



Planning Division

PLANNING COMMISSION STAFF REPORT

TO: Planning Commission

FROM: Stephanie Diaz, Contract Planner

DATE: August 20, 2009

SUBJECT: Planning and Zoning Permit No. 08-540-01, (Planned Development Permit), for a Master Plan to Allow New Development Located at Centerpoint Mall, 2655 Saviers Road.

- 1) **Recommendation:** That the Planning Commission Approve Planning and Zoning Permit No. 08-540-01 for a Planned Development Permit, Administrative Parking Relief and Design Guidelines, subject to certain findings and conditions.
- 2) **Project Description and Applicant:** A request for a Master Plan for the Centerpoint Mall located at 2655 Saviers Road. The Master Plan includes construction of 100,810 square feet of new development and the removal of 7,720 square feet of existing buildings for a net increase of 93,090 square feet of commercial uses. Each phase will be built on an as-needed basis potentially spanning the next ten years. A Development Design Review Permit will be required prior to development of new buildings or additions to existing buildings. The shopping center is currently 376,677 square feet in size. At build-out of the Master Plan, the shopping center size will increase to 469,767 square feet. The existing parking lot would be reconfigured for landscaping, sidewalks, and loading zones. The number of parking spaces would be reduced from 1,845 existing spaces to 1,314 proposed spaces. Administrative Relief of parking is requested for 565 code-required spaces to allow 1,314 spaces rather than the required 1,879 spaces. In addition, a portion of the existing compact parking spaces would be relocated on-site. Road improvements on Saviers Road and Laurel Street will be provided for traffic safety. The Master Plan includes Architecture, Lighting and Sign Guidelines for future development or renovation of existing development. There is no substantial evidence that the proposed project may have a significant effect on the environment, and a mitigated negative declaration (MND # 09-01) will be adopted. Filed by Coastal Architects, 505 South A Street, Oxnard, CA on behalf of Oxnard Center Company.
- 3) **Existing & Surrounding Land Uses:** The site is currently developed with the Centerpoint Mall which includes a main enclosed mall building, several surrounding stand-alone buildings and a landscape parking lot. The site involves APNs 203-0-320, -185 -205, -

245, -265, -285, -295 -315, -335 -355, -375-435. The land use designations and existing land uses are provided below:

LOCATION	ZONING	GENERAL PLAN	EXISTING LAND USE
Project Site	C-PD	Commercial Regional	Shopping Center
North	R-2	School	Santa Clara High School
South	R-1/C-2-PD	Residential Low/Commercial General	Single-family Residential, Commercial, Gas Station
East	C-2	Commercial Neighborhood	Shopping Center
West	R-1/C-O-PD	School/Commercial Office	School, Church, Residential, Mortuary

4) Background Information: The project is located in the Kamala Park Neighborhood and in the redevelopment area known as HERO (Historic Enhancement and Revitalization of Oxnard). The City of Oxnard is not providing supplemental financing or tax incentives for the project. In 1965, the Planning Commission approved Special Use Permit U187 for construction of a “Unimart Store and other commercial outlets.” Several modifications occurred to U187 granting additional square footage and new uses. In 1979, the Planning Commission approved Special Use Permit U813 approving the enclosure of the mall and modifications to the parking lot. This enclosure created the existing interior mall design. Since 1979, several modifications to U813 and U187 have been filed and approved, for tenant improvements to the mall that conform to the City of Oxnard Zoning Ordinance including a new Building C at the corner of “C” Street and Laurel Street. On April 17, 2008 the Planning Commission approved Planned Development Permit PZ 07-540-4 for demolition of an existing building (Yolanda’s Restaurant) and construction of a new building at this location (Building “S”). Several lot line adjustments have been processed to provide individual lots for some of the commercial buildings for financing or leasing purposes. To prevent piecemeal development and provide consistent review standards and design guidelines for future development, a Planned Development permit for a Master Plan has been requested.

5) Environmental Determination: The proposed Master Plan is subject to review in accordance with the California Environmental Quality Act (CEQA). An initial study was prepared to analyze potentially significant adverse environmental effects of the proposed project. Seven areas were identified as potentially being affected including hazardous materials, cultural resources, hydrology, noise, air quality, traffic and geology/soils. Mitigation measures or project description changes were included in the Mitigated Negative Declaration to reduce all impacts to a less than significant level. On April 30, 2009, the applicant agreed to mitigation measures recommended by staff to address the identified adverse effects. The draft document consisting of the initial study and recommended mitigation measures (MND #09-01) was made available for the requisite public review and comment period, from May 6, 2009 through May 26, 2009.

During the review period, a comment letter on the MND was submitted by County of Ventura Environmental Health Division regarding an on-site soil contamination site being monitored by the Los Angeles Regional Water Quality Control Board (LARWQCB). The comment letter is included with the MND. The contamination was caused by the former Regal Cleaners site located within the enclosed mall. Text was added to the MND to describe this site and the mitigation that has been completed and submitted to the LARWQCB. A mitigation measure requiring submittal of evidence of site closure was added to the MND. Additionally, the applicant corrected the square footage numbers for proposed Building G which increased build-out on the site by 11,600 square feet. After analysis, it was determined that no new significant impacts would be associated with the corrected square footage. The MND text has been amended using *ITALICS* to indicate text changes to address these two changes. No recirculation of the document was required. As revised, no significant adverse effects are expected to result from the proposed development, and staff recommends that Planning Commission adopt the revised document, MND #09-01 (see Attachment C).

6) Analysis:

a) General Discussion: The Master Plan addresses the 30-acre Centerpoint Mall site. It includes pad locations and associated parking lot areas for three new buildings (Buildings D, G and M on the site plan) and one replacement building (Building J on the site plan). Also, additions are planned for four existing buildings (Buildings I, H, P, and K). The specific uses in the new buildings are not known at this time but would be typical commercial uses permitted or conditionally permitted in the General Commercial zone. If the Master Plan is approved, individual new buildings or additions that are included in the Master Plan would be reviewed in the Development Design Review permit process. This process will administratively evaluate the proposed development for design, environmental issues, consistency with the approved Master Plan and necessary conditions of approval. The proposed Master Plan Design Guidelines will provide general site, architectural and lighting standards to guide new development. Included in the Design Guidelines is a comprehensive Sign Program that delineates the types and amounts of signage for the mall and individual tenants. The Sign Program is discussed further in section (f) below.

b) General Plan Consistency: The project is consistent with the General Plan land use designation of Commercial Regional. The following chart shows project consistency with General Plan policies:

POLICY	DISCUSSION
<p><i>Standards for Land Use Density/Intensity states: "Regional Shopping Centers in these areas can be multi-story and range up to 3 million square feet in size on as much as 100 acres.(pg V-52)"</i></p>	<p>The proposed Master Plan provides for future development at the Centerpoint Mall a shopping center which is smaller than a regional shopping center. With the Master Plan, the center is proposed to be 469,767 sf in size. Therefore, consistency with this policy may be found.</p>

POLICY	DISCUSSION
<p>Land Use Element Policy 2 Other Infill Development: <i>As stated in the Growth Management Element, it is the City's policy to encourage development of these areas prior to extensive development of larger vacant areas.</i></p>	<p>The project is within the HERO redevelopment area. The City established redevelopment areas to encourage reinvestment and rehabilitation of properties within these areas. The project embraces redevelopment objectives by removing and replacing an older building, providing new additions and new buildings within the existing shopping center.</p>
<p>Land Use Element Policy 3 Infill/Modification Area Policies: <i>This section of the 2020 General Plan is concerned with planning issues within the City's existing areas and neighborhoods.</i></p>	<p>This project is within the existing commercial corridor on Saviers Road on an existing developed site that contains a regional shopping center. The site is within the Kamala Neighborhood which has been identified as a specific infill area.</p>
<p>Growth Management Element <i>Policies consist of two general categories. The first category defines "where" the development will occur and the second defines "how" and "when" the development will proceed. The Oxnard City Urban Restriction Boundary (CURB) sets the primary self-imposed demarcation for the urbanization of the City. The City's Sphere of Influence identifies the probable ultimate physical boundary of the City. Greenbelt Agreements, such as the Save Open Space and Agricultural Resources (SOAR) initiative, designate portions of the planning area that shall remain in agriculture.</i></p>	<p>The project is within the CURB. It is an "in-fill" project. <i>"First priority for development is assigned to those areas necessary to "round out" or "fill in" the boundaries of existing partially developed neighborhoods and infill properties for which public services are already present or can be readily provided at the expense of the developer (page IV-21)." Therefore, the project complies with the Growth Management Element Policies in that it is in an area designated for urban infill and adequate City services are already in place. By concentrating development in the City core, pressure is not put on areas outside of the CURB.</i></p>
<p>Community Design Policy #8: <i>"The City shall continue and formalize in area plans the architectural design themes established in visually distinctive areas of the City.</i></p>	<p>The proposed additions and new buildings are designed to upgrade the architecture related to the Centerpoint Mall.</p>

c) Conformance with Zoning Development Standards: The proposed development is located in the General Commercial Planned Development (C-PD) zone district. In accordance with the City Code, the proposed Master Plan may be permitted with an approved Planned Development Permit. Development standards of the C-2 zone (applicable for the C-PD zone) have been compared with the proposed Master Plan development. The analysis pertains only to the pad locations as actual buildings to be reviewed under the Development Design Review Permit may vary from the general pad shapes shown in the Master Plan. Further zone code analysis will be done for each new building or addition during the Development Design Review permit process.

Conformance with the zone standards for new building pads and proposed building additions is as follows:

DEVELOPMENT STANDARDS FOR BUILDINGS M AND K IN MAIN MALL			
DEVELOPMENT STANDARD (measured from internal parcel lines)	REQUIREMENT	PROPOSED	COMPLIES?
Max. building height	2 stories or 35 feet.	2 stories	YES
Front Yard setback Laurel Street	10 feet from property line.	26 feet	YES
West Side Yard setback Toward C Street	5 feet, but zero when lot abuts another C-2 zoned lot.	160-210 feet	YES
East Side Yard setback Toward Saviers Road	5 feet, but zero when lot abuts another C-2 zoned lot.	145-180 feet	YES
Rear Yard setback Adjacent to Lucky's (Albertson's)	None, except 15 feet required if bldg to be >16 feet in height.	325 feet	YES
Parking lot landscaping	5% min. Landscape fingers required every 10 spaces. 10 feet along frontage.	Mall landscaping is 16%. Reciprocal mall parking lot serves the buildings. Landscape fingers provided. Landscaping provided on frontages.	YES
Trash enclosures & transformers	To be provided and screened.	Trash enclosure provided and screened with landscaping.	YES
Rooftop equipment	May not protrude above height of parapet.	Unknown at this time; To be condition of approval.	YES with condition of approval
Lighting	Comply with City Code.	Photometric plan required at time of plan check review.	YES with condition of approval
Auto Parking	Required Building M parking based on 1/250 is 23 spaces. Required Building K addition parking at 1/250 is 10 spaces. Required mall parking is 1,879 spaces at build-out. No compact parking permitted	Reciprocal mall parking serves the buildings with 1,845 current spaces and 1,314 spaces at build-out. 74 existing compact parking spaces to be relocated on-site for the mall Master Plan.	Administrative Relief required for mall parking space count and compact spaces.
Motorcycle Parking	3/first 100 spaces and 1/40 spaces thereafter = 34 spaces	Reciprocal mall motorcycle parking provides 34 spaces.	YES

DEVELOPMENT STANDARDS FOR BUILDING FOOTPRINTS D, G & J AND ADDITIONS, H, I, AND P			
DEVELOPMENT STANDARD (measured from internal parcel lines)	REQUIREMENT	PROPOSED	COMPLIES?
Max. building height	2 stories or 35 feet.	Unknown	YES per Conditions of Approval
Front Yard setback	10 feet from front property line.	New Bldg D 15 feet New Bldg G 60 feet Add. Bldg H 35 feet Add. Bldg I 5 ft & 5 ft Replace Bldg J 10 ft & 25ft Add. Bldg P 15 feet	YES with Bldg I condition to maintain minimum 10 ft setbacks.
Side Yard setback: South or East	5 feet, but zero when lot abuts another C-2 zoned lot.	New Bldg D 12 feet New Bldg G 105 feet Add. Bldg H 15 feet Add. Bldg I 10 feet Replace Bldg J NA Add. Bldg P 5 feet	YES
Side Yard setback: North or West		New Bldg D 60 feet New Bldg G 25 feet Add. Bldg H 35 feet Add. Bldg I NA Replace Bldg J 175 feet Add. Bldg P 210 ft	YES
Rear Yard setback:	None, except 15 feet required if bldg to be >16 feet in height.	New Bldg D 35 feet New Bldg G 30 feet Add. Bldg H 25 feet Add. Bldg I 35 feet Replace Bldg J 105feet Add. Bldg P 240 feet	YES
Parking lot landscaping	5% min. Landscape fingers required every 10 spaces. 10 feet along frontage.	Mall landscaping is 16%. Reciprocal mall parking serves the buildings. Landscape fingers provided.	YES
Trash enclosures & transformers	To be provided and screened.	Trash enclosures provided and screened with landscaping.	YES
Rooftop equipment	May not protrude above height of parapet.	Unknown at this time; To be condition of approval.	YES with condition of approval
Lighting	Comply with City Code.	Photometric plan required at time of plan check review.	YES with condition of approval

DEVELOPMENT STANDARDS FOR BUILDING FOOTPRINTS D, G & J AND ADDITIONS, H, I, AND P			
DEVELOPMENT STANDARD (measured from internal parcel lines)	REQUIREMENT	PROPOSED	COMPLIES?
Auto Parking	Required parking for each building is based on 1/250. Required mall parking at build-out is 1,879 spaces. No compact parking permitted	Parcels contain various numbers of parking spaces. Reciprocal mall parking serves the buildings. 1,314 spaces will serve the mall at build-out.	Yes with approval of Administrative Relief required for mall parking
Motorcycle Parking	3/first 100 spaces and 1/40 spaces thereafter = 34 spaces for mall	34 spaces provided for mall	YES

d) Site Design: The Centerpoint Mall project site includes the main enclosed mall building with a major tenant at each end (currently Lucky's grocery store and former Mervyn's). Additionally, there are seven separate buildings located in the parking area surrounding the main mall. Existing buildings include:

Existing Main Mall

- Building A Various Small Tenants and Rite Aid
- Building B Various Small Tenants
- Building E Major Tenant (former Mervyn's)
- Building F Major Tenant (Lucky's/Savon)
- Building K Auto Zone
- Building P Ventura County Health

Existing Separate Buildings

- Building C Las Islas Women's Clinic & Medical Group
- Building H Taco Bell
- Building I Las Islas Urgent Care and VCMC Clinic
- Building J Ritmo/Oxnard Dental
- Building L Bank of America
- Building N Wells Fargo Bank
- Building S New Construction - Commercial

All existing development as listed above is included in the Master Plan. The following new development or additions are proposed in the Master Plan:

New Building Pads:

- Building D 40,000 sf commercial
- Building G 17,600 sf commercial
- Building M 5,746 sf (in main mall)

Replacement Building:

Building J - Ritmo 10,885 sf (net new 3,165 sf)

Additions to Existing Buildings:

Building H – Taco Bell 2,000 sf
Building I – Las Islas 2,000 sf
Building K – Auto Zone 2,579 sf
Building P – Ventura Co. 20,000 sf

New building pads (Buildings D & G) are located in open parking lot spaces that are not currently developed with structures. Building G (17,600 sf) will fill in frontage area along Saviers Road reducing the view of the large asphalt parking lot. Building D (40,000 sf) will fill in the frontage area along “C” Street also reducing the view of the large asphalt parking lot. The remaining open areas in the parking lot along Saviers Road, Channel Islands Boulevard and C Street are required view corridors for the major tenants that anchor the mall at the north and south ends. Therefore, the locations of Buildings D & G are generally fixed. The pad for Building M is located at the north end of the enclosed mall, behind the Auto Zone store. This building will provide maintenance and storage for the mall. Building J will replace the Ritmo building currently located at the corner of Saviers Road and Laurel Street.

Additions are proposed to existing buildings. The majority of the additions are small with the exception of 20,000 sf to be added to Building P (Ventura County Health). The Master Plan site plan has been broken down into “Areas” associated with each new building or addition. These “Areas” include the parking lot, landscaping and public improvements that are associated with each project. Since the Master Plan only indicates pads for development, conditions of approval are provided for building height, building setbacks, lighting and screening of roof-top equipment. The maximum square footage for new buildings or additions is limited by the numbers presented in the Master Plan.

- e) **Circulation:** The project site is bounded by Laurel Street on the north, Saviers Road on the east, Channel Islands Boulevard on the south and “C” Street on the west. There are two existing driveways providing ingress and egress at the mall from each of these streets. No change in the number or location of driveways is proposed. To encourage pedestrian circulation into and through the site, primary driveways on Saviers Road and “C” Street into the mall would be narrowed to 25-feet to provide pedestrian sidewalks alongside the driveways. This includes the addition of a pedestrian sidewalk for the northern C Street driveway to provide pedestrian access along the south side of the driveway from the “C” Street bus center to the mall. To create a pedestrian-friendly atmosphere, the walkways along the entrance drives will have light standards and poles for small decorative banners.

The City Traffic Engineer has reviewed the proposed driveway widths and corner turning radii and determined that a fire truck or semi-truck could make the turning movement and

therefore, supports the 25-foot wide driveways. The interior circulation and parking lot access are designed to meet City engineering standards. The Fire Department will require main drives that serve fire hydrants to be 26-foot wide. Small adjustments may be needed for some main drive aisles. This will be accomplished during the Development Design Review process for each new project. No vehicle circulation safety issues have been identified by the City Traffic Engineer.

A traffic and parking study by Austin-Foust Associates, Inc, January 2009, was prepared for the Master Plan build-out (see MND Attachment C). Based on 2007 traffic counts, the study determined that the increase in traffic associated with new development at the center is 79 vehicles per hour in the AM peak hours and 121 vehicles per hour in the PM peak hours with a 1,473 average daily trip increase. (The traffic counts are considered appropriate as they were calculated when the Mervyn's store was open.) The majority of trips in the morning are inbound and most trips in the afternoon are outbound. Because the site expansion will not generate 100 or more trips in the AM peak hour, the project will not significantly impact the Congestion Management Plan network during the AM time frame. During PM peak hour, the north-south travel on Saviers Road will increase by 36 vehicles per hour in each direction. East-west travel on Channel Islands Blvd will increase by 24 vehicles per hour each way. The intersection of Saviers Road and Channel Islands Blvd currently operates at LOS "C." There is a PM peak on Saviers of 1,400 vehicles per hour and 950 vehicles per hour on Channel Islands Blvd. According to the traffic study, the addition of 36 vehicles per hour north-south on Saviers and 24 vehicles per hour east-west on Channel Islands Blvd. does not create a significant impact. Payment of the Ventura County traffic fee is a standard condition of approval and will be applied to this project.

With the potential for 1,473 average daily trip increase associated with the project at build-out, public street improvements will be required to address safety and circulation. It is expected that the mall's Laurel Street driveway will have an increase in traffic movements as it provides good access to area streets. It has been observed that vehicles are currently stacking-up on eastbound Laurel Street to make a left-turn on to Saviers Road blocking the through-traffic. With additional traffic from Master Plan build-out, a second left-turn lane would be needed to mitigate vehicle stacking on Laurel.

Additionally, a traffic safety issue has been observed in relation to the Santa Clara High School driveways. Vehicles have been observed turning right out of the school driveway, then traveling westbound on Laurel Street in the opposing traffic lane to make a turn into the shopping center. The vehicles travel in the opposing lane because an exiting small median prevents left turns into the mall. By travelling in the opposing lane, vehicles avoid the median and make left turns. The project proposes mitigation by the addition of a phased raised median extension. The first phase will extend the existing raised median to the edge of the school's eastern driveway to prevent right-turns being made into the opposing traffic lane on Laurel. The second phase will extend the median all the way to Saviers Road to prevent left turns out of the eastern school driveway as this turning movement will become unsafe with the two left-turn pockets that will be constructed on Laurel at Saviers (this will occur

when Building J, Ritmo, is replaced). These improvements will increase safety for vehicles making turning movements at the school driveways.

To accommodate Master Plan traffic and mitigate additional vehicles stacking and turning into the shopping center from southbound Saviers Road, a right-turn pocket is proposed at each of the two Saviers Road mall driveways. The need for a future southbound right-turn lane onto Channel Islands Boulevard has been identified by the City to address right-turn vehicle stacking. With the previous approval of Building S (Yolanda's replacement) at the shopping center, right-of-way dedication along the Saviers Road frontage of Building S was required. That dedication has been provided to the City. To allow completion of the right-turn pocket, right-of-way dedication along the Saviers Road frontage of the Wells Fargo building (on the corner of Saviers and Channel Islands Blvd.) is required for this Master Plan. The following public improvements are proposed as part of the Master Plan for safety and better circulation. These improvements are required to be dedicated and/or constructed in a phased manner, timed with construction of adjacent buildings proposed in the Master Plan.

- Extend raised median on Laurel Street at Santa Clara High School to edge of eastern school driveway to prevent right turns out of the school driveways into opposing traffic lane to improve safety. Master Plan to provide at first building permit.
- Dedicate right-of-way for widening Laurel Street at the Saviers Road intersection at building permit for Building J to accommodate Laurel Street widening for potential project traffic added to Laurel Street.
- Widen the Laurel St./Saviers Rd. intersection to add a second left-turn pocket from east-bound Laurel Street onto Saviers Road to address existing and potentially greater queuing with project traffic on Laurel west of the Saviers intersection. Provide striping for the second left-turn pocket. Extend raised median on Laurel Street up to Saviers Road to address safety of turning movements from the high school onto Laurel Street. Modify signal pole and mast arm length to accommodate eastbound Laurel Street new left turn lane. Master Plan to provide at reconstruction of Building J on the corner of Saviers and Laurel.
- On Saviers Road south of Laurel at the northern shopping center driveway, provide a south-bound right-turn pocket into the center with a bus stop and shelter to provide a separate lane for turns into the shopping center and to allow semi-truck deliveries to move out of the traffic flow to turn into the center. Master Plan to provide with reconstruction of Building J or construction of Building G, whichever occurs first.
- On Saviers Road at the southern shopping center driveway, provide a southbound right-turn pocket into the shopping center driveway to provide a separate lane for turns into the shopping center. Master Plan to provide with construction of Building G.

- On Saviers Road at the Wells Fargo Bank, dedicate an 11' 1" wide strip along the Saviers Road parcel frontage for a southbound right-turn lane onto Channel Islands Blvd. The Master Plan shall provide the right-of-way at the time the Wells Fargo building is remodeled or demolished to remove the portion of the building that encroaches.

f) **Parking:** The Centerpoint Mall currently has a comprehensive parking lot on all sides of the mall providing 1,845 parking spaces (more than required by code). With the new development, additions to existing buildings, and reorientation of parking spaces for emergency vehicle access, some parking spaces will be lost. At Master Plan build-out 1,314 spaces will be provided. According to parking ordinance standards of one space per 250 square feet, a total of 1,879 parking spaces would be required. With the Master Plan the on-site parking lot would provide 565 spaces less than required by ordinance as outlined below:

Required Parking

Master Plan: 469,767 sf @ 1/250 = 1,879 spaces

Proposed Parking

Standard:	1,168 spaces
Compact:	74 spaces
Handicap:	33 spaces
Motorcycle:	34 spaces
<u>Drive-Thru:</u>	<u>5 spaces</u>
Total:	1,314 spaces

Request for Administrative Relief

565 parking spaces
74 existing compact spaces to be reconfigured

The parking ordinance contains a provision for an applicant to request an Administrative Relief from Parking Provisions when parking as required by the Code is not provided. The Code states that Administrative Relief may be granted by the Commission for the proposed use under one or more of the following circumstances, as supported by a parking study:

Shared Parking: Parking spaces may be used jointly by uses with different hours of operation or different peak hours of operation. (The mall has office uses that operate mainly on weekdays and retail uses with high weekend use such as the grocery store).

Decrease in Required Parking Spaces: Required parking may be decreased when the requirement is shown to be excessive. (The parking study indicates that the parking demand for the Master Plan is accommodated by the proposed number of parking spaces.)

Compact Parking Spaces: Compact parking may be provided in lieu of some full size parking spaces however no more than 25% of required full-size spaces may be compact spaces. (A total of 74 compact parking spaces are proposed, less than 6% of the parking to be provided.)

The following parking information is based on the “Oxnard Centerpoint Traffic and Parking Study” prepared by Austin-Foust Associates, Inc, January 2009. (It should be noted that this study is based on an earlier proposal for an addition of 87,525 square feet; the current proposal is for an addition of 93,090 net new square feet.) Parking counts were conducted at the center on three weekdays and two Saturdays in August and September 2006. The entire center experienced a maximum parking demand of 775 spaces. This is equivalent to 2.34 spaces/1000 sf. The Parking Ordinance standard is 2.5 spaces/1000 sf. Using the actual demand of 2.34 spaces/1000 s.f., the Master Plan build-out would have a maximum demand of 1,099 spaces. As the Master Plan is proposing 1,314 parking spaces, the parking lot would meet the expected demand. The parking study states that industry standards for commercial parking lots allows up to 90% occupancy to be acceptable. That occupancy would be a demand for 1,182 spaces which the Master Plan will provide.

Additionally, five loading zones are required for a shopping center over 150,000 s.f. A total of 24 loading zones are proposed in the Master Plan. Each separate building has one or two loading zones while the main mall and major tenants have four or more. Some of the loading zones are shorter than the code requirement of 40 feet because they serve smaller tenants and are used by smaller delivery trucks (Fed Ex, UPS, etc.) The mall and major buildings have full size loading zones.

- g) Design Guidelines: Architecture, Lighting, Master Sign Program:** The Master Plan includes Design Guidelines (Attachment D) for Architecture and Lighting as well as a Master Sign Program. The document presents “architectural expectations” for remodels, additions and new buildings. The architectural style is “Contemporary Retail” based on strong massing with varying proportions and exposed structural elements. (The newly completed Building C – Las Islas Women’s Clinic and Building S - former Yolandas represent this style.) Strong earth tone colors of tans, browns and greens are proposed for building colors. Storefront systems, awnings and architectural projections are also addressed. The lighting plan calls for contemporary lighting with a maximum height of 26-feet that meets City photometric requirements and is required to be consistent in design throughout the main lots. Entry drives may have alternate ornamental light fixtures. General exterior building lighting is to have flat lens, full cut-off fixtures with the light sources shielded. Decorative street furniture is also addressed including benches, trash receptacles, newspaper racks as well as seasonal flags on poles.

The Master Sign Program is included in the Design Guidelines (Attachment D). This program addresses the total amount of signage permitted on the site for tenants and mall identification. This includes signage amounts for interior and exterior mall tenants, major tenants, new buildings and shopping center identification signage. The total amount of

signage permitted on the site and on individual buildings meets Sign Ordinance requirements. In some cases, the allowed signage amount for specific existing buildings was reduced to obtain square footage to be assigned for the mall identification signs. Signage amounts for proposed buildings such as Building G and Building D are a “placeholder” number based on the buildings’ frontage measurement. When a building is designed and submitted for a Development Design Review Permit, specific signage for that building will be addressed per the Master Sign Program. Once the Master Sign Program is approved, non-conforming signs must be changed to signs that conform to the Master Plan when a tenant change or change of copy is requested. Maximum letter height is 36 inches except for the Major Tenants where a maximum letter height of 72 inches is allowed.

In addition to providing signage opportunities for mall tenants and individual buildings, the Master Sign Program allows the following center identification signs:

- One Freestanding Sign (32’high) on Saviers Road
- Five Ground Signs (3’6” high) one on Saviers Road, two on Channel Islands Blvd. and two on C St. (all at driveway entrances)
- Four Mall Marque Signs, one at each entry to the enclosed mall

Two aspects of the Sign Program do not meet Sign Ordinance standards, but may be approved as part of a Master Sign Program. The first is a Freestanding Sign is limited to 24 feet in height, but may be approved up to 32 feet in height, as proposed. The second item involves ground signs. The Sign Ordinance allows a shopping center to have one ground sign for each frontage. The Master Sign Program calls for two mall identification ground signs on Channel Islands Blvd. and two on C Street. Each of these signs is proposed to be located at an existing driveway. Given the large size of this site and the long frontages, two ground signs on these frontages may be considered appropriate. (An existing tenant ground sign at Taco Bell is required to be removed when the tenants change or the proposed addition is built.) In total, the sign program provides for a mall identification sign at each major driveway and at each major entrance to the enclosed mall. The Sign Ordinance specifies that the Commission may approve a Master Sign Program for a master planned commercial project that includes sign types and configurations not otherwise permitted by the Sign Ordinance.

- h) Landscaping and Open Space:** The Master Plan includes a landscape plan that addresses the parking lot areas, property frontages along roadways and driveway entrances. The 30-acre site has 16% landscaped area (216,469 s.f.). Parking lots are required by City Code to have a minimum of 5% landscaping. All required landscape planters in parking lots are provided as well as landscape screening for trash enclosures. The Master Landscape Plan includes tree, shrub and ground cover species for new buildings including palm trees, flax shrubs and fescue grasses. The main tree species for the mall parking lot areas include New Zealand Christmas Tree, Olive, African Sumac and several types of palms. Existing trees to be removed are outlined in the Horticultural

Report prepared by "Trees, etc." The economic appraised value of these trees shall be put back into the size of new tree sizes. Quite a few existing palms that have been removed from the mall area for past projects and parking lot improvements are stored on-site and are available for immediate planting. Precise landscape plans for new buildings will be addressed during the Development Design Review process.

- i) **Permit Process for Future Master Plan Projects:** An approved Planned Development Permit for the Master Plan provides a baseline for the amount, type and design of structures as well as parking, signs and landscaping. Once the Master Plan and the Design Guidelines are adopted, new development must be generally consistent with the approval. Applications for new development that are consistent with that approved under the Master Plan would be processed administratively through the Development Design Review process. Proposed signs that are consistent with the Design Guidelines/Sign Program only require a sign permit that would be administratively approved.

7) Development Advisory Committee: The Development Advisory Committee (DAC) reviewed this project on September 2008 and January 7, 2009. The DAC discussed road improvements, drainage, parking lot circulation, and timing of improvements. All DAC comments have been addressed in plan changes or conditions of approval.

8) Community Workshop: On February 10, 2008, the applicant mailed notices of the Community Workshop meeting to all property owners within the Kamala Park, Black Stock North, Bryce Canyon North and Cal Giser Neighborhoods. The applicant also posted such notice on the project site with a brief description of the project and contact information. The Community Workshop was conducted on February 25, 2008. One member of the public spoke regarding the loss of the Yolanda's building. He suggested that a convenience market such as Trader Joe's would be a good use on the site.

Appeal Procedure: In accordance with Section 16-545 of the City Code, the Planning Commission's action may be appealed to the City Council within 18 days after the decision date. Appeal forms may be obtained from the City Clerk and must be submitted with the appropriate fees before the end of the appeal period

Attachments:

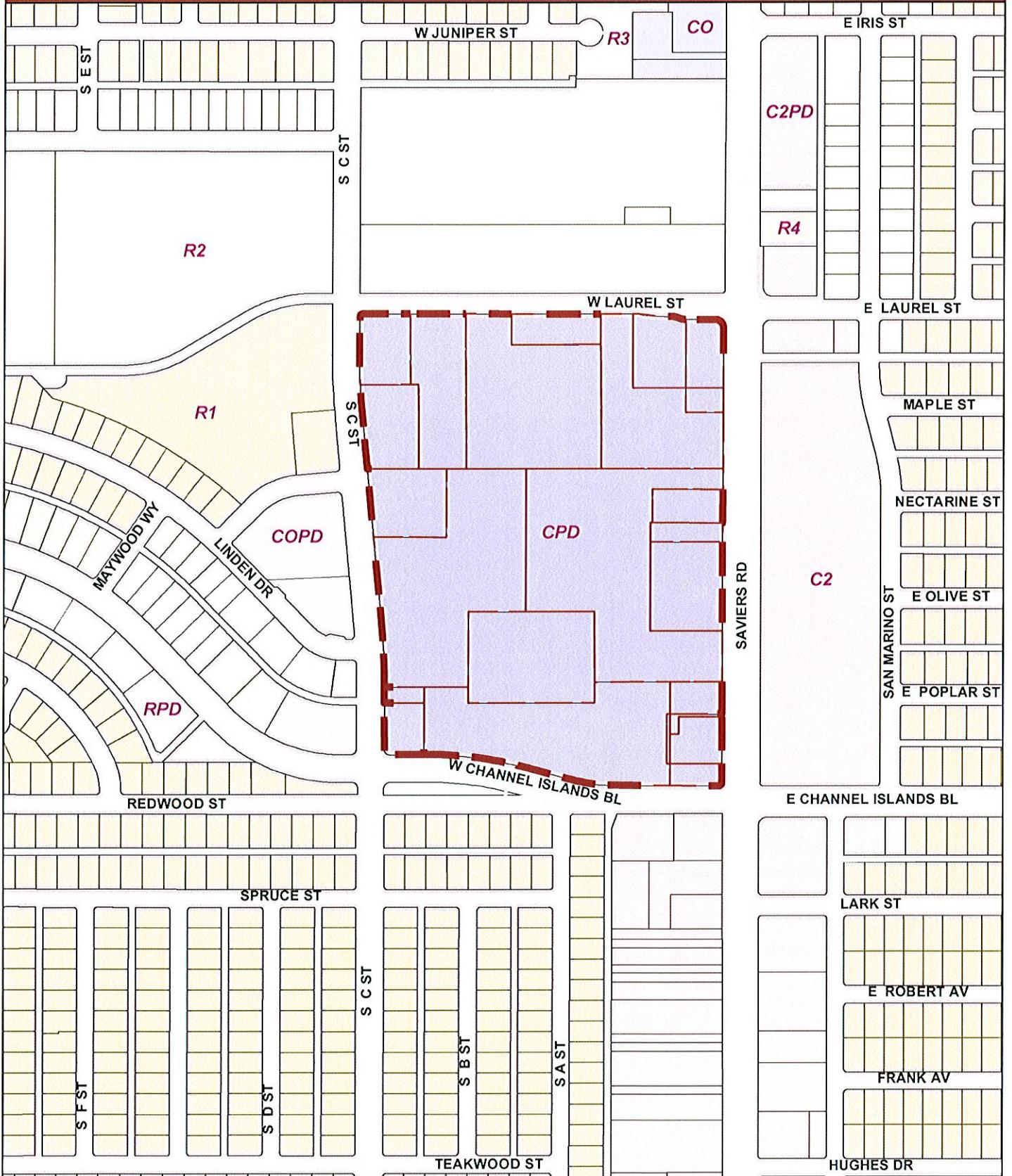
- A. Maps (Vicinity, General Plan, Zoning)
- B. Reduced Project Plans
- C. Environmental Document MND #09-01, with Comments
- D. Design Guidelines with Master Sign Program
- E. Resolution

Prepared by:	<u>SLD</u> SLD
Approved by:	<u>SM</u> SM

ATTACHMENT A

Maps (Vicinity, General Plan, Zoning)

Zone Map



PZ 08-540-01
Location: 2655 Saviers Rd
Centerpoint Mall Master Plan

0 75 150 300 450 600 Feet

Zone Map



1:4,484

ATTACHMENT B

Reduced Project Plans



EXISTING BUILDINGS

Address	Tenant	Type of Occupant	S.F.	All Building sf Parked at 1/250
2701 Saviers	Bldg. 'E'	Comm.	76000.00	304
2401 Saviers	Bldg. 'F'	Comm.	86808.00	347
2831 Saviers	Bldg. 'N'	Comm.	6472.00	26
2475 Saviers	Bldg. 'L'	Comm.	10700.00	43
2501 Saviers	Bldg. 'A' -Tenant	Comm.	3000.00	12
2679 Saviers	Bldg. 'B' -Tenant	Comm.	8000.00	32
2661 Saviers	Bldg. 'A' -Tenant	Comm.	20970.00	84
150a Laurel St	Bldg. 'K' -Storage	Comm.	2471.00	10
150 Laurel St	Bldg. 'K' -Tenant	Comm.	4950.00	20
2643 Saviers	Bldg. 'A' -Tenant	Comm.	1650.00	7
2651-A Saviers	Bldg. 'A' -Tenant	Comm.	1090.00	4
2653-A Saviers	Bldg. 'A' -Tenant	Comm.	2760.00	11
2655-B Saviers	Bldg. 'A' -Tenant	Comm.	1755.00	7
2657-A Saviers	Bldg. 'A' -Tenant	Comm.	1200.00	5
2503 Saviers	Bldg. 'A' -Tenant	Comm.	465.00	2
2655 Saviers	Bldg. 'A' -Admin	Comm.	4060.00	16
2655 Saviers	Bldg. 'A' -Storage	Comm.	535.00	2
2623 Saviers	Bldg. 'A' -Tenant	Comm.	1400.00	6
2631 Saviers	Bldg. 'A' -Tenant	Comm.	1250.00	5
2633 Saviers	Bldg. 'A' -Tenant	Comm.	1250.00	5
2643-A Saviers	Bldg. 'A' -Tenant	Comm.	1800.00	7
2651 Saviers	Bldg. 'A' -Tenant	Comm.	2029.00	8
2653 Saviers	Bldg. 'A' -Tenant	Comm.	2580.00	10
2655-C Saviers	Bldg. 'A' -Tenant	Comm.	1600.00	6
2655-D Saviers	Bldg. 'A' -Tenant	Comm.	4898.00	20
2665 Saviers	Bldg. 'A' -Tenant	Comm.	630.00	3
2661-A Saviers	Bldg. 'A' -Tenant	Comm.	570.00	2
2661-B Saviers	Bldg. 'A' -Tenant	Comm.	568.00	2
2661-C Saviers	Bldg. 'A' -Tenant	Comm.	682.00	3
2661-D Saviers	Bldg. 'A' -Tenant	Comm.	675.00	3
2661-E Saviers	Bldg. 'A' -Tenant	Comm.	675.00	3
2661-F Saviers	Bldg. 'A' -Tenant	Comm.	900.00	4
2667 Saviers	Bldg. 'B' -Tenant	Comm.	1312.00	5
2669 Saviers	Bldg. 'B' -Tenant	Comm.	1312.00	5
2671 Saviers	Bldg. 'B' -Tenant	Comm.	1750.00	7
2673 Saviers	Bldg. 'B' -Tenant	Comm.	2232.00	9
2675 Saviers	Bldg. 'B' -Tenant	Comm.	3450.00	14
2677 Saviers	Bldg. 'B' -Tenant	Comm.	2000.00	8
2679-A Saviers	Bldg. 'B' -Tenant	Comm.	2250.00	9
2681-B Saviers	Bldg. 'B' -Tenant	Comm.	1575.00	6
2681-A Saviers	Bldg. 'B' -Tenant	Comm.	1575.00	6
2683 Saviers	Bldg. 'B' -Tenant	Comm.	2400.00	10
2685-B Saviers	Bldg. 'B' -Tenant	Comm.	2400.00	10
2685-A Saviers	Bldg. 'B' -Tenant	Comm.	2400.00	10



2689 Saviers	Bldg. 'B' -Tenant	Comm.	2304.00	9
2691 Saviers	Bldg. 'B' -Tenant	Comm.	2304.00	9
2693-A Saviers	Bldg. 'B' -Tenant	Comm.	3000.00	12
2693-A Saviers	Bldg. 'B' -Tenant	Comm.	3300.00	13
2695 Saviers	Bldg. 'B' -Storage	Comm.	1304.00	5
2500 C St.	Bldg. 'P'	Comm.	20400.00	82
2697 Saviers	Bldg. 'B' -Tenant	Comm.	957.00	4
2699 Saviers	Bldg. 'B' -Tenant	Comm.	1650.00	7
2699-A Saviers	Bldg. 'B' -Tenant	Comm.	760.00	3
2687 Saviers	Bldg. 'B' -Tenant	Comm.	1200.00	5
2687-A Saviers	Bldg. 'B' -Tenant	Comm.	1200.00	5
2689-B Saviers	Bldg. 'B' -Tenant	Comm.	748.00	3
2689-A Saviers	Bldg. 'B' -Tenant	Comm.	1575.00	6
201 W. C.I. Blvd	Bldg. 'H'	Comm.	3000.00	12
325 W. C.I. Blvd	Bldg. 'I'	Comm.	13426.00	54
2400 S. C St.	Bldg. 'C'	Comm.	20000.00	80
2801 Saviers	Building-S	Comm.	12780.00	51

BUILDINGS REMOVED in (future)

2407 Saviers	Bldg. 'J'	Comm.	1920.00	8
2409 Saviers	Bldg. 'J'	Comm.	2800.00	11
2411 Saviers	Bldg. 'J'	Comm.	3000.00	12

Sub-Total	376677.00	1507
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Existing provide on-site 1845

BUILDINGS REMOVED in (future)

Bldg. 'J'	Sub-Total	7720.0	31
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Existing Building Total =	368957.00	1476
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PROPOSED BUILDINGS

Building	Type	Type of Occupant	S.F.	All Building sf Parked at 1/250
Building 'D'	New	Comm.	40000.00	160
Building 'G'	New	Comm.	17600.00	70
325 W. C.I. Blvd	Building 'I'	Addition	2000.00	8
201 W. C.I. Blvd	Building 'H'	Addition	2000.00	8
2411 Saviers	Building 'J'	Replacment	10885.00	44
150a Laurel St	Building 'M'	New	3000.00	12
	Building 'M'	New 2nd Flr	2746.00	11
2500 C St.	Building 'P'	2nd Flr Infill	20000.00	80
150 Laurel St	Building 'K'	Addition	2579.00	10
	Sub-Total		100810.00	403

TOTAL MASTER PLAN BUILD-OUT =	Total	Parking at
	Bldg s.f.	1sp/250
	469767.00	1879



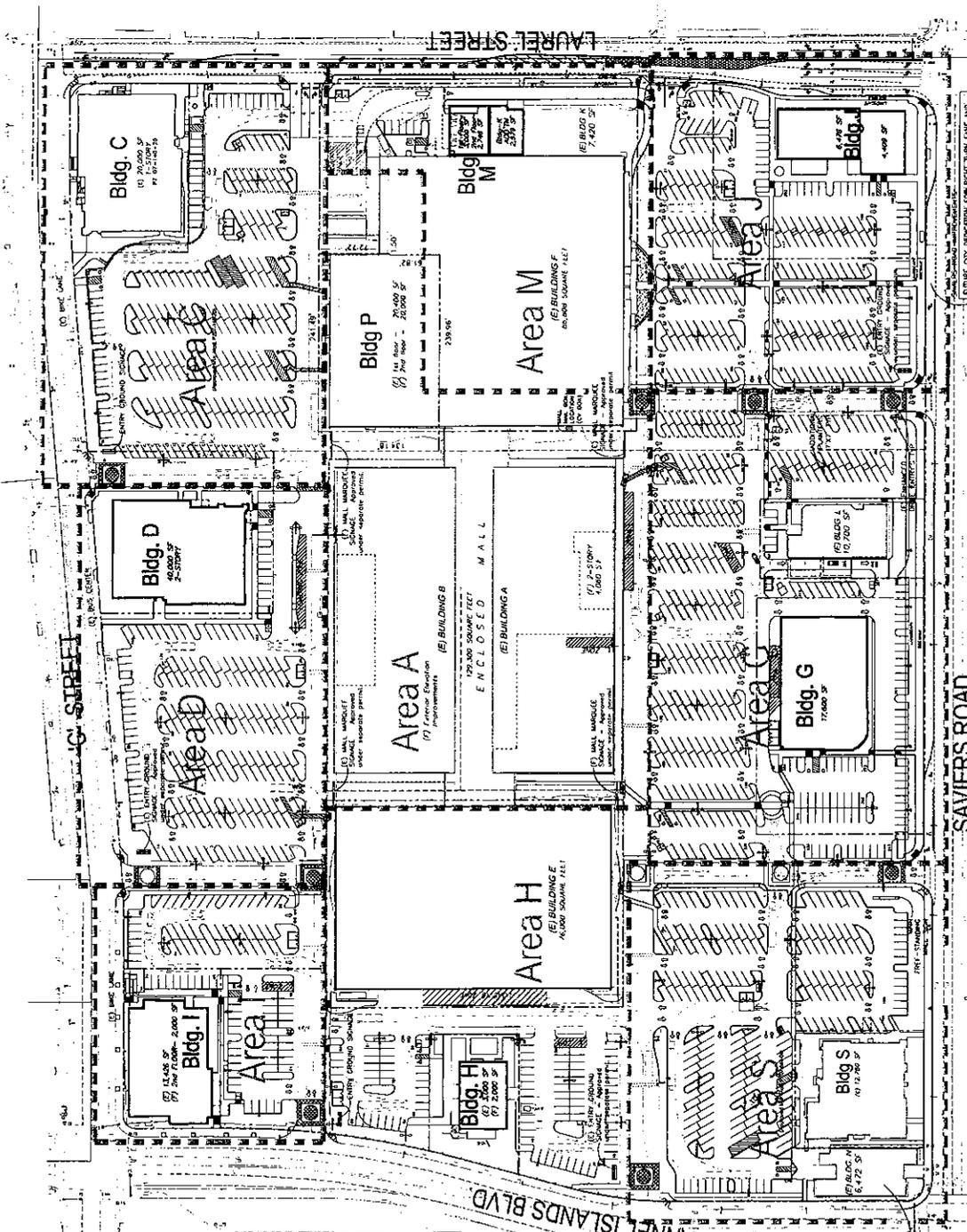
net increase 93090.00

	Total	All Building sf Parked at 1/250
PROPOSED PARKING AREAS ANALYSIS		
Building 'J'	212	
Building 'G'	325	
Building 'S'	188	
Building 'H'	94	
Building 'I'	98	
Building 'D'	177	
Building 'C'	210	
Building 'M' 6 + 4 Serv Bays=	10	
Proposed Site Parking Total	1314 spaces	
Spaces Deficient	1314-1879=	-565 -30%
	s.f. per space =	357.51

GENERAL NOTES
 1. EXISTING BUILDING FOOTPRINTS
 2. EXISTING DRIVEWAYS
 3. EXISTING SIDEWALKS
 4. EXISTING CURBS
 5. EXISTING UTILITIES
 6. EXISTING TREES TO BE MAINTAINED
 7. EXISTING TREES TO BE REMOVED
 8. EXISTING TREES TO BE REPLANTED
 9. EXISTING TREES TO BE PRESERVED
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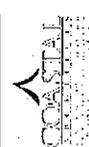
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APRIL 30, 2009
 MASTER PLAN FUTURE IMPROVEMENT PLAN A1
 VICINITY MAP



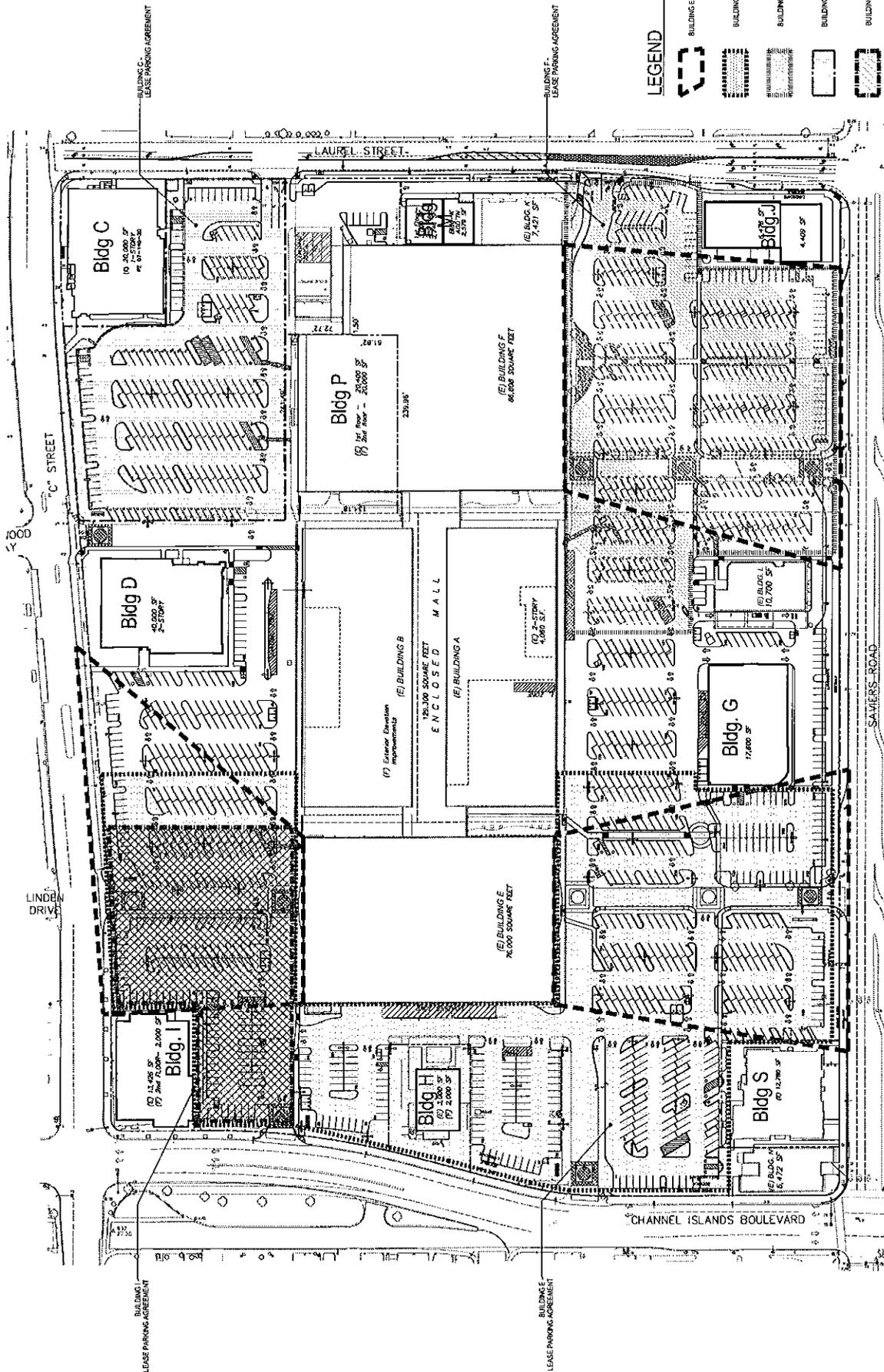
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Centerpoint Mall Master Plan
 CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



APRIL 30, 2009
 MASTER PLAN FUTURE IMPROVEMENT PLAN A1
 VICINITY MAP

030904



LEGEND

- BUILDING E-F LEASE CONSTRAINT AGREEMENT
- BUILDING E LEASE PARKING AGREEMENT
- BUILDING F LEASE PARKING AGREEMENT
- BUILDING G LEASE PARKING AGREEMENT
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1 MASTER SITE PLAN
1" = 60'-0"

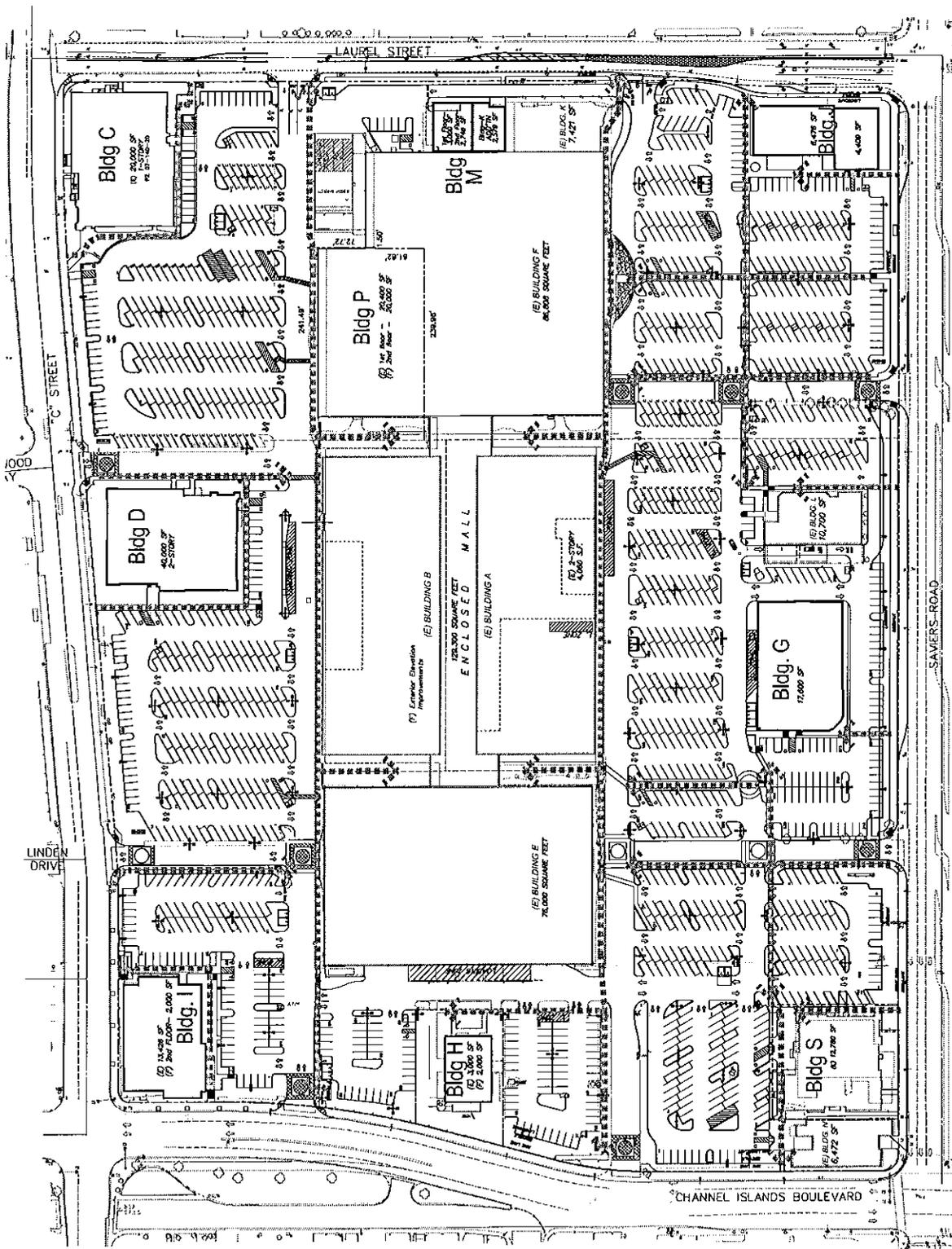
LEASE AGREEMENTS SITE PLAN A2.1
April, 30, 2009



Centerpoint Mall Master Plan
CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



050904



1 MASTER SITE PLAN
1" = 50'-0"



Centerpoint Mall Master Plan

CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033

PEDESTRIAN CIRCULATION PLAN A2

April 30, 2009

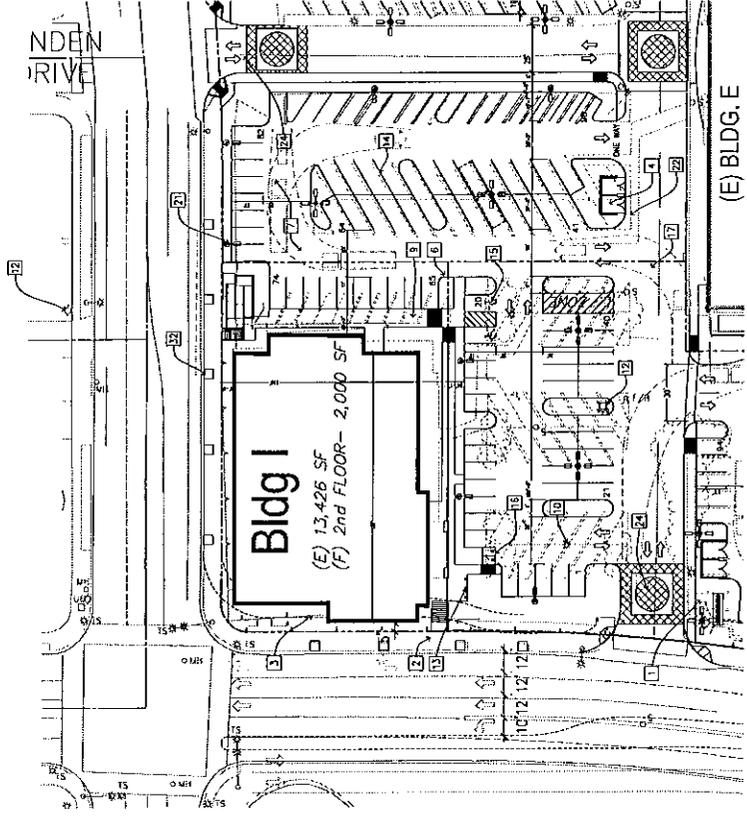


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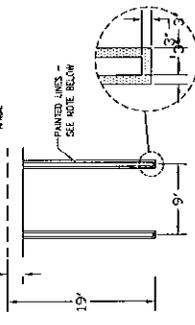
KEYNOTES:

- 17. RELOCATED BUS STOP AND BENCH.
- 18. EXISTING CONCRETE PAVEMENT TO REMAIN IN PLACE.
- 19. LANE OF ADJACENT ROAD TO BE RELOCATED.
- 20. NEW PAVEMENT AREA.
- 21. PROPOSED LANDSCAPE JAWL.
- 22. DRIVE CURB TO BE REMOVED.
- 23. EXISTING PARKING AREA PER CITY.
- 24. EXISTING DRIVE CURB TO REMAIN WITH NEW CURB IN PLACE.
- 25. DRIVE CURB TO BE RELOCATED.
- 26. ADJUSTED PARKWAY AND SIDEWALK PER CITY PERMITS.
- 27. SIDEWALK NOT SET FOR 6" TO 8" FINISH USE.
- 28. PROPOSED TREE PLANTING LOCATION.
- 29. EXISTING LOCATION OF TRUCK COMPACTOR.
- 30. EXISTING SIGNAGE TO BE REMOVED.
- 31. TRANSPORTATION ON LOCAL PAVEMENT.
- 32. EXISTING SIDEWALK PER CITY PERMITS.
- 33. SIDEWALK NOT SET FOR 6" TO 8" FINISH USE.
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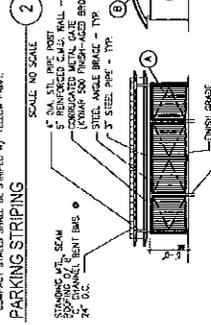
PARTIAL SITE ANALYSIS:
 MULTITENANT RETAIL BUILDING WITH RECREATIONAL ACCESS AND PARKING
 OCCUPANCY GROUP
 CONSTRUCTION TYPE
 PARTIAL SITE AREA
 BUILDING - 1
 Parking Site Coverage
 PARKING PROVIDED
 HC
 2 Stairs
 4 Stairs
 96 Stairs
 Loading Zones
 Bicycles
 73,214 SF
 15,426 SF
 21.2%



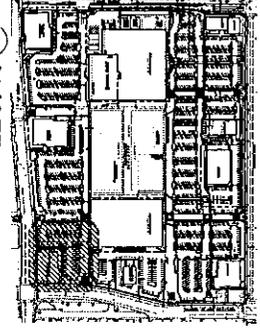
1 PARTIAL SITE PLAN '1'
 1" = 30'-0"



STANDARD PARKING STALL SHALL BE STRIPED W/ WHITE PAINT.
 COMPACT STALLS SHALL BE STRIPED W/ YELLOW PAINT.



SCALE: NO SCALE
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2 KEY PLAN
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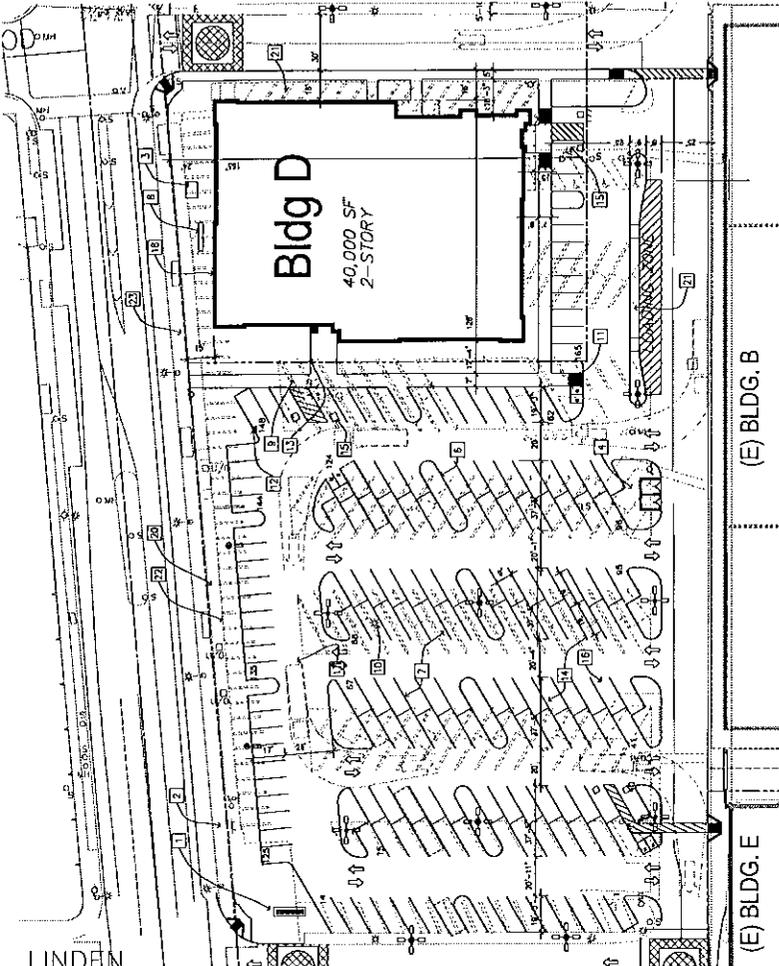
Centerpoint Mall Master Plan
 CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



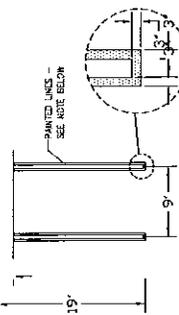
KEYNOTES:

- 17. RELOCATE BUS STOP AND BENCH.
- 18. RELOCATE CONCRETE RAMP TO ADJACENT SIDE OF ROADWAY TO ALLOW FOR TRUCK TURNING MANEUVER.
- 19. RELOCATE TRUCK TURNING MANEUVER TO ADJACENT SIDE OF ROADWAY.
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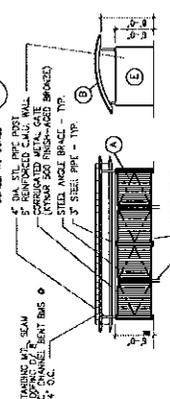
PARTIAL SITE ANALYSIS:
 MULTI-TENANT RETAIL BUILDING WITH REDUCED ACCESS AND PARKING
 OCCUPANCY GROUP
 CONSTRUCTION TYPE
 PARTIAL SITE AREA
 BUILDING - 0
 Partic. Site Coverage
 PARKING:
 PROPOSED
 6 Spaces
 1455 SQ. FT.
 5 Spaces
 177 Spaces
 2 Spaces
 6 per Foot
 9
 1/8
 123,111 SF
 40,000 SF
 52.2%



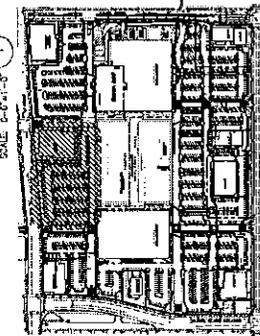
1 PARTIAL SITE PLAN 'D'
 1" = 30'-0"



STANDARD PARKING STALL SHALL BE STRIPPED W/ WHITE PAINT.
 COMPACT STALLS SHALL BE STRIPPED W/ YELLOW PAINT.
 PARKING STRIP



1. COLOR SELECTION FOR EXTERIOR GLASS SHALL BE TO FRAME - TYP.
 2. FOR ADDITIONAL INFO, VERIFY W/ BRAND'S STANDARD PLATE #804
 TRASH ENCLOSURE ELEV.
 SCALE: 1/4" = 1'-0"



2 KEY PLAN
 1/8" = 1'-0"

Centerpoint Mall Master Plan
 CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



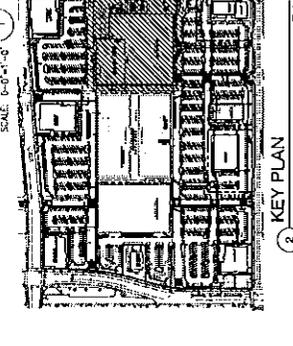
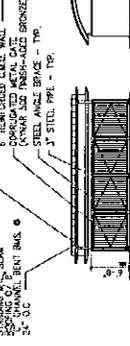
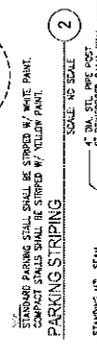
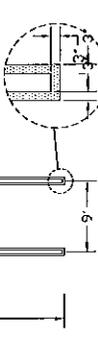
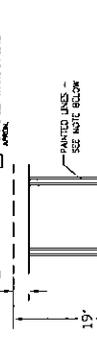
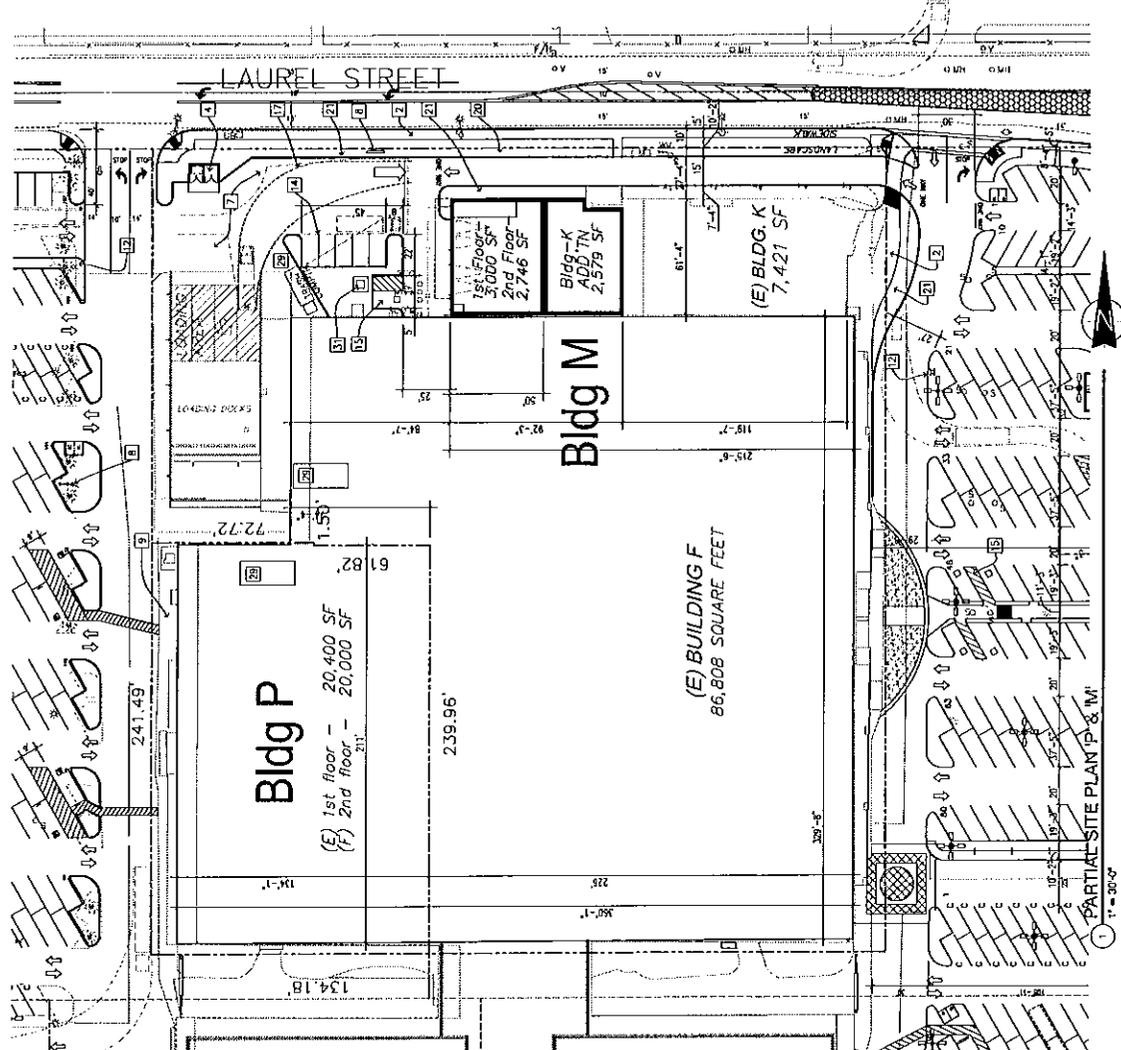
KEYNOTES:

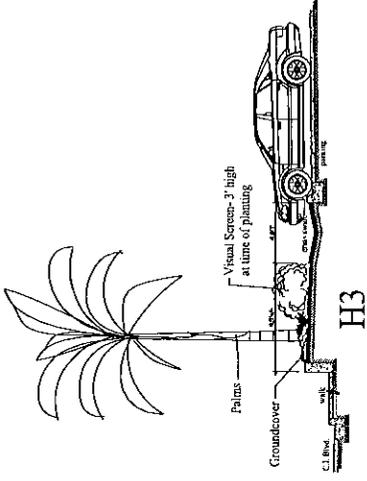
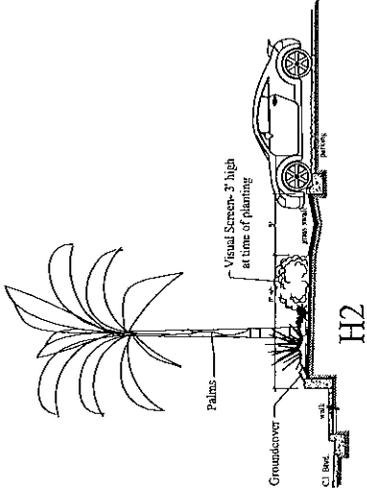
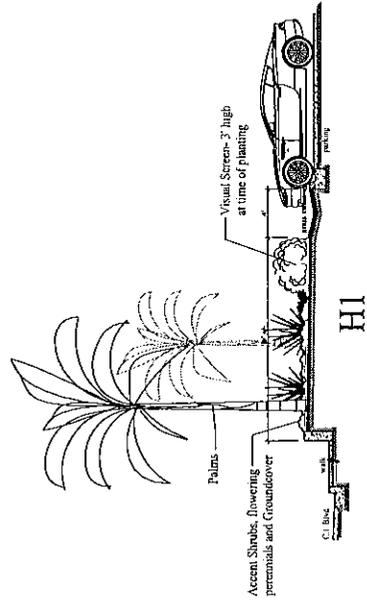
1. TRUCK TRAILER MARKS
2. IF TRUCK TRAILER MARKS
3. FIRE DEPARTMENT CONNECTION
4. LINE OF ABANDONED ROW
5. 800 SMALL AREA
6. PROPOSED LANDSCAPE AREAS
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PARTIAL SITE ANALYSIS:

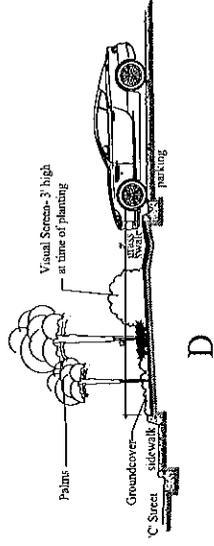
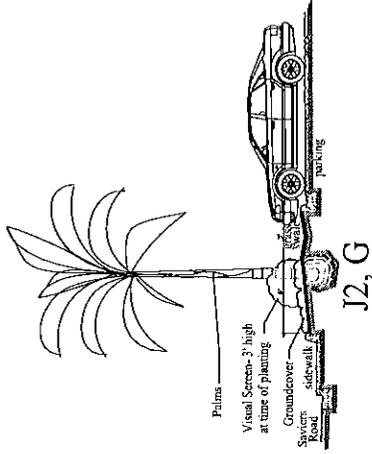
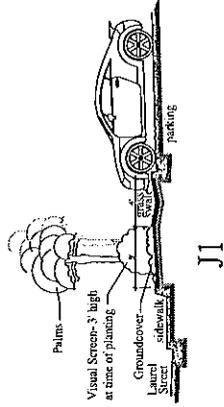
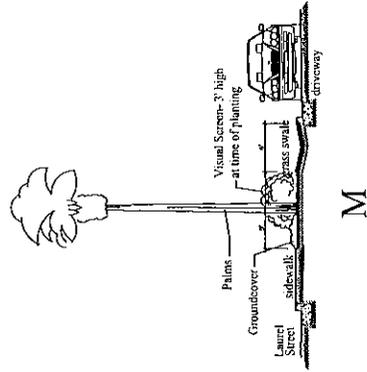
MULTI-FAMILY RETAIL BUILDING WITH RESIDENTIAL ACCESS AND PARKING	14,225 SF
OCCUPANCY GROUP	14,225 SF
CONSTRUCTION TYPE	14,225 SF
PARTIAL SITE AREA	14,225 SF
EXISTING BUILDINGS:	
(A) Auto Zone (Future Bldg)	36,800 SF
(B) Auto Zone (Future Bldg)	4,550 SF
(C) Maintenance	2,479 SF
(D) V.I. Health Services	20,000 SF
PROPOSED BUILDING:	
Building - 1 (Retail FC - Health Servs.)	20,000 SF
Building - 2 (Retail FC - Health Servs.)	5,840 SF
Building - 3 (Retail FC - Health Servs.)	2,746 SF
Building Area	142,558
Site Coverage	77.0%

- PROVIDED:**
- 1. Streets
 - 2. Sidewalk
 - 3. Signage
 - 4. Signage
 - 5. Signage
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Channel Islands Blvd.-- Phase 'H'



Laurel Street

Saviers Road

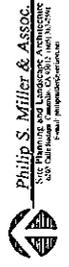
'C' Street

LANDSCAPE MASTERPLAN SECTIONS

SCALE: 1/4" = 1'-0"



Centerpoint Mall Master Plan
CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033

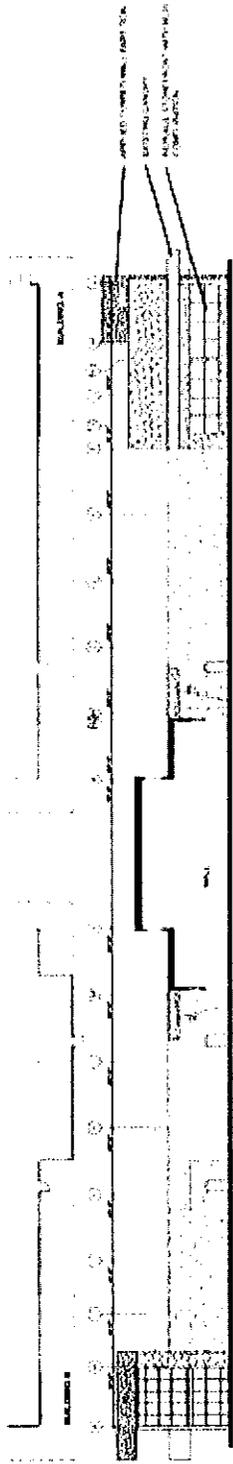


Philip S. Miller & Assoc.
Site Planning and Landscape Architecture
450 Fairmead Parkway, Oxnard, CA 93033
Phone: (805) 461-1111
Fax: (805) 461-1112

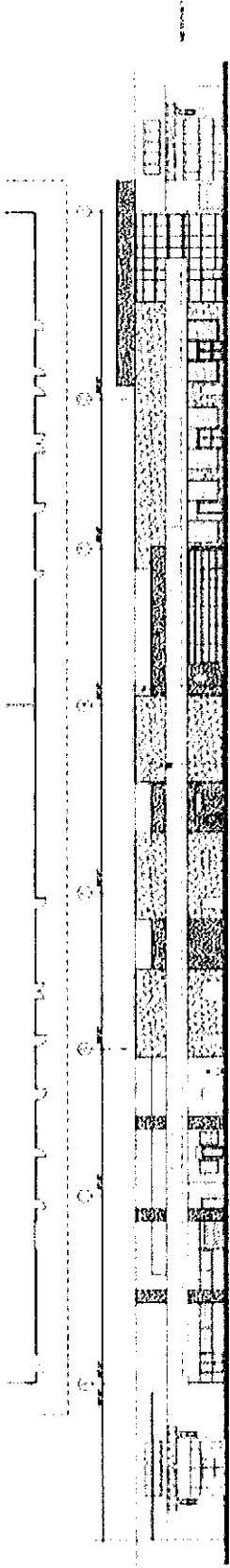
Oct. 16, 2004



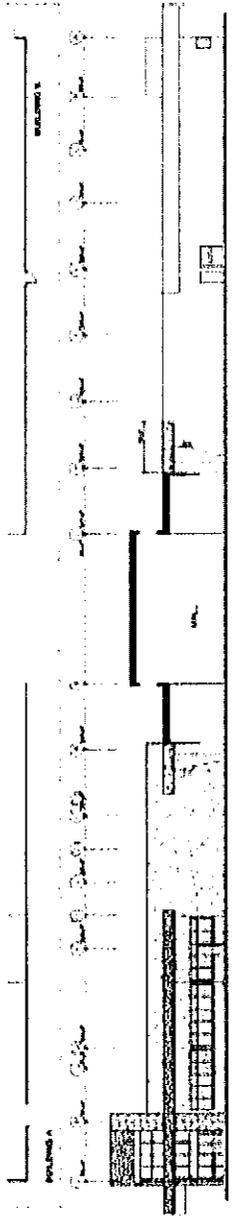
03/04



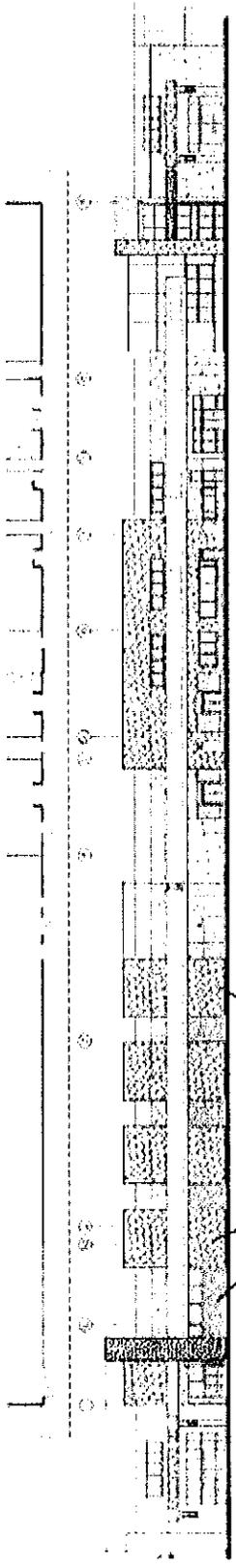
SOUTH ELEVATION



WEST ELEVATION

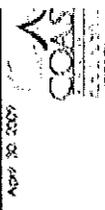


NORTH ELEVATION



EAST ELEVATION

BUILDING A/B
PROPOSED FACADE ELEVATIONS



Centerpoint Mall Master Plan
CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



ATTACHMENT C

Mitigated Negative Declaration #09-01



PLANNING DIVISION
214 SOUTH C STREET
OXNARD, CALIFORNIA 93030

**NOTICE OF INTENT
TO ADOPT A MITIGATED NEGATIVE DECLARATION
MND # 09-01**

The City of Oxnard Planning Division has reviewed an application on the following proposed project:

PLANNING AND ZONING PERMIT NO. 08-540-1 (Planned Development Permit), request for approval of a Master Plan to guide future development of the Centerpoint Mall site located at 2655 Saviers Road. New structures would be built on an as-needed basis, potentially spanning the next ten years. Currently, the shopping center site consists of 369,057 square feet. Build-out of the Master Plan would increase development on the site to 450,759 square feet. The parking lot would be reconfigured to provide landscaping, travel aisles and loading zones. The number of parking spaces would be reduced from 1,845 existing spaces to 1,320 proposed spaces. Administrative Relief of Parking Provisions is requested to provide fewer parking spaces than required by ordinance. Entrance drive improvements with new sidewalks to facilitate pedestrian access are also proposed. Several improvements to Saviers Road and Laurel Street are proposed for traffic safety. The Master Plan includes Architecture, Lighting and Sign Guidelines. Filed on behalf of Oxnard Center Mall by Coastal Architects, 505 South A Street, Oxnard, CA 93030.

In accordance with Section 15070 of the California Code of Regulations, the Planning Division of the City of Oxnard has determined that there is no substantial evidence that the proposed project would have a significant effect on the environment, and that a mitigated negative declaration (MND) may be adopted.

The draft document may be reviewed online, from the City webpage. Go to www.planning.cityofoxnard.org, then "Environmental Documents" (on the right side of the page) to select and view the draft document.

Alternatively, the draft document is available for review at the Oxnard Planning Division office, 214 South "C" Street (8:00 a.m. to 6:00 p.m., Monday through Thursday, and 8:00 a.m. to 5:00 p.m. on alternate Fridays), and at the Oxnard Public Library, 251 South "A" Street (9:00 a.m. to 8:00 p.m., Monday through Thursday, and 9:00 a.m. to 5:30 p.m. on Saturday and 1:00 p.m. to 5:00 p.m. on Sunday).

The public review period begins on Wednesday May 6, 2009 and ends on May 26, 2009. All comments should be provided in writing and received before 5:00 p.m. on the last day of the review period. Inquiries should be directed to Stephanie Diaz, Planning Consultant, at (805) 385- 3918 and written comments may be mailed or faxed (805/385-7417) to the City of Oxnard, Planning Division, 214 South "C" Street, Oxnard, CA 93030.

4/30/09

Date



Susan L. Martin, AICP
Planning Division Manager



PLANNING DIVISION
214 SOUTH "C" STREET
OXNARD, CALIFORNIA 93030

MITIGATED NEGATIVE DECLARATION NO. 09-01

On the basis of an initial study, and in accordance with Section 15070 of the California Code of Regulations, the Planning Division has determined that there is no substantial evidence that the proposed project may have a significant effect on the environment:

PLANNING AND ZONING PERMIT NO. 08-540-1 (Planned Development Permit), request for approval of a Master Plan to guide future development of the Centerpoint Mall site located at 2655 Saviers Road. New structures would be built on an as-needed basis, potentially spanning the next ten years. Currently, the shopping center site consists of 376,677 square feet. Build-out of the Master Plan would increase development on the site to 469,767 square feet. The parking lot would be reconfigured to provide landscaping, travel aisles and loading zones. The number of parking spaces would be reduced from 1,845 existing spaces to 1,314 proposed spaces. Administrative Relief of Parking Provisions is requested to provide fewer parking spaces than required by ordinance. Entrance drive improvements with new sidewalks to facilitate pedestrian access are also proposed. Several improvements to Saviers Road and Laurel Street are proposed for traffic safety. The Master Plan includes Architecture, Lighting and Sign Guidelines. Filed on behalf of Oxnard Center Mall by Coastal Architects, 505 South A Street, Oxnard, CA 93030.

Attached is a copy of the initial study documenting the reasons to support the finding of no significant effect on the environment. Mitigation measures are included in the initial study to reduce the identified potential effects to a less than significant level:

- Air Quality
- Cultural Resources
- Geology/Soils
- Hazardous Materials
- Hydrology
- Noise
- Traffic

In response to a comment received during the public review period and the applicant's correction of the net new square footage, minor changes have been made in the MND and Initial Study. No new significant impacts were created. These changes are shown in *Italics*.

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
Air Quality (Short-term)	Temporary Minor Impact	<p>C-1 Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.</p> <p>C-2 During smog season (May through October), Developer shall lengthen the construction period so as to minimize the number of vehicles and equipment operating at the same time.</p> <p>C-3 At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.</p> <p>C-4 During construction, Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.</p> <p>C-5 During construction, Developer shall control dust by the following activities:</p> <ul style="list-style-type: none"> • All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads. • All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally-safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible. <p>C-6 During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.</p> <p>C-7 During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and</p>	Less Than Significant Impact	<p>At plan check: Planning Division</p> <p>Onsite: Development Services</p>

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
		<p>excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.</p>		
Cultural Resources	Potentially Significant	<p>C-8 Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.</p> <p>E-1 Developer shall contract with a qualified archaeologist to conduct a Phase I cultural resources survey of the project site prior to issuance of any grading permits. The survey shall include 1) an archaeological and historical records search through the California Historical Resources Information System at CalState Fullerton; and 2) a field inspection of the project site. Upon completion, the Phase I survey report shall be submitted to the Planning Division for compliance verification. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to initiation of the Phase I activities.</p> <p>The contract shall include provisions in case any cultural resources are discovered onsite. In the event that any historic or prehistoric cultural resources are discovered, work in the vicinity of the find shall be halted immediately. The archaeologist shall evaluate the discovery and determine the necessary mitigations for successful compliance with all applicable regulations. Developer or its successor in interest shall be responsible for paying all salaries, fees and the cost of any future mitigation resulting from the survey.</p> <p>E-2 Developer shall contract with a Native American monitor to be present during all subsurface grading, trenching or construction activities on the project site. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building permits.</p>	Less Than Significant Impact	Prior to grading permit issuance: Planning Division. Onsite: Development Services
Geology and Soils	Potentially Significant	<p>F-1 Developer shall submit a site-specific soils investigation, which shall be prepared by a licensed geotechnical engineer. At a minimum, such study shall include liquefaction and compressible soil characteristics on the subject site, and shall identify any necessary construction techniques or other mitigation measures to prevent significant liquefaction/compressible soils impacts upon the proposed project. All recommendations of said report shall be incorporated into the project. Developer shall submit the report to the Building Official for review and approval prior to issuance of building permits.</p>	Less Than Significant Impact	Development Services

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
Hazardous Materials	Potentially Significant	G-1 Prior to demolition or excavation within the boundaries of the two sites, documentation of appropriate closure shall be required.	Less Than Significant Impact	Planning Division
Hydrology and Water Quality	Potentially Significant	H-1 Developer shall submit calculations verifying compliance with NPDES storm water quality requirements prior to issuance of a Grading or Building Permit for each phase/building	Less Than Significant Impact	Development Services
Transportation and Traffic	Potentially Significant	O-1 Extend raised median on Laurel Street at Santa Clara High School to edge of eastern school driveway to prevent right turns out of the school driveways into opposing traffic lane to improve safety. Master Plan to provide at first building permit.	Less Than Significant Impact	Development Services and Traffic Engineering
		O-2 Dedicate right-of-way for widening Laurel Street at the Saviers Road intersection at building permit for Building J to accommodate Laurel Street widening for potential project traffic added to Laurel Street.		
		O-3 Widen the Laurel St./Saviers Rd. intersection to add a second left-turn pocket from east-bound Laurel Street onto Saviers Road to address existing and potentially greater queuing with project traffic on Laurel west of the Saviers intersection. Provide striping for the second left-turn pocket. Extend raised median on Laurel Street up to Saviers Road to address safety of turning movements from the high school onto Laurel Street. Modify signal pole and mast arm length to accommodate eastbound Laurel Street new left turn lane. Master Plan to provide at reconstruction of the Ritmo building on the corner of Saviers and Laurel.		
		O-4 On Saviers Road south of Laurel at the northern shopping center driveway, provide a south-bound right-turn pocket into the center with a bus stop and shelter to provide a separate lane for turns into the shopping center and to allow semi-truck deliveries to move out of the traffic flow to turn into the center. Master Plan to provide with reconstruction of Ritmo building or construction of Building G, whichever occurs first.		
		O-5 On Saviers Road at the southern shopping center driveway, provide a southbound right-turn pocket into the shopping center driveway to provide a separate lane for turns into the shopping center. Master Plan to provide with construction of Building G.		
		O-6 On Saviers Road at the Wells Fargo Bank, dedicate an 11' 1" wide strip along the		

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
Noise	Potentially Significant	<p>Saviors Road parcel frontage for a southbound right-turn lane onto Channel Islands Blvd. The Master Plan shall provide the right-of-way at the time the Wells Fargo building is remodeled or demolished to remove the portion of the building that encroaches.</p> <p>K-1. During all excavation and grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.</p> <p>K-2. Construction times shall be limited to 7 a.m. to 7 p.m. daily or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.</p>	Less Than Significant Impact	Development Services

- Attachments:
- A. Initial Study/MND No. 09-01
 - B. Reduced Project Plans
 - C. Air Emission Modeling
 - D. Traffic Study
 - E. Applicant's Agreement to Mitigation Measures
 - F. Letter from County of Ventura Environmental Health Division, dated May 19, 2009
 - G. Letter from NSN Engineering



Planning Division
214 S. "C" Street
Oxnard, CA 93030
805/385-7858
FAX 805/385-7417

Attachment A

INITIAL STUDY NEGATIVE DECLARATION 09-01

Centerpoint Mall Master Plan and Building M Remodel
Planned Development Permit and Administrative Relief for Parking
PZ 08-540-1

2655 Saviers Road
Between Saviers Road and C Street at Channel Islands Boulevard

Coastal Architects on Behalf of Oxnard Center Company
505 South 'A' Street, 2nd floor
Oxnard, CA 93030

April 2009

Updated for Response to Comments June 2009

Introduction

This *Initial Study* has been prepared in accordance with relevant provisions of the *California Environmental Quality Act (CEQA) of 1970*, as amended, and the *CEQA Guidelines* as revised. *Section 15063(c)* of the *CEQA Guidelines* indicates that the purposes of an Initial Study are to:

1. Provide the Lead Agency (i.e., the City of Oxnard) with information to use as the basis for deciding whether to prepare an Environmental Impact Report (EIR) or Negative Declaration;
2. Enable an applicant or Lead Agency to modify a project, mitigating adverse impacts before an EIR is prepared, thereby enabling the project to qualify for a Negative Declaration;
3. Assist the preparation of an EIR, if one is required, by:
 - Focusing the EIR on the effects determined to be significant;
 - Identifying the effects determined not to be significant;
 - Explaining the reasons why potentially significant effects would not be significant; and
 - Identifying whether a program EIR, tiering, or another appropriate process can be used for analysis of the project's environmental effects.
4. Facilitate environmental assessment early in the design of a project;
5. Provide documentation of the factual basis for the finding in a Negative Declaration that a project will not have a significant effect on the environment;

6. Eliminate unnecessary EIRs; and
7. Determine whether a previously prepared EIR could be used with the project.

The City of Oxnard *Threshold Guidelines - Initial Study Assessment* (February 1995) was used along with other pertinent information for preparing the *Initial Study* for this project.

The purpose of the *Threshold Guidelines* is to inform the public, project applicants, consultants and City staff of the threshold criteria and standard methodology used in determining whether or not a project (individually or cumulatively) could have a significant effect on the environment. Furthermore, the *Threshold Guidelines* provide instructions for completing the *Initial Study* and determining the type of environmental document required for individual projects.

Determining the significance of environmental impacts is a critical and often controversial aspect of the environmental review process. It is critical because a determination of significance may require that the project be substantially altered, or that mitigation measures be readily employed to avoid the impact or reduce it below the level of significance. If the impact cannot be reduced or avoided, an Environmental Impact Report (EIR) must be prepared. An EIR is a detailed statement that describes and analyzes the significant environmental impacts of a proposed project, discusses ways to reduce or avoid them, and suggests alternatives to the project, as proposed. The preparation of an EIR can be a costly and time-consuming process.

Determining the significance of impacts is often controversial because the decision requires staff to use their judgment regarding a subject that is not clearly defined by the law. The State CEQA *Guidelines* define the term "significant impact on the environment" as a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project. However, there is no iron-clad definition of what constitutes a substantial change because the significance of an activity may vary according to location.

To help clarify and standardize decision-making in the environmental review process, Oxnard has developed thresholds of environmental significance. Thresholds are measures of environmental change that are quantitative for subjects like noise, air quality, and traffic; and qualitative for subjects like aesthetics, land use compatibility, and biology. These thresholds are used in the absence of other empirical data to define the significance of impacts. For some projects, however, special studies and/or the professional judgment of City staff may enter into the decision-making process. Therefore, Oxnard's thresholds are intended to serve as guidelines, and to augment existing CEQA provisions governing the definition of significance.

The City's environmental thresholds will be periodically updated as new information becomes available, or as standards regarding acceptable levels of environmental change are reevaluated. For example, the air quality thresholds adopted by Oxnard were established through State and Federal legislation. These standards, and the methodology used to compute them, may change over time. When this occurs, the City will evaluate the data and, if necessary, modify the thresholds to reflect improved awareness.

When other agencies have jurisdiction over a given site, the project proponent will have to meet the design, mitigation, and monitoring requirements imposed by those agencies, as well as any additional requirements established by the City of Oxnard.

CITY OF OXNARD

INITIAL STUDY ENVIRONMENTAL CHECKLIST FORM

1. Project Title: Centerpoint Mall Master Plan and Building M Remodel.
2. Lead Agency Name and Address: City of Oxnard, Planning Division, 214 S. "C" Street, Oxnard, CA 93030
3. Contact Person and Phone Number: Stephanie Diaz, Contract Planner, 805-385-3918
4. Project Location: Between Saviers Road and C Street at Channel Islands Boulevard, 2655 Saviers Road, APNs 203-0-320, -185 -205, -245, -265, -285, -295 -315, -335 -355, -375-435.
5. Project Applicant Name and Address: Michael Sanchez, Coastal Architects, 505 S. A Street, Oxnard, CA, 93030 on behalf of Oxnard Center Company
6. General Plan Designation: Commercial Regional
7. Zoning: Commercial Planned Development (CPD)
8. Description of Project: Master Plan for existing shopping center to allow the construction of 89,422 square feet and the removal of 7,720 square feet for a net new total of 81,702 square feet. This includes removal of three existing uses (Ritmo, Oxnard Dental and Oasis) in one building, the construction of two new buildings (Bldgs D and G), a new replacement building for Ritmo (Bldg J) and additions to four existing buildings (Bldgs I, H, M and P).

The proposed building square footage is as follows:

Building D - 40,000 sf office, new
Building G - 6,000 sf retail, new
Building I - 2,000 sf medical office, addition
Building H - 2,000 sf food sales, addition
Building J - 11,097 sf retail, replacement
Building M - 8,325 sf, storage, maintenance
Building P - 20,000 sf office, addition

Buildings proposed to be removed are as follows:

Ritmo/Oasis/Ox Dental – 7,720 sf retail/office

Each phase will be built on an as-needed basis potentially spanning the next ten years. The shopping center is currently 369,057 square feet in size. With the project, the shopping center size will increase to 450,759 square feet. The existing parking lot would be reconfigured and the number of parking spaces would be reduced from 1,845 existing spaces to 1,320 proposed spaces. Administrative Relief

Each phase will be built on an as-needed basis potentially spanning the next ten years. The shopping center is currently 376,677 square feet in size. With the project, the shopping center size will increase to 469,767 square feet. The existing parking lot would be reconfigured and the number of parking spaces would be reduced from 1,845 existing spaces to 1,314 proposed spaces. Administrative Relief of Parking Provisions is requested to provide 1,314 spaces rather than the 1,879 spaces required by ordinance and to relocate a portion of the existing compact parking spaces. The parking lot would be reconfigured in several areas addressing parking and loading-zone lay-out, lighting and landscaping. In the main mall, only Building M, the existing Auto Zone store, is proposed to be remodeled to change the façade and to add a second story. Entrance drive improvements to facilitate pedestrian access with new sidewalks are also proposed. Road improvements on Saviers and Laurel will be provided for traffic safety. See Attachment 1 and 2 for site plans. The Master Plan includes Architecture, Lighting and Sign Guidelines.

9. Surrounding Land Uses and Setting:

DIRECTION	ZONING	LAND USE DESIGNATION	LAND USE
North	R-2	School	Santa Clara High School (Private)
East	C2	Commercial General	Commercial Shopping/Center
South	R-3/C2-PD	Residential Low/Commercial General	Self Storage/Residential complex
West	C2-PD	School, Commercial Office & Residential High	St Anthony School/Church, Residential

The project is located in the Kamala Park Neighborhood and in the redevelopment area known as HERO (Historic Enhancement and Revitalization of Oxnard). The City of Oxnard is not providing supplemental financing or tax incentives for the project. In 1965, the Planning Commission approved Special Use Permit U-187 for construction of a “Unimart Store and other commercial outlets.” Several modifications occurred to U187 granting additional square footage and new uses. In 1979, the Planning Commission approved Special Use Permit U813 approving the enclosure of the mall and modifications to the parking lot. This enclosure created the existing interior mall design. Since 1979, several modifications to U813 and U187 have been filed and approved, for tenant improvements to the mall that conform to the City of Oxnard Zoning Ordinance including a new Building C at the corner of “C” Street and Laurel Street. On April 17, 2008 the Planning Commission approved Planned Development Permit PZ 07-540-4 for demolition of an existing building (Yolandas Restaurant) and construction of a new building at this location (Building “S”). Several lot line adjustments have been processed to provide individual lots for some of the commercial buildings.

10. Other agencies whose approval is required (e.g., permits, financing approval, or participating agreement): None known at this time.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

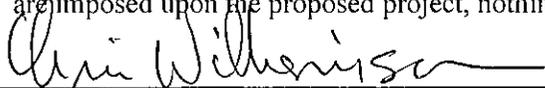
The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" or as indicated by the checklist on the following pages.

- | | | |
|---|---|--|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agricultural Resources | <input checked="" type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input checked="" type="checkbox"/> Cultural Resources | <input checked="" type="checkbox"/> Geology/Soils |
| <input checked="" type="checkbox"/> Hazards & Hazardous Materials | <input checked="" type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Land Use/Planning |
| <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Population/Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input checked="" type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

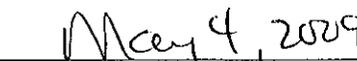
DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find the proposed project COULD NOT have a significant effect on the environment and a NEGATIVE DECLARATION will be prepared.
- I find that although the project could have a significant effect on the environment there will not be a significant effect in this case because revisions in the project have been made or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



Signature



Date

CHRIS WILLIAMSON

Print Name

SENIOR PLANNER

Title

EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analyses,” cited in support of conclusions reached in other sections may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a. Earlier Analysis Used—Identify and state where they are available for review.
 - b. Impacts Adequately Addressed—Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c. Mitigation Measures—For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. The explanation of each issue should identify: a) The significance criteria or threshold, if any, used to evaluate each question; and b) The mitigation measure identified, if any, to reduce the impact to less than significance.

A. AESTHETICS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect on a scenic vista? (2020 General Plan, VIII - Open Space/ Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? (2020 General Plan, VIII - Open Space/ Conservation Element; XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
3. Substantially degrade the existing visual character or quality of the site and its surroundings? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
4. Create a source of substantial light or glare, which would adversely affect day or nighttime views in the area? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

Discussion:

- 1-3) The project site is located within a completely developed portion of the city. According to the 2020 General Plan, the aesthetic resources in this area are north-south streets that provide views of the mountains to the north and east-west streets that provide views eastward toward the Santa Monica Mountains. Both Saviers Road, which borders the project site on the east, and Channel Islands Boulevard, which borders the project to the south, are designated scenic highways and provide such views. Construction of the Master Plan project on this site will not affect such views along Saviers Road or Channel Islands Boulevard, and there are no other scenic vistas in the immediate vicinity of the project site. In addition, all aspects of the project are subject to review by the Development Advisory Committee (DAC) and a follow-on Development Design Review Permit for new or remodeled buildings. The project will be required to meet the city's applicable landscape and zoning standards. The Master Plan project includes Sign and Architecture Guidelines to assure aesthetic development of future components on the site. Other natural scenic resources within the City include beaches, coastline, agricultural areas and parks. This project site is not within or adjacent to these scenic resources. **Therefore, no impacts related to scenic resources and visual quality are expected to result from this project.**
- 4) The proposed project will not create new additional sources of light since this project site is currently developed as a low-rise shopping mall and has existing parking lot and building lighting. The City has adopted policies and standards to regulate the minimum and maximum illumination standards, as well as to reduce light spillover onto adjacent properties and to avoid hazards for motorists. Current city standards are more specific and stricter than standards that were in effect at the time the existing shopping center was originally built. As a standard condition of project approval, the Planning Division will require review and approval of a photometric plan

prepared by an electrical engineer certifying the exterior illumination intensities provided by any new or replacement light standards and/or exterior building lighting devices, such as wall mounted light fixtures, are designed to provide lighting within the property limits. **The impacts due to lighting are expected to be less than significant.**

Mitigation: Based on the discussion provided above, less than significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: No monitoring is required.

Result After Mitigation: NA

B. AGRICULTURAL RESOURCES*

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Conflict with existing zoning for agricultural use, or a Williamson Act contract? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
3. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

* In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agricultural and farmland.

Discussion:

1-3) The site is located in an urbanized area and is currently substantially developed with a regional shopping center. This site is not included in a Williamson Act Contract. The nearest significant agricultural property is unincorporated farmland located to the northeast. Unincorporated areas that are located outside of the City's Urban Restriction Boundary (CURB) and agricultural farmland is subject to the provisions of the 1998 "Save Open Space and Agricultural Resources" (SOAR) initiative which was adopted by the voters in Ventura County in 1998. Development of the subject site is not expected to affect available use of existing agricultural land. **No significant impacts related to agriculture are expected.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: No monitoring is required.

Result After Mitigation: NA

C. AIR QUALITY*

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Conflict with or obstruct implementation of the applicable air quality plan? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Violate any air quality standard or contribute substantially to an existing or projected air quality violation? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is nonattainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
4. Expose sensitive receptors to substantial pollutant concentrations? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
5. Create objectionable odors affecting a substantial number of people? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

* Where available, the significant criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.

Discussion:

It is the applicant's responsibility to contact the Ventura County Air Pollution Control District (APCD) to verify compliance with any permitting needs of the APCD.

1-3) Short-term Impacts: Short-term impacts to air quality will likely result from demolition, grading, and other

construction activities associated with the project (e.g., earth-moving and heavy equipment vehicle operations) and may be spread over a period up to ten years as the Master Plan is built out. According to the Urbemis report, Attachment 3, construction emissions for the entire project is 85.84 ROG and 54.46. The APCD thresholds for ROG and NO_x do not apply to construction emissions since they are temporary. For construction emissions that exceed the 25 lbs per day threshold, APCD recommends imposition of mitigation measures for fugitive dust and construction equipment. **With standard APCD construction conditions of approval, less than significant impacts are anticipated.**

Depending upon the types of building materials that were used and the year in which the buildings to be demolished were built, there may be asbestos in the buildings. Exposure to asbestos may cause serious health effects. Asbestos is likely to be found in buildings constructed before 1979. Demolition or renovation activities involving asbestos materials also are subject to APCD Rule 62.7, Asbestos, Demolition and Renovation. The District's Compliance Division should be contacted at 805/645-1443 to determine any asbestos inspection and compliance requirements before commencing demolition or renovation of any building. **Compliance with APCD Rule 62.7 is adequate to ensure that asbestos will not cause a significant adverse impact.**

With inclusion of standard air quality mitigation measures identified in the City's Threshold Guideline—Initial Study Assessment (February 1995) and compliance with APCD permitting and asbestos requirements, **short-term air quality impacts would be considered less than significant.**

Long-term Impacts: The City's adopted threshold for Reactive Organic Compounds (ROC) and Nitrogen Oxide (NO_x) emissions is 25 pounds per day (ppd). Project-specific emissions have been calculated using the *URBEMIS 2007, Version 9.2.4* computer modeling program. This site is currently developed with a shopping center and office uses. The increase in air pollutant emissions resulting from the new projects were determined by adding the square footage of the new uses. It has been determined that the proposed commercial development, consisting of retail, medical office, general office and restaurant uses, will generate increased emissions. The increase is projected to be 10.21 pounds per day ROG, and 10.58 pounds per day NO_x. (See Attachment 3, Air Emission Modeling) The NO_x pollutant amount is less than the 25-ppd threshold limit. *The addition of 11,300 s.f. to the original project description would not increase the pollutant emissions to threshold levels. This is considered a less than significant impact.*

4. Sensitive Receptors: Although the Ventura County Air Basin is currently an attainment area for CO, exhaust emissions can potentially cause a direct, localized "hotspot" impact at or near the proposed development. CO is a product of incomplete combustion of fossil fuel; unlike ozone, CO is emitted directly out of a vehicle exhaust pipe and is heavier than air. The optimum conditions for a CO hotspot is cool and calm weather (a stable and reduced air mixing layer) at a congested major roadway intersection with sensitive receptors nearby, and where vehicles are either idling or moving at a stop-and-go pace. Sensitive receptors within the project vicinity include residents to the west of the site, park users to the northwest of the site and Santa Clara High School to the north of the site. The only intersection with congestion adjacent to the project site is the Saviers Road/Channel Islands Blvd intersection. According to the project traffic report (Oxnard Centerpoint Traffic and Parking Study, Austin-Faust Assos., January 7, 2009), this intersection is operating at Level of Service "C." The project will only cause an increase in intersection operation of 0.01 in ICU. The APCD defines CO hotspots as intersections that operate at LOS E or F. Less than significant impacts are anticipated for sensitive receptors.

5. Odors. The project consists of retail, food and office uses and is not expected to generate any objectionable odors. **No significant impacts are anticipated.**

Mitigation: The following mitigation measures shall apply:

Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.

- C-2 During smog season (May through October), Developer shall lengthen the construction period so as to minimize the number of vehicles and equipment operating at the same time.
- C-3 At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.
- C-4 During construction, Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
- C-5 During construction, Developer shall control dust by the following activities:
- All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads.
 - All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally-safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible.
- C-6 During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.
- C-7 During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.
- C-8 Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.
- C-9 Comply with APCD Rule 62.7 regarding asbestos detection and removal.

Monitoring: Planning staff shall check all plans prior to permit issuance to ensure that the above measures are incorporated into the project. The Building Official, or designee, shall monitor all applicable measures in the field until construction is completed. Proof of compliance with APCD asbestos requirements shall be provided to Planning staff.

Result after mitigation: With incorporation and monitoring of the above mitigation measures, air quality impacts would be considered less than significant.

D. BIOLOGICAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
3. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
4. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
5. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

D. BIOLOGICAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
6. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? (2020 General Plan, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

The site is a previously graded and fully developed parcel with a regional shopping center and associated parking lots in an urbanized neighborhood. The project site does not lie within an existing or proposed habitat or conservation plan area and does not contribute to regional wildlife movement since it is surrounded by commercial development and a major roadway. An arborist’s report by Trees, etc, dated September 23, 2007, describes the existing on-site trees. As of the report date, 370 large non-native trees, 438 palm shrubs as well as 681 palm and shrubs around buildings existed on-site. In accordance with the Oxnard Tree Preservation and Protection ordinance, the trees were evaluated to determine the value of those trees. Each tree was evaluated by their surface root systems, trunks, foliage, and branches. An appraised value for the trees is provided in the report. The project landscape plan prepared by Philip Miller & Assoc indicates that 58 non-native trees will be removed. Most removals are in existing parking lot landscape planters and currently block visual access in the parking areas. Although the subject trees are not considered to be native or protected vegetation, the City’s standard condition will require the developer to pay the assessed value into upgrading trees on-site. **No significant impacts related to biology are expected.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: No monitoring is required.

Result After Mitigation: NA

E. CULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>

E. CULTURAL RESOURCES	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
Would the project:				
2. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
3. Directly or indirectly destroy a unique paleontological resource or site or unique geological feature? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
4. Disturb any human remains, including those interred outside of formal cemeteries? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-4) The Oxnard Plain, on which the City lies, has a history of human habitation for thousands of years. Portions of Ventura County were occupied by early Native American cultures from about 3,500 years ago to approximately the first century A.D. Chumash Indians settled in the area around 1500 A.D. Literature searches undertaken through the UCLA Institute of Archaeology, conducted between 1984 and 1986 identified seven archaeological sites in the County. Records checks conducted through the South Central Coastal Information Center (SCCIC) indicated that cultural resources have been found in various places throughout the City.

The site is currently developed with buildings, parking and landscaping. This development has existed on the property since 1962 which has graded and disturbed the entire site. Due to the history of previously recorded and discovered archaeological and ethnographic resources within the general area of the proposed project site, the potential exists that previously unknown, subsurface resources might exist on site that could be disturbed by grading and other subsurface activities associated with the proposed development. **With mitigation, this is considered to be a less than significant impact.**

Mitigation:

E-1 Developer shall contract with a qualified archaeologist to conduct a Phase I cultural resources survey of the project site prior to issuance of any grading permits. The survey shall include 1) an archaeological and historical records search through the California Historical Resources Information System at CalState Fullerton; and 2) a field inspection of the project site. Upon completion, the Phase I survey report shall be submitted to the Planning Division for compliance verification. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to initiation of the Phase I activities.

The contract shall include provisions in case any cultural resources are discovered onsite. In the event that any historic or prehistoric cultural resources are discovered, work in the vicinity of the find shall be halted immediately. The archaeologist shall evaluate the discovery and determine the necessary mitigations for

successful compliance with all applicable regulations. Developer or its successor in interest shall be responsible for paying all salaries, fees and the cost of any future mitigation resulting from the survey.

E-2 Developer shall contract with a Native American monitor to be present during all subsurface grading, trenching or construction activities on the project site. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building permits.

Monitoring: Planning staff will review the Archaeological / Native American monitoring contract(s) prior to issuance of any grading permits. Planning staff will ensure the monitoring reports are received prior to Planning Division inspection for final building permit sign-off. Development Services staff will monitor onsite construction activities, as necessary.

Result after Mitigation: Upon implementation of the above mitigation measures, the cultural impacts will be less than significant.

F. GEOLOGY AND SOILS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
a. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of known fault? Refer to Division of Mines and Geology Special Pub. 42. (2020 General Plan, IX-Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
b. Strong seismic ground shaking? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
c. Seismic-related ground failure, including liquefaction? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
d. Landslides? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Result in substantial soil erosion, or the loss of topsoil? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

F. GEOLOGY AND SOILS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
3. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
4. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-4) The City of Oxnard is located in an area that has a high potential for seismic ground shaking (City of Oxnard, 2020 General Plan Figure IX-2). The City of Oxnard 2020 General Plan lists fault systems that are located within the vicinity of the City of Oxnard (City of Oxnard Table IX-1 and Figure IX-2). There are no known active faults within the City limits. However active and potentially active faults are present in the surrounding region and may extend into the subsurface beneath the City. In addition, the City of Oxnard 2020 General Plan identifies the proposed project site as being located in an area that is marked by high to moderate potential for liquefaction (City of Oxnard Figure IX-2). These ground failure risks are considered to be potentially significant impacts, therefore, a geotechnical engineering evaluation of the proposed project site will be required as part of the construction plans in order to avoid creating any significant impacts to soils and geologic resources. No landslides affect the project site. With the implementation of necessary mitigation measures to address construction concerns, **impacts due to geology and soils would be reduced to less than significant levels.**

Mitigation for Items 1.b, 3 and 4:

F-1 Developer shall submit a site-specific soils investigation, which shall be prepared by a licensed geotechnical engineer. At a minimum, such study shall include liquefaction and compressible soil characteristics on the subject site, and shall identify any necessary construction techniques or other mitigation measures to prevent significant liquefaction/compressible soils impacts upon the proposed project. All recommendations of said report shall be incorporated into the project. Developer shall submit the report to the Building Official for review and approval prior to issuance of building permits.

Monitoring: The Building Official will review the soils investigation report and shall determine if any applicable recommendations are to be incorporated into the project.

Result After Mitigation: Upon implementation of the mitigation measure, impacts are anticipated to be less than significant.

G. HAZARDS AND HAZARDOUS MATERIALS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
4. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
6. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? <i>(2020 General Plan, IX - Safety Element)</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
7. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? <i>(2020 General Plan, IX - Safety Element; City of Oxnard Emergency Preparedness Plan and Response Manual)</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

G. HAZARDS AND HAZARDOUS MATERIALS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
8. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1-4 This application consists of a request to construct commercial buildings and associated parking and landscaping. The proposed operations will consist of sales of goods and services, delivery of merchandise to the various businesses on the site and circulation and parking on the project site. Any proposed tenant that includes on-site chemicals or bio waste will be reviewed and permitted through the Oxnard Fire Department. Due to the uses allowed in the C-PD zone this project is limited to the types of businesses that are unlikely to involve hazardous materials on-site storage. The project will not emit any known hazardous materials during or after construction. To minimize hazards, as well as assist the business operators and the Fire Department in responding to emergencies involving hazardous materials, the City's Certified Unified Permitting Agency (CUPA) requires compliance with the City's hazardous materials regulations.

*During the review period for this MND, the City received a comment letter from the County of Ventura Environmental health Division. The letter indicated that the a former leaking underground fuel tank site (ID# 93-0300643) under the at the corner of Laurel Street and Saviers Road was closed in 1993. The County questioned whether MTBE was assessed prior to closure of that site. In addition, an open LARWQCB case exists for the chlorinated solvents from the former Regal Cleaners (located in the Mall adjacent to the former Mervyns building). These pollutants were present in a plume traveling southwest across the mall site. The applicant submitted a letter from NSN Engineering that provided information on the clean-up status of this site. This company has been overseeing the clean-up of this site since 1996. The site has been designated as "low risk" by CRWQCB (file 95-062). The most recent ground water monitoring was done in March 2009. It was found that pollutant concentrations have significantly decreased in all monitoring wells. A Site Closure Request was filed in 2007 and is currently under consideration by the water board. Action on this request is expected within this year. Prior to demolition or excavation within the boundaries of the two sites, documentation of appropriate closure shall be required. **With this mitigation, less than significant adverse impacts related to Hazards and Hazardous Materials are anticipated.***

5-8) The proposed project site is not located within an airport approach or clear zone adjacent to the Oxnard Airport as depicted on Figure IX-4 of the 2020 General Plan, nor is it located near a private airstrip. Moreover, the proposed project would not interfere with an adopted emergency response plan or emergency evacuation plans. Finally, the proposed project site is located within an urban area and consists of a developed lot. No wildlands exist within the vicinity of the proposed project site that could result in hazards related to wildland fires. **Therefore no impacts are anticipated from hazards related to airports or wildland fires from the proposed project.**

Mitigation:

G-1. *Prior to demolition or excavation within the boundaries of the two sites, documentation of appropriate closure shall be required.*

Monitoring: Planning Division to review documentation of appropriate closure.

Result After Mitigation: Less than significant impact.

H. HYDROLOGY AND WATER QUALITY

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Violate any water quality standards or waste discharge requirements? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
4. Create or contribute runoff water, which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
5. Otherwise substantially degrade water quality? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>

H. HYDROLOGY AND WATER QUALITY

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
6. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
7. Place within a 100-year flood hazard area structures which would impede or redirect flood flows? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
8. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
9. Inundation by seiche, tsunami, or mudflow? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

Discussion:

1,3&5) Water quality impacts associated with the proposed use would primarily be those associated with motor vehicles and landscape maintenance. The primary source of contaminants would be oil, grease, and particulates emitted by motor vehicles driving in the parking lot. There are no surface water bodies or wetlands within the vicinity of the proposed project. Minor changes to existing drainage patterns, absorption rates, and runoff rates would occur on the subject site and surrounding areas by the reorganization of the existing parking lots that cover the surface of the site. Generally, there would be little change in impervious surfaces on-site. In general, the amount of impervious surfaces would be similar or less than the amount of the previous development. In addition, the National Pollution Discharge Elimination System (NPDES) has been introduced since the previous buildings were built, and the proposed project will be required to comply with the NPDES program, which will result in cleaner water introduced into the City's storm drain system than with the previous project. In order to comply with the (NPDES) requirements for a permit to discharge storm water and NPDES requirements for a construction permit, new construction projects are required to control storm water pollution during construction and after construction is complete by the use of best management practices (BMPs) that are appropriate and applicable to the project. The City of Oxnard has incorporated several standard conditions requiring the applicant to incorporate BMPs into these types of project, and the project would be subject to the review and approval of the City of Oxnard in order to verify compliance with NPDES requirements. A Storm Water Pollution and Prevention Plan will be required to mitigate water quality issues. *Also, see discussion of hazardous materials related to ground water contamination under Item G above.* **With mitigation, less than significant impacts to water quality are anticipated as a result of this project.**

2) The project site is located in the Calleguas Water District. The project would be served by City municipal water; the City obtains most of its water from the Calleguas Water District, which in turn purchases most its water from the Metropolitan Water District of Southern California. Other sources of water include local well water from United Water Conservation District and City wells. In order to address water supply needs at a regional level, representatives of the City of Oxnard, the Port Hueneme Water Agency (PHWA), the United Water Conservation District (UWCD), and the Calleguas Municipal Water District (CMWD) meet regularly. A collective effort to ensure contented delivery of high quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program, which will result locally in the construction of a new, regional groundwater desalination facility to serve Oxnard and Port Hueneme. **Less than significant impacts are anticipated.**

6-9) The proposed project site is located outside of the 100-year flood plain, within an area of minimal flooding (FEMA Map #060417 0020 C). In addition, the proposed project is not located near the coastline, creeks, or drainage courses and, as such, is not located within an area that is prone to inundation by seiche, tsunami, or mudflow. **Therefore, no significant impacts are anticipated from these risks.**

Mitigation:

H-1 Developer shall submit calculations verifying compliance with NPDES storm water quality requirements prior to issuance of a Grading or Building Permit for each phase/building.

Monitoring: Development Services Staff shall review and approve the calculations prior to issuance of a Grading or Building Permit for each phase/building.

Result After Mitigation: Less than significant impact.

I. LAND USE AND PLANNING

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Physically divide an established community? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? (2020 General Plan; City adopted Specific Plans; Local Coastal Program; and Zoning Ordinance; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
3. Conflict with any applicable habitat conservation plan or natural community conservation plan? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1-3) The proposed project is a Master Plan for build-out of several new structures and renovation of the existing shopping center. The center is located on a rectangular site bounded by Saviers Road, Channel Islands Blvd, C Street and Laurel Street. Commercial uses surround the site with a high school to the north. The project provides infill development on an already developed shopping center site. The General Plan land use designation of the subject site is Commercial Regional, which is consistent with the zoning designation of Commercial Planned Development (CPD).

The 2020 General Plan describes the *Commercial Regional* Land Use Designation as shopping centers that can be "...multi-story and range up to 3 million square feet in size on as much as 100 acres." The Master Plan project will result in a site with on and two story structures with a total of 450,759 square feet on a 30-acre site. The proposed development is consistent with this land use designation in terms of its site and building sizes. The retail, office and restaurant businesses proposed for this project are permitted land uses in the C-PD zone district. The Master Plan indicates building "pad" areas for future buildings. When each building is submitted, a Development Design Review Permit will address zone regulations including height and setbacks as well as architecture. The two following charts outline the C-PD zone regulations and the consistency for Building M addition (part of existing mall building) and the new building "pads." These project footprints meet or exceed all applicable zoning standards. The Parking Ordinance parking requirement is not being met but is addressed by a Parking Study and a request for Administrative Relief of Parking Requirements.

DEVELOPMENT STANDARDS FOR BUILDING M (AUTO ZONE) PROPOSED 8,326 SF ADDITION TO CREATE A 15,747 SF BUILDING			
DEVELOPMENT STANDARD (measured from internal parcel lines)	REQUIREMENT	PROPOSED	COMPLIES?
Max. building height	2 stories or 35 feet.	2 stores	YES
Front Yard setback Laurel Street	10 feet from property line.	26 feet	YES
West Side Yard setback Toward C Street	Zero when lot abuts another C-2 zoned lot.	NA	YES
East Side Yard setback Toward Saviers Road	Zero when lot abuts another C-2 zoned lot.	NA	YES
Rear Yard setback Adjacent to Albertsons	15 feet if >16 feet in height.	NA	YES
Parking lot landscaping	5% min. Landscape fingers required every 10 spaces. 10 feet along frontage.	Mall landscaping is 16%. Parcel contains no parking. Reciprocal mall parking serves the building. Landscape fingers provided. 15 feet landscaping on Laurel St frontage.	YES
Trash enclosures & transformers	To be provided and screened.	Trash enclosure provided and screened with landscaping.	YES
Rooftop equipment	May not protrude above height of parapet.	Unknown at this time; To be condition of approval.	YES with condition of approval

DEVELOPMENT STANDARDS FOR BUILDING M (AUTO ZONE) PROPOSED 8,326 SF ADDITION TO CREATE A 15,747 SF BUILDING			
DEVELOPMENT STANDARD (measured from internal parcel lines)	REQUIREMENT	PROPOSED	COMPLIES?
Lighting	Comply with City Code.	Photometric plan required at time of plan check review.	YES with condition of approval
Auto Parking	Required Building M parking based on 1/250 is 63 spaces. Required mall parking is 1,803 spaces at build-out. No compact parking permitted	Parcel contains 10 parking spaces. Reciprocal mall parking serves the building. 1,320 spaces will serve the mall at build-out. 74 existing compact parking spaces to be relocated on-site. 89 existing compact parking spaces to be removed.	Parking Relief required for mall parking.
Motorcycle Parking	3/first 100 spaces and 1/40 spaces thereafter = 34 spaces for mall	Parcel contains no motorcycle parking. 34 spaces provided for mall.	YES

DEVELOPMENT STANDARDS FOR BUILDING FOOTPRINTS D, G, H, I, J AND P			
DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Max. building height	2 stories or 35 feet.	NA	YES per Conditions of Approval
Front Yard setback:	10 feet from front property line.	Bldg D 16 feet Bldg G 65 feet Bldg H 35 feet Bldg I 5 feet Bldg J 28 feet Bldg P NA	YES except Bldg I (existing legal non-conforming)
Parking lot landscaping	5% min. Landscape fingers required every 10 spaces. 10 feet along frontage.	Mall landscaping is 16%. Reciprocal mall parking serves the buildings. Landscape fingers provided.	YES
Trash enclosures & transformers	To be provided and screened.	Trash enclosures provided and screened with landscaping.	YES
Rooftop equipment	May not protrude above height of parapet.	Unknown at this time; To be condition of approval.	YES with condition of approval
Lighting	Comply with City Code.	Photometric plan required at time of plan check review.	YES with condition of approval
Auto Parking	Required parking for each building is based on 1/250. Required mall parking is 1,803 spaces at build-out. No compact parking permitted	Parcels contain various numbers of parking spaces. Reciprocal mall parking serves the buildings. 1,320 spaces will serve the mall at build-out.	Parking Relief required for mall parking
Motorcycle Parking	3/first 100 spaces and 1/40 spaces thereafter = 34 spaces for mall	Various parcels contain motorcycle parking. 34 spaces provided for mall	YES
Loading Zones	5 spaces, for entire mall	24 spaces, for entire mall	YES

The proposed shopping center development is consistent with the type and intensity of development within the vicinity of the proposed project site, and the proposed project would not: (a) physically divide the neighborhood or the community; (b) conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project; and, (c) conflict with any applicable habitat conservation plan or natural community conservation plan. **Therefore, no significant impacts to land use are anticipated.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required.

Result After Mitigation: NA

J. MINERAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1-2) The proposed project is anticipated to have a nominal increase in fuel consumption and other energy sources due to construction activities and future day-to-day business operations. As shown in Figure VII-7 of the 2020 General Plan, the City of Oxnard has mineral/sand/gravel deposits primarily along the Santa Clara River channel, along the 101 Freeway corridor and along the eastern edge of the City extending west of Oxnard Boulevard. The project will not create a unique demand on available mineral resources in the City, since the project site is not located in an area of importance for mineral deposits. The project does not fall within any of the areas listed as having significant mineral deposits. **Therefore, no significant adverse effects on natural and mineral resources are expected.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required.

Result After Mitigation: NA

K. NOISE

Would the project result in:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

K. NOISE

Would the project result in:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
3. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
4. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
6. For a project located within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1-4) The project site is currently developed as shopping center and has been continuously developed since 1962. Noise related to the project would be generated by related vehicle trips on adjacent roads and truck deliveries. Long-term noise levels generated by the future occupancy of the shopping center development are expected to be consistent with the current commercial development located nearby. According to Figure X-1 and X-5, of the Noise Element in the General Plan, the project site and the vicinity are located in an area designated within the 60 CNEL Noise Contour, both existing and in the year 2020. Saviers Road produces noise levels within these contours. The closest "sensitive" land uses (as defined by the General Plan Noise Element) is the High School to the north of the project site. Noise levels are expected to be in conformance with the 2020 General Plan. Short-term noise impacts generated by construction activities are anticipated to occur and can be mitigated with conditions on construction. **Short-term noise will be reduced to less than significant with the application of mitigation measures.**

5-6) The project site is not located near the Oxnard Airport and is outside the noise contours as depicted on Figure X-2 in the 2020 General Plan. No private airstrips are in the vicinity of the project. **Therefore, no significant adverse effects related to noise from airports is anticipated from this project.**

Mitigation:

- K-1. During all excavation and grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.
- K-2. Construction times shall be limited to 7 a.m. to 7 p.m. daily or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.

Monitoring: Development Services shall monitor these mitigation measures at the time of permit issuance and during construction.

Result after mitigation: Upon implementation of the above mitigation measures, the project will result in less than significant effects on the environment related to noise.

L. POPULATION AND HOUSING

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through an extension of roads or other infra-structure)? <i>(2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)</i>	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? <i>(2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1 & 2) The proposed commercial development is consistent with the Regional Commercial land use designation of the 2020 General Plan. The site has been developed as commercial shopping center since 1962. The proposed commercial development is not anticipated to substantially affect the population of the City and it will not displace housing or people. **Therefore, no significant impacts to population and housing are anticipated.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required

Result After Mitigation: NA

M. PUBLIC SERVICES*

Would the project result in substantial adverse physical impacts to the following:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Fire protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Police protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Schools? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
4. Parks? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
5. Other public facilities? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

* Include potential effects associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services.

Discussion:

1) Fire. The Oxnard Fire Department provides fire protection to the City of Oxnard. Seven fire stations and a staff of more than 90 uniformed Fire Department personnel currently serve the City. Furthermore, the Fire Department can access additional manpower and equipment through an automatic aid agreement with Ventura County and a mutual aid agreement with the City of Ventura and Point Mugu Naval Air Station. The project includes adequate fire hydrants, access, signage, fire alarms, addressable smoke detectors, and will meet all requirements of the Uniform Fire Code to minimize any potential impacts on Fire services. With the inclusion of standard Fire Department conditions, **impact on fire services is considered to be less than significant.**

2) Police. The Oxnard Police Department provides police protection in this area at a police protection ratio of 1.3 police officers for each 1,000 City residents (2020 General Plan Update, Background Report, 2005 p. 4-57). The 2020 General Plan EIR (pgs. 4.13-16) indicates that the current staffing ratio of officers to population should be maintained to provide adequate police service as the City's population increases. The City will monitor the need for additional public facilities and/or personnel as part of the Five-Year Development Plan. Through this action, the City would ensure that police services are available to serve the proposed project and cumulative development. The increase in tax base generated by the project and cumulative projects would help fund the project's share of necessary police service expansion within the City. The mall administration currently provides on-site security patrols and an Oxnard Police Department store-front office is located at the mall. **Impacts on police services are anticipated to be less than significant.**

3) Schools. The Oxnard School District (K-8th grade) and the Oxnard Union High School District (9th –12th grade) serve the neighborhood. There are no residential uses proposed that would impact schools. Santa Clara High School is located across Laurel Street to the north of the project site. To address traffic safety in relation to the school, the project proposes the addition of a phased raised median extension. The first phase will extend the existing raised median to the edge of the school's eastern driveway to prevent right-turns being made into the opposing traffic lane on Laurel. The second phase will extend the median all the way to Saviers Road to prevent left turns out of the eastern school driveway due to the two left-turn pockets on Laurel at Saviers. These improvements will increase safety for

vehicles making turning movements at the school driveways. (See the mitigation measures outlined under the Traffic section.) **No significant impacts are anticipated for schools.**

4) Parks. The proposed project does not propose to add any residential units, which generally increase the use of existing parks. **No impacts will occur to parks.**

5) Public Services. Water service to this area is currently provided by Calleguas Municipal Water District. Furthermore, the maintenance and development of the water facilities under the jurisdiction of the Calleguas Municipal Water District (CMWD) are provided for through a Capital Construction Charge. The proposed project consists of additions and remodeling of an existing shopping center. All required infrastructure is in place surrounding the site. Water will be provided to new development by the City of Oxnard's Public Works Water Division, and wastewater disposal will be provided by the City's Public Works Wastewater Division. To address the project's share of demands on public infrastructure, the City requires developers of new projects to pay the following development fees: *Planned Traffic Circulation System Facilities Fees* (Traffic Impact); *Planned Water Facilities Fee*; *Planned Wastewater Facilities Fee*; *Planned Drainage Facilities Fee*; and *Growth Requirement Capital Fee*. **Therefore, less than significant impacts to public facilities are anticipated.**

Mitigation: Based on the discussion provided above, less than significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required.

Result After Mitigation: NA

N. RECREATION

	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X
2. Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X

Discussion:

1-2) The proposed project does not propose to add any residential units, which generally increase the use of existing parks. **Therefore, no significant adverse effects on the environment related to recreation are expected to result from the project.**

Mitigation: Based on the discussion provided above, no significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required.

Result After Mitigation: NA

O. TRANSPORTATION/TRAFFIC

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Cause an increase in traffic, which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Exceed, either individually or cumulatively, a level of service standard established by the County congestion management agency for designated roads or highways? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Result in a change in traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>
4. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/ Circulation)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
5. Result in inadequate emergency access? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

O. TRANSPORTATION/TRAFFIC

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
6. Result in inadequate parking capacity? (<i>Zone Ordinance - Parking Regulations and Parking Lot Design Standards</i>)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
7. Conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (<i>Bicycle Facilities Master Plan</i>)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

Discussion:

1-2) Traffic Trips. The City of Oxnard has identified *Level of Service* (LOS) C as the transportation planning goal that the city ultimately desires to achieve, where environmentally feasible. The City evaluates project impacts at intersections based upon the change in LOS attributable to the project. If an intersection operates in the LOS C, D, E or F ranges with the existing plus “pending projects” volumes and a change in the ICU (Intersection Capacity Utilization) of 0.02 (2%) or greater is attributable to the project, the impact is considered significant. The level of service must then be mitigated to the ICU level identified without the project traffic. The project proposes a net new increase of 93,090 square feet of retail/office uses within the shopping center.

The following traffic information is based on the “Oxnard Centerpoint Traffic and Parking Study” prepared by Austin-Foust Associates, Inc, January 2009, Attachment 2. (It should be noted that this study is a “worst case” analysis as it is based on an earlier proposal for an addition of 102,000 square feet; the current proposal is for an addition of 93,090 net new square feet.) Based on 2007 traffic counts, the study determined that the increase in traffic associated with new development at the center is 79 vehicles per hour in the AM peak hours and 121 vehicles per hour in the PM peak hours with 1,473 average daily trip increase. The report also found that the peak traffic generation is generally in line with ITE trip rates. The distribution of the project’s increased traffic is highly directional with the majority of trips in the AM inbound and in the PM virtually all are outbound. Because the site expansion will not generate 100 or more trips in the AM peak hour, the project will not significantly impact the Congestion Management Plan network during the AM time frame. During PM peak hour, the north-south travel on Saviers Road will increase by 36 vehicles per hour in each direction. East-west travel on Channel Islands Blvd will increase by 24 vehicles per hour each way. The intersection of Saviers Road and Channel Islands Blvd currently operates at LOS “C” with a PM peak on Saviers of 1,400 vehicles per hour and 950 vehicles per hour on Channel Islands Blvd. According to the traffic study, the addition of 36 vehicles per hour north-south and 24 vehicles per hour east-west does not create a significant impact. (The intersection capacity would change only 0.01.) Payment of the Ventura County traffic fee is a standard condition of approval and will be applied to this project. **A less than significant impact is anticipated for traffic trips.**

3-5) Circulation and Safety. The project curb cuts, driveways, interior circulation and site access are designed to meet City engineering standards and meet City Fire Department standards to ensure fire apparatus can reach all parts of the site, and no vehicle circulation safety issues have been identified by the City Traffic Engineer. Existing driveways on Saviers Road, Channel Island Blvd, C Street, and Laurel Street are proposed to remain. Primary driveways from Saviers and C Street into the mall would be narrowed to 25 feet to provide pedestrian sidewalks alongside the driveways. This includes the addition of a pedestrian sidewalk for the northern C Street driveway to provide pedestrian access along the south side of the driveway from the bus center to the mall. The City standard for these driveways is usually 30 – 36 feet in width. The City Traffic Engineer has reviewed the proposed driveway widths

and corner turning radii and determined that a semi-truck could make the turning movement and therefore, supports the 25- foot wide drives.

With the potential for 1,473 average daily trip increase with the project, public street improvements will be required to address safety and circulation. It is expected that the Laurel Street driveway will have a marked increase in traffic movements as it provides better egress to area streets than other driveways. It has been observed that vehicles are currently stacking-up on eastbound Laurel Street to make a left-turn on to Saviers Road blocking the through-traffic. With additional traffic from Master Plan build-out, a second left-turn lane would be needed to mitigate vehicle stacking on Laurel. Additionally, a traffic safety issue has been observed in relation to the Santa Clara High School driveways. Vehicles have been observed turning right out of the school driveway, then traveling westbound on Laurel Street in the opposing traffic lane to make a turn into the shopping center. The project proposes mitigation by the addition of a phased raised median extension. The first phase will extend the existing raised median to the edge of the school's eastern driveway to prevent right-turns being made into the opposing traffic lane on Laurel. The second phase will extend the median all the way to Saviers Road to prevent left turns out of the eastern school driveway as this turning movement will become unsafe with the two left-turn pockets that will be constructed on Laurel at Saviers. These improvements will increase safety for vehicles making turning movements at the school driveways. Additionally, to accommodate Master Plan traffic and mitigate additional vehicles stacking and turning into the shopping center from southbound Saviers Road, a right-turn pocket is proposed at each mall driveway. The need for a future southbound right-turn lane onto Channel Islands Boulevard has been identified by the City to address right-turn vehicle stacking. With the previous approval of Building S at the shopping center, right-of-way dedication along the Saviers Road frontage of Building S was required. That dedication has been provided to the City. To allow completion of the right-turn pocket, right-of way dedication along the Saviers Road frontage of the Wells Fargo building (on the corner of Saviers and Channel Islands Blvd.) is required.

The following public improvement mitigation measures are proposed as part of the project on streets adjacent to the shopping center for increased safety and improved traffic circulation:

- Extend raised median on Laurel Street at Santa Clara High School to edge of eastern school driveway to prevent right turns out of the school driveways into opposing traffic lane to improve safety. Master Plan to provide at first building permit.
- Dedicate right-of-way for widening Laurel Street at the Saviers Road intersection at building permit for Building J to accommodate Laurel Street widening for potential project traffic added to Laurel Street.
- Widen the Laurel St./Saviers Rd. intersection to add a second left-turn pocket from east-bound Laurel Street onto Saviers Road to address existing and potentially greater queuing with project traffic on Laurel west of the Saviers intersection. Provide striping for the second left-turn pocket. Extend raised median on Laurel Street up to Saviers Road to address safety of turning movements from the high school onto Laurel Street. Modify signal pole and mast arm length to accommodate eastbound Laurel Street new left turn lane. Master Plan to provide at reconstruction of the Ritmo building on the corner of Saviers and Laurel.
- On Saviers Road south of Laurel at the northern shopping center driveway, provide a south-bound right-turn pocket into the center with a bus stop and shelter to provide a separate lane for turns into the shopping center and to allow semi-truck deliveries to move out of the traffic flow to turn into the center. Master Plan to provide with reconstruction of Ritmo building or construction of Building G, whichever occurs first

- On Saviers Road at the southern shopping center driveway, provide a southbound right-turn pocket into the shopping center driveway to provide a separate lane for turns into the shopping center. Master Plan to provide with construction of Building G.
- On Saviers Road at the Wells Fargo Bank, dedicate an 11' 1" wide strip along the Saviers Road parcel frontage for a southbound right-turn lane onto Channel Islands Blvd. The Master Plan shall provide the right-of-way at the time the Wells Fargo building is remodeled or demolished to remove the portion of the building that encroaches.

These improvements are required to be dedicated and/or constructed in a phased manner, timed with construction of adjacent buildings proposed in the Master Plan. **Less than significant traffic and safety impacts are anticipated with these improvements.**

6) Parking. The shopping center currently has 1,845 parking spaces. The code requirement for the existing center at 1 space/250 sf is 1,476 spaces. The project is proposing to provide 1,320 spaces to serve the center at Master Plan build-out. This is 483 spaces less than required by code (a deficiency of 27%). The proposed parking lot that surrounds the mall buildings would have 1,179 standard parking spaces, 74 existing/relocated compact parking spaces, 33 handicap spaces, and 34 motorcycle spaces. The existing Bank of America building also has a drive-through with 4 auto stacking spaces. The Code requires five loading zones for a shopping center over 150,000 sf. A total of 24 loading zones are provided. Each separate building has one or two loading zones while the main mall and major tenants have four or more. Some of the loading zones for the smaller buildings are shorter than the code requirement of 40 feet because these smaller tenants are usually serviced by smaller deliver trucks (Fed Ex, UPS, etc.) The mall and major building have full size loading zones.

The following parking information is based on the "Oxnard Centerpoint Traffic and Parking Study" prepared by Austin-Foust Associates, Inc, January 2009. (It should be noted that this study is a "worst case" analysis as it is based on an earlier proposal for an addition of 87,525 square feet; the current proposal is for an addition of 81,702 net new square feet.) Parking counts were conducted at the center on three weekdays and two Saturdays in August and September 2006. The entire center experienced a maximum parking demand of 775 spaces. This is equivalent to 2.34 spaces/1000 sf. The Parking Ordinance standard is 2.5 spaces/1000 sf. Using the actual demand of 2.34 spaces/ 1000 sf, the entire center under the Master Plan would have a maximum demand of 1,073 spaces. (This is based on the larger sized addition and therefore could be a "worst case.>"). As the Master Plan is proposing 1,320 parking spaces, the study finds that the parking lot would operate at a maximum occupancy of 82% full. The study reports that industry standards for parking lots allows up to 90% occupancy to be acceptable.

Approval of Administrative Relief from Parking Provisions is required for the proposed parking plan including the reduction in required parking spaces and the relocation of 74 existing compact parking spaces. **Less than significant parking impacts are anticipated.**

7) Alternative Transportation. C Street on the west side of the shopping center is a designated bike route and currently has a striped bike lane on the street. C Street is the most favorable location for this bike path than on Saviers Road which is an arterial. Channel Islands Blvd. is also a bike route with an un-striped bike lane. The project will be required to provide bicycle racks in accordance with City Code requirements. The C Street bus center is located in the right-of way on C Street adjacent to the project site. The project will not negatively affect the bus center as no work is proposed in the right-of-way. The project will provide enhanced entry drives with better pedestrian access. This includes a sidewalk along the C Street driveway for people that use the bus system to arrive or depart from the mall. **Less than significant impacts are anticipated.**

Mitigation: Based on the discussion provided above, less than significant traffic and parking impacts are expected to occur as a result of this project; therefore no mitigation measures are required for these issues areas. Mitigation is required for traffic safety and circulation as outlined in item 3-5 in the discussion above.

Monitoring: None Required

Result After Mitigation: NA

P. UTILITIES AND SERVICE SYSTEMS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
4. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
5. Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

P. UTILITIES AND SERVICE SYSTEMS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
6. Be served by a landfill with sufficient permitted capacity to accommodate the project’s solid waste disposal needs? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
7. Comply with federal, state, and local statutes and regulations related to solid waste? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

Discussion:

1-3, 5) The project represents additional demand on master-planned utilities and service systems that have been anticipated in the General Plan build-out scenario for this area. In reviewing this project based on recent and similar projects, the City expects that this project can be served without significantly impacting existing utility services.

The proposed project will be required to comply with measures to treat stormwater runoff by implementing Best Management Practices (BMPs) in accordance with National Pollutant Discharge Elimination System (NPDES) requirements. **Therefore less than significant impacts are anticipated.**

4) The City of Oxnard currently provides the existing property with water and will provide the new project with water that is supplied by Calleguas Municipal Water District (Calleguas). Calleguas purchases water from the Metropolitan Water District of Southern California (MWD). MWD supplies surplus water from the Colorado River and the State Water Project for municipal, industrial, and agricultural users within its service area. Standard City conditions require all projects to provide drought-tolerant landscaping to reduce the amount of water consumed by the project. **Therefore, less than significant impacts to water supplies and resources are anticipated.**

6-7) Standard conditions of approval require compliance with the City’s recycling programs, which require solid waste recycling and disposal plans and reporting during construction and operation of the facility. **Therefore, less than significant impacts to landfill and solid waste resources are anticipated.**

Mitigation: Based on the discussion provided above, less than significant impacts are expected to occur as a result of this project; therefore no mitigation measures are required or proposed.

Monitoring: None required.

Result After Mitigation: NA

Q. MANDATORY FINDINGS OF SIGNIFICANCE	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
2. Does the project have impacts that are individually limited, but cumulatively considerable (<i>"Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects</i>)?	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>
3. Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	X	<input type="checkbox"/>

No new significant adverse effects are expected to result from the proposed project. Mitigation measures are either incorporated into the project or made a part of the Mitigated Negative Declaration.

SUMMARY OF MITIGATION MEASURES INCORPORATED INTO THE PROJECT

Air Quality

- C-1 Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.
- C-2 During smog season (May through October), Developer shall lengthen the construction period so as to minimize the number of vehicles and equipment operating at the same time.
- C-3 At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.
- C-4 During construction, Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
- C-5 During construction, Developer shall control dust by the following activities:
- All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads.
 - All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally-safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible.
- C-6 During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.
- C-7 During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.
- C-8 Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.

Cultural Resources

- E-1 Developer shall contract with a qualified archaeologist to conduct a Phase I cultural resources survey of the project site prior to issuance of any grading permits. The survey shall include 1) an archaeological and historical records search through the California Historical Resources Information System at CalState Fullerton; and 2) a field inspection of the project site. Upon completion, the Phase I survey report shall be submitted to the Planning Division for compliance verification. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to initiation of the Phase I activities.

The contract shall include provisions in case any cultural resources are discovered onsite. In the event that any historic or prehistoric cultural resources are discovered, work in the vicinity of the find shall be halted immediately. The archaeologist shall evaluate the discovery and determine the necessary mitigations for successful compliance with all

applicable regulations. Developer or its successor in interest shall be responsible for paying all salaries, fees and the cost of any future mitigation resulting from the survey.

- E-2 Developer shall contract with a Native American monitor to be present during all subsurface grading, trenching or construction activities on the project site. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building permits.

Geology

- F-1 Developer shall submit a site-specific soils investigation, which shall be prepared by a licensed geotechnical engineer. At a minimum, such study shall include liquefaction and compressible soil characteristics on the subject site, and shall identify any necessary construction techniques or other mitigation measures to prevent significant liquefaction/compressible soils impacts upon the proposed project. All recommendations of said report shall be incorporated into the project. Developer shall submit the report to the Building Official for review and approval prior to issuance of building permits.

Hazardous Materials

- G-1 *Prior to demolition or excavation within the boundaries of the two sites, documentation of appropriate closure shall be required.*

Hydrology

- H-1 Developer shall submit calculations verifying compliance with NPDES storm water quality requirements prior to issuance of a Grading or Building Permit for each phase/building.

Noise

- K-1. During all excavation and grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.
K-2. Construction times shall be limited to 7 a.m. to 7 p.m. daily or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.

Traffic

- O-1 Extend raised median on Laurel Street at Santa Clara High School to edge of eastern school driveway to prevent right turns out of the school driveways into opposing traffic lane to improve safety. Master Plan to provide at first building permit.
O-2 Dedicate right-of-way for widening Laurel Street at the Saviers Road intersection at building permit for Building J to accommodate Laurel Street widening for potential project traffic added to Laurel Street.
O-3 Widen the Laurel St./Saviers Rd. intersection to add a second left-turn pocket from east-bound Laurel Street onto Saviers Road to address existing and potentially greater queuing with project traffic on Laurel west of the Saviers intersection. Provide striping for the second left-turn pocket. Extend raised median on Laurel Street up to Saviers Road to address safety of turning movements from the high school onto Laurel Street. Modify

signal pole and mast arm length to accommodate eastbound Laurel Street new left turn lane. Master Plan to provide at reconstruction of the Ritmo building on the corner of Saviers and Laurel.

- O-4 On Saviers Road south of Laurel at the northern shopping center driveway, provide a south-bound right-turn pocket into the center with a bus stop and shelter to provide a separate lane for turns into the shopping center and to allow semi-truck deliveries to move out of the traffic flow to turn into the center. Master Plan to provide with reconstruction of Ritmo building or construction of Building G, whichever occurs first.
- O-5 On Saviers Road at the southern shopping center driveway, provide a southbound right-turn pocket into the shopping center driveway to provide a separate lane for turns into the shopping center. Master Plan to provide with construction of Building G.
- O-6 On Saviers Road at the Wells Fargo Bank, dedicate an 11' 1" wide strip along the Saviers Road parcel frontage for a southbound right-turn lane onto Channel Islands Blvd. The Master Plan shall provide the right-of-way at the time the Wells Fargo building is remodeled or demolished to remove the portion of the building that encroaches.

ADDITIONAL REFERENCES

1. California, State of, Air Resources Board, *URBEMIS 2002 Program*.
2. California, State of, Governor's Office, Office of Planning and Research, Office of Permit Assistance, *Hazardous Waste and Substances Sites - List Pursuant to AB 3750*, current edition.
3. California, State of, Office of Planning and Research, *California Environmental Quality Act Statutes*, Sacramento, California: January 1, 2002.
4. California, State of, Office of Planning and Research, *Guidelines for Implementation of the California Environmental Quality Act*, Sacramento, California: February 1, 2001.
5. California, State of, Office of Planning and Research, *Planning, Zoning and Development Laws*, November 2000.
6. City of Oxnard, *The Municipal Code of the City of Oxnard - Zoning Ordinance*, current edition.
7. City of Oxnard, Development Services Department, Planning Division, *Coastal Zoning Regulations and Zone Maps*, current edition.
8. City of Oxnard, Development Services Department, Planning Division, *Coastal Land Use Plan*, current edition.
9. City of Oxnard, Community Development Department, Planning Division, *Zone Maps*, current edition.
10. City of Oxnard, Fire Department, *Fire Protection Planning Guide*, January 1990.
11. Ventura County Air Pollution Control District, *Air Quality Management Plan*, current edition.
12. Ventura County Air Pollution Control District, *Ventura County Air Quality Assessment Guidelines*, October 2003.
13. Institute of Transportation Engineers, *Trip Generation Manual*, Seventh Edition, Washington, DC, 2003.
14. United States Federal Emergency Management Agency, National Flood Insurance Program, *FIRM Flood Insurance Rate Maps for the City of Oxnard*, October 1985.
15. City of Oxnard, Public Works Department, *Master Sewer Plan*, current edition.
16. City of Oxnard, Public Works Department, *Master Drainage Plan*, current edition.
17. City of Oxnard, Public Works Department, *Master Water Plan*, current edition.
18. California State University - Fullerton South Central Coastal Information Center, *California Historical Resources Information System*, Department of Anthropology, Fullerton, California.
19. Ventura County Airport Land Use Commission, *Oxnard Airport Master Land Use Plan*, 1990.
20. Ventura County Cultural Heritage Board, *Ventura County Historical Landmarks & Points of Interest—August 1991*, Ventura County Recreation Services.
21. Ventura County, Property Administration Agency, *Final Report: Cultural Heritage Survey, Phase I*, Oxnard and Santa Paula, 1981.

Environmental Impact Reports

22. City of Oxnard, *FEIR 94-1 for the Oxnard Factory Outlet, Phase III*.
23. City of Oxnard, *FEIR 95-2 for the Shopping Center at Lockwood and Rose Avenue*.
24. City of Oxnard, *FEIR 95-3 for the Redevelopment Project*.
25. City of Oxnard, *FEIR 96-1 for the Northshore Project at Harbor Boulevard and Fifth Street*.

26. City of Oxnard, *FEIR 96-2 for the Northwest Specific Plan.*
27. City of Oxnard, *FEIR 97-1 for the Ormond Beach Specific Plan.*
28. City of Oxnard, *FEIR 98-1 (Supplemental) for the Northeast Specific Plan.*
29. City of Oxnard, *FEIR 98-2 (Supplemental) for the Westport at Mandalay Bay Project (Tract 5196).*

Specific Plans

30. City of Oxnard, *Northfield and Seagate Specific Plan*, July 3, 1984.
31. City of Oxnard, *Mandalay Bay Specific Plan*, January 9, 1985.
32. City of Oxnard, *Oxnard Town Center Specific Plan*, October 1, 1986.
33. City of Oxnard, *Rose-Santa Clara Corridor Specific Plan*, July 15, 1986.
34. City of Oxnard, *McInnes Ranch Business Park Specific Plan*, December 20, 1988.
35. City of Oxnard, *Northeast Community Specific Plan*, February 8, 1994.
36. City of Oxnard, *Northwest Community Specific Plan*, July 20, 1998.

ATTACH B

GENERAL NOTES

1. ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE IN FEET AND INCHES.

2. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

3. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

4. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

5. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

6. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

7. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

8. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

9. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

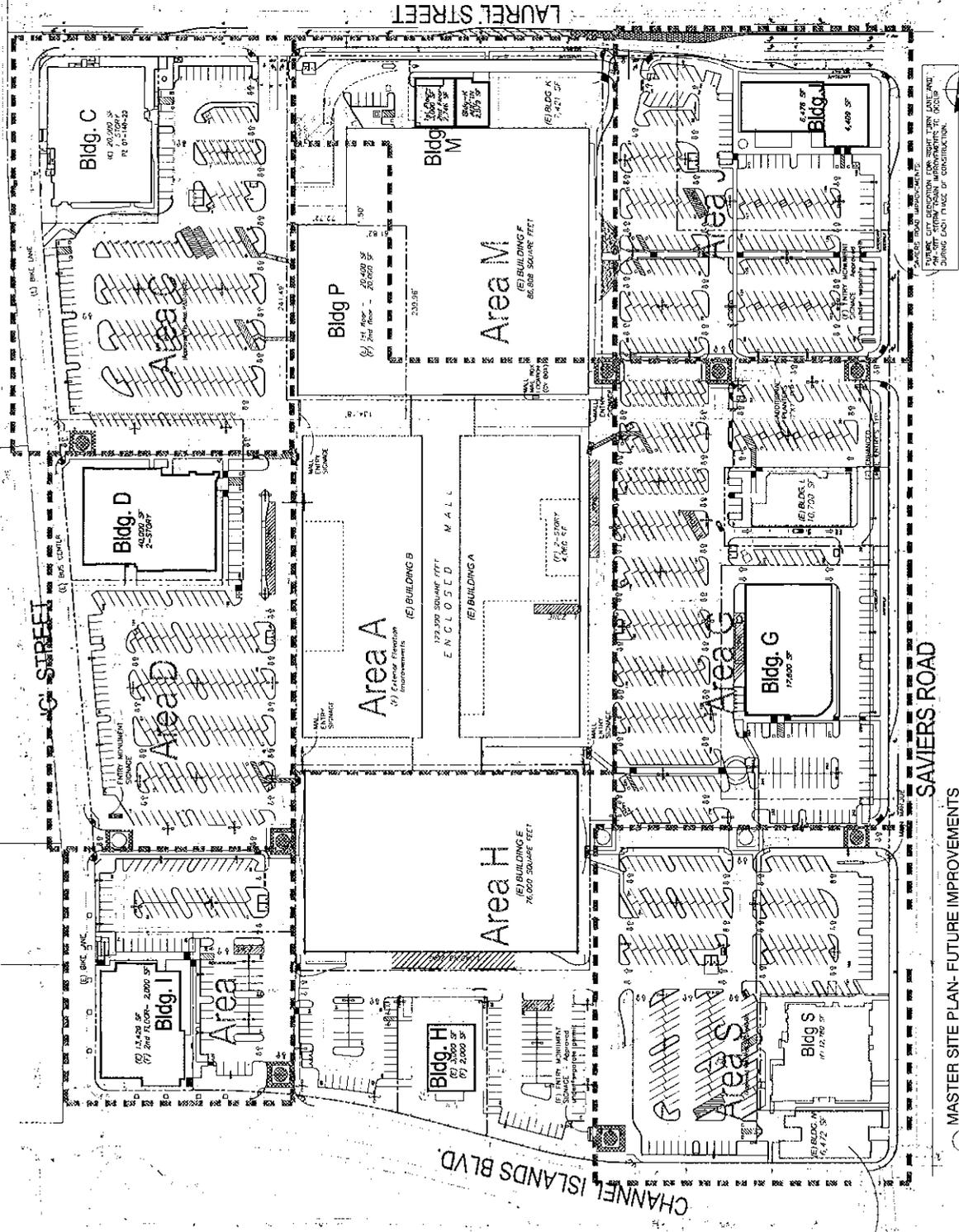
10. ALL DISTANCES ARE TO THE CENTERLINE UNLESS OTHERWISE NOTED.

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMITTING	04/30/2009
2	ISSUED FOR PERMITTING	04/30/2009
3	ISSUED FOR PERMITTING	04/30/2009
4	ISSUED FOR PERMITTING	04/30/2009
5	ISSUED FOR PERMITTING	04/30/2009
6	ISSUED FOR PERMITTING	04/30/2009
7	ISSUED FOR PERMITTING	04/30/2009
8	ISSUED FOR PERMITTING	04/30/2009
9	ISSUED FOR PERMITTING	04/30/2009
10	ISSUED FOR PERMITTING	04/30/2009



MASTER PLAN FUTURE IMPROVEMENT PLAN A1

April 30, 2009



1 MASTER SITE PLAN- FUTURE IMPROVEMENTS

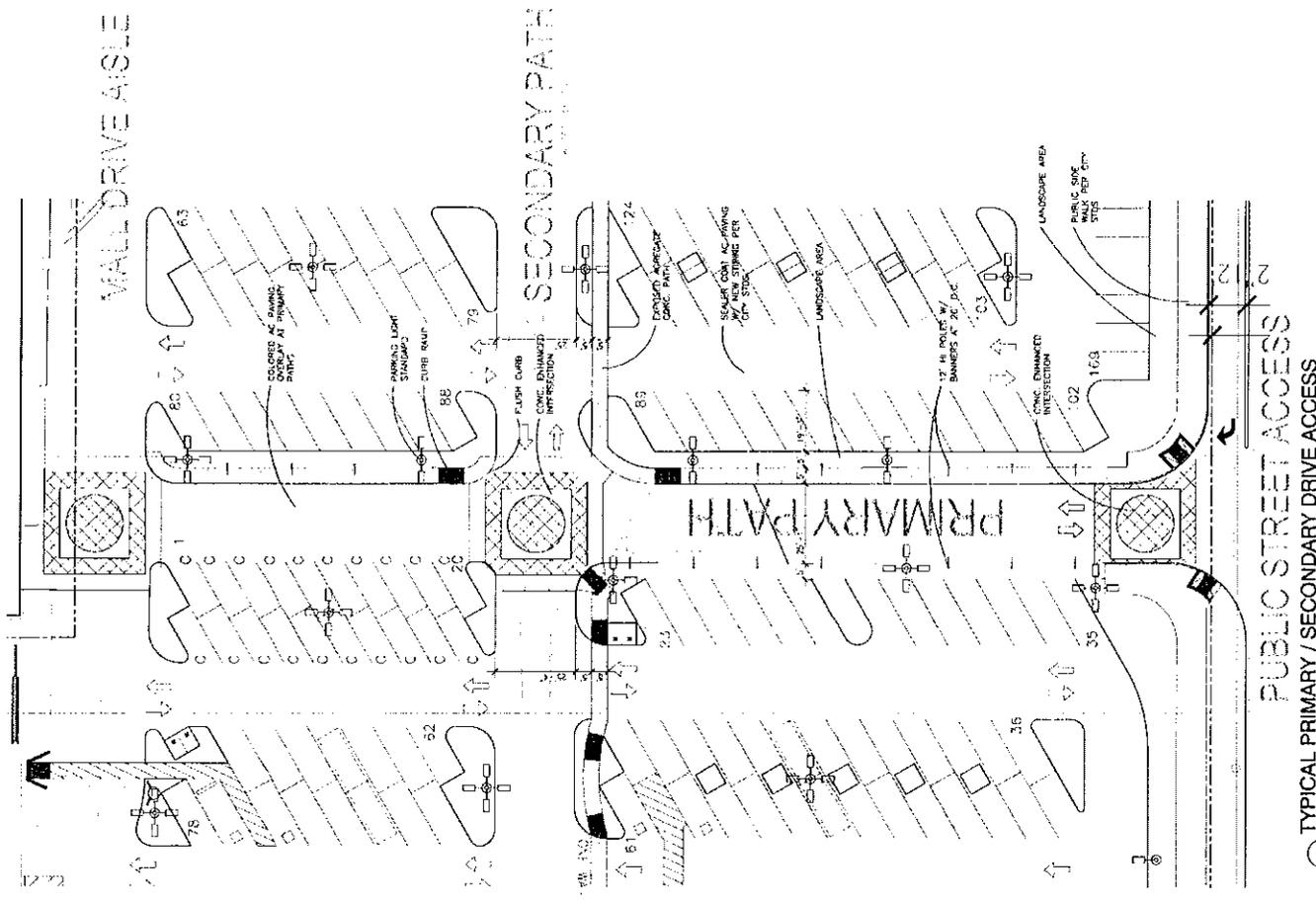
1" = 60'-0"

Centerpoint Mall Master Plan

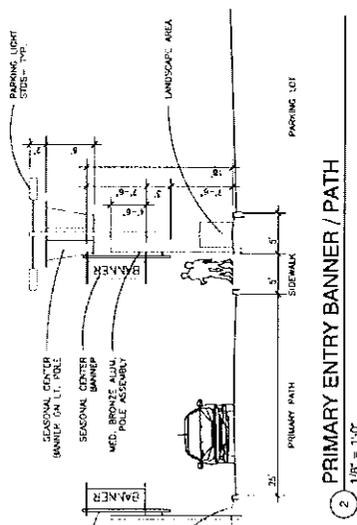
CHANNEL ISLANDS BLVD. & SAVIERS ROAD, OXNARD CA 93033



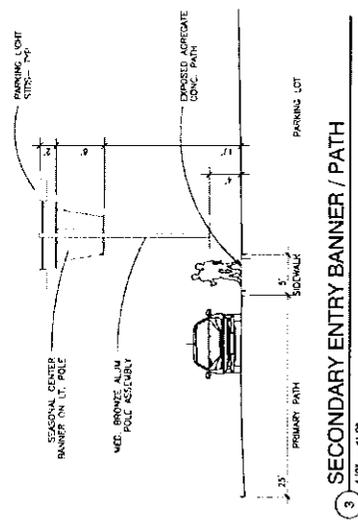
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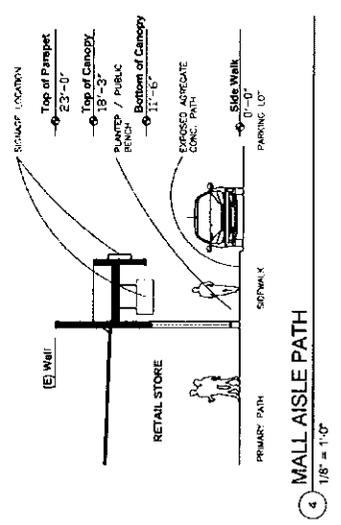
1 TYPICAL PRIMARY / SECONDARY DRIVE ACCESS
1" = 20'-0"



2 PRIMARY ENTRY BANNER / PATH
1/8" = 1'-0"



3 SECONDARY ENTRY BANNER / PATH
1/8" = 1'-0"



4 MALL AISLE PATH
1/8" = 1'-0"

Centerpoint Mall Master Plan

CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033

PLANT LIST FOR LANDSCAPES BUILDINGS

PLANT	COMMON NAME	HEIGHT	SPREAD	PLANTING	MAINTENANCE
...

RIMETER LANDSCAPING

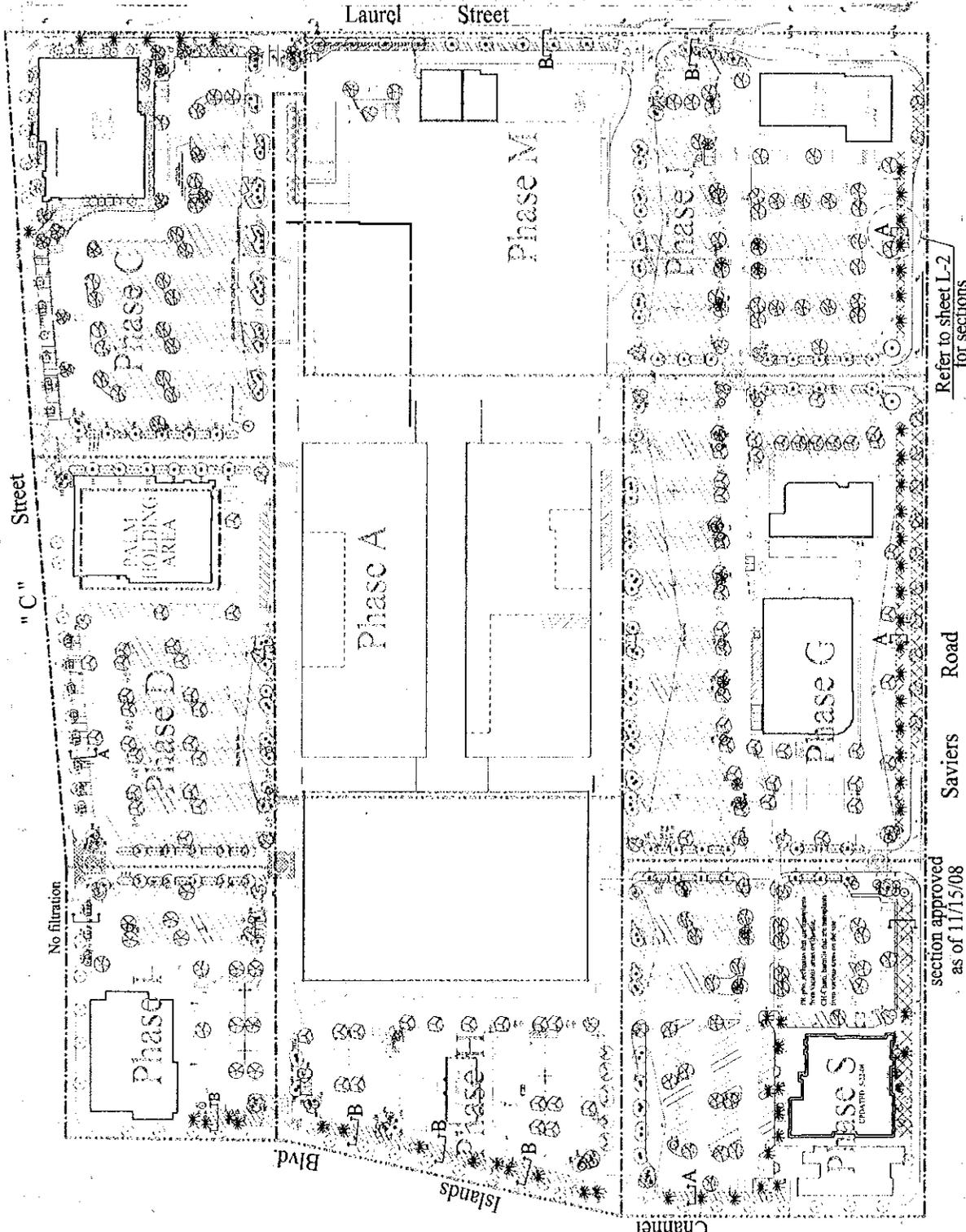
1" = 1" scale of rimeter
 1" = 1" scale of rimeter
 1" = 1" scale of rimeter

TREE LEGEND

SYMBOL	DESCRIPTION
...	...

TREE REMOVAL

The following numbers indicate trees that are to be removed. The numbers are based on the tree's trunk diameter at 4.5 feet above the ground. The numbers are based on the tree's trunk diameter at 4.5 feet above the ground. The numbers are based on the tree's trunk diameter at 4.5 feet above the ground.



Refer to sheet L-2 for sections

section approved as of 11/15/08

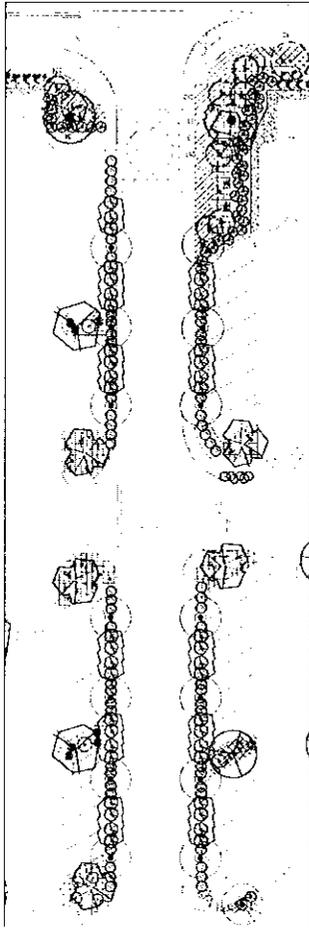
LS-

Philip S. Miller & Assoc.
Site Planning and Landscape Architecture
1000 S. AVENUE, SUITE 100, OXNARD, CA 93033

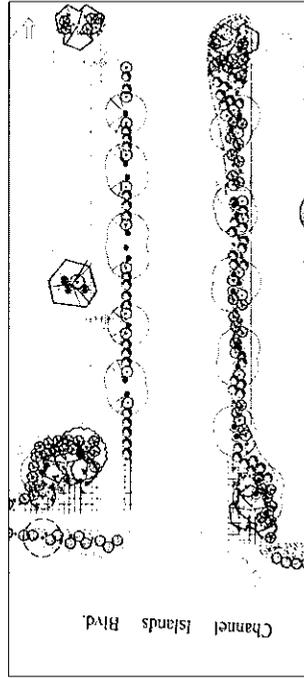
LANDSCAPE MASTERPLAN SCALE: 1" = 50' ft.

OXNARD CENTER

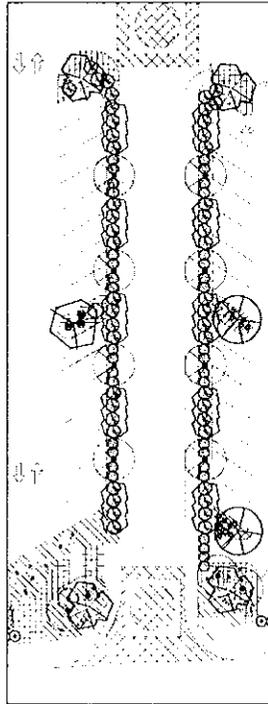
OXNARD CENTER MASTER PLAN
CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



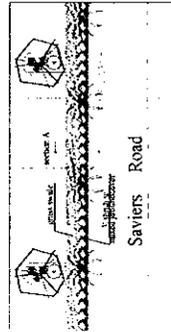
1. ENTRANCE and DRIVEWAY DETAIL at SAVIERS ROAD



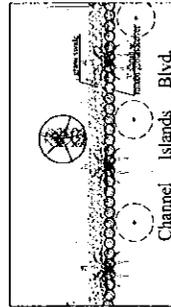
2. ENTRANCE and DRIVEWAY DETAIL at CHANNEL ISLANDS BLVD.



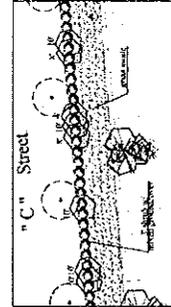
3. ENTRANCE and DRIVEWAY DETAIL at "C" STREET



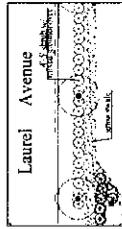
4. PLANTINGS ALONG SAVIERS



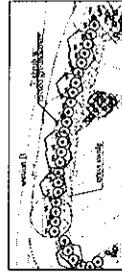
5. PLANTINGS ALONG CH. ISLANDS



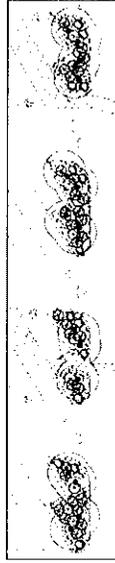
6. PLANTINGS ALONG "C" STREET



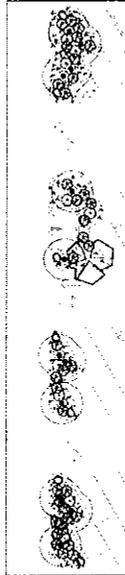
7. LOADING ZONE SCREENING



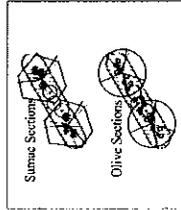
8. PLANTINGS ALONG LAURELE AVE



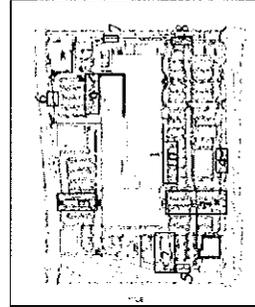
9. DRIVE ALONG WEST EDGE OF MALL



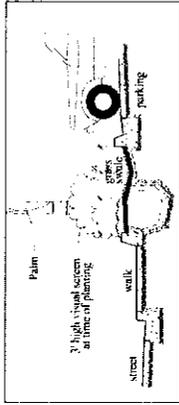
10. DRIVE ALONG EASTERN EDGE OF MALL



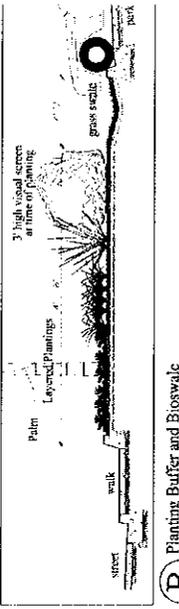
TYPICAL FINGER PLANTER LAYOUT



LEGEND



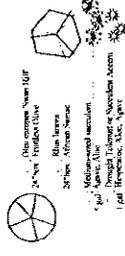
A Planting Buffer and Bioswale (see plan for location of section 'A')



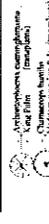
B Planting Buffer and Bioswale (see plan for location of section 'B')

PLANTING LEGEND

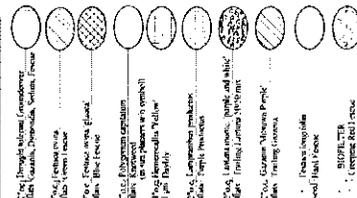
PARKING LOT PLANTINGS



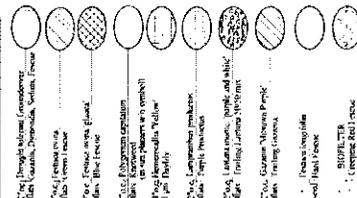
PALMS



SHRUBS & VINES



GROUNDCOVERS



Summary Report for Summer Emissions (Pounds/Day)

File Name:

Project Name: Centerpoint Mall Master Plan

Project Location: Ventura County APCD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

CONSTRUCTION EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10 Dust</u>	<u>PM10 Exhaust</u>	<u>PM10</u>	<u>PM2.5 Dust</u>	<u>PM2.5 Exhaust</u>	<u>PM2.5</u>	<u>CO2</u>
2007 TOTALS (lbs/day unmitigated)	6.26	44.73	24.73	0.00	18.82	2.74	21.56	3.93	2.52	6.45	3,610.41
2008 TOTALS (lbs/day unmitigated)	85.84	54.46	35.57	0.01	18.85	3.33	22.19	3.95	3.06	7.01	5,294.60

AREA SOURCE EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	0.95	1.40	5.77	0.00	0.02	0.02	1,619.25

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	10.21	10.58	121.23	0.09	17.40	3.28	9,894.73

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SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	11.16	11.98	127.00	0.09	17.42	3.30	11,513.98

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2008	1.05	1.17	1.06	0.00	0.09	0.07	0.16	0.02	0.07	0.08	152.89
Asphalt 12/28/2007-01/11/2008	0.01	0.06	0.04	0.00	0.00	0.01	0.01	0.00	0.00	0.00	5.67
Paving Off-Gas	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off Road Diesel	0.01	0.06	0.03	0.00	0.00	0.01	0.01	0.00	0.00	0.00	4.41
Paving On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46
Paving Worker Trips	0.00	0.00	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.81
Fine Grading 11/30/2007-01/11/2008	0.02	0.13	0.07	0.00	0.08	0.01	0.09	0.02	0.01	0.02	10.57
Fine Grading Dust	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.02	0.00	0.02	0.00
Fine Grading Off Road Diesel	0.01	0.13	0.06	0.00	0.00	0.01	0.01	0.00	0.01	0.01	10.11
Fine Grading On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading Worker Trips	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46
Building 01/11/2008-08/22/2008	0.14	0.98	0.95	0.00	0.00	0.06	0.06	0.00	0.05	0.06	135.59
Building Off Road Diesel	0.11	0.84	0.41	0.00	0.00	0.05	0.05	0.00	0.05	0.05	71.92
Building Vendor Trips	0.01	0.11	0.09	0.00	0.00	0.00	0.01	0.00	0.00	0.00	18.77
Building Worker Trips	0.01	0.03	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	44.90
Coating 08/08/2008-09/05/2008	0.88	0.00	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.06
Architectural Coating	0.88	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Coating Worker Trips	0.00	0.00	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.06

Phase Assumptions

Phase: Fine Grading 11/30/2007 - 1/11/2008 - Default Fine Site Grading Description

Total Acres Disturbed: 3.78

Maximum Daily Acreage Disturbed: 0.94

Fugitive Dust Level of Detail: Default

20 lbs per acre-day

On Road Truck Travel (VMT): 0

Off-Road Equipment:

1 Graders (174 hp) operating at a 0.61 load factor for 6 hours per day

1 Rubber Tired Dozers (357 hp) operating at a 0.59 load factor for 6 hours per day

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- 1 Tractors/Loaders/Backhoes (108 hp) operating at a 0.55 load factor for 7 hours per day
- 1 Water Trucks (189 hp) operating at a 0.5 load factor for 8 hours per day

Phase: Paving 12/28/2007 - 1/11/2008 - Default Paving Description

Acres to be Paved: 0.94

Off-Road Equipment:

- 4 Cement and Mortar Mixers (10 hp) operating at a 0.56 load factor for 6 hours per day
- 1 Pavers (100 hp) operating at a 0.62 load factor for 7 hours per day
- 1 Rollers (95 hp) operating at a 0.56 load factor for 7 hours per day
- 1 Tractors/Loaders/Backhoes (108 hp) operating at a 0.55 load factor for 7 hours per day

Phase: Building Construction 1/11/2008 - 8/22/2008 - Default Building Construction Description

Off-Road Equipment:

- 1 Cranes (399 hp) operating at a 0.43 load factor for 4 hours per day
- 2 Forklifts (145 hp) operating at a 0.3 load factor for 6 hours per day
- 1 Tractors/Loaders/Backhoes (108 hp) operating at a 0.55 load factor for 8 hours per day

Phase: Architectural Coating 8/8/2008 - 9/5/2008 - Default Architectural Coating Description

- Rule: Residential Interior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250
- Rule: Residential Exterior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250
- Rule: Nonresidential Interior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250
- Rule: Nonresidential Exterior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250

Detail Report for Annual Operational Unmitigated Emissions (Tons/Year)

File Name:

Project Name: Centerpoint Mall Master Plan

Project Location: Ventura County APCD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

OPERATIONAL EMISSION ESTIMATES (Annual Tons Per Year, Unmitigated)

Source	ROG	NOX	CO	SO2	PM10	PM25	CO2
Strip mall	0.84	1.01	10.11	0.01	1.41	0.27	761.08
General office building	1.06	1.20	12.25	0.01	1.67	0.32	911.55
Warehouse	0.07	0.07	0.68	0.00	0.10	0.02	51.49
TOTALS (tons/year, unmitigated)	1.97	2.28	23.04	0.02	3.18	0.61	1,724.12

Does not include correction for passby trips

Does not include double counting adjustment for internal trips

Analysis Year: 2010 Season: Annual

Emfac: Version : Emfac2007 V2.3 Nov 1 2006

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Strip mall		42.94	1000 sq ft	14.17	608.46	4,498.34
General office building		11.01	1000 sq ft	60.00	660.60	5,352.51

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Warehouse	4.96	1000 sq ft	8.30	41.17	304.36	
				1,310.23	10,155.21	

Vehicle Fleet Mix

Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	47.0	1.5	98.1	0.4
Light Truck < 3750 lbs	8.7	2.3	93.1	4.6
Light Truck 3751-5750 lbs	23.6	0.8	99.2	0.0
Med Truck 5751-8500 lbs	11.5	0.9	99.1	0.0
Lite-Heavy Truck 8501-10,000 lbs	1.7	0.0	82.4	17.6
Lite-Heavy Truck 10,001-14,000 lbs	0.5	0.0	60.0	40.0
Med-Heavy Truck 14,001-33,000 lbs	0.7	0.0	28.6	71.4
Heavy-Heavy Truck 33,001-60,000 lbs	0.2	0.0	0.0	100.0
Other Bus	0.0	0.0	0.0	0.0
Urban Bus	0.0	0.0	0.0	0.0
Motorcycle	4.5	66.7	33.3	0.0
School Bus	0.1	0.0	0.0	100.0
Motor Home	1.5	6.7	80.0	13.3

Travel Conditions

	Residential			Commercial	
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work
Urban Trip Length (miles)	10.8	7.3	7.5	9.5	7.4
Rural Trip Length (miles)	16.8	7.1	7.9	14.7	6.6

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
Trip speeds (mph)	35.0	35.0	35.0	35.0	35.0	35.0
% of Trips - Residential	32.9	18.0	49.1			
% of Trips - Commercial (by land use)						
Strip mall				2.0	1.0	97.0
General office building				35.0	17.5	47.5
Warehouse				2.0	1.0	97.0

Operational Changes to Defaults

ATTACH D

OXNARD CENTERPOINT TRAFFIC AND PARKING STUDY

Prepared by:

Austin-Foust Associates, Inc.
2223 Wellington Avenue, Suite 300
Santa Ana, California 92701-3161
(714) 667-0496

January 7, 2009

City of Oxnard
OXNARD CENTER MASTER SITE PLAN
TRAFFIC AND PARKING STUDY

Oxnard Center Company is planning to revise its existing 370,955 square feet (sf) Master Plan by removing 14,720 sf of existing food, retail and office use and expanding with another 102,245 sf of retail and office uses. This traffic study evaluates the change in traffic patterns and determines the need for any additional traffic mitigation. The general vicinity of the site is shown in Figure 1.

7,720

370,955

89,472

81702 Net New

102,245

TRAFFIC ANALYSIS

The proposed expansion will bring the center size to 458,480 sf, which includes a net increase of 87,525 sf over the existing 370,955 sf that is essentially fully occupied (i.e., over 98 percent at the time of the traffic counts in 2007). The uses removed include food, retail and a medical facility, while the new proposed developments include a mixture of uses comprised mostly of general office space (60,000 sf) and retail (38,250 sf). Figure 2 illustrates the proposed new Master Plan for Oxnard Center. Since the time of the counts, 7,000 sf (of the 14,720 sf of uses to be removed) has already been demolished and construction of 12,780 sf of retail space (of the proposed 102,245 expansion) to replace it is almost complete. Because the 12,780 sf of retail space is not finished or occupied, our analysis categorized this new development as still a proposed use and the 7,000 sf that has been demolished as an existing use to be removed.

The initial task of the traffic study is to forecast the change in trip generation. Table 1 shows the net change in traffic as well as the traffic currently attributable to the existing center. Examination of Table 1 reveals four interesting facts. These are:

1. The proposed change in use, although large in terms of square footage (i.e., an increase of over 85,000 sf), results in quite a modest increase in traffic (79 vehicles per hour (vph) in the AM and 121 vph in the PM peak hours).
2. The existing peak traffic generation of this center is generally in line with ITE trip rates.
3. The distribution of the project's increased traffic is highly directional (i.e., the majority of trips in the AM are largely inbound and in the PM virtually all are outbound).
4. Because the proposed site expansion will not generate 100 or more trips in the AM peak hour, the project will not significantly impact the CMP network system during this time frame and will instead focus on the traffic evaluation of the PM peak hour.

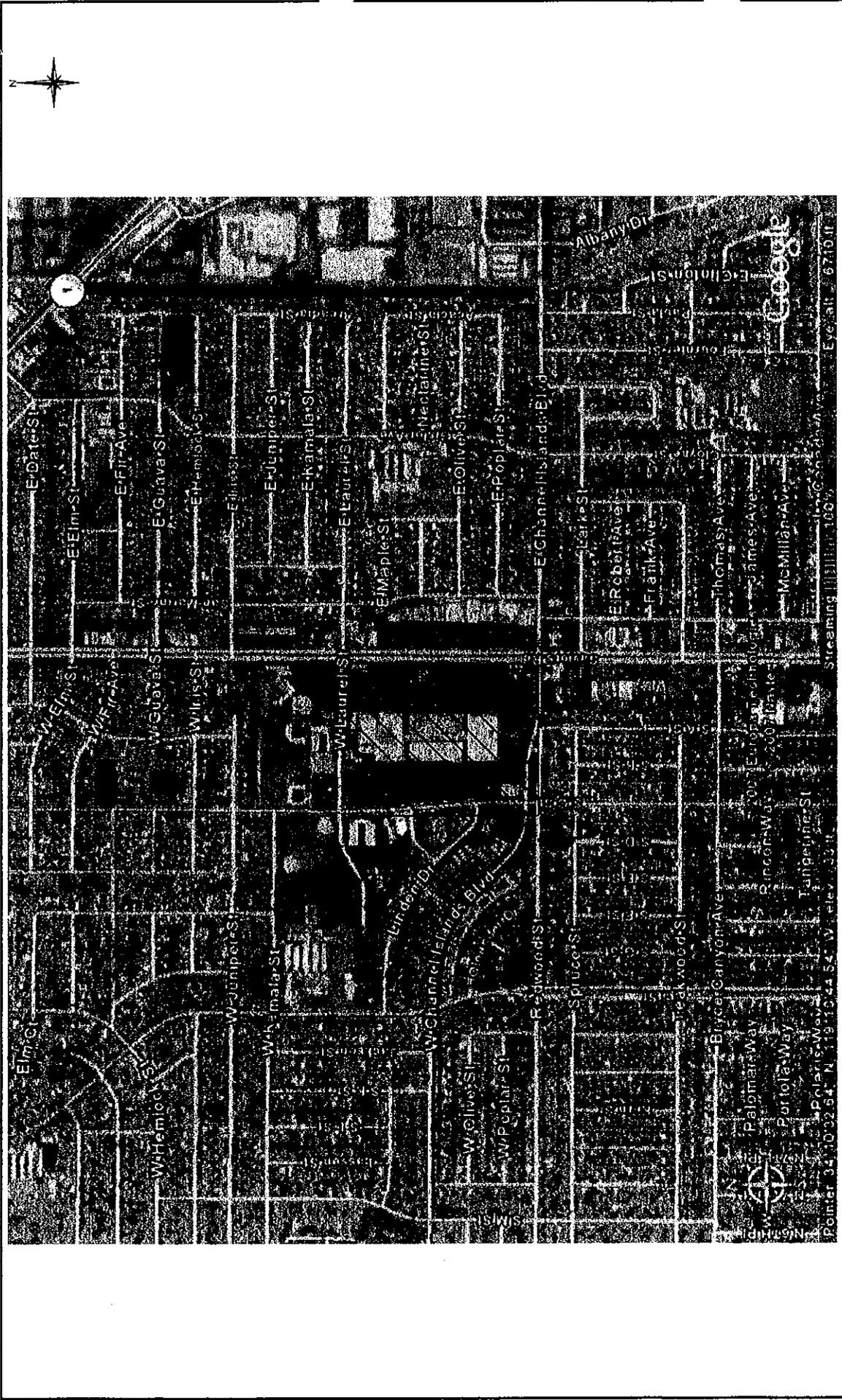


Figure 1
LOCATION MAP

Table 1 PROJECT TRIP GENERATION SUMMARY								
Use	Size	AM Peak Hour			PM Peak Hour			ADT
		In	Out	Total	In	Out	Total	
Existing Center (Actual Count)	370,955 sf	253	152	405	844	1,011	1,855	N/A
Existing Center (ITE Rates Individual Uses)	370,955 sf	676	494	1,140	1,347	1,455	2,802	26,720
Existing Center (ITE Rate Shopping Center)	370,955 sf	234	148	382	668	723	1,391	15,921
Proposed Removals	14,720 sf	50	44	94	56	45	101	1,209
Proposed New Uses	102,245 sf	121	52	173	76	146	222	2,682
Net Change in Trips		+71	+8	+79	+20	+101	+121	+1,473
N/A = Not Available								

A look at the distribution of traffic currently exiting the center indicates about 30 percent travel each north and south on Saviers Road while 20 percent goes east and west on Channel Islands Boulevard. This, in turn, means during the PM peak hour the north - south travel on Saviers Road will increase by 36 vph each direction whereas east - west travel on Channel Islands Boulevard will increase by 24 vph each way. In view of the fact that the intersection of Saviers Road and Channel Islands Boulevard currently operates at LOS "C" and that the PM peak flow rates on Saviers Road is almost 1,400 vph and Channel Islands Boulevard is 950 vph, the addition of 36 vph north - south and 24 vph east - west does not create a significant impact (change of 0.01 in ICU, see appendix). This analysis does not stretch beyond the project's one-mile sphere of influence since the site is surrounded mostly by residential uses, which is assumed to be the source of most of the additional trips, and the Saviers Road at Channel Islands intersection fell under the one percent threshold for analysis.

The master site plan notes a future improvement at the Saviers Road and Channel Islands Boulevard intersection of the addition of a southbound right-turn lane. As previously mentioned, the addition of the project traffic does not create a significant impact at this intersection and requires no mitigation measures. This right turn lane improvement is a future City widening project and is not the responsibility of the Oxnard Center Expansion, but the project is allowing for the right-of-way for this improvement when the existing Wells Fargo building, which is currently situated at the northwest corner of the intersection, is removed.

PARKING ANALYSIS

Austin-Foust Associates, (AFA) conducted a prior parking study on the center in 2006 when the Centerpoint Retail Center contained 341,333 sf of mixed use (retail, office and medical office) with a total of 1,845 parking spaces available. The issue is, "Will sufficient parking remain after the completion of the expansion and occupation?". The previous study answered this question with existing site parking counts of the entire center on three weekdays and two Saturdays in August and September 2006 when the vacancy rate in the entire center was less than three percent (10,000 sf vacant out of a total 341,000 sf). These counts reveal that the entire center experienced a maximum parking accumulation count of 775 spaces, which is equivalent to 2.34 spaces per occupied TSF. This peak occurred on a Saturday, but the weekday counts were just about as high – ranging from 674 to 711 spaces occupied. This relatively low parking utilization is due, at least in part, to the City Bus Center located directly adjacent to the site on "C" Street. This bus center was observed as quite busy during the parking counts.

Currently, the entire master plan revisions to the existing center call for removal of 14,720 sf of existing food, retail and medical uses (7,000 sf food, 4,920 sf retail and 2,800 of medical office) and replacement with 102,245 sf of office and retail uses. This results in a net increase of 87,525 sf in the center bringing the total to 458,480 sf. Construction of these new buildings results in a total of 1,320 parking spaces or 2.88 spaces per TSF of building area. Based upon the peak parking of the center in our previous study (i.e., 775 spaces for 331,000 sf of occupied space) the maximum demand for parking is 2.34 spaces per TSF. For the entire 458,480 sf center this equates to 1,073 spaces compared to a supply of 1,320 (maximum 81.3 percent filled). A 82 percent peak occupancy is a conservative amount compared with industry standards which recommends parking lots be designed where the peak expected demand should account for more than 90-95 percent of the parking supply for office type uses and 85-90 percent for retail uses.

Another favorable consideration is that the actual peak occupancy occurs on Saturdays when the bulk of the expansion is essentially vacant (62,000 sf of office and medical typically closed on weekends).

CONCLUSION

In summary, it is concluded that despite a sizable planned increase in square footage (87,525 sf) to the existing 370,995 sf mixed-use Oxnard Center, the net traffic increase in the AM falls under 100 peak hour trips, will not significantly impact the CMP network system during this time frame and instead focused on the traffic evaluation of the PM peak hour. In view of the fact that the intersection of Saviers Road and Channel Islands Boulevard currently operates at LOS "C" and that the PM peak flow rates on Saviers Road is almost 1,400 vph and Channel Islands Boulevard is 950 vph, the addition of 36 vph north - south and 24 vph east - west does not create a significant impact at this location.

The master site plan notes a future improvement at the Saviers Road and Channel Islands Boulevard intersection of the addition of a southbound right-turn lane. The addition of the project traffic does not create a significant impact at this intersection and requires no mitigation measures. This right turn lane improvement is a future City widening project and is not the responsibility of the Oxnard Center Expansion, but the center is making provision for the right-of-way to be available when the existing building (Wells Fargo) is removed.

The revised master plan for the entire site calls for 1,320 total parking spaces to serve 458,480 sf of a mixed-use office-retail center. The current parking activity in the existing center creates a peak parking demand of 2.34 spaces per TSF, which occurs on Saturdays. The proposed expansion of the center, which is largely office use, is essentially vacant on Saturdays. Even so, based on the peak parking demand, a maximum utilization of the parking facilities in the future is 82 percent or precisely what good design practice for a retail center calls for.

APPENDIX A

GENERAL NOTES

1. SEE GENERAL NOTES TO THESE DRAWINGS FOR A COMPLETE LIST OF NOTES.

2. ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE SPECIFIED.

3. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

4. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

5. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

6. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

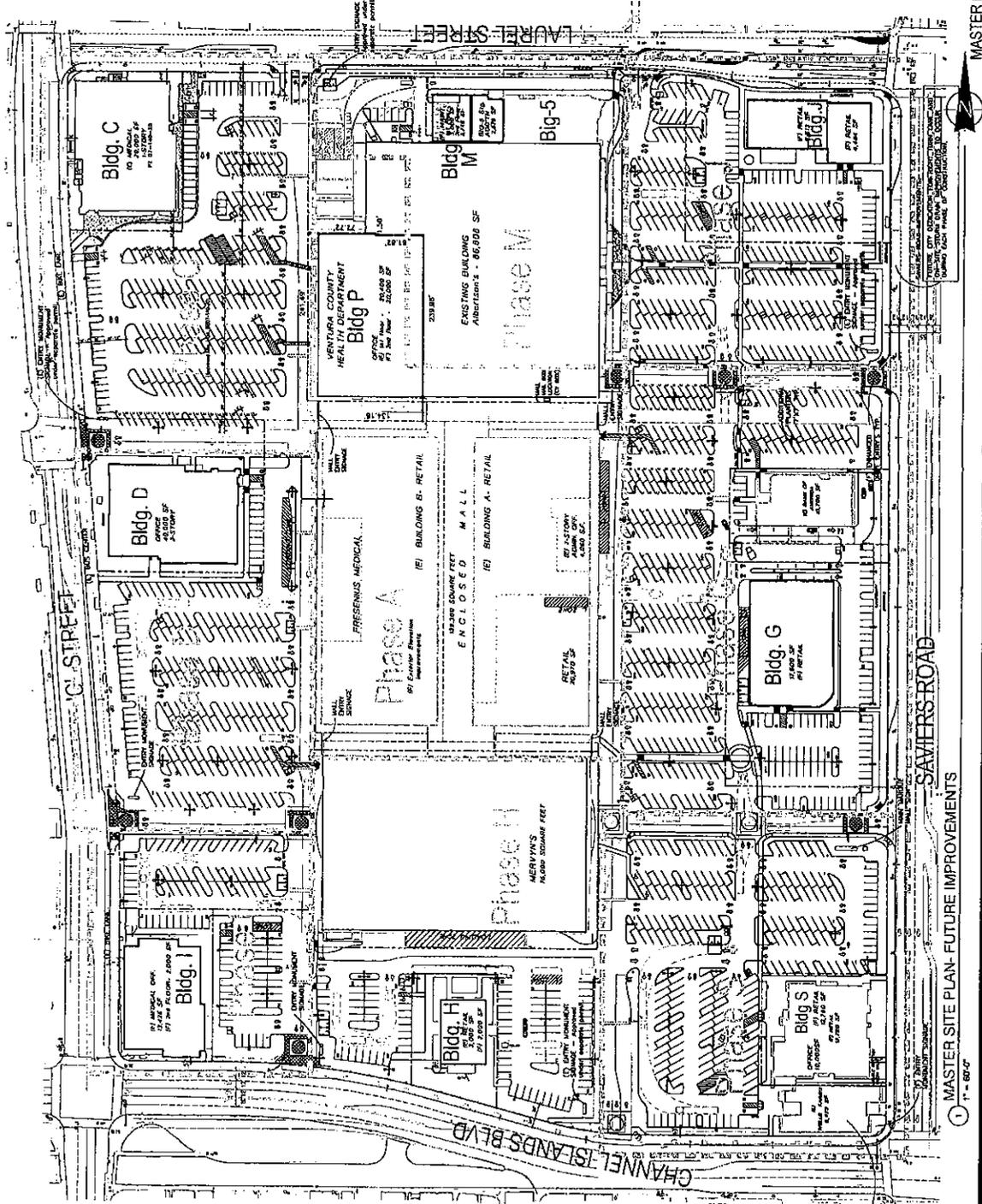
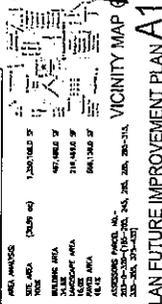
7. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

8. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

9. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

10. ALL DISTANCES ARE MEASURED FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.

NO.	DESCRIPTION	DATE
1	PRELIMINARY PLAN	10/15/08
2	REVISED PLAN	10/15/08
3	REVISED PLAN	10/15/08
4	REVISED PLAN	10/15/08
5	REVISED PLAN	10/15/08
6	REVISED PLAN	10/15/08
7	REVISED PLAN	10/15/08
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100	REVISED PLAN	10/15/08



Oct. 15, 2008

COASTAL

2008-0004

MASTER PLAN FUTURE IMPROVEMENT PLAN A1

Centerpoint Mall Master Plan

CHANNEL ISLANDS BLVD. & SAVIERS ROAD, OXNARD CA 93033

Centerpoint Mall

2008-0004

1" = 60'-0"



EXISTING BUILDINGS

Address	Tenant	Type of Occupant	S.F.	All Building sf Parked at 1/250
2701 Saviers	Mervyn's	Retail	76000.00	304
2401 Saviers	Alberston's	Retail	86808.00	347
2831 Saviers	Wells Fargo Bank	Bank	6472.00	26
2475 Saviers	Bank of America	Bank	10700.00	43
2501 Saviers	Mora's Boots	Retail	3000.00	12
2679 Saviers	BMA Dialysis	Medical	8000.00	32
2661 Saviers	Rite Aid	Retail	20970.00	84
150a Laurel St	Maintenance	Repair	2471.00	10
150 Laurel St	Auto Zone	Retail	4950.00	20
2643 Saviers	Wells Fargo Mortgage	Retail	1650.00	7
2651-A Saviers	Liberty Tax	Office	1090.00	4
2653-A Saviers	V.C. Health Services	Office	2760.00	11
2655-B Saviers	EB Games	Retail	1755.00	7
2657-A Saviers	Hair Image	Retail	1200.00	5
2503 Saviers	Princess Nails	Retail	465.00	2
2655 Saviers	Administrative Office	Office	4060.00	16
2655 Saviers	Storage	Storage	535.00	2
2623 Saviers	Photo Galleria	Retail	1400.00	6
2631 Saviers	T.J. Music	Retail	1250.00	5
2633 Saviers	Sense	Retail	1250.00	5
2643-A Saviers	Glitter	Retail	1800.00	7
2651 Saviers	Sneaker Time	Retail	2029.00	8
2653 Saviers	VIP	Retail	2580.00	10
2655-C Saviers	Daniel's Jewelry	Retail	1600.00	6
2655-D Saviers	Payless	Retail	4898.00	20
2665 Saviers	Indian Spirit	Retail	630.00	3
2661-A Saviers	5 de Mayo Jewelry	Retail	570.00	2
2661-B Saviers	Money Pancho Express	Food	568.00	2
2661-C Saviers	Mong. BBQ House	Food	682.00	3
2661-D Saviers	Chinese Combo #1	Food	675.00	3
2661-E Saviers	It's Greek to Me	Food	675.00	3
2661-F Saviers	Muro's	Retail	900.00	4
2667 Saviers	Crescent Jewelers	Retail	1312.00	5
2669 Saviers	Sports Zone	Retail	1312.00	5
2671 Saviers	World Place	Retail	1750.00	7
2673 Saviers	Kids Land	Retail	2232.00	9
2675 Saviers	Lim's	Retail	3450.00	14
2677 Saviers	Party N Gifts	Retail	2000.00	8
2679-A Saviers	Az Toys	Retail	2250.00	9
2681-B Saviers	Coquis 1dollar Store	Retail	1575.00	6
2681-A Saviers	Sports World	Retail	1575.00	6
2683 Saviers	Fashion Point	Retail	2400.00	10
2685-B Saviers	Vacant	Retail	2400.00	10
2685-A Saviers	Fashion 101	Retail	2400.00	10

see Revised 7/2/2009



2689 Saviers	Fernandez Jewelers	Retail	2304.00	9
2691 Saviers	Don Victor	Retail	2304.00	9
2693-A Saviers	Don Roberto Jewelers	Retail	3000.00	12
2693-A Saviers	Royal Family Shoes	Retail	3300.00	13
2695 Saviers	Maintenance	Storage	1304.00	5
2500 C St.	Ventura Public Health	Medical	20400.00	82
2697 Saviers	V.C. Health Services	Office	957.00	4
2699 Saviers	Vacant	Retail	1650.00	7
2699-A Saviers	Vacant	Retail	760.00	3
2687 Saviers	Oxnard Police Dept.	Office	1200.00	5
2687-A Saviers	Dance Studio	Retail	1200.00	5
2689-B Saviers	Las Islas Billing Dept.	Office	748.00	3
2689-A Saviers	Sky Dental	Medical	1575.00	6
201 W. C.I. Blvd	Taco Bell	Food	3100.00	12
325 W. C.I. Blvd	Las Islas Clinic	Medical	13426.00	54
2400 S. C St.	Vent. Cnty. Medical Center	Medical	20000.00	80
2801 Saviers	Building-S Multi-Tenant Retail	Retail	12780.00	51

Sub-Total 369057.00 1476

Existing provide on-site 1845

BUILDINGS REMOVED in (future)

2407 Saviers	Oasis	Retail	1920.00	8
2409 Saviers	Oxnard Dental	Medical	2800.00	11
2411 Saviers	Ritmo Latino	Retail	3000.00	12

Sub-Total 7720.0 31

Existing Building Total = 361337.00 1445

PROPOSED BUILDINGS

Building	Type	Type of Occupant	S.F.	All Building sf Parked at 1/250
Building 'D'	New	Office	40000.00	160
Building 'G'	New	Retail	6000.00	24
325 W. C.I. Blvd Building 'I'	Addition	Medical	2000.00	8
201 W. C.I. Blvd Building 'H'	Addition	Food	2000.00	8
2411 Saviers Building 'J'	Replacment	Retail	11097.00	44
150a Laurel St Building 'M'	Addition	Maintn.	3000.00	12
Building 'M'	2nd Flr Addtn	Maintn.	2746.00	11
2500 C St. Building 'P'	2nd Flr Infill	Office	20000.00	80
150 Laurel St Big-5	Addition	Retail	2580.00	10
			Sub-Total	89423.00 358

TOTAL MASTER PLAN BUILD-OUT =	Total	Parking at
	Bldg s.f.	1sp/250
	458,480	1834



	Total	All Building sf Parked at 1/250
PROPOSED PARK Building 'J'	212	
Building 'G'	319	
Building 'S'	192	
Building 'H'	94	
Building 'I'	98	
Building 'D'	177	
Building 'C'	218	
Building 'M' 6 + 4 Serv Bays=	10	
Proposed Site Parking Total		1320 spaces
Spaces Deficient	1320-1803=	-483 -27%
	s.f. per space =	341.48

ITE TRIP RATES, 7th EDITION

LAND USE		AM PEAK HOUR			PM PEAK HOUR			ADT
		IN	OUT	TOTAL	IN	OUT	TOTAL	
<u>Trip Rates</u>								
Single Family (210)	DU	0.19	0.56	0.75	0.65	0.36	1.01	9.57
Apartments (220)	DU	0.10	0.41	0.51	0.40	0.22	0.62	6.72
Low-Rise Condo (231)	DU	0.17	0.50	0.67	0.45	0.33	0.78	7.89 *
Senior Adult Housing - Detached (251)	DU	0.08	0.12	0.20	0.16	0.10	0.26	3.71
Senior Adult Housing - Attached (252)	DU	0.04	0.04	0.08	0.07	0.04	0.11	3.48
Hotel (310)	Room	0.34	0.22	0.56	0.31	0.28	0.59	8.17
General Light Industrial (110)	TSF	0.81	0.11	0.92	0.12	0.86	0.98	6.97
Industrial Park (130)	TSF	0.69	0.15	0.84	0.18	0.68	0.86	6.96
Manufacturing (140)	TSF	0.56	0.17	0.73	0.27	0.47	0.74	3.82
Private School K-12 (536)	Stu	0.48	0.31	0.79	0.07	0.10	0.17	2.48
Church (560)	TSF	0.39	0.33	0.72	0.34	0.32	0.66	9.11
Hospital (610)	TSF	0.80	0.40	1.20	0.39	0.79	1.18	17.57
Office (710)	TSF	1.36	0.19	1.55	0.25	1.24	1.49	11.01
Medical/Dental Office (720)	TSF	1.96	0.52	2.48	1.00	2.72	3.72	36.13
Office Park (750)	TSF	1.55	0.19	1.74	0.21	1.29	1.50	11.42
Business Park (770)	TSF	1.20	0.23	1.43	0.30	0.99	1.29	12.76
General Commercial (Shop Ctr) (820) ***	TSF	0.63	0.40	1.03	1.80	1.95	3.75	42.94
Free-Standing Discount Store (815)	TSF	0.57	0.27	0.84	2.53	2.53	5.06	56.02
Specialty Retail (814)	TSF	0.60	0.76	1.36	1.19	1.52	2.71	44.32 **
Supermarket (850)	TSF	1.98	1.27	3.25	5.33	5.12	10.45	102.24
Quality Restaurant (931)	TSF	0.54	0.27	0.81	5.02	2.47	7.49	89.95
High-Turnover Restaurant (932)	TSF	5.99	5.53	11.52	6.66	4.26	10.92	127.15
Fast Food w/Drive-Through (934)	TSF	27.09	26.02	53.11	18.01	16.63	34.64	496.12
Home Improvement Superstore (862)	TSF	0.65	0.55	1.20	1.15	1.30	2.45	29.80

* Condo ADT interpolated between SFD and Apt ADT

** Specialty Retail AM peak hour rates estimated as 50% of PM peak hour rates

*** Shopping Center Fitted Curve Equations:

ADT Ln(T) = .65Ln(X) + 5.83	=	#NUM!	IN	OUT
AM Ln(T) = .60Ln(X) + 2.29	=	#NUM!	#NUM!	#NUM!
PM Ln(T) = .66Ln(X) + 3.40	=	#NUM!	#NUM!	#NUM!

PROJECT: Oxnard Center - Proposed uses

Trip Generation

Single Family	0 DU	0	0	0	0	0	0	0
Apartments	0 DU	0	0	0	0	0	0	0
Condo	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Detached	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Attached	0 DU	0	0	0	0	0	0	0
Hotel	0 Room	0	0	0	0	0	0	0
General Light Industrial	0 TSF	0	0	0	0	0	0	0
Industrial Park	0 TSF	0	0	0	0	0	0	0
Manufacturing	0 TSF	0	0	0	0	0	0	0
Private School K-12	0 Stu	0	0	0	0	0	0	0
Church	0 TSF	0	0	0	0	0	0	0
Hospital	0 TSF	0	0	0	0	0	0	0
Office	60.00 TSF	82	11	93	15	74	89	661
Medical Office	2.00 TSF	4	1	5	2	5	7	72
Office Park	0 TSF	0	0	0	0	0	0	0
Business Park	0 TSF	0	0	0	0	0	0	0
General Commercial	0 TSF	0	0	0	0	0	0	0
Free-Standing Discount Store	0 TSF	0	0	0	0	0	0	0
Specialty Retail	38.25 TSF	23	29	52	46	58	104	1,695
Supermarket	TSF	0	0	0	0	0	0	0
Quality Restaurant	TSF	0	0	0	0	0	0	0
High-Turnover Restaurant	2 TSF	12	11	23	13	9	22	254
Fast Food w/Drive-Through	0 TSF	0	0	0	0	0	0	0
Home Depot	0 TSF	0	0	0	0	0	0	0
TOTAL		121	52	173	76	146	222	2,682

ITE TRIP RATES, 7th EDITION

LAND USE		AM PEAK HOUR			PM PEAK HOUR			ADT
		IN	OUT	TOTAL	IN	OUT	TOTAL	
<u>Trip Rates</u>								
Single Family (210)	DU	0.19	0.56	0.75	0.65	0.36	1.01	9.57
Apartments (220)	DU	0.10	0.41	0.51	0.40	0.22	0.62	6.72
Low-Rise Condo (231)	DU	0.17	0.50	0.67	0.45	0.33	0.78	7.89 *
Senior Adult Housing - Detached (251)	DU	0.08	0.12	0.20	0.16	0.10	0.26	3.71
Senior Adult Housing - Attached (252)	DU	0.04	0.04	0.08	0.07	0.04	0.11	3.48
Hotel (310)	Room	0.34	0.22	0.56	0.31	0.28	0.59	8.17
General Light Industrial (110)	TSF	0.81	0.11	0.92	0.12	0.86	0.98	6.97
Industrial Park (130)	TSF	0.69	0.15	0.84	0.18	0.68	0.86	6.96
Manufacturing (140)	TSF	0.56	0.17	0.73	0.27	0.47	0.74	3.82
Private School K-12 (536)	Stu	0.48	0.31	0.79	0.07	0.10	0.17	2.48
Church (560)	TSF	0.39	0.33	0.72	0.34	0.32	0.66	9.11
Hospital (610)	TSF	0.80	0.40	1.20	0.39	0.79	1.18	17.57
Office (710)	TSF	1.36	0.19	1.55	0.25	1.24	1.49	11.01
Medical/Dental Office (720)	TSF	1.96	0.52	2.48	1.00	2.72	3.72	36.13
Office Park (750)	TSF	1.55	0.19	1.74	0.21	1.29	1.50	11.42
Business Park (770)	TSF	1.20	0.23	1.43	0.30	0.99	1.29	12.76
General Commercial (Shop Ctr) (820) ***	TSF	0.63	0.40	1.03	1.80	1.95	3.75	42.94
Free-Standing Discount Store (815)	TSF	0.57	0.27	0.84	2.53	2.53	5.06	56.02
Specialty Retail (814)	TSF	0.60	0.76	1.36	1.19	1.52	2.71	44.32 **
Supermarket (850)	TSF	1.98	1.27	3.25	5.33	5.12	10.45	102.24
Quality Restaurant (931)	TSF	0.54	0.27	0.81	5.02	2.47	7.49	89.95
High-Turnover Restaurant (932)	TSF	5.99	5.53	11.52	6.66	4.26	10.92	127.15
Fast Food w/Drive-Through (934)	TSF	27.09	26.02	53.11	18.01	16.63	34.64	496.12
Home Improvement Superstore (862)	TSF	0.65	0.55	1.20	1.15	1.30	2.45	29.80

* Condo ADT interpolated between SFD and Apt ADT

** Specialty Retail AM peak hour rates estimated as 50% of PM peak hour rates

*** Shopping Center Fitted Curve Equations:

			IN	OUT
ADT Ln(T) = .65Ln(X) + 5.83	=	#NUM!		
AM Ln(T) = .60Ln(X) + 2.29	=	#NUM!	#NUM!	#NUM!
PM Ln(T) = .66Ln(X) + 3.40	=	#NUM!	#NUM!	#NUM!

PROJECT: Oxnard Center - Existing uses to be removed

Trip Generation

Single Family	0 DU	0	0	0	0	0	0	0
Apartments	0 DU	0	0	0	0	0	0	0
Condo	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Detached	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Attached	0 DU	0	0	0	0	0	0	0
Hotel	0 Room	0	0	0	0	0	0	0
General Light Industrial	0 TSF	0	0	0	0	0	0	0
Industrial Park	0 TSF	0	0	0	0	0	0	0
Manufacturing	0 TSF	0	0	0	0	0	0	0
Private School K-12	0 Stu	0	0	0	0	0	0	0
Church	0 TSF	0	0	0	0	0	0	0
Hospital	0 TSF	0	0	0	0	0	0	0
Office	0 TSF	0	0	0	0	0	0	0
Medical Office	2.8 TSF	5	1	7	3	8	10	101
Office Park	0 TSF	0	0	0	0	0	0	0
Business Park	0 TSF	0	0	0	0	0	0	0
General Commercial	0 TSF	0	0	0	0	0	0	0
Free-Standing Discount Store	0 TSF	0	0	0	0	0	0	0
Specialty Retail	4.92 TSF	3	4	7	6	7	13	218
Supermarket	0 TSF	0	0	0	0	0	0	0
Quality Restaurant	0 TSF	0	0	0	0	0	0	0
High-Turnover Restaurant	7.00 TSF	42	39	81	47	30	77	890
Fast Food w/Drive-Through	0.00 TSF	0	0	0	0	0	0	0
Home Depot	0 TSF	0	0	0	0	0	0	0
TOTAL		50	44	95	56	45	100	1,209

ITE TRIP RATES, 7th EDITION

LAND USE		AM PEAK HOUR			PM PEAK HOUR			ADT
		IN	OUT	TOTAL	IN	OUT	TOTAL	
<u>Trip Rates</u>								
Single Family (210)	DU	0.19	0.56	0.75	0.65	0.36	1.01	9.57
Apartments (220)	DU	0.10	0.41	0.51	0.40	0.22	0.62	6.72
Low-Rise Condo (231)	DU	0.17	0.50	0.67	0.45	0.33	0.78	7.89 *
Senior Adult Housing - Detached (251)	DU	0.08	0.12	0.20	0.16	0.10	0.26	3.71
Senior Adult Housing - Attached (252)	DU	0.04	0.04	0.08	0.07	0.04	0.11	3.48
Hotel (310)	Room	0.34	0.22	0.56	0.31	0.28	0.59	8.17
General Light Industrial (110)	TSF	0.81	0.11	0.92	0.12	0.86	0.98	6.97
Industrial Park (130)	TSF	0.69	0.15	0.84	0.18	0.68	0.86	6.96
Manufacturing (140)	TSF	0.56	0.17	0.73	0.27	0.47	0.74	3.82
Private School K-12 (536)	Stu	0.48	0.31	0.79	0.07	0.10	0.17	2.48
Church (560)	TSF	0.39	0.33	0.72	0.34	0.32	0.66	9.11
Hospital (610)	TSF	0.80	0.40	1.20	0.39	0.79	1.18	17.57
Office (710)	TSF	1.36	0.19	1.55	0.25	1.24	1.49	11.01
Medical/Dental Office (720)	TSF	1.96	0.52	2.48	1.00	2.72	3.72	36.13
Office Park (750)	TSF	1.55	0.19	1.74	0.21	1.29	1.50	11.42
Business Park (770)	TSF	1.20	0.23	1.43	0.30	0.99	1.29	12.76
General Commercial (Shop Ctr) (820) ***	TSF	0.63	0.40	1.03	1.80	1.95	3.75	42.94
Free-Standing Discount Store (815)	TSF	0.57	0.27	0.84	2.53	2.53	5.06	56.02
Specialty Retail (814)	TSF	0.60	0.76	1.36	1.19	1.52	2.71	44.32 **
Supermarket (850)	TSF	1.98	1.27	3.25	5.33	5.12	10.45	102.24
Quality Restaurant (931)	TSF	0.54	0.27	0.81	5.02	2.47	7.49	89.95
High-Turnover Restaurant (932)	TSF	5.99	5.53	11.52	6.66	4.26	10.92	127.15
Fast Food w/Drive-Through (934)	TSF	27.09	26.02	53.11	18.01	16.63	34.64	496.12
Home Improvement Superstore (862)	TSF	0.65	0.55	1.20	1.15	1.30	2.45	29.80

* Condo ADT interpolated between SFD and Apt ADT

** Specialty Retail AM peak hour rates estimated as 50% of PM peak hour rates

*** Shopping Center Fitted Curve Equations:

			IN	OUT
ADT Ln(T) = .65Ln(X) + 5.83	=	15,922		
AM Ln(T) = .60Ln(X) + 2.29	=	344	210	134
PM Ln(T) = .66Ln(X) + 3.40	=	1,487	714	773

PROJECT: ITE Trip Generation for Existing as a Shopping Center

Trip Generation

Single Family	0 DU	0	0	0	0	0	0	0
Apartments	0 DU	0	0	0	0	0	0	0
Condo	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Detached	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Attached	0 DU	0	0	0	0	0	0	0
Hotel	0 Room	0	0	0	0	0	0	0
General Light Industrial	0 TSF	0	0	0	0	0	0	0
Industrial Park	0 TSF	0	0	0	0	0	0	0
Manufacturing	0 TSF	0	0	0	0	0	0	0
Private School K-12	0 Stu	0	0	0	0	0	0	0
Church	0 TSF	0	0	0	0	0	0	0
Hospital	0 TSF	0	0	0	0	0	0	0
Office	TSF	0	0	0	0	0	0	0
Medical Office	TSF	0	0	0	0	0	0	0
Office Park	TSF	0	0	0	0	0	0	0
Business Park	TSF	0	0	0	0	0	0	0
General Commercial	370.96 TSF	234	148	382	668	723	1,391	15,929
Free-Standing Discount Store	TSF	0	0	0	0	0	0	0
Specialty Retail	TSF	0	0	0	0	0	0	0
Supermarket	TSF	0	0	0	0	0	0	0
Quality Restaurant	TSF	0	0	0	0	0	0	0
High-Turnover Restaurant	TSF	0	0	0	0	0	0	0
Fast Food w/Drive-Through	TSF	0	0	0	0	0	0	0
Home Depot	TSF	0	0	0	0	0	0	0
TOTAL		234	148	382	668	723	1,391	15,929

ITE TRIP RATES, 7th EDITION

LAND USE		AM PEAK HOUR			PM PEAK HOUR			ADT
		IN	OUT	TOTAL	IN	OUT	TOTAL	
<u>Trip Rates</u>								
Single Family (210)	DU	0.19	0.56	0.75	0.65	0.36	1.01	9.57
Apartments (220)	DU	0.10	0.41	0.51	0.40	0.22	0.62	6.72
Low-Rise Condo (231)	DU	0.17	0.50	0.67	0.45	0.33	0.78	7.89 *
Senior Adult Housing - Detached (251)	DU	0.08	0.12	0.20	0.16	0.10	0.26	3.71
Senior Adult Housing - Attached (252)	DU	0.04	0.04	0.08	0.07	0.04	0.11	3.48
Hotel (310)	Room	0.34	0.22	0.56	0.31	0.28	0.59	8.17
General Light Industrial (110)	TSF	0.81	0.11	0.92	0.12	0.86	0.98	6.97
Industrial Park (130)	TSF	0.69	0.15	0.84	0.18	0.68	0.86	6.96
Manufacturing (140)	TSF	0.56	0.17	0.73	0.27	0.47	0.74	3.82
Private School K-12 (536)	Stu	0.48	0.31	0.79	0.07	0.10	0.17	2.48
Church (560)	TSF	0.39	0.33	0.72	0.34	0.32	0.66	9.11
Hospital (610)	TSF	0.80	0.40	1.20	0.39	0.79	1.18	17.57
Office (710)	TSF	1.36	0.19	1.55	0.25	1.24	1.49	11.01
Medical/Dental Office (720)	TSF	1.96	0.52	2.48	1.00	2.72	3.72	36.13
Office Park (750)	TSF	1.55	0.19	1.74	0.21	1.29	1.50	11.42
Business Park (770)	TSF	1.20	0.23	1.43	0.30	0.99	1.29	12.76
General Commercial (Shop Ctr) (820) ***	TSF	0.63	0.40	1.03	1.80	1.95	3.75	42.94
Free-Standing Discount Store (815)	TSF	0.57	0.27	0.84	2.53	2.53	5.06	56.02
Specialty Retail (814)	TSF	0.60	0.76	1.36	1.19	1.52	2.71	44.32 **
Supermarket (850)	TSF	1.98	1.27	3.25	5.33	5.12	10.45	102.24
Quality Restaurant (931)	TSF	0.54	0.27	0.81	5.02	2.47	7.49	89.95
High-Turnover Restaurant (932)	TSF	5.99	5.53	11.52	6.66	4.26	10.92	127.15
Fast Food w/Drive-Through (934)	TSF	27.09	26.02	53.11	18.01	16.63	34.64	496.12
Home Improvement Superstore (862)	TSF	0.65	0.55	1.20	1.15	1.30	2.45	29.80

* Condo ADT interpolated between SFD and Apt ADT

** Specialty Retail AM peak hour rates estimated as 50% of PM peak hour rates

*** Shopping Center Fitted Curve Equations:

			IN	OUT
ADT Ln(T) = .65Ln(X) + 5.83	=	18,272		
AM Ln(T) = .60Ln(X) + 2.29	=	390	238	152
PM Ln(T) = .66Ln(X) + 3.40	=	1,710	821	889

PROJECT: ITE Trip Generation for Proposed Shopping Center

Trip Generation

Single Family	0 DU	0	0	0	0	0	0	0
Apartments	0 DU	0	0	0	0	0	0	0
Condo	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Detached	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Attached	0 DU	0	0	0	0	0	0	0
Hotel	0 Room	0	0	0	0	0	0	0
General Light Industrial	0 TSF	0	0	0	0	0	0	0
Industrial Park	0 TSF	0	0	0	0	0	0	0
Manufacturing	0 TSF	0	0	0	0	0	0	0
Private School K-12	0 Stu	0	0	0	0	0	0	0
Church	0 TSF	0	0	0	0	0	0	0
Hospital	0 TSF	0	0	0	0	0	0	0
Office	TSF	0	0	0	0	0	0	0
Medical Office	TSF	0	0	0	0	0	0	0
Office Park	TSF	0	0	0	0	0	0	0
Business Park	TSF	0	0	0	0	0	0	0
General Commercial	458.48 TSF	289	183	472	825	894	1,719	19,687
Free-Standing Discount Store	TSF	0	0	0	0	0	0	0
Specialty Retail	TSF	0	0	0	0	0	0	0
Supermarket	TSF	0	0	0	0	0	0	0
Quality Restaurant	TSF	0	0	0	0	0	0	0
High-Turnover Restaurant	TSF	0	0	0	0	0	0	0
Fast Food w/Drive-Through	TSF	0	0	0	0	0	0	0
Home Depot	TSF	0	0	0	0	0	0	0
TOTAL		289	183	472	825	894	1,719	19,687

ITE TRIP RATES, 7th EDITION

LAND USE		AM PEAK HOUR			PM PEAK HOUR			ADT
		IN	OUT	TOTAL	IN	OUT	TOTAL	
<u>Trip Rates</u>								
Single Family (210)	DU	0.19	0.56	0.75	0.65	0.36	1.01	9.57
Apartments (220)	DU	0.10	0.41	0.51	0.40	0.22	0.62	6.72
Low-Rise Condo (231)	DU	0.17	0.50	0.67	0.45	0.33	0.78	7.89 *
Senior Adult Housing - Detached (251)	DU	0.08	0.12	0.20	0.16	0.10	0.26	3.71
Senior Adult Housing - Attached (252)	DU	0.04	0.04	0.08	0.07	0.04	0.11	3.48
Hotel (310)	Room	0.34	0.22	0.56	0.31	0.28	0.59	8.17
General Light Industrial (110)	TSF	0.81	0.11	0.92	0.12	0.86	0.98	6.97
Industrial Park (130)	TSF	0.69	0.15	0.84	0.18	0.68	0.86	6.96
Manufacturing (140)	TSF	0.56	0.17	0.73	0.27	0.47	0.74	3.82
Private School K-12 (536)	Stu	0.48	0.31	0.79	0.07	0.10	0.17	2.48
Church (560)	TSF	0.39	0.33	0.72	0.34	0.32	0.66	9.11
Hospital (610)	TSF	0.80	0.40	1.20	0.39	0.79	1.18	17.57
Office (710)	TSF	1.36	0.19	1.55	0.25	1.24	1.49	11.01
Medical/Dental Office (720)	TSF	1.96	0.52	2.48	1.00	2.72	3.72	36.13
Office Park (750)	TSF	1.55	0.19	1.74	0.21	1.29	1.50	11.42
Business Park (770)	TSF	1.20	0.23	1.43	0.30	0.99	1.29	12.76
General Commercial (Shop Ctr) (820) ***	TSF	0.63	0.40	1.03	1.80	1.95	3.75	42.94
Free-Standing Discount Store (815)	TSF	0.57	0.27	0.84	2.53	2.53	5.06	56.02
Specialty Retail (814)	TSF	0.60	0.76	1.36	1.19	1.52	2.71	44.32 **
Supermarket (850)	TSF	1.98	1.27	3.25	5.33	5.12	10.45	102.24
Drive-In Bank (912)	TSF	6.91	5.43	12.34	22.87	22.87	45.74	246.49
High-Turnover Restaurant (932)	TSF	5.99	5.53	11.52	6.66	4.26	10.92	127.15
Fast Food w/Drive-Through (934)	TSF	27.09	26.02	53.11	18.01	16.63	34.64	496.12
Home Improvement Superstore (862)	TSF	0.65	0.55	1.20	1.15	1.30	2.45	29.80

* Condo ADT interpolated between SFD and Apt ADT

** Specialty Retail AM peak hour rates estimated as 50% of PM peak hour rates

*** Shopping Center Fitted Curve Equations:

				<u>IN</u>	<u>OUT</u>
ADT Ln(T) = .65Ln(X) + 5.83	=	#NUM!			
AM Ln(T) = .60Ln(X) + 2.29	=	#NUM!	#NUM!	#NUM!	
PM Ln(T) = .66Ln(X) + 3.40	=	#NUM!	#NUM!	#NUM!	

PROJECT: ITE Trip Generation of Existing Shopping Center (Individual Uses)

Trip Generation

Single Family	0 DU	0	0	0	0	0	0	0
Apartments	0 DU	0	0	0	0	0	0	0
Condo	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Detached	0 DU	0	0	0	0	0	0	0
Senior Adult Housing - Attached	0 DU	0	0	0	0	0	0	0
Hotel	0 Room	0	0	0	0	0	0	0
General Light Industrial	0 TSF	0	0	0	0	0	0	0
Industrial Park	0 TSF	0	0	0	0	0	0	0
Manufacturing	0 TSF	0	0	0	0	0	0	0
Private School K-12	0 Stu	0	0	0	0	0	0	0
Church	0 TSF	0	0	0	0	0	0	0
Hospital	0 TSF	0	0	0	0	0	0	0
Office	10.8 TSF	15	2	17	3	13	16	119
Medical Office	66.2 TSF	130	34	164	66	180	246	2,392
Office Park	0 TSF	0	0	0	0	0	0	0
Business Park	0 TSF	0	0	0	0	0	0	0
General Commercial	TSF	0	0	0	0	0	0	0
Free-Standing Discount Store	76 TSF	43	21	64	192	192	384	4,258
Specialty Retail	92.181 TSF	55	70	125	110	140	250	4,085
Supermarket	86.8 TSF	172	110	282	463	444	907	8,874
Drive-In Bank	17.172 TSF	119	93	212	393	393	786	4,233
High-Turnover Restaurant	9.6 TSF	58	53	111	64	41	105	1,221
Fast Food w/Drive-Through	3.1 TSF	84	81	165	56	52	108	1,538
Home Depot	0 TSF	0	0	0	0	0	0	0
TOTAL		676	464	1,140	1,347	1,455	2,802	26,720

51. Saviers & Channel Islands

Existing						
	LANES	CAPACITY	AM PK HOUR		PM PK HOUR	
			VOL	V/C	VOL	V/C
NBL	1	1600	270	.17*	320	.20*
NBT	3	4800	680	.17	1010	.25
NBR	0	0	150		210	
SBL	1	1600	130	.08	210	.13
SBT	3	4800	470	.11*	1090	.25*
SBR	0	0	70		90	
EBL	1	1600	100	.06	150	.09
EBT	2	3200	1020	.32*	550	.17*
EBR	1	1600	90	.06	250	.16
WBL	1	1600	260	.16*	250	.16*
WBT	2	3200	540	.17	720	.23
WBR	1	1600	100	.06	160	.10
TOTAL CAPACITY UTILIZATION				.76		.78

Existing + Project						
	LANES	CAPACITY	AM PK HOUR		PM PK HOUR	
			VOL	V/C	VOL	V/C
NBL	1	1600	274	.17*	321	.20*
NBT	3	4800	698	.18	1015	.26
NBR	0	0	150		210	
SBL	1	1600	130	.08	210	.13
SBT	3	4800	472	.11*	1115	.25*
SBR	0	0	70		90	
EBL	1	1600	100	.06	150	.09
EBT	2	3200	1022	.32*	570	.18*
EBR	1	1600	90	.06	255	.16
WBL	1	1600	260	.16*	250	.16*
WBT	2	3200	554	.17	724	.23
WBR	1	1600	100	.06	160	.10
TOTAL CAPACITY UTILIZATION				.76		.79

TRAFFIC DATA SERVICES, INC.
PARKING STUDY

LOCATION: CENTERPOINT MALL
CITY: OXNARD

FILENAME: 09601PK1
DATE: 8/31/06
DAY: THURSDAY

Time Period Beginning	ZONES								ZONES				TOTAL
	1	2	3	4	5	6	7	8	9	10	11	12	
SPACES:	89	271	318	104	3	232	407	247					1671 *
6:00 AM													
7:00 AM													
8:00 AM													
9:00 AM	66	80	101	10	2	6	56	89					410
10:00 AM	70	119	122 ✓	12	2	23	109	114					671
11:00 AM	84	104	111	8	3	84	112	97					603
12:00 PM	71	120	90	15	3	111	135	129					674 ✓
1:00 PM	73	124	91	13	4	74	130	130					639
2:00 PM	74	118	102	5	2	33	123	109					566
3:00 PM	74	111	110	9	1	25	126	104					560
4:00 PM	70	109	94	7	0	49	138	132					599
5:00 PM	49	99	93	11	0	54	131	141					578
6:00 PM	33	110	41	12	1	79	126	144					546
7:00 PM	21	87	37	12	0	107	118	131					513
8:00 PM													
9:00 PM													
10:00 PM													

COMMENTS: * *NOTE: SOME PARKING SPACES COVERED UP BY CONSTRUCTION MATERIALS*

TRAFFIC DATA SERVICES, INC.
PARKING STUDY

LOCATION: CENTERPOINT MALL
CITY: OXNARD

FILENAME: 09601PK2
DATE: 9/01/06
DAY: FRIDAY

Time Period Beginning	ZONES								ZONES				TOTAL
	1	2	3	4	5	6	7	8	9	10	11	12	
SPACES:	89	271	318	104	3	232	407	247					1671 *
6:00 AM													
7:00 AM													
8:00 AM													
9:00 AM	56	57	62	4	0	15	91	181					466
10:00 AM	67	83	80	4	1	22	144	144					545
11:00 AM	71	89	89	8	3	32	182	148					622
12:00 PM	64	86	96	16	6	96	168	175					707 ✓
1:00 PM	55	91	92	11	2	72	194	172					689
2:00 PM	67	91	105	10	1	55	160	139					628
3:00 PM	69	89	106 ✓	11	2	37	188	151					653
4:00 PM	61	89	97	6	1	69	209	166					698
5:00 PM	48	88	59	9	0	131	221	155					711
6:00 PM	27	81	51	10	0	152	163	167					651
7:00 PM	14	71	44	7	1	156	125	138					556
8:00 PM													
9:00 PM													
10:00 PM													

COMMENTS:

TRAFFIC DATA SERVICES, INC.
PARKING STUDY

LOCATION: CENTERPOINT MALL
CITY: OXNARD

FILENAME: 09601PK3
DATE: 9/02/06
DAY: SATURDAY

Time Period	ZONES								ZONES				TOTAL
	1	2	3	4	5	6	7	8	9	10	11	12	
SPACES:	89	271	318	104	3	232	407	247					1671 *
6:00 AM													
7:00 AM													
8:00 AM													
9:00 AM	18	40	63 ✓	3	0	11	67	79					281
10:00 AM	29	67	44	6	0	32	145	126					449
11:00 AM	37	82	47	3	0	43	203	156					571
12:00 PM	36	88	62	7	0	70	182	164					609
1:00 PM	30	87	58	19	2	76	237	266					775 ✓
2:00 PM	41	92	49	4	0	70	189	161					606
3:00 PM	39	92	56	9	0	98	184	154					632
4:00 PM													
5:00 PM													
6:00 PM													
7:00 PM													
8:00 PM													
9:00 PM													
10:00 PM													

COMMENTS:

TRAFFIC DATA SERVICES, INC.
PARKING STUDY

LOCATION: CENTERPOINT MALL
CITY: OXNARD

FILENAME: 09601PK4
DATE: 9/05/06
DAY: TUESDAY

Time Period Beginning	ZONES								ZONES				TOTAL
	1	2	3	4	5	6	7	8	9	10	11	12	
SPACES:	89	271	318	104	3	232	407	247					1671 *
6:00 AM													
7:00 AM													
8:00 AM													
9:00 AM	73	76	75	1	2	17	116	84					444
10:00 AM	88	103	100	14	1	24	166	125					621
11:00 AM	87	104	111	10	1	33	173	143					662
12:00 PM	82	106	91	17	1	61	159	141					658
1:00 PM	67	92	118	13	1	47	170	123					631
2:00 PM	71	110	141 ✓	7	2	26	139	146					642
3:00 PM	72	112	132	10	1	25	144	129					625
4:00 PM	72	116	120	11	1	30	175	156					681 ✓
5:00 PM	59	96	100	6	0	48	166	141					616
6:00 PM	36	87	62	5	0	65	126	174					555
7:00 PM	33	76	67	10	0	62	114	152					514
8:00 PM													
9:00 PM													
10:00 PM													

COMMENTS:

TRAFFIC DATA SERVICES, INC.
PARKING STUDY

LOCATION: CENTERPOINT MALL
CITY: OXNARD

FILENAME: 09601PK5
DATE: 9/09/06
DAY: SATURDAY

Time Period Beginning	ZONES								ZONES				TOTAL
	1	2	3	4	5	6	7	8	9	10	11	12	
SPACES:	89	271	318	104	3	232	407	247					1671*
6:00 AM													
7:00 AM													
8:00 AM													
9:00 AM	5	21	18	2	0	11	84	26					167
10:00 AM	14	68	31	5	0	31	171	74					394
11:00 AM	17	73	47	9	1	55	202	138					542
12:00 PM	21	85	49	11	0	69	215	151					601
1:00 PM	19	91	52	13	1	58	189	189					612*
2:00 PM	24	97	58	11	0	61	174	178					603
3:00 PM	18	101	63✓	14	0	67	163	172					598
4:00 PM													
5:00 PM													
6:00 PM													
7:00 PM													
8:00 PM													
9:00 PM													
10:00 PM													

COMMENTS:

GENERAL NOTES	
EXISTING PARKING SPACES	1,842
STANDARD	35
HANDICAPPED	4
MOTORCYCLE	1
DRIVE THRU LANE	1
TOTAL SPACES	1,843

4,000 PARKING SPACES REQUIRED FOR EXISTING BUILDINGS	
HEALTHY'S	70,000 SF
WALGREEN'S	50,000 SF
WALMART	130,000 SF
WALMART SUPERCENTERS	180,000 SF
WALMART STORES	20,000 SF
WALMART EXPRESS	4,000 SF
WALMART GROCERY	4,000 SF
WALMART HOME CENTER	4,000 SF
WALMART NEIGHBORHOOD MARKET	4,000 SF
WALMART SUPERCENTERS	4,000 SF
WALMART STORES	4,000 SF
WALMART EXPRESS	4,000 SF
WALMART GROCERY	4,000 SF
WALMART HOME CENTER	4,000 SF
WALMART NEIGHBORHOOD MARKET	4,000 SF
TOTAL SQUARE FOOTAGE	384,000 SF
TOTAL SPACES REQUIRED	384,000 SF / 2200 = 174,545 SPACES

THE PARKING SPACES REQUIRED FOR PROPOSED BUILDING	
PROPOSED:	60
Building C	20,000 SF
Building D	10,000 SF
Building E	5,000 SF
Building F	2,000 SF
TOTAL SPACES REQUIRED	60,000 SF @ 1200 = 50 SPACES

PROPOSED PARKING SPACES	
Building C	20,000 SF
Building D	10,000 SF
Building E	5,000 SF
Building F	2,000 SF
Total Spaces Proposed	37,000

TOTAL OF PARKING SPACES	
EXISTING	1,843
PROPOSED	37,000
TOTAL	38,843

PARKING SPACES (SHORT)	
PARKING SPACES	276,157
TOTAL EXISTING SPACES	1,843
TOTAL PROPOSED SPACES	37,000
TOTAL REQUIRED SPACES	1,843



SITE PLAN A1

COASTAL ARCHITECTS
 ARCHITECTURE + PLANNING
 1139 OJA COURT, OXNARD CA 93033
 (805) 461-1144 FAX (805) 461-1143

CENTERPOINT MALL
 MASTER SITE PLAN
 SAVIERS ROAD & CHANNEL ISLANDS BLVD., OXNARD CA

Oxnard Center Company
 2425 Saviers Road, Oxnard CA 93033 (805) 487-1142



MEMORANDUM

TO: Pat Farrell

FROM: Joe Foust, P.E.

DATE: August 3, 2009

SUBJECT: **OXNARD CENTER MASTER SITE PLAN TRAFFIC AND PARKING STUDY**

Austin-Foust Associates, Inc. (AFA) prepared the Oxnard Center Master Site Plan Traffic and Parking Study in January 2009. It has since come to our attention that the center currently contains a number of compact parking stalls. The City is requesting further parking review in order to grant administrative approval for these compact parking stalls. Stated in Oxnard's City Code (SEC. 16-651. ADMINISTRATIVE RELIEF FROM PARKING PROVISIONS, paragraph-3), the City may grant administrative relief in the form of compact parking spaces for no more than 25% of the total spaces. Of the 1,320 parking spaces to be provided, 74 of them would be compact parking spaces. These 74 compact parking spaces make up less than six percent of the total parking supply, well below the 25% that the City may approve.

Secondly, based upon the peak parking of the center in our previous study (i.e., 775 spaces for 331,000 sf of occupied space) the maximum demand for parking is 2.34 spaces per TSF. For the entire 458,480 sf center this equates to 1,073 spaces compared to a supply of 1,320 (maximum 81.3 percent filled). Industry standards recommends parking lots be designed where the peak expected demand should account for more than 90-95 percent of the parking supply for office type uses and 85-90 percent for retail uses. Even without the 74 compact parking spaces included in the center's total parking supply (1,320 spaces – 74 compact parking spaces = 1,246 parking spaces), the center's peak parking demand would still result in an ideal industry standard of only 86% utilization. Such a supply would adequately meet all parking needs of the center (1,073 space demand/1,246 space supply = 86%).

In conclusion, 74 compact parking spaces within the center would be acceptable, given the abundant supply of full-size parking spaces. It is not expected these 74 spaces will inconvenience drivers, impair safety or traffic circulation on the site.

1053002mm.doc

ATTACH E

Date: 4-30-2009

From: Mr. Pat Farrell
Centerpoint Mall
2655 Saviers Road
Oxnard, CA 93030

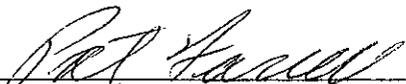
RE: Initial Study and Mitigated Negative Declaration (MND 09-01), Centerpoint Mall Master Plan;
Planning and Zoning Permit No. 08-540-01 Planed Development Permit

To: Planning Division Manager

Pursuant to Section 15070 (Negative or Mitigated Negative Declaration Process) of the State Guidelines implementing the California Environmental Quality Act, I/we, acting as agents for the property owner/developer, hereby agree to all of the following:

- 1) The draft initial study identifies potentially significant effects from the project, but the study also identifies mitigation measures that would avoid or mitigate the effects to a level where clearly no significant effects would occur;
- 2) The mitigation measures are hereby incorporated into the project prior to releasing the draft initial study and mitigated negative declaration for public comment;
- 3) I/we agree to the mitigation measures as necessary to avoid or mitigate significant effects that would otherwise arise from the project. I/we accept the mitigation measures included in the draft initial study and have resolved all questions and concerns regarding the mitigation measures;
- 4) If during the public comment period and/or decision-making process, substitute or additional mitigation measures are proposed, the appropriate process must take place for determining whether or not to substitute or apply additional measures;
- 5) This agreement is binding upon the applicant for this project and any successors in interest or assignees.

This acknowledgment is binding upon the applicant and any successors in interest or assignees:


Signature

4-30-2009
Date

PAT FARRELL
Print Name

GENERAL MANAGER
Title

This acknowledgment is to be attached to the draft initial study and mitigated negative declaration for the project and then released for the applicable public comment period.

RESOURCE MANAGEMENT AGENCY
county of ventura

Environmental Health Division
Robert Gallagher
Director

May 19, 2009

RECEIVED

MAY 21 2009

PLANNING DIVISION
CITY OF OXNARD

Stephanie Diaz
Planning Division
City of Oxnard
214 South "C" Street
Oxnard, CA 93030

**MITIGATED NEGATIVE DECLARATION (MND #09-01) FOR PLANNED
DEVELOPMENT PERMIT NO. 08-540-1, CENTERPOINT MALL AT 2655 SAVIERS
ROAD, OXNARD**

Environmental Health Division (EHD) staff reviewed the document submitted for the subject project and comments that the subject project is located on a former leaking underground fuel tank (LUFT) site identified as case ID #93-0300643, Centerpoint Mall. Due to the time period in which the case was closed, further evaluation of the site may be necessary. In addition, information found on the Geotracker database indicates that the Los Angeles Regional Water Quality Control Board (LARWQCB) has an open case located at 2655 Saviers Rd., Oxnard, identified as case ID #0419, Regal Cleaners. Please contact Yue Rong of LARWQCB at 213/576-6710 for more information on the status of the cases.

Please contact me at 805/654-2811 if you have any questions.



MELINDA TALENT
LAND USE SECTION
ENVIRONMENTAL HEALTH DIVISION

c: Erin O'Connell, EHD
Yue Rong, LARWQCB



NSN Engineering
Environmental, Geotechnical, Management and Construction

Mrs. Stephanie Diaz
City of Oxnard Planning Division
214 South "C" Street
Oxnard, CA 93030

Dear Mrs. Diaz:

This letter is in response to the "Mitigated Negative Declaration (MDN 09-01) for Planned Development Permit Number 08-540-01 Centerpoint Mall, 2655 Saviers Road, Oxnard, California, Letter" dated May 19, 2009 prepared by Resource Management Agency Environmental Health Division County of Ventura. This letter provides additional information regarding the environmental history and present status of the California Regional Water Quality Control Board (CRWQCB) Site Clean up Program file 95-062 for this site.

The CRWQCB Case Manager, You Rong, identified in the aforementioned letter, is not in charge with the site, as indicated on the Geotracker. Presently, no case manager is assigned for this project. The site had been designated by the CRWQCB as "low risk" and over the last several years, the CRWQCB has reassigned several inspectors for oversight of the project. However, due to the shortage of staff, heavy work load, and the "low risk" designated status of the site, no response to our Site Closure Request Report Request, dated September 17, 2007 has been received. During this time, the Oxnard Center Company made a concerted effort to maintain close communication with the CRWQCB and illicit a response to the Site Closure Request. The following presents for your perusal a brief environmental status of the project.

During a site assessment conducted in 1995, chlorinated solvents were detected in soil and groundwater beneath the site. The contamination resulted from dry cleaning operations conducted at former Regal Cleaners business. The Regal Cleaners used to be a tenant in the Center Pointe Mall located at the above referenced address. The chlorinated solvent hydrocarbon detected in soil and groundwater were tetrachloroethene (PCE) and low concentrations of trichloroethene (TCE)

The site underwent soil and groundwater remediation from 1996 through 2002. Vapor extraction and air sparging were used as remediation technologies at the site. The vapor extraction system was shut down in March 2002, when PCE influent concentrations had reached trace levels and displayed asymptotic behavior. The operation of the remediation system in pulse mode had not indicated a rebound in influent concentrations.

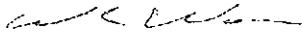
Based on the verification soil and groundwater sampling conducted in February 2001, most of the PCE and TCE concentrations in the soil had been reduced to concentrations below the Soil Screening Levels (SSL) protective of the groundwater. These levels were calculated based on the CRWQCB *Interim Site Assessment and Clean-up Guidebook, dated May 1996*. The residual impacted soil area was limited and did not appear to pose a further threat to groundwater. The groundwater monitoring program implemented at the site commenced in 1996 and has continued to date. The last groundwater monitoring session was conducted in March, 2009. Based on the groundwater monitoring data the PCE concentrations in all groundwater wells have decreased significantly since initial assessment in 1996. Very low residual contaminant concentrations (PCE only) above Drinking Water Standards have remained present in several groundwater monitoring wells.

Based on this, a Site Closure Request was submitted to the CRWQCB on September 17, 2007 and a "No Further Action Letter" for this site was requested. As indicated above, to date, no response has been received from the CRWQCB.

We hope that, the CRWQCB, who was in the process of hiring and reassigning the project to a new case manager prior to the State hiring freeze, will reallocate resources to this project and provide the requested "No Further Action Letter".

If you need additional information, or you wish us to submit a copy of the First Semi-annual Groundwater Monitoring Report, dated March 30 2009, prepared by NSN Engineering and submitted to the CRWQCB, please do not hesitate to call me at 310-947-1602, or email me at nnorocea@roadrunner.com

Sincerely,



Nick Norocea, PE

NSN Engineering

1922 Ava Avenue, Hermosa Beach, California 90254, telephone: 310-947-1602, fax: 310-798-8921,
email: nnorocea@roadrunner.com

ATTACHMENT D

Design Guidelines and Master Sign Program

DESIGN GUIDELINES

For

ARCHITECTURE
LIGHTING
SIGNAGE

MASTER PLAN PROGRAM

For the

CENTERPOINT MALL
SAVIERS ROAD & CHANNEL ISLAND BLVD.

ADOPTED: _____.

PERPARED BY:



505 So. A Street, 2nd Fl. Oxnard CA 93030

CENTERPOINT MALL ARCHITECTURE AND LIGHTING

Purpose and Use of this Document

Purpose

This document provides architectural guidelines for all buildings located within the area bordered by the West side of Saviers Road and East side of C Street between Laurel Street and Channel Islands Blvd also known as Centerpoint Mall. This document is intended to provide the overall design direction for projects within the Master Plan (Exhibit 1.0), known as the project site and to identify the architectural requirements for each of the buildings/parcels on the project site. These guidelines serve to ensure that all future development and renovations within the project will be consistent with the architectural expectations for this area.

Included in these guidelines are the following elements:

- Exhibit 1.0 : Master Site Plan and Site Features
- Exhibit 2.1-2.3 : Building Prototypes
- Exhibit 3.0 : Definition and Terms (Illustrative sketches and details)
- Exhibit 4.1 - 4.3 : Primary and Secondary Drive Access
- Exhibit 5.1 - 5.8 : Signage program area allotment
- Exhibit 6.1 - 6.3 : Building signage analysis
- Exhibit 7.0 : Design Check List

Basis and Effect of this Document

This document provides architectural guidelines for all buildings located in Centerpoint Mall. These guidelines are based on the architectural style shown in the various attachments at the back of this document. The suggested elevation and sketches herein depict the minimum architectural expectations for the project. These architectural guidelines translate the suggested architectural elevations, which are not building-specific, into general and specific design requirements for all proposed buildings within the project area. These guidelines further identify and interpret the architectural expectations for the project area and are to be carried out for all buildings or future building remodels.

Review and Determination for Consistency with this Document

For projects within the project area, the City Planning and Environmental Services Manager, upon review by the City's Development Advisory Committee (DAC), shall make a determination about an application's consistency with the requirements of these guidelines. Additionally, all components of a project application are subject to the review and approval of the City Planning and Environmental Services Manager, unless otherwise specified within the City Code.

Application Process

This document does not relieve or modify any existing procedures. City Staff shall determine what type of permit is required for the each application (e.g., minor modification, major modification, zoning clearance, development design review, sign permit etc.), and then apply the requirements as outlines in the City's Zoning Ordinance/City Code. Should an individual property owner elect to vary from these adopted Guidelines, an application for a major modification will be required. A hearing before the Planning Commission will be necessary to modify these Guidelines for one or more property owners.

SITE DESIGN GUIDELINES

Site Design

Site shall be designed to allow the free flow of traffic within the center. The center has been segmented into eight (8) parking lot zones. These zones correspond to adjacent users or major tenants. Parking throughout the Centerpoint Mall has reciprocal parking and access through all parcels. Major entry ways into the center are to be modified through a phasing program approved by the city. Improvements are to include upgrading the drive entry thresholds with textured or colored concrete and saw cut grid patterns; concrete color should be an earth tone such as sand/stone (Exhibit 4.1). Pedestrian walk ways are to be brought from the public right of way and into the center. Bus Center is to be maintained on the west side of the center. Improvements are to occur within each lot as development of the phases are completed.

Pedestrian Environment

Defined pedestrian oriented spatial elements, such as courtyards should be provided. Such elements shall help to manage the bulk of large structures and introduce a human scale element. The internal and external streetscape design should encourage pedestrian activity. Pedestrian walkways should be identified with textured and/or colored paving and buffered from automobile traffic by use of landscape planters, bollards, street furniture, etc. Vertical "gateway" elements should be employed to draw the pedestrian to designated walkways leading from the parking areas to the commercial activity areas. Features within 4 feet of finish grade, shall be more durable than those higher on the building to withstand potential damage, wear, etc. For example, wood siding , foam with plaster overlays and fiberglass elements are not allowed within 4 feet of finish grade as it is more prone to damage than more durable materials such as GFRC (glass fiber reinforced concrete), pre-cast concrete, stone, masonry, etc. Glazing shall not be allowed within 12" of the finish grade.

Recycling and Trash Storage

Storage areas and trash enclosures should be designed to be an adequate size to allow for storage of recyclable materials, including separate containers for glass, paper, plastic, etc when required. Such areas, in addition to the requirements of the City Zoning Ordinance, shall be screened from view by walls and landscaping. The color for trash enclosures will consist of warm tones such as Kynar 500-aged bronze for the corrugated metal screen and Sand-stone for the walls.

Signage

Signage, while addressed in a separate section of these guidelines, is noted here to ensure that the designs for future signs placed on the individual structures shall be complimentary to the architectural design and character of the building. Said signs shall reflect the exterior materials and color(s) of the respective building and shall be illuminated per the Sign Guidelines, or externally (flood lighting spots floods). Specific guidelines are including within the Sign Guidelines text.

Site Lighting

All lighting shall comply with the applicable photometric requirements of the City. The following standards will also apply:

“Gateway” Corners (see Exhibit No.3.0): Special corner treatments at identified corners shall be lighted by decorative, contemporary, lighting suited to the landscaping and sculptural elements located thereon. The maximum height of such lighting shall be 14 feet (as measures from grade to the top of the light fixture); ground mounted spot floods are recommended.

Parking Lot – The maximum height of such lights shall not exceed 26 feet (as measured from grade to the top of the light fixture). All parking lot lighting shall be consistent in design.

General Exterior Fixtures – Wall-pack light fixtures are unacceptable, all exterior fixtures shall be flat lens, full cut-off fixtures with the light source fully shielded unless fixture is of a low light output. No light source shall sign directly upon, or illuminate directly on a surface other that the area required to be lit.

Other “Street Furniture”

Trash cans, pedestrian benches, automatic teller machines and newspaper racks shall be decorative in appearance and shall be complimentary to the architecture, street lights and landscaping on the project site. No plastic furniture shall be allowed as public seating. Wood, Decorative metal and concrete or combination of material shall be allowed.

Seasonal Banners and Poles are allowed for Primary and secondary access. On primary access they are to be placed in light poles at 24 feet maximum height and/or at individual center posts with maximum height of 19 feet (Exhibit 4.2). On Secondary access they shall be placed in light poles only at a maximum height of 23 feet (Exhibit 4.3). Area of Banners shall not exceed 15sf. Banners are to be changed through out the year to keep within seasonal and holiday themes. No tenant advertising is allowed on banners.

ARCHITECTURAL EXPECTATIONS

Architectural Style

The approved architectural style, which applies to Remodels, Additions and New Buildings, is detailed in this document as follows:

“Contemporary Retail” style architecture based on strong, distinctive massing and varying proportions. These proportions are based on simplicity and clean lines with an emphasis on color, exposed structural elements, such as roof overhangs and shade canopies, and varied repetition of geometric fenestration openings. An example of this style is found in the Retail complex at 2350 Vineyard Avenue, adjacent to the high rise. While overall architectural treatment is required, the emphasis shall be placed on features that are appreciated by the pedestrian first and the motorist second. Additionally, overall architecture interest shall be high, with striking massing, proportions and a rich blend of building materials.

General Architectural Requirements

While all of the buildings will be separated by parking and landscape elements, the overall site palette of structures shall be sensitive in massing and building proportions. Vertical elements to attract the eye of both pedestrians and those driving by are encouraged on all buildings. The building mass should be broken up by modulating the walls exterior to the street; i.e. stepping in and out, incorporating recesses and projections and varying in height, colors and materials. On building facades comprised primarily of straight continuous walls greater than 30 linear feet, such walls shall feature appropriate breaks in the wall planes to complement the structure’s massing, proportions and roof elements. Walls with less than 30 linear feet of length will not require a break in the plane of the façade, but may require additional detailing. In contemporary architecture, the bare space between projections is as important as the projections themselves and must be treated accordingly. Additional detailing may be accomplished by wide plaster reglets, geometric openings and color banding. Each piece should stand out as individual and unique yet complete an entire composition. See Color Palette section for applications of appropriate color treatments versus facades.

Green Design Elements

As much as possible, it is encouraged to use renewable resources as well as recycled products within the structure and finishes of the building. These elements should consist within 30% of the building site and at least 30% within the building envelope. The specific types of resources such as recycled wood products, asphalt paving and concrete to be used for base material,

Rear and Interior Sides of Buildings

Many of the structures are free-standing and will be visible from all sides; all elevations shall be subject to equal architectural treatment. An exception to this requirement would be exterior elevations which face loading zones or interior courtyard locations.

Building Materials

- a. Exterior wall construction and finishes are divided into two categories. Those dominant materials which comprise sixty (60) to eighty (80) percent of the building's elevation should consist of painted exterior concrete/ masonry panels. The additional dominate material which comprises twenty (20) to thirty (30) percent of the building's elevation, should consist of exterior plasters or EIFS architectural projection or glass elements. The remaining accent or entry materials may consist of stone or metal surfacing elements. Prohibited materials include wood siding, shingles and pre-engineered types of metal siding.

Exterior materials should be a combination of the following:

- i. Tilt-up Concrete or masonry walls, in combination with plastered or EIFS finished architectural projections.
 - ii. Exterior, waterproof cement plaster (stucco). Texture shall be light to medium dash. Cement plaster walls shall use exposed crack control joints with reveals varying from 1/8" to 1" wide. The pattern of these joints will be designed to reflect the proportions of the wall element and aligned logically with storefront systems, fenestration patterns or changes in the plane of the wall.
 - iii. Exterior Insulation Finish Systems (EIFS), with exposed control joints to align logically with other exterior systems
 - iv. Contemporary metal siding panels or stone accents (at entry towers for new buildings and/or remodels)
 - v. Exposed steel shade elements.
 - vi. Exterior brick veneer used as an accent highlight.
 - vii. Slate or stone veneer base under windows to match existing.
- b. The following materials may be incorporated in conjunction with the above as additional wall surfaces in order to modulate scale and architectural massing:
 - i. Storefront glazing systems with contemporary profiles. Clear glazing to be primarily used. Colored glazing shall be complimentary to other elements of the structure. Clear anodized aluminum framing shall be used throughout the center and to replace existing storefront within the phasing of the center.
 - ii. Ornamental metal screens, louvers, trellises and architectural elements that compliment/accent specific features of the structure.

Color Palette

The predominant color of the exterior walls shall be low reflectance, neutral with warm, earth tone colors. The use of high intensity colors, metallic, black or other accent colors shall be used sparingly and only for trim or accents. Entry features shall have a colored accent or material change wall defining entry.

Material/Color boards for the Center have been submitted to the City with the Master plan approval process. Future building on site shall comply to the colors selected and reviewed by (DDR) Development Design Review prior to approval. Applicant may apply new material/ color boards for review that will

compliment the center's colors and shall be approved by the Development Design Review prior to issuing permit approval.

Building Entrances

Each structure shall have clearly defined, highly visible pedestrian customer entrances with feature such as canopies, porticos, Trellis, wing walls and integral planters as suggested in Exhibit Nos. 2.1 and 2.2. The use of tower elements which break up the massing of parapet style building is encouraged.

Roofs, Parapets, Eaves and Soffit

All roofs are to be concealed "flat" roofs with the exception of architectural entry features and projections requiring roof elements. Roofing material should be standing seam metal roofing with a pre-finished Kynar application. Roof shapes using exposed roofing shall be shed style or curved or a combination thereof. Mansards are not allowed on any structure. Roof eaves shall incorporate gutters, downspouts and leader boxes compliment the shape, massing and colors of the adjacent wall or soffit elements.

Parapet walls that conceal a flat roof beyond are allowed but must be designed as an integral element of the structure's massing and proportions. Long, continuous parapets of the same height are not allowed; modulating of roof lines with different elements (i.e. combining a tower, gable roof or exposed pitch roof with parapet walls) shall reinforce and enhance the structure's massing and proportions.

All exposed roofs shall have a minimum roof-pitch of 3:12 and maximum pitch of 12:12.

Decorative scuppers (the elements which connect the down pipe to the rain gutter) shall be used when parapet walls concealing a roof beyond are present. Said scuppers shall be treated as an architectural accent feature, designed to be ornamental and complimentary to the contemporary lines of structure.

Screening of Equipment

All roof equipment should be completely screened within a horizontal line of sight. In the event parapet screening is not in compliance, a screen enclosure behind the parapet may be used if made to appear an integral part of the building. The parapet roof must be tall enough to completely screen the equipment. Superficial mansard treatments which appear as "plant-ons" or "eyebrows" are not acceptable.

Storefront Systems

All glass shall be transparent (maximum tinting of 25%) and non-reflective (L.O.L., Low E annealed), clear or blue/green (Solex) or others if warranted in the design of the structure. Reflective glazing may only be used sparingly as an accent element.

A 12" min. bulkhead (that portion of the wall below the storefront windows) shall be incorporated into all storefront systems by placing the glass on top of a concrete masonry, stucco or tile bulkhead (maximum 24" tall). An alternative to the bulkhead is to provide a distinctive storefront design by incorporating architectural elements into the system (pre-cast concrete columns, moldings, etc.).

Auxiliary Structures

Canopies and other similar structural elements on buildings shall be treated as a part of the main structure, using the same materials and detailing while respecting the overall massing of the combined structural

elements. While the canopy may be free-standing and detached from the primary structure, the massing, proportions and dimensions of both elements must take into consideration the relationship.

Illumination of Individual Structures

In addition to general safety considerations, lighting shall be used as an integral architectural feature of the structure, enhancing the presentation of the individual structures after dark. Lighting levels should be restrained but sufficiently strong (5 lumens/s.f.) for effective underlining of the structure's design characteristics without washing out the building signage. Floor mounted or wall mounted up lights, as well as wall scones, are recommended as long as any light beam is shielded or controlled so not interfere with adjacent properties, public streets or FAA requirements. Stem-mounted fluorescent billboard-type light fixtures are prohibited. Well-designed neon lighting for architectural accent is allowed and encouraged.

Awnings and Architectural Projections

Fabric awnings, steel trellis or shade structures may be incorporated into design of the structure as an architectural element but shall only be used to shade a door or window and shall be installed immediately above such openings. Architectural projections allow for the creation of shadow lines which add visual interest as well as emphasizing an entry or windows.

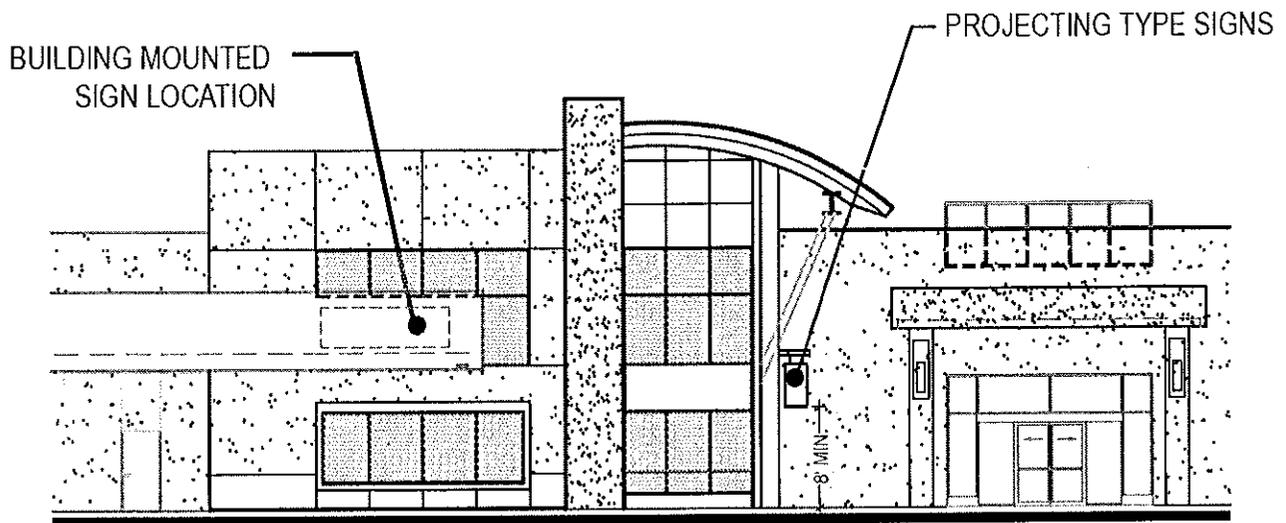
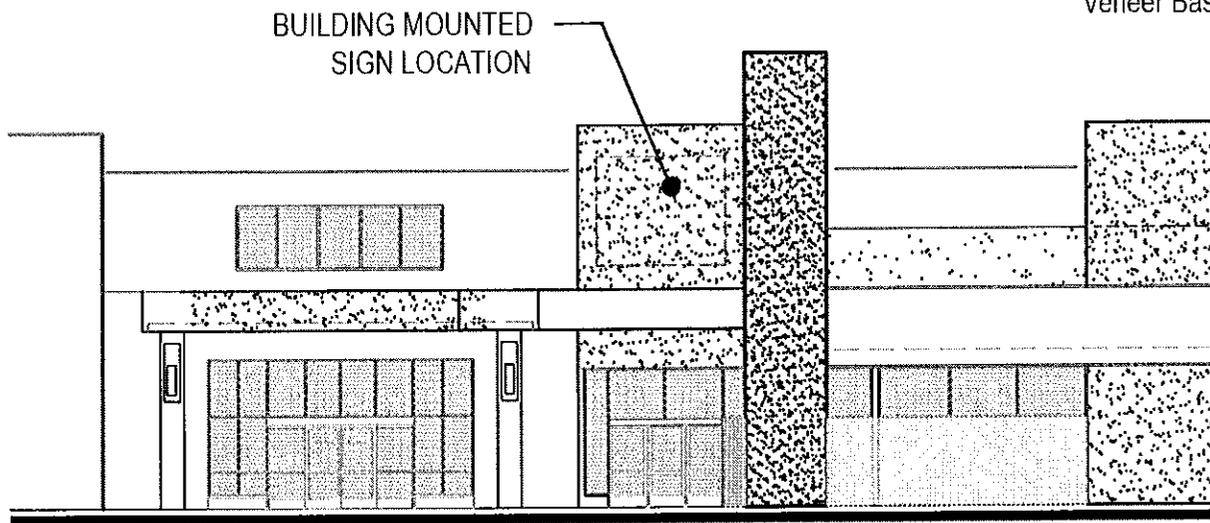
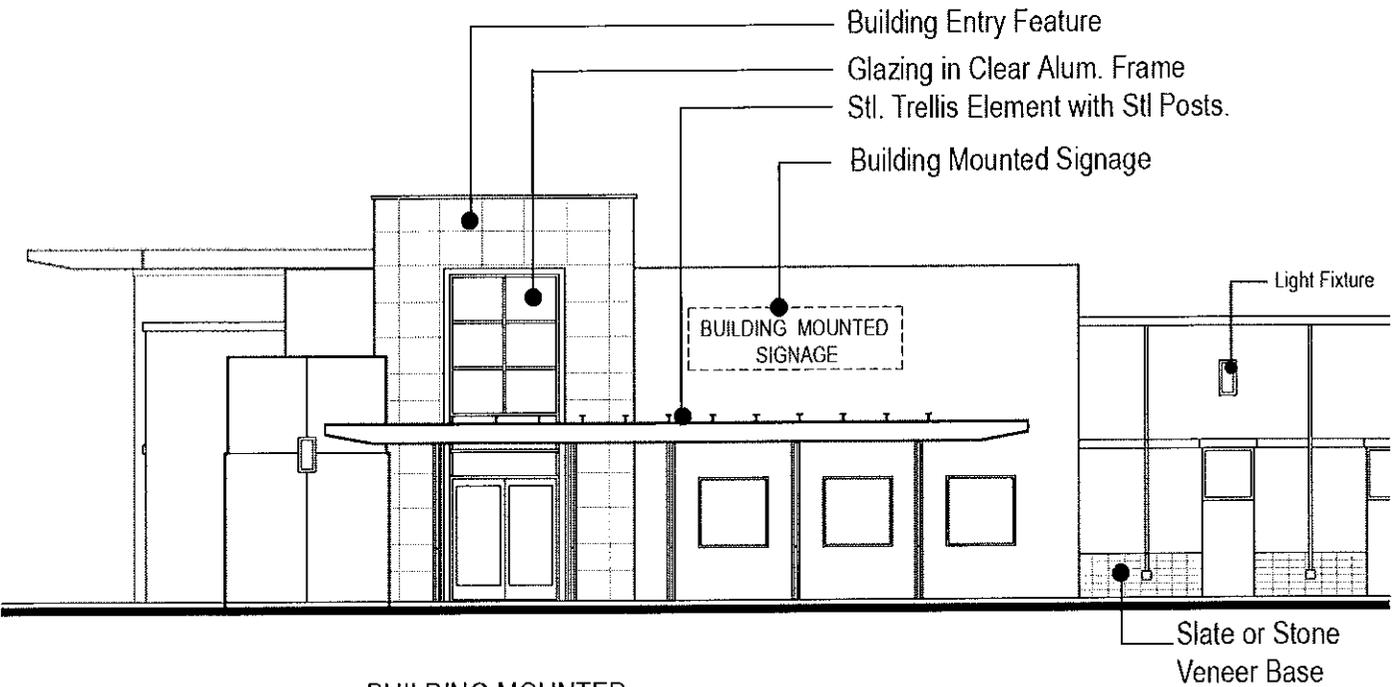


Exhibit 2.1
 BUILDING PROTOTYPES

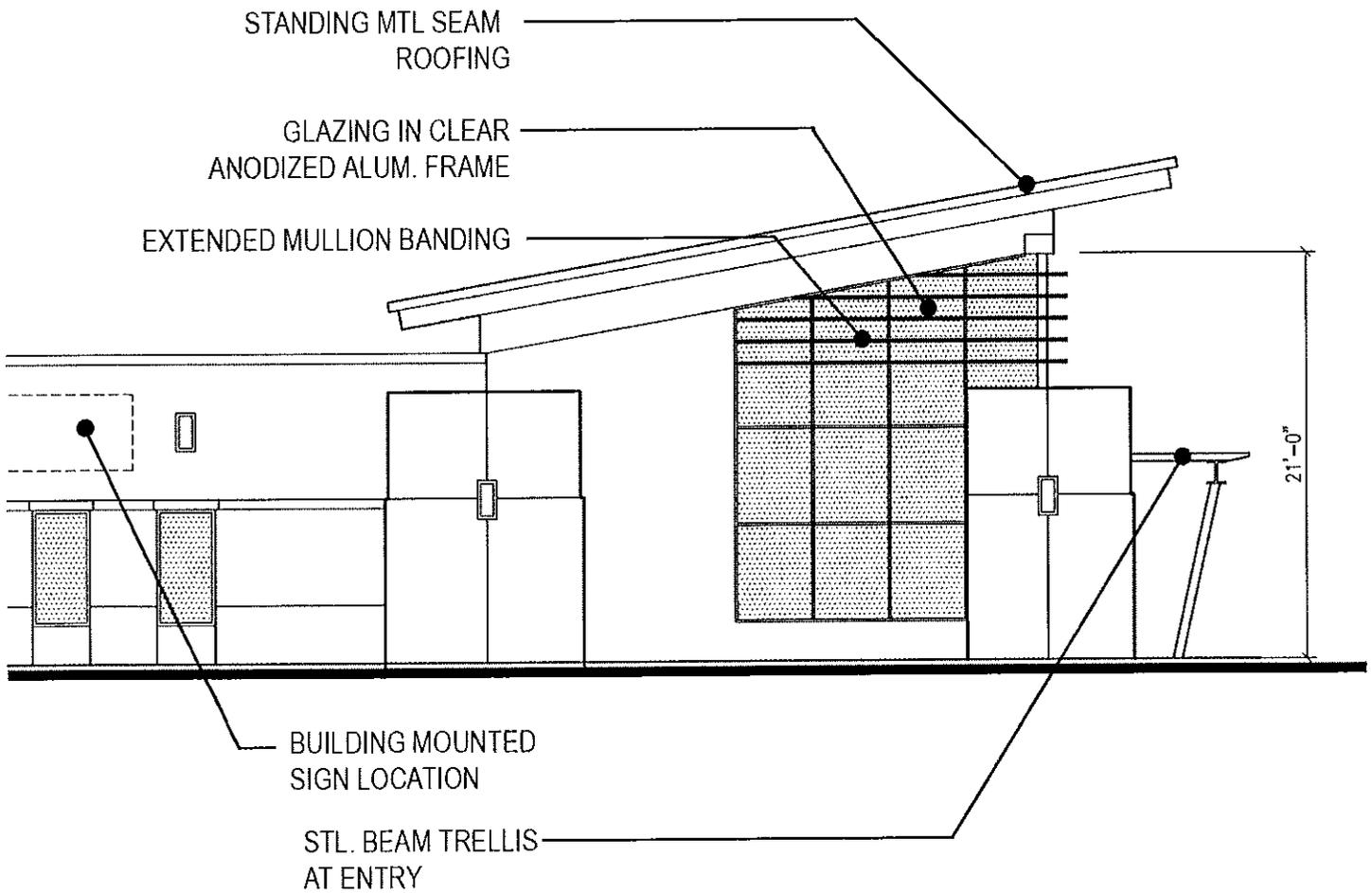
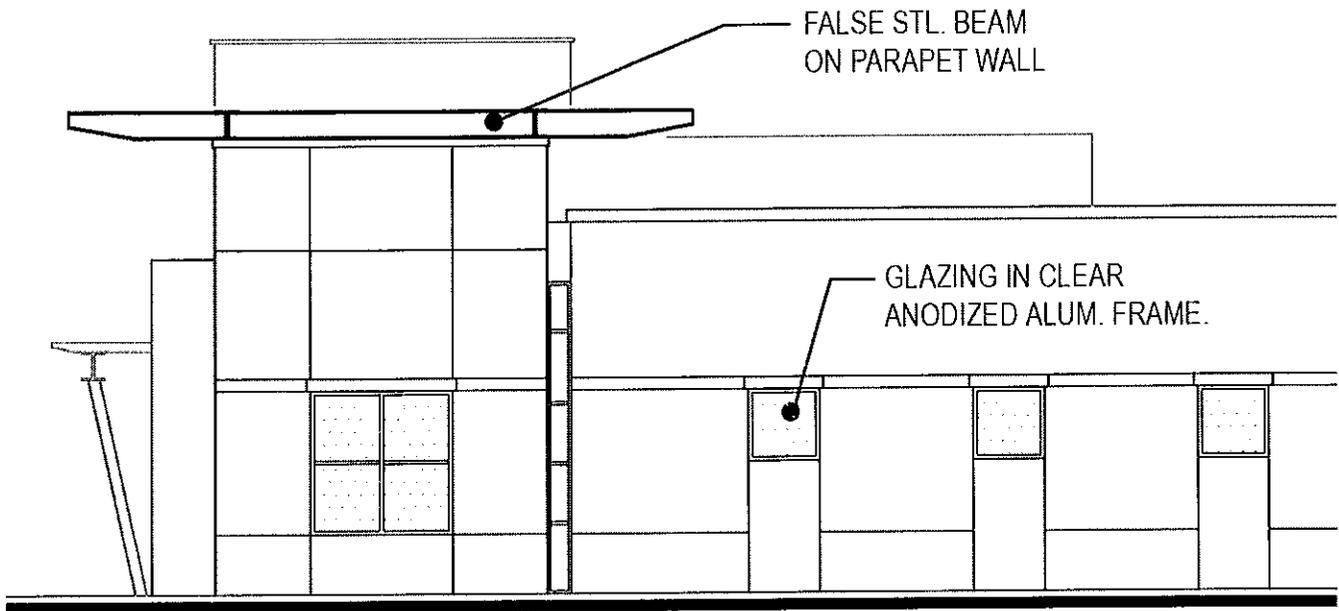
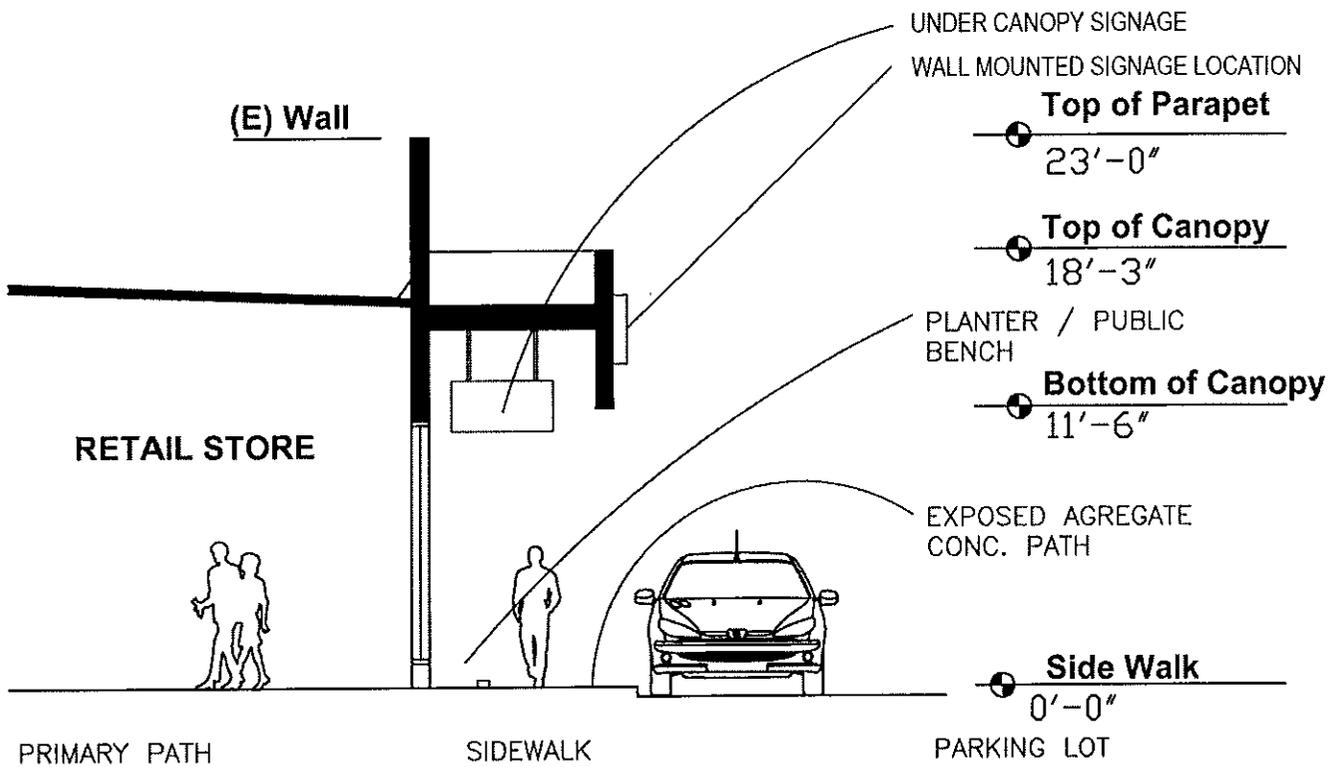


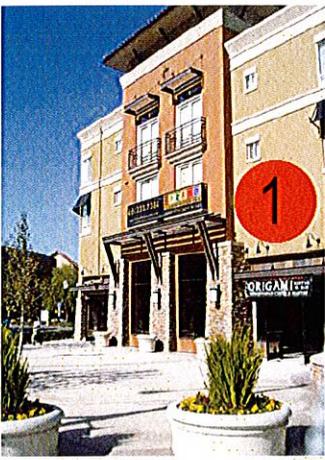
Exhibit 2.2
BUILDING PROTOTYPES



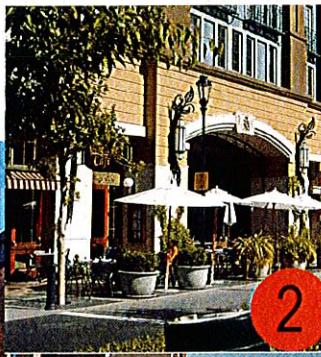
MALL AISLE PATH

NTS

Exhibit 2.3
TYPICAL MALL AISLE PATH



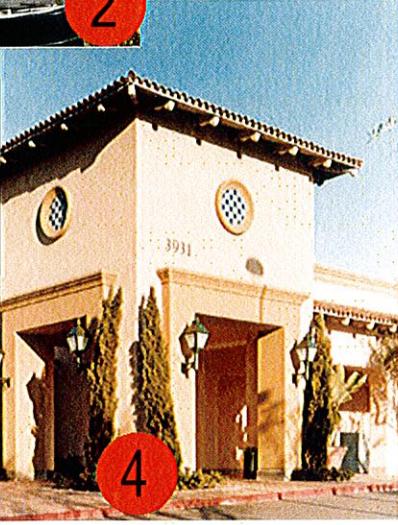
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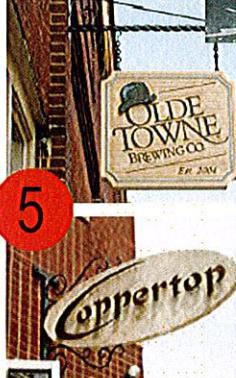
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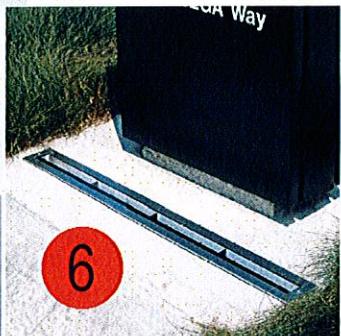
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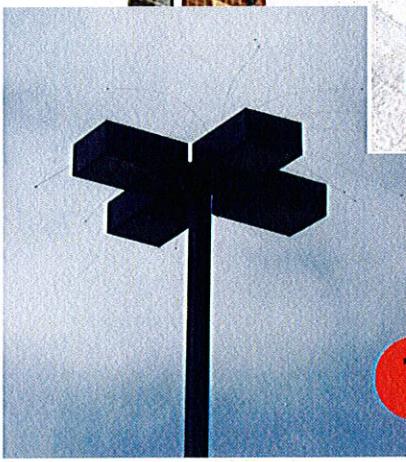
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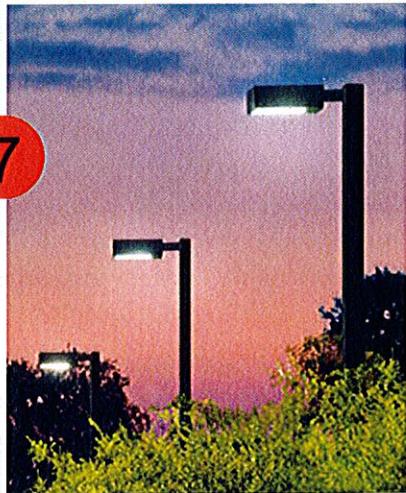
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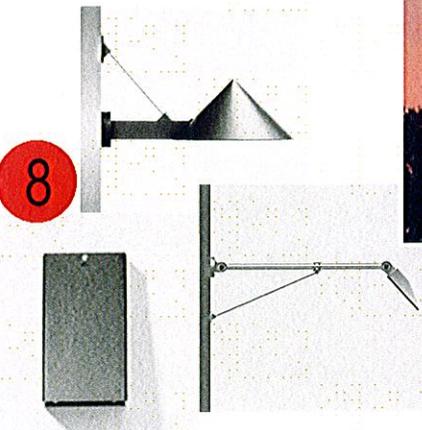
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7



8



DEFINITIONS, TERMS & DETAILS

1. Steel trellis feature with steel bracing. Wall mounted decorative light fixtures.
2. Special treatment and use of materials (stone, brick, splitface, slate tile, copper) for building roofs, entrees and signage.
3. Building forms to be emphasized in height, material or massing to identify the entry, patio or public spaces around or adjacent to the building.
4. Entry tower elements are to be used to create visual interest and establish a dominant feature on the building. Towers shall be detailed with proper proportions and appropriate materials to compliment the structure. This is to include a unique feature (i.e.- sculpture, clock, roof element, special material, etc. . . .).
5. Projecting signage should be sized appropriate for mall tenants that face the parking areas.
6. Monument Sign lighting shall be recessed within the concrete base or be a part of the sign elements.
7. Parking lot lighting shall be low profile and shall compliment the building design style.
8. Building wall mounted light fixtures and accent lighting are to be functional as a light source as well as an architectural feature of the building.

Exhibit 3.0
DEFINITION & TERMS

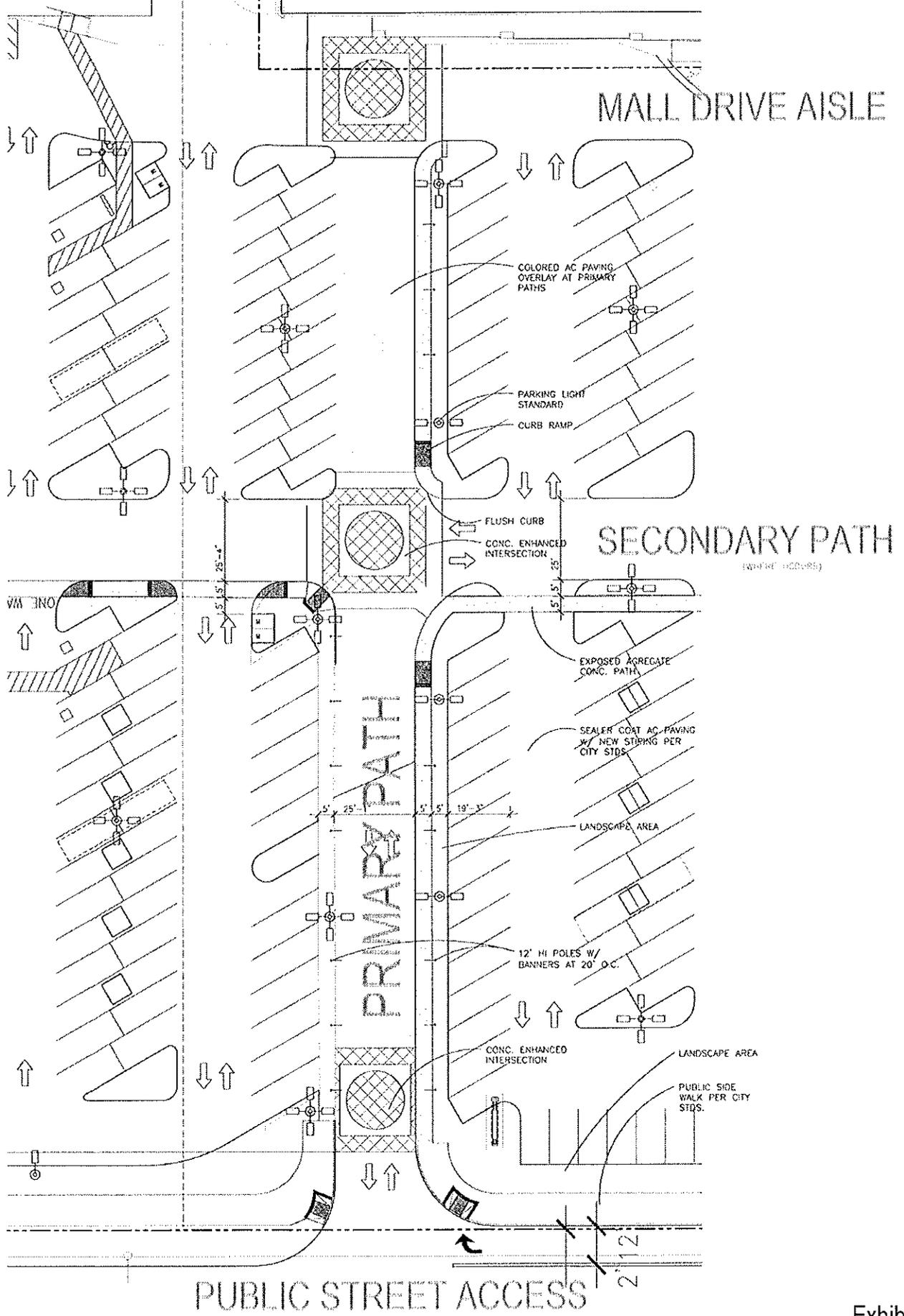


Exhibit 4.1
TYPICAL PRIMARY/SECONDARY ACCESS

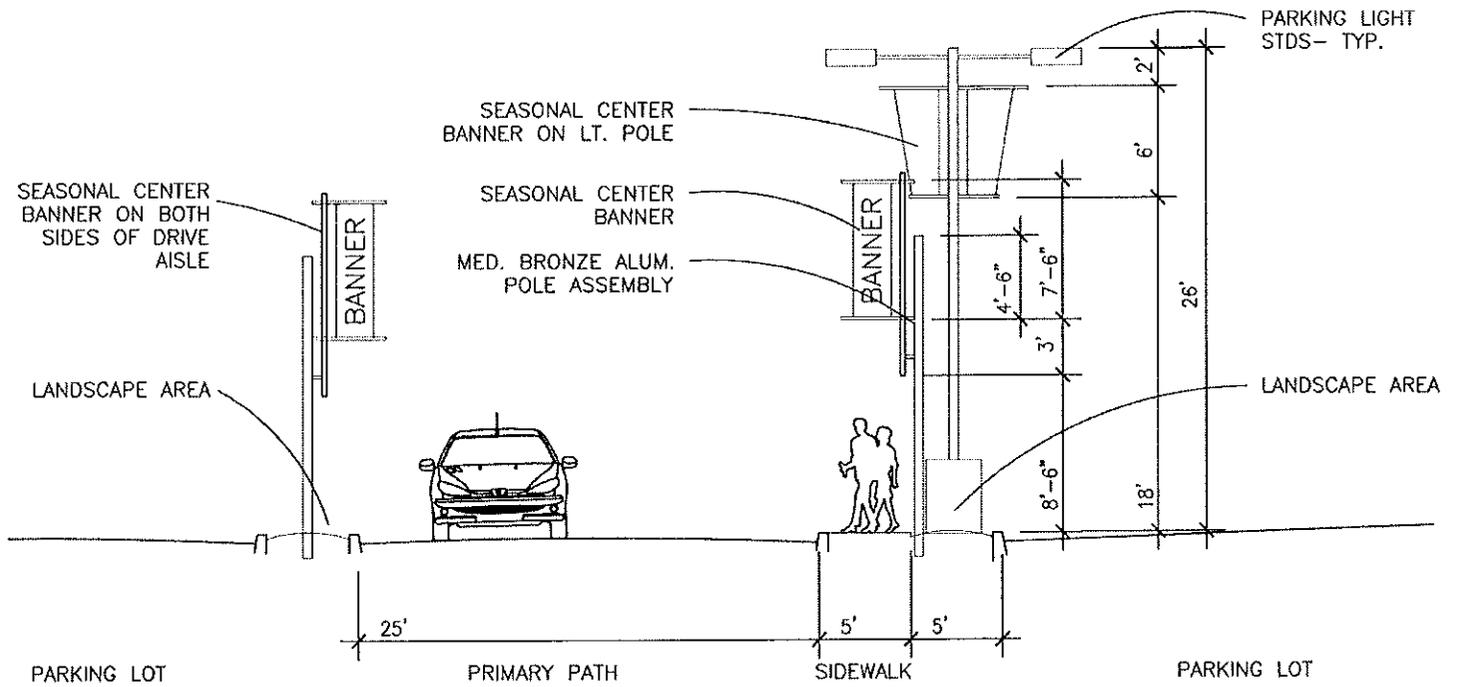


Exhibit 4.2
PRIMARY ENTRY PATH

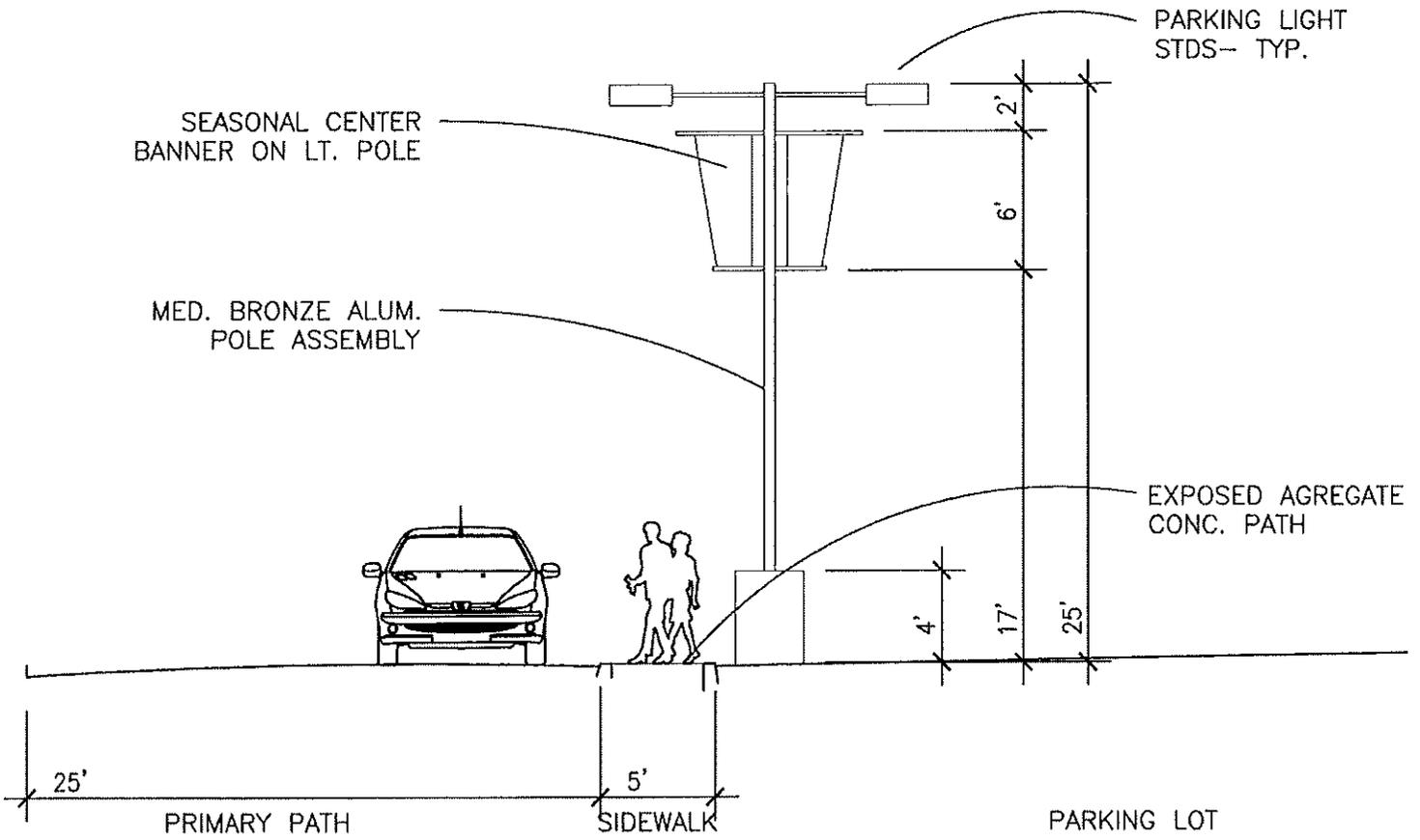


Exhibit 4.3
SECONDARY ACCESS PATH

CENTERPOINT MALL MASTER SIGN PROGRAM

I. PURPOSE AND INTENT

This Master Sign Program establishes requirements for the design and fabrication of all building and site signage at the Centerpoint Mall project sites, including primary identification signs, secondary identification signs and informational signs. Unique and high quality signage is an integral part of the design, image and success of Centerpoint Mall. The intent of this sign program is to ensure that tenant signage in the commercial center is designated and executed in a manner which will achieve these objectives while providing superior identification of tenants and tenant's business, including incorporation of corporate identification where consistent with this program.

II. COMPLIANCE

The City of Oxnard shall review, approve and issue building permits for all signage prior to fabrication and installation. The requirements of the Oxnard Sign Ordinance shall apply unless otherwise indicated below.

III. PROCESS

A. Required Signage- Each structure shall provide a minimum of one major identification sign per store frontage with public entrance. All signage shall conform to the sign sizes, overall allocations and sign location designated in this program.

B. City Review of Sign Design- Individual building owners shall provide the following information to the Planning Manager for use in reviewing the proposed signage and recommend approval or changes as required to conform to this sign guidelines. Review will be made in the context of the architectural features of the individual structures and how the proposed signage is compatible with said architectural style and features. Signs shall not detract from the aesthetic character of the architecture. Sign design submittal package shall include: full store name, logo images, dimensions and colors, and description or samples of interior material finishes.

C. Building Permit Submittals- After approval of the Sign Design by the Planning Manager, the Building Owner shall direct his Sign Contractor to prepare and submit the following to the City for building permit approval:
Required sets of complete and fully dimensioned shop drawings for all signs ("Shop Drawing") based on the approved Sign Design.

Colored elevation(s) drawings of the complete structure, delineating the proposed signage and its impact on the building; show all elevations where signage is proposed.
Three sets of samples of actual paint colors and finishes.
Full store name, logo images and color.

D. Fabrication and Installation- Only after building permits are issued for the approved design, Building Owner shall cause its Sign Contractor to fabricate and install signage in conformance with the Shop Drawings and other materials as approved by the Planning Division and the City. Planning Division shall have the right to perform an in-shop inspection of signage prior to installation.

Building Owner shall cause its signage to be installed so that it is operative on or before the date the proposed building (or portion of space therein) is required to open its store for business to the public.

All costs associated with fabrication and installation of signage, including the costs of all governmental permits, approvals and processing fees, shall be paid by the Building Owner.

IV. DESIGN GUIDELINES

A. Design Objectives-

1. The primary objective of the Design Guidelines is to generate high quality Tenant signage that reflects a sophisticated, contemporary environment. A coordinated sign program that provides for harmony with adjacent structures, yet allows for some individual diversity is encouraged.
2. It is also the objective of the Design Guidelines to limit the overall signage allocations and size of the individual sign letters to create an attractive and uncluttered streetscape.

- B. Acceptable Sign Treatments-** A coordinated approach where signage is composed of several different but related elements and lighting techniques is encouraged. The following treatments are considered appropriate:

- Dimensioned geometric shapes
- Painted metals
- Screens, grids or mesh
- Polished metals
- Cut or fabricated steel
- Wood
- Neon
- Under canopy signs
- Internal LED illumination of channel letters
- Tenant window signs
- Flags, banners for center uses
- Dimensional letter forms with seamless edge treatment

- a) *Acceptable Sign Letter Materials:* Acrylic, Wood, Aluminum, Fiberglass, Metal, Neon.
- b) *Unacceptable Sign Letter Materials:* Foam, Styrofoam, Vinyl, Plastic, Plastic Laminates, Cardboard, Paper.

C. **Prohibited Signs-** The following are prohibited:

- 1. Permanent advertising devices such as attraction boards, posters.
- 2. Exposed junction boxes, transformers, lamps, conduits
- 3. Sign Manufacturers names, stamps or decals
- 4. Vacuum formed type plastic letters
- 5. Paper, cardboard or Styrofoam signs
- 6. Exposed fastenings unless fastenings make an intentional statement
- 7. Simulated materials (i.e., wood grained plastic laminates, etc.)
- 8. Animated lights or other moving sign components
- 9. Conventional internally illuminated box/can type signs.
- 10. All other signs prohibited by the City Sign Ordinance
- 11. Flashing, moving signs
- 12. Box cabinet illuminated signage

a. **Lighting-** In keeping with the sophisticated character of the project, identity signs for Tenants should be illuminated using one of the following techniques:

- 1. Reverse channel neon
- 2. Open face channel neon
- 3. Silhouette illuminated
- 4. Individual illuminated letters
- 5. Front lighting
- 6. Area lighting

All front lighting should be obscured in channels where possible. Where fixtures, shades, or other elements are exposed, they should contribute to the design of the storefront. All exposed or skeletal neon must be backed with an opaque coating as described in acceptable sign treatments, unless otherwise approved by the Planning Division. All housing and tube supports for exposed neon signs must be painted out to match the building background immediately behind and adjacent to the sign. In no event shall sign lighting create a public nuisance or jeopardize public safety.

D. **Colors-** The following guidelines shall be adhered to in selecting colors for the Tenant signage.

- 1. Sign colors should be selected to provide sufficient contrast against building background colors.
- 2. Color within each sign should be harmoniously blended
- 3. Sign colors should be compatible with building background colors
- 4. Signage colors should be coordinated with other on-site signs
- 5. Color or letter returns should be painted dark when against light backgrounds
- 6. Interior or open channel letter should be painted dark when against light backgrounds
- 7. Neon colors should complement related signage elements
- 8. Corporate logos are allowed (franchise or national)

All sign colors are subject to review and approval by the Planning Division Manager as part of the sign submittal process.

- E. **Type Styles-** The use of logos and distinctive type styles is allowed for all signs. Building Owners may adapt established type styles, logos and/or images that are in use on similar buildings operated by them in California, provided that said images are architecturally compatible and coordinated with the sign program. Type case letters depending on sign area. No logo (emblem or symbol e.g. Texaco Star or Golden Arches) shall be more than 36 inches tall, except as otherwise stated in this document.

V. MERCHANT SIGNAGE AND ALLOWABLE SQUARE FOOTAGE

The following signage allocations are based upon City of Oxnard Sign Ordinance standards and are maximums that may not be exceeded. Tenant Signage locations are as shown on Elevations on Exhibits 2.1-2.3 and Site locations shown on Exhibits 5.1, 5.2 and 5.3. Allowable Signage for Tenant is as shown on Exhibits 6.1-6.3.

A. Individual Structures on Separate Parcels and Major Mall Buildings-

The total signage allocation shall not exceed two square feet of sign, for each lineal foot of storefront on the principal street (predominant business exposure). No single sign per elevation shall exceed 300 square feet or 10% of the building frontage. One major identification sign is required on the primary entry elevation. Sign square footage allowed by the primary frontage may be used on other elevations that do not face an arcade or mall.

Where there is a second or third building frontage and entrance facing a pedestrian arcade or mall, one additional ½ square foot of sign area per lineal foot of building frontage facing said arcade or mall is allowed. This signage shall be placed on the wall facing the arcade or mall driveway. No other signage is permitted on these elevations. (Exhibit 5.1).

Major Anchor Tenants may have over 300 square feet of signage per elevation, not to exceed 450 square feet (Exhibit 6.2.1).

Maximum letter height on the building attached letters shall not exceed 36 inches with exception for Major Anchor Tenants in frontages that face Saviers Road and/or 'C' Street maximum letter height shall not exceed 72 inches.

Maximum sign length should not exceed 70% of the tenant frontage.

B. Inline Mall Tenants-

For Exterior Inline Tenants the total signage allocation shall not exceed 2 square feet of sign, for each linear foot of storefront. One major identification sign is required on the primary elevation.

Where there is a second frontage facing an interior mall walkway, one additional ½ square foot of sign area per lineal foot of building frontage facing said arcade or mall is allowed to be used on that frontage only. This signage shall be placed on the wall facing an interior mall walkway.

Maximum letter height on the building attached letters shall not exceed 36 inches.

Interior Mall Tenants having no frontage on a street, may not have exterior wall mounted signage, but they are allowed ½ square foot of sign area per lineal foot of frontage in the interior of the mall where their entrance is located.

C. Other Tenant Signage

All tenants are allowed signage which includes one or more of the following: projecting sign or under canopy sign, storefront or window sign, and/or a space on the Mall Free-standing sign. The Area of these signs shall apply to allowable signage calculation for the Tenant.

Tenants are allowed to have one projecting sign per elevation frontage, limit of two, with a maximum area of 4.5 SF (Exhibit 2.1). One under canopy sign is permitted per tenant as long as the sign does not exceed 4 feet in length and 2 foot in height, with a maximum area of 8 SF (Exhibit 2.3). One window or storefront sign is allowed per tenant, in the entrance frontage, it shall be a maximum of 9 SF and shall be placed at a maximum height of 12 feet above finish floor. A space on the free-standing sign is allowed for a major tenant or as permitted by the landlord, with a maximum area of 4-6 SF per tenant, with landlord approval (Exhibit 5.6). All of the signs above can consist of logos and/or letters, except for the space on the free-standing sign which can only have letters.

Materials of this signage shall be consistent with the Design Guidelines of this master sign program.

Signs hung inside a building 24" or more away from an exterior window shall not count toward the allowable signage per tenant.

D. Master Plan – Non conforming signs

Existing permitted signs as of the date this master program is approved may remain. If a Tenant changes in a space with a non-conforming sign, the sign shall be removed and replaced with a sign that conforms to this Master Sign Program.

Tenants are not allowed to have ground signs. Existing tenant ground signs on site are to be removed at the time the tenant vacates or an addition is constructed to the Building.

VI. MALL IDENTIFICATION SIGNS

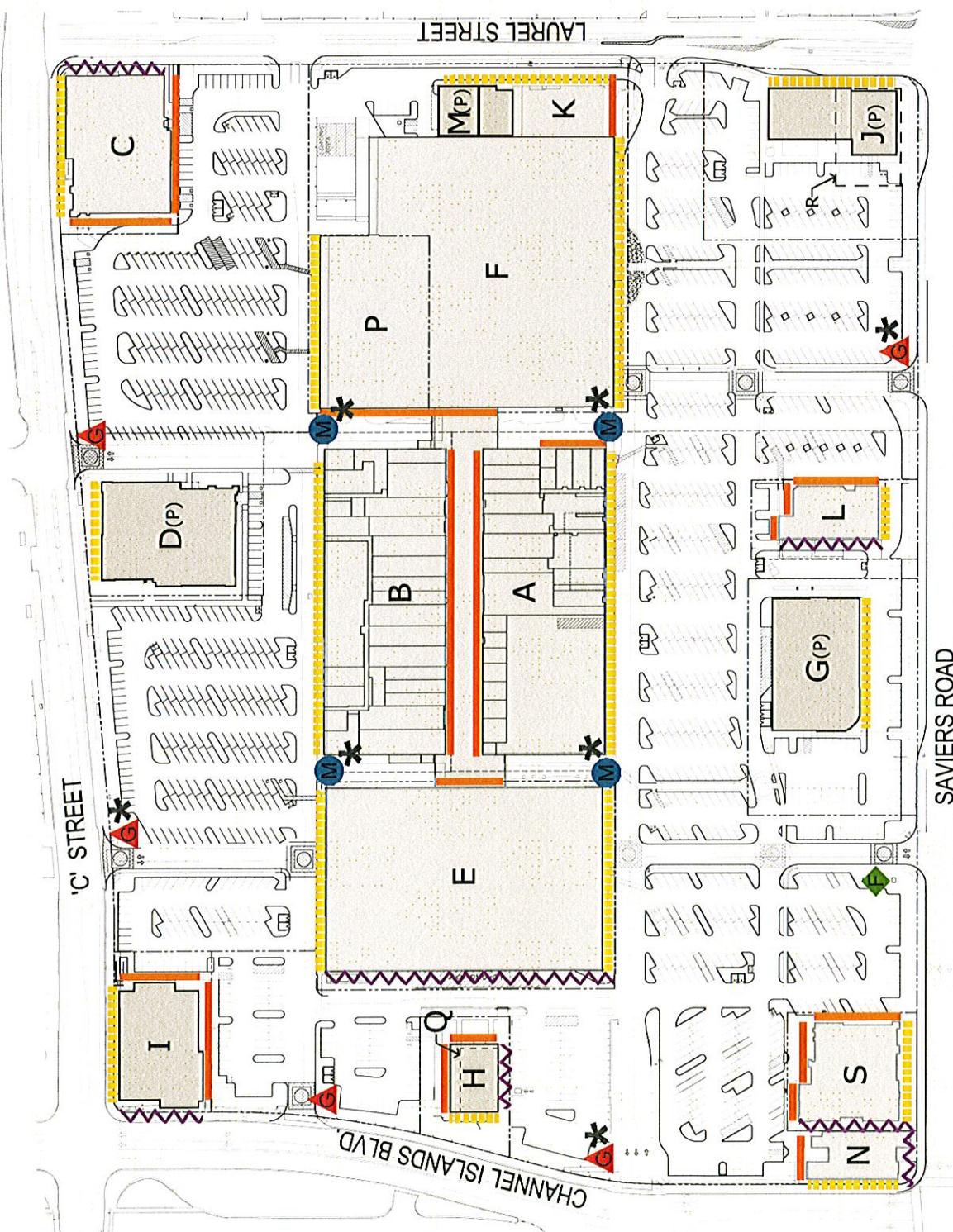
Required Sign Design- On-site Center signs shall be designated and built to the following specifications:

1. Ground Signs – Shall be located at entry points designated on Exhibit 5.1 and shall be placed in landscaped areas. These signs may be one or two sided. These signs shall not be more than 42 inches in height, except they may be up to 6 feet in height if the sign is located 35 feet or more from a curb at any street intersection or driveway. A ground sign shall not exceed 50 square feet in size single sided or 25 sf each side. Ground sign colors shall be coordinated with the architecture and colors of the mall. No ground sign shall be located within 150 feet of another ground sign. Exhibit 5.2 and 5.4 illustrates this sign style.
Note: Because there is an existing non-conforming tenant ground sign on Channel Islands Blvd. that belongs to Building Q, only one mall identification ground sign on that street will be permitted at this time. When existing Tenant on Building Q vacates or an addition is constructed to de Building, such non conforming ground sign shall be removed and a second mall identification ground sign on Channel Islands Blvd. will be approved.
2. Entry Marquee signs are to be placed within the four entry points of the mall facing the parking lots as shown on Exhibit 5.1. Signs are to be single sided with luminous letters. Structure shall have smooth metal finish see Exhibit 5.5 for design configuration.
3. Free-standing sign is to be located in a landscape area facing a Primary Street as shown on exhibit 5.1. Sign is to be double sided with luminous letters. Free-standing sign colors, except for individual tenant name/logo shall be coordinated with the architecture and colors of the mall. Sign Structure is composed of different finishes including Plaster, Stone Veneer and Aluminum. Exhibit 5.3 and 5.6 illustrates this sign style.
4. Materials. The following are considered appropriate for Mall Identification Signs: Masonry, ceramic tile, plaster stucco and aluminum accent bandings; which will incorporate aluminum, acrylic or metal letters.
5. Colors-cohesive design elements.
6. Location. Signs shall be located as identified in the typical elevations and the site plans, Exhibits 5.1, respectively. No signs or any part thereof shall be roof mounted.
7. All site signs are to be maintained.

VI. GENERAL PROVISIONS AND CONSTRUCTION REQUIREMENTS

- A. No temporary wall signs, window signs, pennants, flags, inflatable displays or sandwich boards will be allowed except those signs specifically approved by Landlord and allowed by the City of Oxnard.
- B. Notwithstanding the maximum square specified for a copy area allowances, signs and typography in all cases shall appear balanced and in scale within the context of the sign space and the building as a whole. All signs shall fit comfortably into designated architectural spaces, leaving sufficient margins and negative space on all side. Thickness, height and color of sign lettering shall be visually balanced and in portion to other signs of the building.
- C. Dimensional letters and plaques shall be affixed without visible means of attachment, unless attachments make an intentional design statement.
- D. All sign fabrication work shall be of excellent quality. All logo images and type styles shall be accurately reproduced. Lettering that approximates type styles shall not be acceptable. Planning Division reserves the right to reject any fabrication work deemed to be below standard.
- E. Signs must be made of durable rust-inhibited materials that are appropriate and complementary to the building.
- F. All ferrous and nonferrous metals shall be separated with nonconductive gaskets to prevent electrolysis. In addition to gaskets, stainless steel fasteners shall be used to secure ferrous to nonferrous metals.
- G. Threaded rods or anchor bolts shall be used to mount sign letters which are spaced out from background panel. Angle clips attached to letter sides will not be permitted.
- H. Paint colors and finishes must be reviewed and approved by Landlord and City. Color coating shall exactly match the colors specified on the approved plans.
- I. Surfaces with color mixes and hues prone to fading (e.g., pastels, fluorescent, complex mixtures, and intense reds, yellows and purples) shall be coated with ultraviolet inhibitive clear coat in a semi-gloss finish.
- J. Joining of materials (e.g., seams) shall be finished in such a way to be unnoticeable. Visible welds shall be ground smooth and finished in such a way to be unnoticeable. Visible welds shall be ground smooth and finished with auto body filler. Rivets, crews, and other fasteners that extend to visible surfaces shall be flush, filled so to be unnoticeable.
- K. Finished surfaces of metal shall be free from oil and warping. All sign finishes shall be free of dust, orange peel, drips and runs and shall have uniform surface conforming to the highest standards of the industry.
- L. Channel letters shall have a maximum return depth of 5 inches. Return finish shall have a contrasting color from the face color. Double tube neon shall be used where letter stroke exceeds 30 inches. Letters installed on raceways will not be permitted unless specifically approved by the Planning Division.
- M. Depth of open channel letters shall be 2 inches. All hardware and neon tube supports inside open channel letters shall be painted to match interior letter color. Neon shall be sufficient to make letters read and shall be installed so that the top surface of neon is flush with front with edges of open channel.

- N. Brightness of signs is subject to approval by the Planning Division. Surfaces brightness of illuminated materials shall be consistent in all letters and components of the signs. Light leaks will not be permitted.
- O. All conduit, raceways, crossovers, ballast boxes, transformers and other equipment necessary for sign connection shall be concealed. All bolts, fastenings and clips shall be finished with a rust inhibitive finish.
- P. Underwriter's Laboratory approved labels shall be affixed to all electrical fixtures. Fabrications and installation of electrical signs shall comply with all national and local building code,
- Q. Penetrations into building walls, where required, shall be made waterproof. Location of all openings for conduit sleeves and support in sign panels and building walls shall be indicated by the Sign Contractor on the Shop Drawings submitted to the Planning Division. The Sign Contractor shall install same in accordance with the approved Shop Drawings.
- R. In no case shall any manufacturer's label be visible from the street from normal angles.



SITE LEGEND

-  GROUND SIGN LOCATION
-  MALL ENTRY MARQUEE SIGN LOCATION
-  MAIN MALL FREESTANDING SIGN
-  PRIMARY TENANT FRONTAGE
2 SF X LINEAL FRONTAGE
-  SECONDARY TENANT FRONTAGE
1/2 SF X LINEAL FRONTAGE
-  AREAS WHERE SF FROM PRIMARY
FRONTAGE MAY ALSO BE USED
-  * APPROVED UNDER SEPARATE PERMIT
-  (P) PROPOSED BUILDINGS

MASTER SITE PLAN



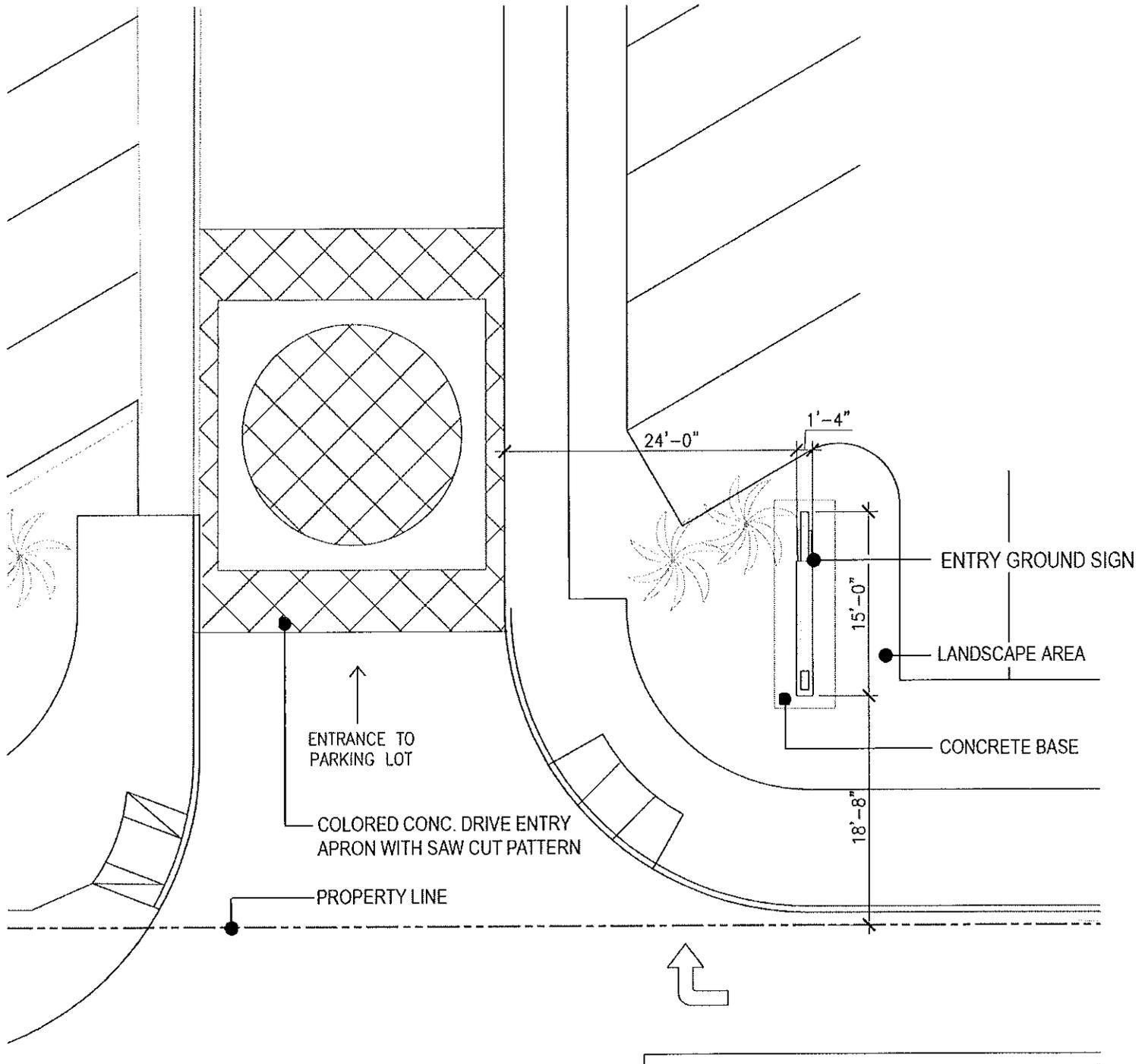


Exhibit 5.2
GROUND SIGN TYP. LOCATION

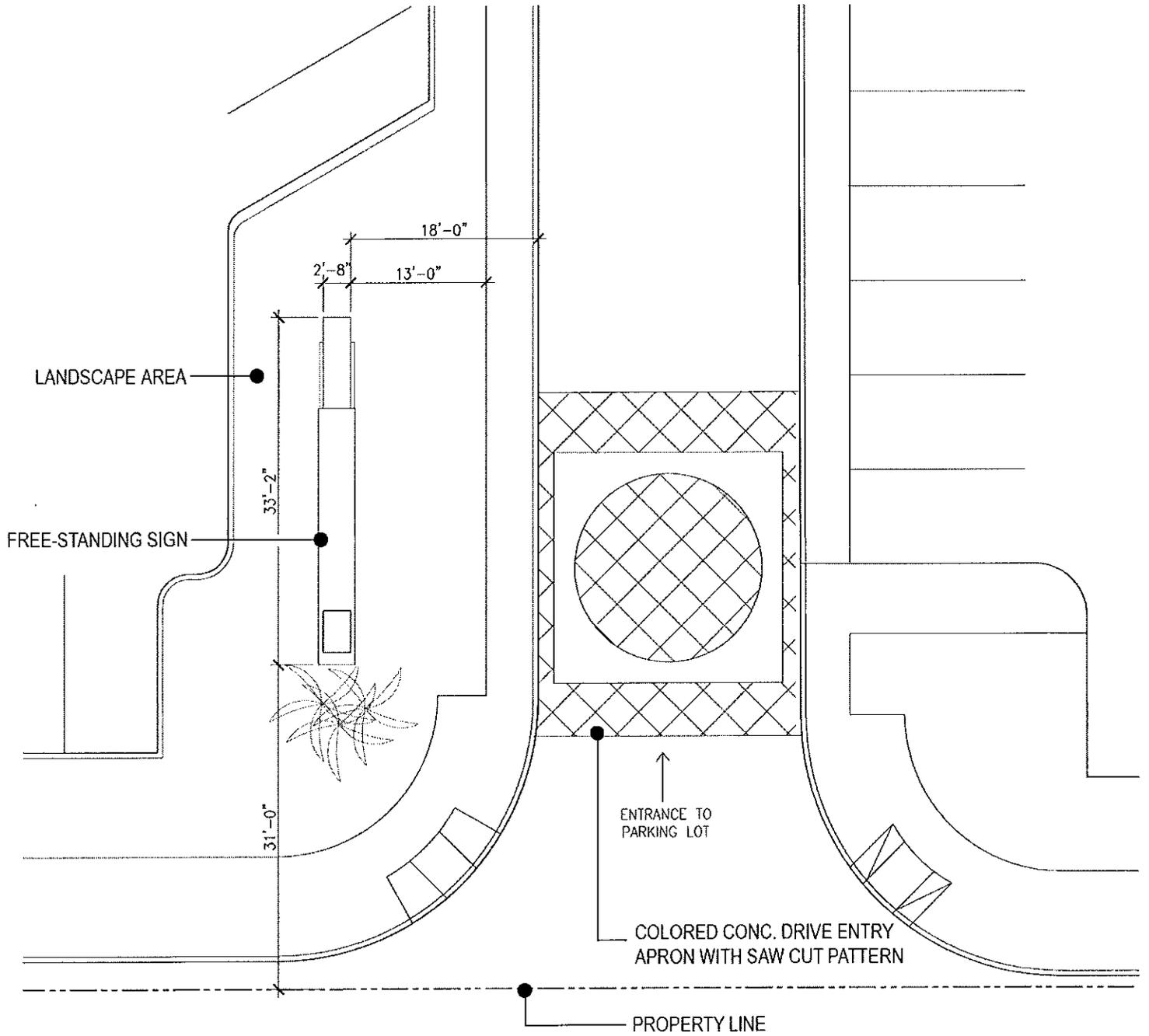
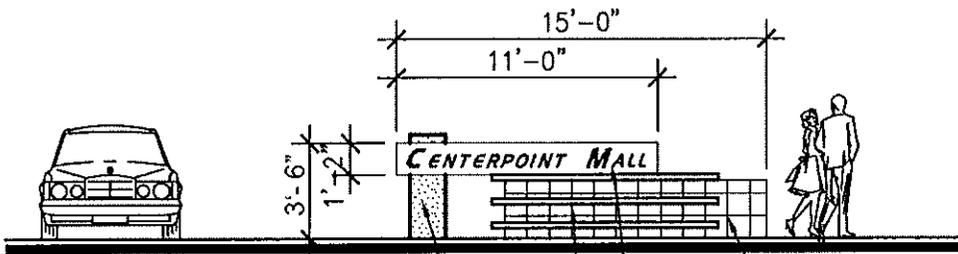
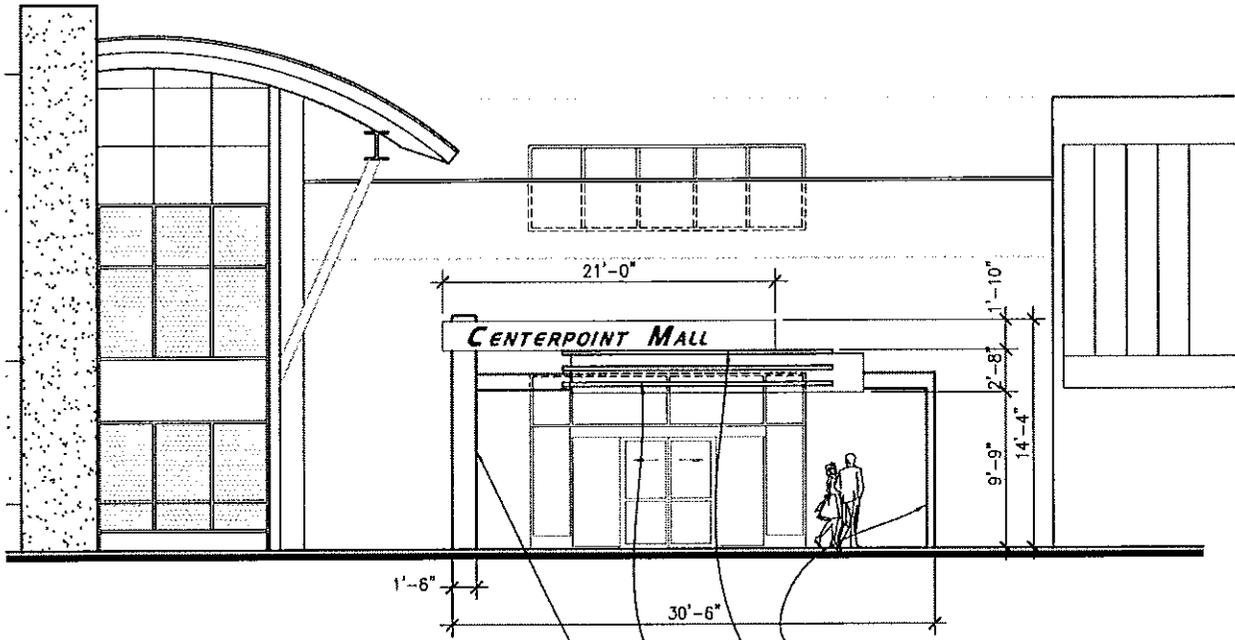


Exhibit 5.3
FREE-STANDING SIGN LOCATION



- STONE VENNER FINISH
- BACK LIT LETTERS 9" HI.
FONT STYLE – AF-PEPSI. 6.5 SF.
AT EACH SIDE
- 3" SQ ALUM. TUBE RAILS
- EXT. PLASTER FINISH COLUMN

Exhibit 5.4
GROUND SIGN - ELEVATION



1'-6"

30'-6"

21'-0"

1'-10"

2'-8"

9'-9"

14'-4"

CENTERPOINT MALL

- EXISTING STL. FRAME
- INDIVIDUAL BACK LIT 16" HI. ALUM. LETTERS. FONT STYLE - AF-PEPSI. 21 SF. ON ONE SIDE
- 3" SQ. ALUM. TUBE RAILS
- 18" SQ. MTL. PANEL COLUMN.

Exhibit 5.5
MALL ENTRY MARQUEE SIGN - ELEVATION

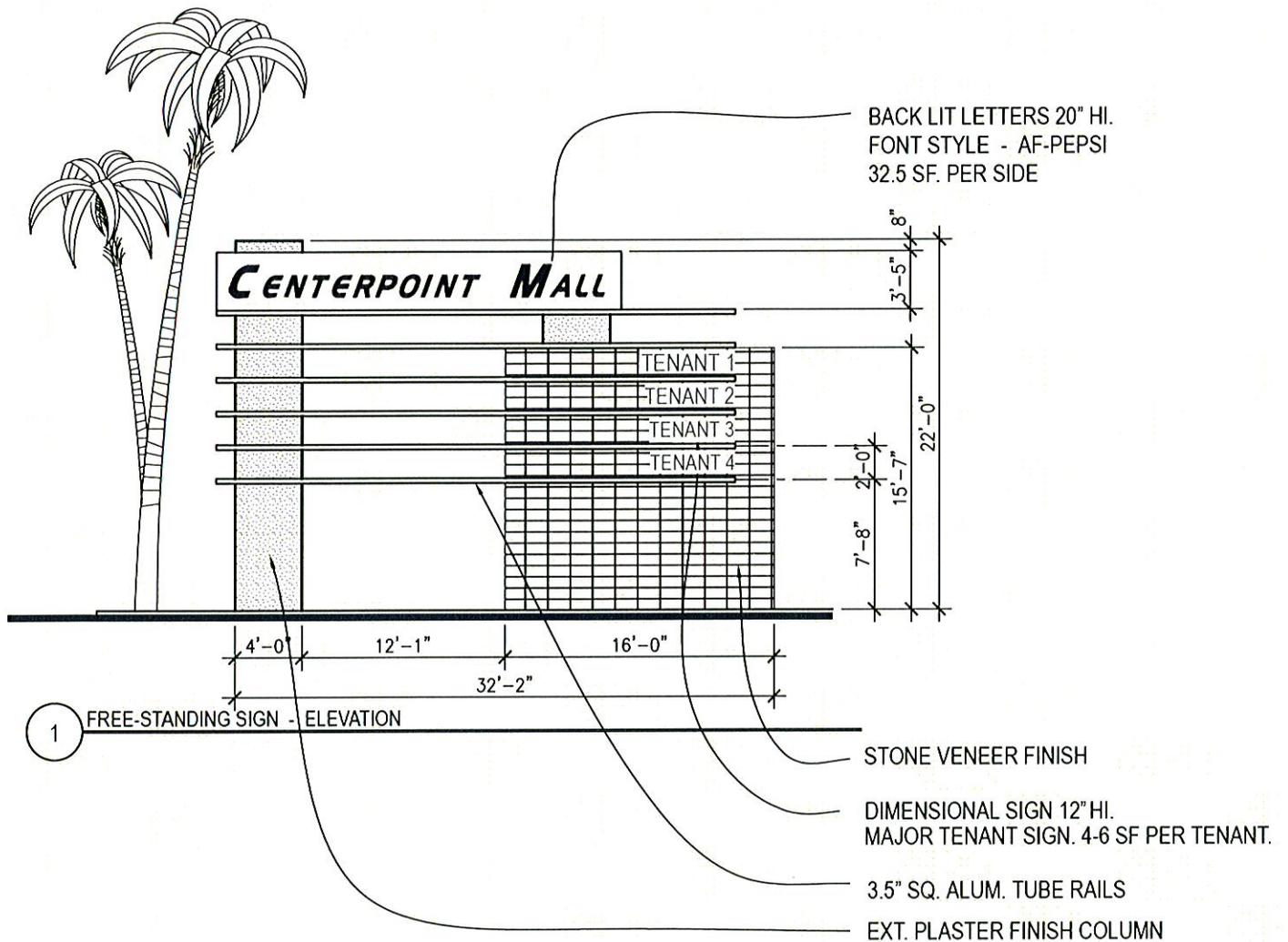
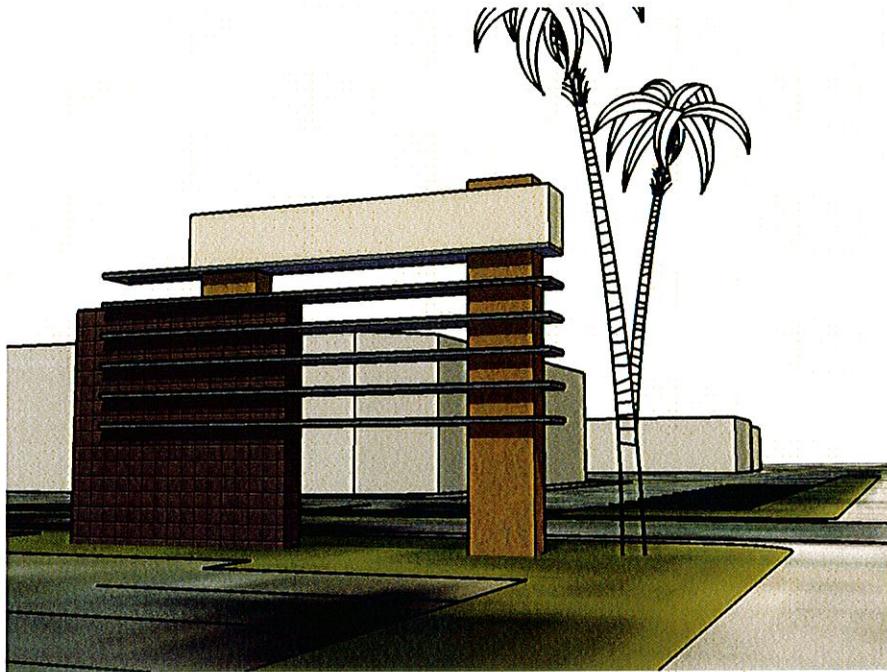
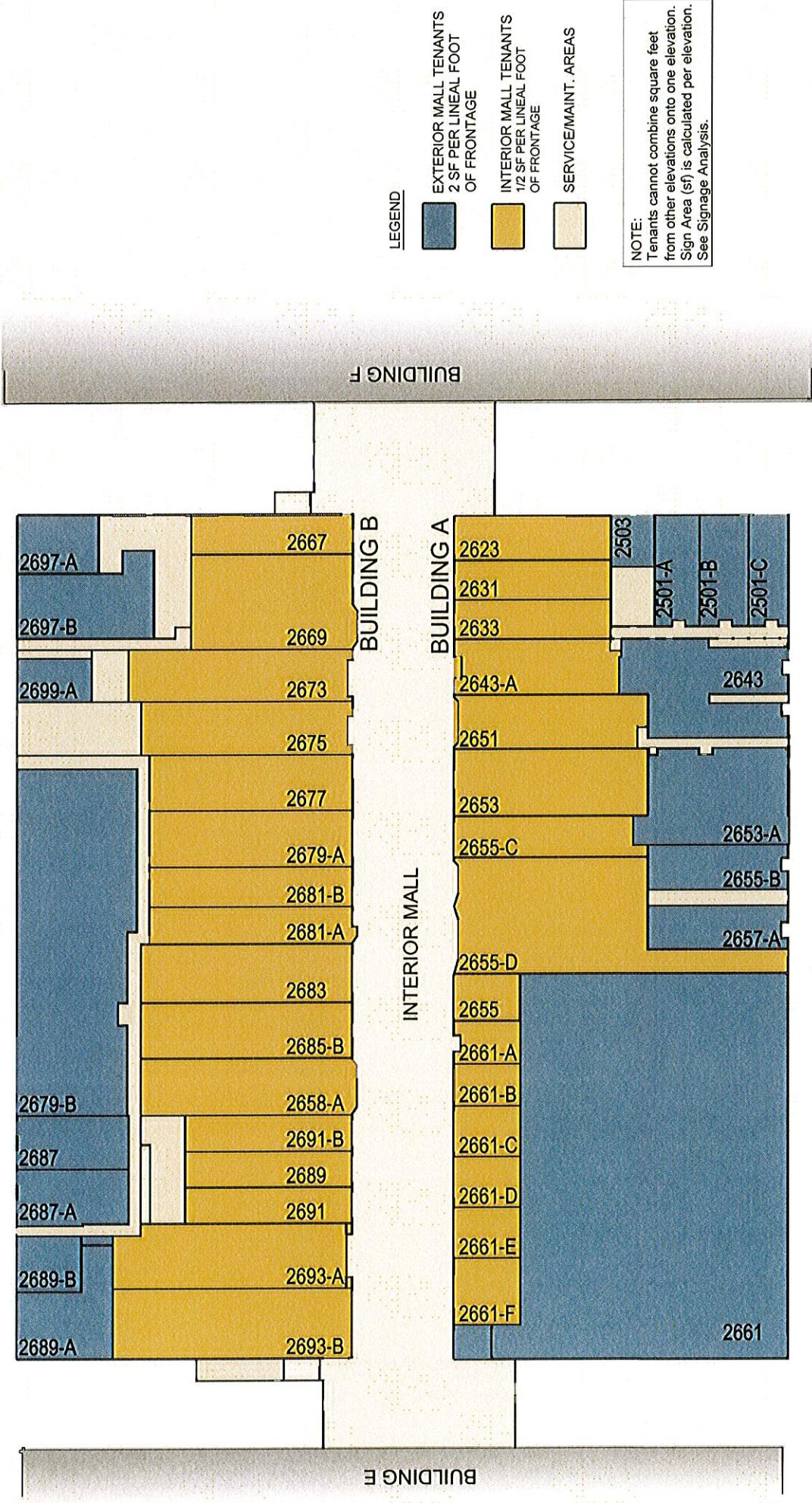
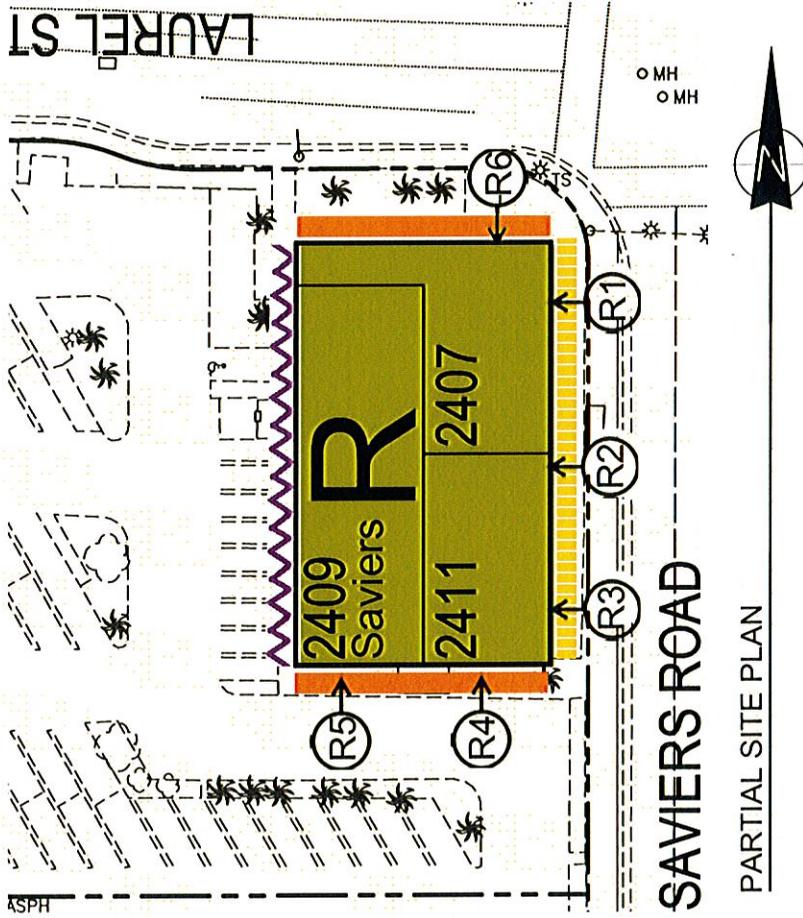


Exhibit 5.6
FREE-STANDING SIGN - ELEVATION

Jan. 30, 2009



MALL TENANT PLAN



SITE LEGEND

-  PRIMARY TENANT FRONTAGE
2 SF X LINEAL FRONTAGE
-  SECONDARY TENANT FRONTAGE
1/2 SF X LINEAL FRONTAGE
-  AREAS WHERE SF FROM PRIMARY
FRONTAGE MAY ALSO BE USED

 EXISTING BUILDING

 SIGNAGE LOCATION
Note: Exact locations of signs to be determined & approved by city officials and Landlord.

NOTE:
Tenants cannot combine square feet from other elevations onto one elevation. Sign Area (sf) is calculated per elevation. See Signage Analysis.

MALL CENTER SIGNAGE

ADDRESS	TYPE OF SIGN	NUMBER	MAX. SIGN AREA ALLOWED (Per side)	No. SIDES PER SIGN	TOTAL SIGN AREA	TOTAL SIGN AREA PER STREET	PRIMARY SIGN LOCATIONS
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PROPOSED SIGNAGE

Saviers Rd.	Ground Sign	x 1	6.50 Sq.Ft.	2	13.00 SF	168.00 SF	PER EXHIBIT 5.1
	Free-standing Sign	x 1	56.50 Sq.Ft.	2	113.00 SF		
	Marquee sign	x 2	21.00 Sq.Ft.	1	42.00 SF		

Channel Islands	Ground Sign	x 2	6.50 Sq.Ft.	2	26.00 SF	26.00 SF	PER EXHIBIT 5.1
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C' Street	Ground Sign	x 2	6.50 Sq.Ft.	2	26.00 SF	68.00 SF	PER EXHIBIT 5.1
	Marquee Sign	x 2	21.00 Sq.Ft.	1	42.00 SF		

TOTAL MALL SIGNAGE	262.00 SF
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TOTAL SIGNAGE AREA FOR MALL IDENTIFICATION ALLOWED (SUBTRACTED FROM BUILDINGS PER EXHIBITS 6.2.1, 6.2.2 AND 6.3)	
EXISTING BUILDINGS	419 SF
PROPOSED BUILDINGS AND ADDITIONS	62 SF
EXTERIOR MALL TENANTS	50.5 SF
TOTAL	531.5 SF

Exhibit 6.1
SIGNAGE ANALYSIS - CENTER

TENANT ADDRESS	LINEAR STORE FRONT	FACTOR	MAX. SIGN AREA ALLOWED (PER TENANT PER ELEVATION)	TOTAL TENANT SIGN AREA	TOTAL NUMBER OF SIGNS ALLOWED ONE PER ELEVATION	MALL IDENTIFICATION SIGNAGE (SUBTRACTED FROM BUILDINGS)
EXISTING BUILDINGS						
2831 Saviers Building N	SOUTH/EAST	131.00	x 2	262.00 Sq.Ft.	291.00 SF	Three (3)
	WEST	58.00	x 0.5	29.00 Sq.Ft.		
Building S Multi Tenant	NORTH	101.00	x 0.5	50.50 Sq.Ft.	344.50 SF	1 Per Tenant per Frontage
	EAST	117.00	x 2	234.00 Sq.Ft.		
	WEST	120.00	x 0.5	60.00 Sq.Ft.		
2475 Saviers Building L	EAST/SOUTH	66.00	x 2	132.00 Sq.Ft.	218.50 SF	Three (3)
	WEST	66.00	x 0.5	33.00 Sq.Ft.		
	NORTH	107.00	x 0.5	53.50 Sq.Ft.		
2701 Saviers Building E Major Tenant	EAST/SOUTH	224.00	x 2	448.00 Sq.Ft.*	896.00 SF	Three (3) *Note: Maximum allowable sign area for East Elevation is 450 SF, because of 400 LF setback from street. Maximum allowable sign area for East Elevation is 400 SF, because of 300 LF setback from street, maximum shall not be exceeded. The remaining Square Footage will be added to Mall Center Signage calculation.
	WEST/SOUTH	224.00	x 2	448.00 Sq.Ft.*		
2401 Saviers Building F Major Tenant	EAST	329.50	x 2	659.00 Sq.Ft.*	659.00 SF	Max Eight (8) on East Side (Due to existing sign permits) *Note: East Elevation shall not exceed maximum allowable sign area per elevation of 450 SF because of 400 LF setback from street. The remaining Square Footage will be added to Mall Center Signage calculation. 209 SF
150 Laurel Building K **	EAST	62.50	x 0.5	31.25 Sq.Ft.	289.25 SF	Two (2)
	NORTH	129.00	x 2	258.00 Sq.Ft.		
2500 'C' Street Building P **	WEST	210.00	x 2	420.00 Sq.Ft.*	489.50 SF	Two (2) *Note: West Elevation exceeds maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation. 120 SF
	SOUTH	139.00	x 0.5	69.50 Sq.Ft.		
2400 'C' Street Building C	SOUTH	124.00	x 0.5	62.00 Sq.Ft.	482.00 SF	Three (3) *Note: West Elevation shall not exceed maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation. 36 SF
	EAST	168.00	x 0.5	84.00 Sq.Ft.		
	WEST/NORTH	168.00	x 2	336.00 Sq.Ft.*		
BUILDING H**	SOUTH/EAST	41.00	x 2	82.00 Sq.Ft.	144.50 SF	Three (3)
	WEST	84.00	0.5	42.00 Sq.Ft.		
	NORTH	41.00	x 0.5	20.50 Sq.Ft.		
BUILDING I	NORTH	105.00	x 0.5	52.50 Sq.Ft.	435.00 SF	Three (3) *Note: West Elevation shall not exceed maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation. 6 SF
	WEST/SOUTH	153.00	x 2	306.00 Sq.Ft.*		
	EAST	153.00	x 0.5	76.50 Sq.Ft.		

**NOTE: AN ADDITION TO THIS BUILDING IS PROPOSED AS PART OF THIS MASTER PLAN. SEE SIGNAGE ANALYSIS FOR PROPOSED ADDITIONS IF ADDITION OCCURS.

TOTAL ALLOWABLE SIGN AREA FOR MALL CENTER
419 SF

Exhibit 6.2.1
SIGNAGE ANALYSIS - SITE BLDGS

TENANT ADDRESS	LINEAR STORE FRONT	FACTOR	MAX. SIGN AREA ALLOWED (PER TENANT PER ELEVATION)	TOTAL TENANT SIGN AREA	TOTAL NUMBER OF SIGNS ALLOWED ONE PER ELEVATION	MALL IDENTIFICATION SIGNAGE (SUBTRACTED FROM BUILDINGS)
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PROPOSED BUILDINGS***

BUILDING D	WEST 121.50	x 2	243.00 Sq.Ft.	243.00 SF	To be determined	To be determined
BUILDING G	EAST 163.50	x 2	327.00 Sq.Ft.	327.00 SF	To be determined	To be determined
BUILDING J Multi Tenant	NORTH 160.00	x 2	320.00 Sq.Ft.	320.00 SF	To be determined	To be determined

PROPOSED ADDITIONS

150 Laurel Building K **	EAST 62.50	x 0.5	31.25 Sq.Ft.	373.25 SF	Two (2) *Note: North Elevation shall not exceed maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation.	42 SF
	NORTH 171.00	x 2	342.00 Sq.Ft.*			
2500 'C' Street Building P **	WEST 210.00	x 2	420.00 Sq.Ft.*	489.50 SF	Two (2) *Note: West Elevation exceeds maximum allowable sign area per elevation of 400 SF because of 300 LF setback from street. The remaining Square Footage will be added to Mall Center Signage calculation.	20 SF
	SOUTH 139.00	x 0.5	69.50 Sq.Ft.			
BUILDING H**	SOUTH/EAST 60.00	x 2	120.00 Sq.Ft.	192.00 SF	Three (3)	
	WEST 84.00	0.5	42.00 Sq.Ft.			
	NORTH 60.00	x 0.5	30.00 Sq.Ft.			
***SIGNAGE AMOUNTS HAVE BEEN CALCULATED ONLY FOR THE PRIMARY FRONTAGE. DURING THE DDR APPROVAL THESE NUMBERS SHALL BE AMENDED TO REFLECT ACTUAL BUILDING CONDITIONS.						TOTAL ALLOWABLE SIGN AREA FOR MALL CENTER 62 SF

TENANT ADDRESS	LINEAR STORE FRONT	FACTOR	MAX. SIGN AREA ALLOWED (PER TENANT PER ELEVATION)	TOTAL TENANT SIGN AREA	PRIMARY SIGN LOCATIONS	TOTAL NUMBER OF SIGNS ALLOWED ONE PER ELEVATION
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EXISTING BUILDINGS TO BE REMOVED

BUILDING R 2411 Saviers	SOUTH 35.50	x 0.5	17.75 Sq.Ft.	95.75 SF	R4 R3	Two (2)
	EAST/WEST 39.00	x 2	78.00 Sq.Ft.			
BUILDING R 2407 Saviers	NORTH 71.00	x 0.5	35.50 Sq.Ft.	113.50 SF	R6 R1	Two (2)
	EAST/WEST 39.00	x 2	78.00 Sq.Ft.			
BUILDING R 2409 Saviers	EAST/WEST 39.00	x 2	78.00 Sq.Ft.	95.75 SF	R2 R5	Two (2)
	SOUTH 35.50	x 0.5	17.75 Sq.Ft.			

Exhibit 6.2.2
SIGNAGE ANALYSIS - SITE BLDGS

MALL TENANTS

TENANT ADDRESS	LINEAR STORE FRONT (MEASURED IN FEET)	FACTOR	MAX. SIGN AREA ALLOWED (PER TENANT PER ELEVATION)	TOTAL TENANT SIGN AREA	TOTAL NO.OF SIGNS ALLOWED (ONE PER ELEVATION)	MALL IDENTIFICATION SIGNAGE (SUBTRACTED FROM BUILDINGS)
EXTERIOR TENANTS - BUILDING A						
2661	EAST 171.25	x 2	342.50 Sq.Ft.*	342.50 SF	One (1) *Note: East Elevation exceeds maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation	42.5 SF
2657-A	EAST 19.83	x 2	39.66 Sq.Ft.	39.66 SF	One (1)	
2655-B	EAST 19.83	x 2	39.66 Sq.Ft.	39.66 SF	One (1)	
2653-A	EAST 43.00	x 2	86.00 Sq.Ft.	86.00 SF	One (1)	
2643	EAST 40.50	x 2	81.00 Sq.Ft.	81.00 SF	One (1)	
2501-C	EAST 49.75	x 2	99.50 Sq.Ft.	108.50 SF	Two (2)	
	NORTH 18.00	x 0.5	9.00 Sq.Ft.			
2501-B	NORTH 22.00	x 0.5	11.00 Sq.Ft.	11.00 SF	One (1)	
2501-A	NORTH 20.00	x 0.5	10.00 Sq.Ft.	10.00 SF	One (1)	
2503	NORTH 20.00	x 0.5	10.00 Sq.Ft.	10.00 SF	One (1)	
EXTERIOR TENANTS - BUILDING B						
2697-A	WEST 26.00	x 2	52.00 Sq.Ft.	52.00 SF	One (1)	
2697-B	WEST 29.00	x 2	58.00 Sq.Ft.	58.00 SF	One (1)	
2699-A	WEST 23.50	x 2	47.00 Sq.Ft.	47.00 SF	One (1)	
2679-B	WEST 154.00	x 2	308.00 Sq.Ft.*	308.00 SF	One (1) *Note: West Elevation exceeds maximum allowable sign area per elevation of 300 SF. The remaining Square Footage will be added to Mall Center Signage calculation	8 SF
2687	WEST 23.50	x 2	47.00 Sq.Ft.	47.00 SF	One (1)	
2687-A	WEST 25.00	x 2	50.00 Sq.Ft.	50.00 SF	One (1)	
2689-B	WEST 25.00	x 2	50.00 Sq.Ft.	50.00 SF	One (1)	
2689-A	WEST 30.00	x 2	60.00 Sq.Ft.	60.00 SF	One (1)	
						TOTAL ALLOWABLE SIGN AREA FOR MALL CENTER 50.5 SF

Exhibit 6.3.1
SIGNAGE ANALYSIS - MALL TENANTS

MALL TENANTS

TENANT ADDRESS	LINEAR STORE FRONT (MEASURED IN FEET)	FACTOR	MAX. SIGN AREA ALLOWED (PER TENANT PER ELEVATION)	TOTAL TENANT SIGN AREA	TOTAL NO.OF SIGNS ALLOWED ONE PER ELEVATION
INTERIOR TENANTS - BUILDING B					
2667	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2669	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2673	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2675	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2677	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2679-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2681-B	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2681-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2683	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2685-B	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2685-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2691-B	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2689	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2691	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2693-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2693-B	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
TOTAL FRONTAGE	373	x 0.5		186.5 SF	1 PER TENANT PER FRONTAGE

INTERIOR TENANTS - BUILDING A

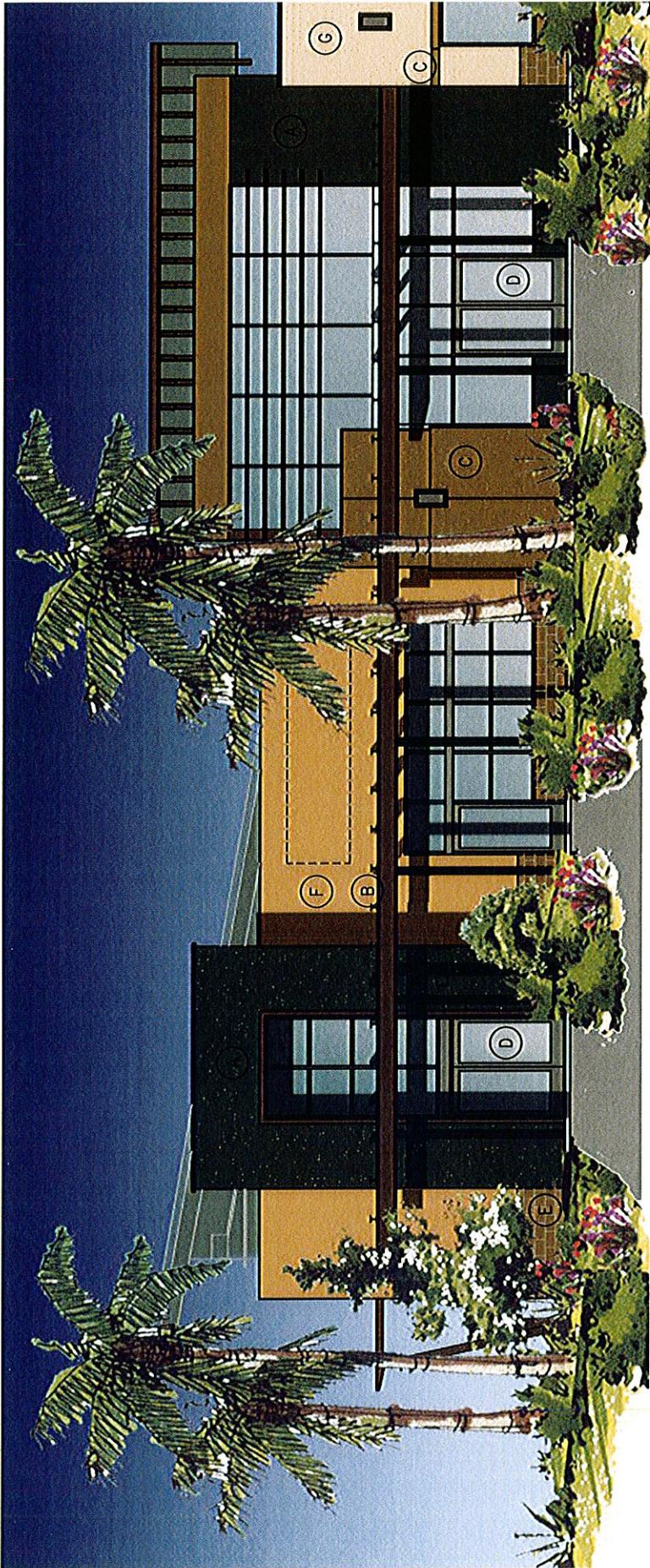
2623	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2631	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2633	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2643-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2651	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2653	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2655-C	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2655-D	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2655	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-A	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-B	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-C	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-D	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-E	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
2661-F	INT. MALL TENANT FRONTAGE	x 0.5	VARIES		
TOTAL FRONTAGE	373	x 0.5		186.5 SF	1 PER TENANT PER FRONTAGE

Exhibit 6.3.2
SIGNAGE ANALYSIS - MALL TENANTS

DESIGN GUIDELINES CHECKLIST

CRITERIA	ACCEPTABLE	NEEDS IMPROVEMENT	COMMENTS
MASSING-PROPORTIONS			
Height to Width Ratio: 1:15 to 1:3			
Varied Massing, Proportions			
Defined Entrance			
Parapet Wall Proportioned			
BUILDING DIMENSIONS			
Max. Building Height			
Max. Building Square Footage			
BUILDING MATERIALS			
Cement Plaster, light to medium dash texture			
Exterior Insulation Finish Systems (EIFS)			
Contemporary metal siding panels			
Brick Veneer or Stone Accents			
Exposed Steel Shade Elements			
Metal Screens, Louvers, Trellises			
Metal Roofing with pre-finished Kynar			
GLAZING			
Transparent or blue/green non-reflective glass			
Clear anodized aluminum framing			
Storefront with contemporary profiles			
Shading: awnings, trellis, overhangs			
COLORS			
Primary color of structure			
Secondary and Accent colors			
DETAILS			
Decorative Scuppers			
Decorative Reglets			
Decorative Color Banding			
ROOF			
Roof Pitch at Exposed Roof of 3:12-12:12			
Material per Guidelines text; standing seam			
Proportioned Auxiliary Structures (Canopies)			
SIGNAGE			
Signs with illumination per Guideline text			
LANDSCAPING			
Plant Species per Master Landscape Plan			
Plan per Project conditions/requirements			
GREEN DESIGN ELEMENTS			
Recycled wood products			
Recycled asphalt or concrete			

Exhibit 7.0
DESIGN GUIDELINES - CHECKLIST



(J) ACCENT FEATURE:
18"X18" SLATE TILE



(G) STUCCO WALL:
DUNN EDWARDS
DE6192 NOMADIC TAUPE



(F) STUCCO WALL:
DUNN EDWARDS
DE6207 EGYPTIAN SAND



(E) BASE COURSE:
MASONRY, COMMON BOND
SPLIT FACE 'HARVEST'



(D) STOREFRONT SYSTEM:
CLEAR ALUM. FRAME W/
SOLAR BRONZE GLAZING



(C) ACCENT FEATURE:
DUNN EDWARDS
DE6216 BARREL STOVE



(B) METAL TRELLIS:
DUNN EDWARDS
DE6063 BLACK WALNUT



(A) ENTRY FACADE:
CUSTOM COLOR
BINDER GREEN

COLOR BOARD - S

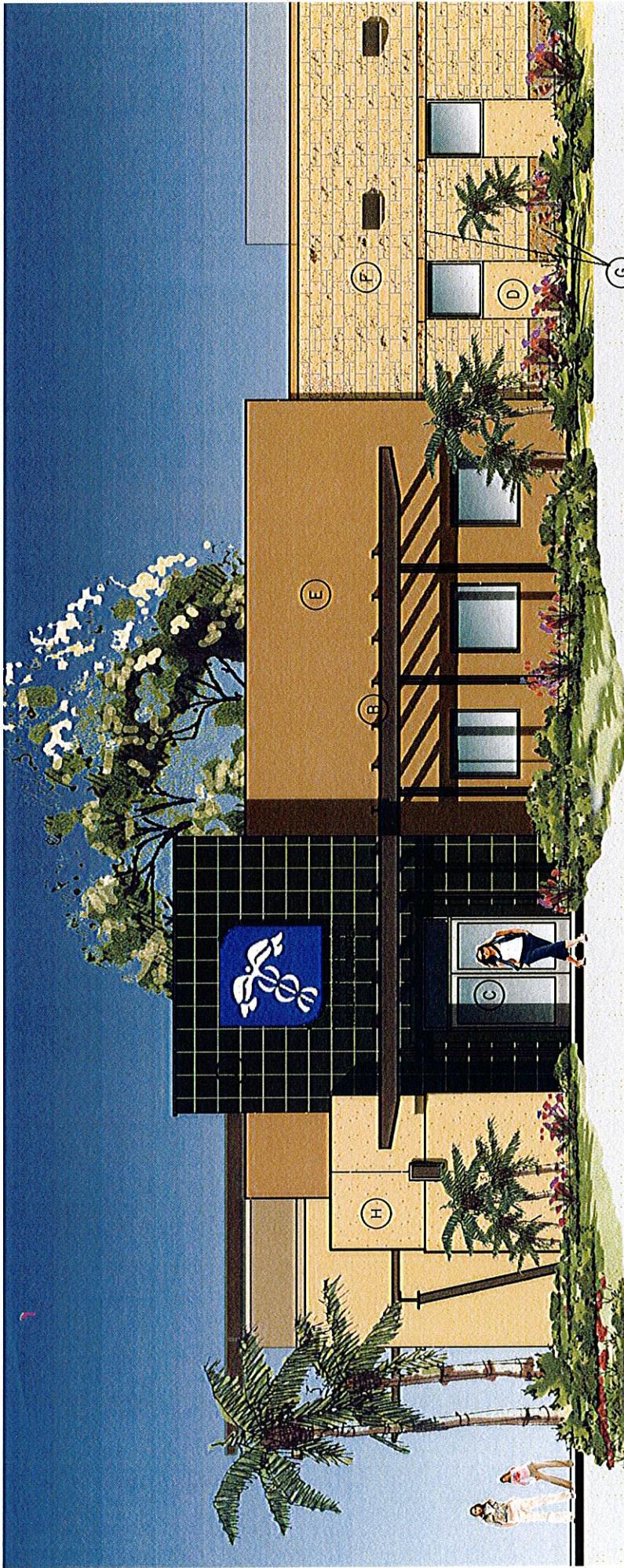
Jan. 30, 2009



Centerpoint Mall Master Plan
CHANNEL ISLAND BLVD. & SAVIERS ROAD, OXNARD CA 93033



200 SAVIERS ROAD OXNARD CA 93033



(A)

ENTRY FACADE:
SLATE TILE 18X18
BLENDED COLOR MIX



(B)

METAL TRELLIS:
DUNN EDWARDS
DE 6392 MINK



(C)

STOREFRONT SYSTEM:
CLEAR ALUM. FRAME W/
SOLAR BRONZE GLAZING



(D)

PLASTER TO MATCH MASONRY:
DUNN EDWARDS
DE6150 GOURMET HONEY



(E)

PLASTER WALL:
DUNN EDWARDS
DE 6130 WOODEN PEG



(F)

MASONRY FIELD:
ANGELUS BLOCK
CHAMPAGNE-
ASHLER PATTERN



(G)

MASONRY BASE:
ANGELUS BLOCK
HARVEST- SPLIT FACE



(H)

PLASTER WALL:
DUNN EDWARDS
DE6150 GOURMET HONEY



(A)

ENTRY FACADE:
DUNN EDWARDS
DE6174 RIDGECREST



(B)

METAL TRELLIS:
DUNN EDWARDS
DE6392 MINK



(C)

STOREFRONT SYSTEM:
CLEAR ALUM. FRAME W/
SOLAR BRONZE GLAZING



(D)

WATER BLAST TEXTURE:
DUNN EDWARDS
DE6230 CENTER RIDGE



(E)

BASE COURSE:
DUNN EDWARDS
DE 6230 CENTER RIDGE



(F)

CONCRETE PANEL:
DUNN EDWARDS
DE6280 ENGLISH FOREST



(G)

CONCRETE PANEL:
DUNN EDWARDS
DE6192 NOMADIC TAUPE

ATTACHMENT E

Planning Commission Resolution (Planned Development Permit)

RESOLUTION NO. 2009 – PZ 08-540-01

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OXNARD APPROVING PLANNING AND ZONING PERMIT NO. 08-540-01 (PLANNED DEVELOPMENT PERMIT), FOR APPROVAL OF A MASTER PLAN TO ALLOW PHASED DEVELOPMENT FOR THE CENTERPOINT MALL, LOCATED AT 2655 SAVIERS ROAD (APN #'S 203-0-320, -185 -205, -245, -265, -285, -295 -315, -335 -355, -375-435), SUBJECT TO CERTAIN FINDINGS AND CONDITIONS. FILED BY COASTAL ARCHITECTS, 505 SOUTH A STREET, OXNARD, CA 93030 ON BEHALF OF OXNARD CENTER COMPANY.

WHEREAS, the Planning Commission of the City of Oxnard has considered an application for Planning and Zoning Permit No. 08-540-01, filed by Coastal Architects in accordance with Section 16-530 through 16-553 of the Oxnard City Code; and

WHEREAS, in accordance with the California Environmental Quality Act, the Planning Manager provided public notice of the intent of the City to adopt a mitigated negative declaration for this project, and the Planning Commission considered the proposed mitigated negative declaration, together with any comments received during the public review process, finds on the basis of the whole record before it (including the initial study and any comments received) that with the imposition of mitigation measures as conditions of approval, there is no substantial evidence that the project will have a significant effect on the environment, further finds that the mitigated negative declaration reflects the independent judgment of the City, and adopts the mitigated negative declaration; and

WHEREAS, the documents and other materials that constitute the record of proceedings upon which the decision to adopt the mitigated negative declaration is based is located in the Planning Division of the City of Oxnard, and the custodian of the record is the Planning Manager; and

WHEREAS, the Planning Commission finds, after due study, deliberation and public hearing, that the following circumstances exist:

1. The proposed use is in conformance with the General Plan and other adopted policies of the City of Oxnard.
2. The proposed use will not adversely affect or be materially detrimental to the adjacent uses, buildings or structures or to the public health, safety or general welfare.
3. The site for the proposed use is adequate in size and shape to accommodate the setbacks, parking, landscaping, and other City standards except as may be specifically excepted by the special findings and conditions of this resolution.
4. The site for the proposed use will be served by streets and highways adequate in width and structure to carry the kind and quantity of traffic such use will generate.

5. The site for the proposed use will be provided with adequate sewerage, water, fire protection and storm drainage facilities.

WHEREAS, in approving the Administrative Relief of Parking Provisions to provide 1,314 parking spaces, 565 parking spaces less than required by the parking ordinance code, and to provide 74 relocated compact parking spaces, the Planning Commission finds that the site plan on file with the Planning Division:

1. Preserves the intent of the parking ordinance as there is shared use of parking due to different hours of operation and peak demands for the mall uses;
2. Provides sufficient parking to serve the intended use as demonstrated in a parking study, including the proximity to the C Street bus center; and
3. The modification will not be detrimental to the public health, safety or welfare.

WHEREAS, in approving a Master Sign Program that includes two mall identification ground signs on two different frontages and allows a freestanding sign with a height of 32 feet, the Planning Commission finds that:

1. The Centerpoint Mall Master Plan, that is subject to the Master Sign Program, has been approved subject to a planned development permit;
2. The amount of sign area does not exceed that which would be allowed by the sign ordinance and otherwise meets the intent of the sign provisions of the sign ordinance;
3. The type and appearance of the proposed signs is consistent with the design guidelines of the Centerpoint Mall Master Plan;
4. The Master Sign Program provides for unique and unified sign program that cannot be accommodated through the strict application of the provisions of the sign ordinance.

WHEREAS, the Planning Commission finds that the applicant agrees with the necessity of and accepts all elements, requirements, and conditions of this resolution as being a reasonable manner of preserving, protecting, providing for, and fostering the health, safety, and welfare of the citizenry in general and the persons who work, visit or live in this development in particular.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Oxnard hereby approves this permit subject to the following conditions. The decision of the Planning Commission is final unless appealed in accordance with the provisions of Section 16-545 of the Oxnard City Code.

**STANDARD CONDITIONS OF APPROVAL
FOR LAND USE PERMITS**

Note: The abbreviations below identify the City department or division responsible for determining compliance with these standard conditions. The first department or division listed has responsibility for compliance at plan check, the second during inspection and the third at final inspection, prior to issuance of a certificate of occupancy, or at a later date, as specified in the condition. If more than one department or division is listed, the first will check the plans or inspect the project before the second confirms compliance with the condition. The italicized code at the end of each condition provides internal information on the source of each condition: Some are standard permit conditions (e.g. *G-1*) while some are taken from environmental documents (e.g. *MND-S2*).

DEPARTMENTS AND DIVISIONS			
CA	City Attorney	PL	Planning Division
DS	Dev Services/Eng Dev/Inspectors	TR	Traffic Division
PD	Police Department	B	Building Plan Checker
SC	Source Control	FD	Fire Department
PK	Public Works, Landscape Design	CE	Code Compliance

GENERAL PROJECT CONDITIONS

1. This permit is granted for the property described in the application on file with the Planning Division, and may not be transferred from one property to another. (PL, *G-1*).
2. This permit is granted for the plans dated August 20, 2009, (“the plans”) on file with the Planning Division. The project shall conform to the plans, except as otherwise specified in these conditions, or unless a minor modification to the plans is approved by the Planning and Environmental Services Manager (“Planning Manager”) or a major modification to the plans is approved by the Planning Commission. A minor modification may be granted for minimal changes or increases in the extent of use or size of structures or of the design, materials or colors of structures or masonry walls. A major modification shall be required for substantial changes or increases in such items. (PL, *G-2*)
3. This permit shall automatically become null and void 36 months from the date of its issuance, unless Developer has diligently developed the proposed project, as shown by the issuance of a grading, foundation, or building permit and the construction of substantial improvements. (PL, *G-3*)
4. All required off-site and on-site improvements for the project, including structures, paving, and landscaping, shall be completed prior to occupancy for each phase unless the Development Services Manager allows Developer to provide security or an executed

- agreement approved by the City Attorney to ensure completion of such improvements. (DS, G-4)
5. By commencing any activity related to the project or using any structure authorized by this permit, Developer accepts all of the conditions and obligations imposed by this permit and waives any challenge to the validity of the conditions and obligations stated therein. (CA, G-5)
 6. Developer agrees, as a condition of adoption of this resolution, at Developer's own expense, to indemnify, defend and hold harmless the City and its agents, officers and employees from and against any claim, action or proceeding to attack, review, set aside, void or annul the approval of the resolution or any condition attached thereto or any proceedings, acts or determinations taken, done or made prior to the approval of such resolution that were part of the approval process. Developer's commencement of construction or operations pursuant to the resolution shall be deemed to be an acceptance of all conditions thereof. (CA, G-6)
 7. Any covenants, conditions, and restrictions (CC&Rs) applicable to the project property shall be consistent with the terms of this permit and the City Code. If there is a conflict between the CC&Rs and the City Code or this permit, the City Code or this permit shall prevail. (CA, G-7)
 8. Developer shall complete the "Notice of Land Use Restrictions and Conditions" form, using the form provided by the City, for recording with the Ventura County Recorder. Before the City issues building permits, Developer shall submit the original completed, signed and notarized document, together with the required fees to the Planning Manager. (PL, G-8)
 9. Developer shall provide off-street parking for the project, including the number of spaces, stall size, paving, striping, location, and access, as required by the Master Plan. (PL/B, G-9)
 10. Before placing or constructing any signs on the project property, Developer shall obtain a sign permit from the City. Except as provided in the Master Sign Program, Developer may not add or change any signs on the project property. (PL/B, G-10)
 11. Developer shall obtain a building permit for any new construction or modifications to structures, including interior modifications, authorized by this permit. (B, G-11)
 12. Developer shall not permit any combustible refuse or other flammable materials to be burned on the project property. (FD, G-12)
 13. Developer shall not permit any materials classified as flammable, combustible, radioactive, carcinogenic or otherwise potentially hazardous to human health to be handled, stored or used on the project property, except as provided in a permit issued by the Fire Chief. (FD, G-13)

14. If Developer, owner or tenant fails to comply with any of the conditions of this permit, the Developer, owner or tenant shall be subject to a civil fine pursuant to the City Code. (CA, *G-14*)
15. Prior to issuance of building permits for each phase, Developer shall correct all violations of the City Code existing on the project property. (PL, *G-15*).

LANDSCAPE STANDARD CONDITIONS

16. Before the City issues building permits or a proposed use is initiated in each Master Plan Area, Developer shall submit two copies of landscape and irrigation plans, along with the appropriate permit application and fees, to the Development Services Division and obtain approval of such plans. (PK/DS, *PK-2*)
17. Before the City issues a certificate of occupancy for a structure pursuant to the Master Plan, Developer shall install landscape and automatic irrigation systems that have been approved by Parks and Facilities Superintendent. (PK, *PK-3*)
18. All trees planted or placed on the project property by Developer shall be at least 24-inch-box size. All shrubs and vines shall be at least five-gallon size, except as otherwise specified by this permit. (PK, *PK-6*)
19. Developer shall install an irrigation system that includes a water sensor shut off device as a water conservation measure. (PK, *PK-22*)

LANDSCAPE SPECIAL CONDITIONS

20. The Developer's Landscape Architect or Architect shall provide the City with written confirmation that they have reviewed the civil engineering construction drawings and that the NPDES requirements are not in conflict with meeting the City's landscape requirements. (PK)
21. All typical parking lot island landscape finger planters must maintain clear sight visibility for both the pedestrian as well as the driver pulling out of the parking stall. No more than a maximum of three palm trees, *Washingtonia robusta*, shall be permitted per double loaded landscape finger planters. The City reserves the right to modify the landscape treatment in quantity and quality in order to address providing clear sight visibility for the pedestrian and driver. (PK)
22. Sheet LS-1 (Centerpoint Mall Master Plan) shall be revised to reflect the most current median configuration on Laurel Street prior to issuance of first building permit after Master Plan approval. (PK)
23. In no case shall new trees be planted in grass swales or treatment swales; nor are any existing trees to remain in any proposed grass swales or treatment swales. Existing trees may however be reused and transplanted to other areas outside of the swale basin. (PK)

24. As each new building or addition to an existing building is processed, plan submittal shall include the following information on the Landscape Plans:
 - A. Each drawing submittal package shall contain a tree listing (by number from the approved arborist's Report) of the tree(s) removed, saved in place, or to be transplanted throughout the site.
 - B. If existing trees are removed from the site and not reused, the economic appraisal value (based on the Arborist's Report) shall be put back into new tree sizes for the project in addition to meeting the City's minimum tree size of 24' box size. A Tree Tabulation Chart shall be provided on the Landscape Plans which clearly demonstrates the value of the tree(s) removed (based on the Arborist's Report) and a calculation showing how that value of the removed tree(s) was put back into new tree sizes for the project. (PK)
25. Before the City issues a certificate of occupancy for any new building or addition, Developer shall install landscape and automatic irrigation systems that have been approved by the Parks and Facilities Superintendent for the associated area. (PK)
26. Before the city issues a certificate of occupancy for any new building or addition, Developer shall provide a watering schedule to the building owner or manager and to the Parks and Facilities Superintendent for the associated area. The irrigation system shall include automatic rain shut-off devices, or instructions on how to override the irrigation system during rainy periods. (PK)
27. A 36" high continuous visual screen is required, at time of planting, to help screen parking lot cars from the adjacent roadway. (PK)
28. Where exaggerated curbs occur adjacent to sidewalks (Sections H1, H2, H3, of Plan Sheet LS-3) the groundcovers in the adjacent landscape planters shall be trained to spill over the exaggerated curb to act as a graffiti deterrent. (PK)
29. Prior to Certificate of Occupancy for replacement Building J (corner of Saviers Rd. and Laurel St.), the median and roadway improvements on Laurel Street are required to be improved. The median improvements shall include landscaping, irrigation and stamped paving per the City's direction. A Landscape Maintenance Assessment District shall be formed by the property mall owner to fund the on going landscape maintenance of the median landscape improvements. The property mall owner will be required to pay the appropriate fees and or deposits in order to form the future City administered Landscape Maintenance Assessment District. (PK)
30. A final copy of the approved sheet LS-1 (Centerpoint Mall Master Plan) shall be provided to staff and retained in the file to be used to confirm that subsequent landscape submittals

for new buildings or additions to existing buildings conform to the approved Master Plan.
(PK)

FIRE DEPARTMENT STANDARD CONDITIONS

31. Where a fire hydrant is located on a new fire apparatus access drive, Developer shall construct the drive width to a minimum of 26-feet.
32. Prior to issuance of first Building Permit, Developer shall mark curbs adjacent to designated fire lanes in parking lots to prohibit stopping and parking in the fire lanes. Developer shall mark all designated fire lanes in accordance with the California Vehicle Code. (FD/B, *F-1*)
33. All roof covering materials on the project property shall be of non-combustible or fire retardant materials approved by the Fire Chief and in compliance with the City Code. (FD, *F-2*)
34. Before the City issues building permits, Developer shall obtain the Fire Chief's approval of a plan to ensure fire equipment access and the availability of water for fire combat operations to all areas of the project property. The Fire Chief shall determine whether or not the plan provides adequate fire protection. (FD/DS, *F-3*)
35. At Developer's expense, Developer shall obtain two certified fire flow tests for the project property. The first test shall be completed before City approval of first building plans and the second shall be completed after construction and prior to the issuance of a certificate of occupancy. A mechanical, civil, or fire protection engineer must certify the tests. Developer shall obtain permits for the tests from the Engineering Division. Developer shall send the results of the tests to the Fire Chief and the City Engineer. (FD/DS, *F-4*)
36. All structures on the project property shall conform to the minimum standards prescribed in Title 19 of the California Code of Regulations. (FD, *F-5*)
37. The project shall meet the minimum requirements of the "Fire Protection Planning Guide" published by the Fire Department. (FD, *F-6*)
38. At all times during construction, developer shall maintain paved surfaces capable of handling loads of 46,000 pounds which will provide access for fire fighting apparatus to all parts of the project property. (FD/DS, *F-7*)
39. Developer shall identify on plans, all hydrants, standpipes and other fire protection equipment on the project property as required by the Fire Chief. (FD, *F-8*)
40. Developer shall provide central station monitoring of the fire sprinkler system and all control valves. (FD, *F-10*)

41. The turning radius of all project property driveways and turnaround areas used for emergency access shall be a minimum of 48 feet outside diameter for a semi-trailer. (FD, *F-11*)
42. Developer shall provide automatic fire sprinklers as required by the City Code and shall contact the Fire Chief to ascertain the location of all connections. (FD, *F-12*)
43. Developer shall install in each new structure or addition to an existing structure, in the Master Plan according to Fire Code, a detection/alarm system with a central station monitor that will automatically notify the Fire Department in the event of a fire in the structure. The alarm system shall include a UL or State Fire Marshal approved device, which shall not exceed design specifications, that reports the location of the fire and allows the central station monitor to inform the Fire Department of the point of entry into the structure that is nearest the fire. (FD, *F-13*)
44. Developer shall comply with Certified Unified Program Agency (CUPA) requirements regarding the storage, handling and generation of hazardous materials or waste. Prior to the issuance of building permits, Developer shall contact the CUPA division of the Fire Department to ensure that such requirements are followed. (FD, *F-16*)

FIRE DEPARTMENT SPECIAL CONDITIONS

45. Prior to issuance of first Building Permit for any new building or addition to an existing building, all existing Double detector check valves and/or Fire Department Connections shall be identified by signage to indicate which buildings or occupancies are serviced by that device. (FD)
46. Fire Department shall review and approve emergency access, drive aisle width, placement of fire hydrants, double detector check valves and Fire Department connections on any new construction within the Master Plan. (FD)
47. Before the City issues a certificate of occupancy to any building, the Developer shall install a Knox Box key vault at a location on the building to be determined by the Fire Department. (FD)

PLANNING DIVISION STANDARD CONDITIONS

48. Plans submitted by Developer with building permit applications shall show on the building elevation sheets all exterior building materials and colors, including product and finish manufacturer name, color name and number, and surface finish type (such as: stucco with sand finish, plaster with smooth finish) to be used in construction consistent with the Master Plan. (PL/B, PL-1)
49. Any application for a minor modification to the project shall be accompanied by four copies of plans reflecting the requested modification, together with applicable processing fees. (PL, *PL-2*)

50. Before the City issues building permits, Developer shall include a reproduction of all conditions of this permit as adopted by resolution of the Planning Commission and/or the City Council in all sets of construction documents and specifications for the project. (PL, *PL-3*)
51. Before the City issues building permits, Developer shall provide to the Planning Division Manager color photographic reductions (8 1/2" by 11") of full-size colored elevations and any other colored exhibit approved by the Planning Commission. Developer may retain the full-size colored elevations after the reductions are so provided. (PL, *PL-4*)
52. Developer acknowledges that because of population limitations placed on the City by the Air Quality Management Program, approval of this permit does not guarantee that the City will issue building permits. The City's issuance of building permits may be delayed as a result of implementation of an air quality plan. (PL, *PL-5*)
53. Developer may not modify any use approved by this permit unless the Planning Division Manager determines that Developer has provided the parking required by the Master Plan for the modified use. (PL, *PL-7*)
54. During the plan check review process, the Developer shall provide a lighting plan that provides design details (light standards, bollards, wall mounted packs, etc.) and illumination site information within alleyways, pathways, streetscapes, and open spaces proposed throughout the Proposed Area of the Master Plan. An electrical engineer shall prepare the site lighting plan demonstrating that adequate lighting ranges will be provided throughout the development without creating light spillover, light pollution, or conflicts with surrounding factors such as tree locations, off-site or adjacent lighting. (PL)
55. Prior to issuance of building permits, Developer shall demonstrate that light standards illustrated on conceptual lighting plan do not conflict with tree locations. Developer shall submit a plan showing both the lighting and landscape on the same sheet.
56. Project on-site lighting shall be of a type and in a location that does not constitute a hazard to vehicular traffic, either on private property or on adjoining streets. To prevent damage from vehicles, standards in parking areas shall be mounted on reinforced concrete pedestals or otherwise protected. Developer shall recess or conceal under-canopy lighting elements so as not to be directly visible from a public street. Developer shall submit a lighting plan showing standard heights and light materials pursuant to the Master Plan for design review and approval of the Planning Division Manager. (PL/B, *PL-8*)
57. In order to minimize light and glare on the project property, all parking lot and exterior structure light fixtures shall be high cut-off type that divert lighting downward onto the property and shall not cast light on any adjacent property or roadway. (PL, *PL-9*)
58. Developer agrees to participate in a water conservation program that includes refitting water fixtures existing on the project property with water conserving devices within

residences or businesses in the City's water service area, if such a program is in effect when building permits are issued for this project. Among the requirements of such a program might be refitting existing toilets, faucets, shower heads, landscaping irrigation or other fixtures and items that consume water within the structure. (PL, *PL-14*)

59. Because of water limitations placed upon the City by its water providers, approval of this permit does not guarantee that the City will issue building permits. Issuance of building permits may be delayed as a result of implementation of a water conservation or allocation plan. (PL, *PL-15*)
60. Prior to issuance of building permits, Developer shall pay a document imaging fee for the planning files in an amount calculated by planning staff at the time of building permit review based on fees then in effect. (PL/B, *PL-16*).
61. Developer shall recess or screen roof heating and cooling systems and other exterior mechanical equipment from adjoining property and public streets for all new buildings or additions to existing buildings in the Master Plan. Plumbing vents, ducts and other appurtenances protruding from the roof of structures shall be placed so that they will not be visible from the front of the property or other major public vantage points. Developer shall include a note on the construction plumbing drawings of exterior elevations to indicate to contractors that roof features shall be grouped and located in the described manner. Roof vents shall be shown on construction drawings and painted to match roof material color. (PL/B, *PL-41*)
62. Developer shall install all roof and building drainpipes and downspouts inside building elements. These items shall not be visible on any exterior building elevations. (PL, *PL-42*)
63. For any exterior utility meter panels, Developer shall paint such panels to match the structure upon which it is located. Such panels shall be located to take advantage of screening (e.g. landscaping or other building elements) from public right-of-ways, to the maximum extent feasible. (PL, *PL-43*)
64. Prior to issuance of a certificate of occupancy for any new building or addition to an existing building, Developer shall remove all construction materials and vehicles from the subject property. (PL/B, *PL-47*)
65. Developer shall limit outdoor storage of materials to the locations shown on the project plans. Developer shall screen storage areas from adjacent properties and streets by a wall or similar screening. All fence and wall materials shall match the predominant design and materials of the main structure on the project property. (PL/B, *PL-48*)

PLANNING DIVISION SPECIAL CONDITIONS

66. Development of any new building or addition to an existing building in the Master Plan shall require approval of a Development Design Review Permit.

67. All new buildings, additions to existing buildings or façade remodels shall be consistent with the Master Plan Design guidelines and approved color palette.
68. All new or replacement signage shall be consistent with the approved Master Plan Design Guidelines and Master Sign Program. When a tenant changes, any sign(s) on the tenant space shall be consistent with the Master Sign Program.
69. Developer shall install toilets that have automatic flush sensors in all public restrooms. Such toilets shall be included on the plans submitted for a building permit and shall be maintained and in working order at all times. (PL)
70. Developer shall install individual mirrors above each sink in a public restroom to the satisfaction of the Planning Division Manager. The details of such mirrors shall be approved prior to issuance of a building permit. Developer shall remove graffiti from the mirrors or replace the mirrors within 24 hours of graffiti appearance. (PL)
71. New buildings or additions to existing buildings shall be limited to a building height of 2 stories or 35 feet. (PL)
72. Any addition to Building I (located at the corner of Channel Islands Boulevard and C Street) shall maintain a front yard setback of 10 feet. (PL)
73. Rooftop equipment on any new building or additions to existing buildings may not protrude above the height of the parapet. (PL)
74. A photometric plan shall be provided for new lighting associated with parking lot changes in each Master Plan Area. (PL)
75. The maximum square footage for new buildings or additions to existing buildings shall be as stated in the Master Plan unless a Minor or Major Modification is approved.
76. Developer shall remove any and all graffiti from the project premises, including but not limited to graffiti within the building, such as in restrooms or fitting rooms, within 24 hours of its appearance. The surface of such affected areas shall be matched to blend in with the underlying colors and/or design, and shall not look like a paint patch. (PL)
77. Before the City issues building permits, Developer shall provide a Graffiti Deterrent Plan, subject to the approval the Planning Division Manager. Such plan shall include such elements as clear film on windows and/or mirrors, as well as washable paint and sealers on the building and perimeter walls. (PL)
78. General condition (for all new construction and additions):
 - A. Developer shall participate in the City's Art in Public Places Program by paying the Public Art fee prior to issuance of building permits, in accordance with City Council Resolution No. 13,103.

Air Quality

79. Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment. (MND, C-1)
80. During smog season (May through October), Developer shall lengthen the construction period so as to minimize the number of vehicles and equipment operating at the same time. (MND, C-2)
81. At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust. (MND, C-3)
82. During construction, Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities. (MND, C-4)
83. During construction, Developer shall control dust by the following activities:
 - A. All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads.
 - B. All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally-safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible. (MND, C-5)
84. During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less. (MND, C-6)
85. During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite. (MND, C-7)
86. Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways. (MND, C-8)

87. Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements. (PL)
88. Developer shall comply with APCD Rule 62.7 regarding asbestos during remodeling or construction additions to existing buildings. (PL)

Cultural Resources

89. Developer shall contract with a qualified archaeologist to conduct a Phase I cultural resources survey of the project site prior to issuance of any grading permits. The survey shall include: (1) an archaeological and historical records search through the California Historical Resources Information System at CalState Fullerton; and (2) a field inspection of the project site. Upon completion, the Phase I survey report shall be submitted to the Planning Division for compliance verification. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to initiation of the Phase I activities.

The contract shall include provisions in case any cultural resources are discovered onsite. In the event that any historic or prehistoric cultural resources are discovered, work in the vicinity of the find shall be halted immediately. The archaeologist shall evaluate the discovery and determine the necessary mitigations for successful compliance with all applicable regulations. Developer or its successor in interest shall be responsible for paying all salaries, fees and the cost of any future mitigation resulting from the survey. (MND, E-1)

90. Developer shall contract with a Native American monitor to be present during all subsurface grading, trenching or construction activities on the project site. The monitor monthly report to the Planning Division summarizing the activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building permit signature. (MND, E-2)

Geology and Soils

91. Developer shall submit a site-specific soils investigation, which shall be prepared by a licensed geotechnical engineer. At a minimum, such study shall include liquefaction and compressible soil characteristics on the subject site, and shall identify any necessary construction techniques or other mitigation measures to prevent significant liquefaction/compressible soils impacts upon the proposed project. All recommendations of said report shall be incorporated into the project. Developer shall submit the report to the Building Official for review and approval prior to issuance of building permits. (MND F-1)

Hazards and Hazardous Materials

92. Prior to demolition or excavation within the boundaries of the two underground contamination sites, documentation of appropriate closure shall be required. (MND G-1)

Hydrology and Water Quality

93. Developer shall submit calculations verifying compliance with NPDES storm water quality requirements prior to issuance of a Grading or Building Permit for each phase/building. (MND H-1)

Noise

94. During all excavation and grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards. (MND K-1)
95. Construction times shall be limited to 7 a.m. to 7 p.m. daily or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive. (MND K-2)

Transportation/Traffic

96. Developer shall extend the raised median on Laurel Street to the easterly driveway of Santa Clara High School to block right turns out of the school driveway. Developer shall also install associated striping and signage on Laurel Street. Master Plan shall provide for these improvements to occur with the first building permit issued under the Master Plan. Developer shall re-stripe Laurel Street from C Street to Saviers Road to accommodate left-turn lanes as directed by the City Traffic Engineer. Left-turn pockets shall be 10 feet wide with 150 feet of vehicle storage and a 75 foot bay taper. Master Plan shall provide for these improvements to occur with the first building permit issued under the Master Plan. Master Plan eliminates west bound left-turn access from Laurel Street to the easterly shopping center driveway. Due to elimination of this turning movement, Developer shall re-design proposed driveway location to provide a straight shopping center drive aisle to increase the number of parking spaces being provided. Redesign to be approved by the City Traffic Engineer. (TR O-1)
97. Developer shall widen the western leg of Laurel Street/Saviers Road intersection to add a second left-turn pocket and provide a raised median. The Laurel Street east bound left-turn and right-turn pockets shall be designed and constructed with a 150 foot (minimum) vehicle storage length and a 90 foot taper. Developer shall install striping and perform traffic signal modification at the southeast and southwest corners of the Laurel Street/Saviers Road intersection as part of the road widening. Master plan shall provide for these improvements

and any corresponding right-of-way dedication to occur concurrent with construction of Building J or Building G, whichever occurs first. (TR O-2 and O-3)

98. Developer shall construct a dedicated southbound right-turn pocket on Saviers Road at the northerly shopping center driveway. The right-turn pocket shall include construction of a bus stop with shelter. The right-turn pocket shall be 12 feet wide with a minimum full lane width length of 210 feet (100 foot vehicle storage area, 60 foot transition area, and 50 foot concrete bus pad) and a 50 foot long bay taper. Developer shall dedicate the required right-of-way to accommodate these improvements. Master plan shall provide for these improvements and corresponding right-of-way dedication to occur concurrent with construction of Building J or Building G, whichever occurs first. (TR O-4)
99. Developer shall construct a dedicated southbound right-turn pocket on Saviers Road at the southerly shopping center driveway. The right-turn pocket shall be 12 feet wide with a minimum full lane width of 100 feet for vehicle storage and 60 foot long bay taper. Developer shall dedicate the required right-of-way to accommodate these improvements. Master Plan shall provide for these improvements and corresponding right-of-way dedication to occur concurrent with construction of Building G. (TR O-5)
100. Master Plan shall provide for Developer to construct a southbound right-turn pocket on Saviers Road at Channel Islands Boulevard. Developer shall dedicate 11 feet 1 inch of right-of-way measured from the existing project easterly property line to accommodate the right-turn pocket. The dedication shall extend from the existing southerly property line of the lot containing Building S to the southerly property line of lot containing existing Building N. Master plan shall provide for these improvements and corresponding right-of-way dedication to occur when Building N is remodeled or demolished to remove the portion of the building within the right-of-way dedication area. (TR O-6)

ENVIRONMENTAL RESOURCES DIVISION

101. To ensure that solid waste generated by the project is diverted from the landfill and reduced, reused or recycled, Developer shall complete and submit a "City of Oxnard C&D Environmental Resources Management & Recycling Plan" ("Plan") to the City for review and approval for each new building or addition to a n existing building. The Plan shall provide that at least 50% of the waste generated on the project be diverted from the landfill. The Plan shall include the entire project area, even if tenants are pursuing or will pursue independent programs. The Plan shall be submitted to and approved by the Environmental Resources Division prior to issuance of a building permit. The Plan shall include the following information: material type to be recycled, reused, salvaged or disposed; estimated quantities to be processed; management method used; destination of material including the hauler name and facility location. Developer shall use the Plan form.
102. Developer shall follow the approved "City of Oxnard C&D Environmental Resources Management & Recycling Plan" and provide for the collection, recycling, and/or reuse of materials (i.e., concrete, wood, metal, cardboard, green waste, etc.) and document results during construction and/or demolition of each new building or addition to an existing building.

After completion of demolition and/or construction, Developer shall complete and submit the "City of Oxnard C&D Environmental Resources Management & Recycling Report For Work Completed" ("Work Completed Report") and provide legible copies of weight tickets, receipts, or invoices for materials sent to disposal or reuse/recycling facilities. For other discarded or salvaged materials, Developer shall provide documentation, on the disposal facility's letterhead, identifying where the materials were taken, type of materials, and tons or cubic yards disposed, recycled or reused, and the project generating the discarded materials. Developer shall submit and obtain approval of the Work Completed Report prior to issuance of a certificate of occupancy.

103. Developer shall arrange for materials collection during construction, demolition, and occupancy with the City's Environmental Resources Division or Developer shall arrange for self-hauling to an authorized facility.
104. Developer shall make provisions to divert at least 50% of the waste material generated during occupancy of building with new construction through source reduction, recycling, reuse, and green waste programs. Developer shall complete and submit a "City of Oxnard C&D Environmental Resources Management & Recycling Occupancy Plan" ("Occupancy Plan") to the City's Environmental Resources Division. An Occupancy Plan must be submitted and approved prior to issuance of a certificate of occupancy. A "City of Oxnard C&D Environmental Resources Management & Recycling Occupancy Report" shall be submitted to the Environmental Resources Division annually on the anniversary date of the certificate of occupancy for approval.

DEVELOPMENT SERVICES DIVISION STANDARD CONDITIONS

105. Developer shall pay plan check and processing fees in effect at the time of construction plan submittal and shall pay development fees, encroachment permit fees, and other applicable fees in effect at the time the City issues building permits. (DS-1)
106. Developer shall install on-site and off-site utility services underground in accordance with City ordinances in effect at the time City issues the building permit. Services shall be installed underground to the nearest suitable riser pole as determined by the appropriate utility service provider. (DS-12)
107. A civil engineer licensed in the State of California shall prepare the public improvement plans and documents for this project in accordance with City standards and shall submit all such plans to the City Engineer. Such plans and documents shall include, but not be limited to, grading, street, drainage, sewer, water and other appurtenant improvement plans; a master utility plan showing the layout and location of all on-site and off-site utility improvements that serve the project; construction cost estimates, soils reports, and all pertinent engineering design calculations. City will not accept an application for the final map or parcel map for the project or issue a grading, site improvement or building permit until the City Engineer has approved all improvement plans. (DS-15)

108. The conditions of this resolution shall prevail over all omissions, conflicting notations, specifications, dimensions, typical sections, and the like, that may or may not be shown on the improvement plans. (DS-21)
109. "Standard Specifications for Public Works Construction," latest edition, and any modifications thereto by City, and City of Oxnard Standard Land Development Specifications and all applicable City Standard Plans, shall be the project specifications, except as noted otherwise on the approved improvement plans. City reserves the right to upgrade, add to, or revise these specifications and plans and all other City ordinances, policies, and standards. If the improvements required of this project are not completed within 12 months from the date of City's approval of the improvement plans, Developer shall comply with and conform to any and all upgraded, additional or revised specifications, plans, ordinances, policies and standards. (DS-27)
110. Developer shall retain a Civil Engineer licensed in the State of California to ensure that the construction work conforms to the approved improvement plans and specifications and to provide certified "as-built" plans after project completion. Developer's submittal of the certified "as-built" plans is a condition of City's final acceptance of the project. (DS-29)
111. Storm drain, sewer and water facilities shall conform to applicable City Master Plans. Developer shall prepare plans for these facilities in accordance with City's engineering design criteria in effect at the time of improvement plan submittal. Developer shall submit plans with pertinent engineering analyses and design calculations for review and approval by the City Engineer prior to issuance of a site improvement permit. (DS-34)
112. Developer shall dispose of sewage and solid waste from the project by City's wastewater and solid waste systems in a manner approved by the City Engineer. (DS-38)
113. Developer shall install adequately sized water services and meters to each lot or unit in accordance with City standards in effect at the time City issues building permits. There shall be no interconnections between structures. (DS-42)
114. Developer shall provide adequate vehicle sight distance as specified by CalTrans specifications at all driveways and intersections. (TR-71)
115. Developer, property owner or tenant shall participate in a Traffic Management Association (TMA) if one is formed and shall implement and participate in all programs and strategies established by the TMA. This condition shall be included in all leases and rental agreements for the project property. (TR-72)
116. Developer shall install bike racks for each Master Plan Area in association with new construction or additions to existing buildings, in accordance with City standards at locations approved by City Traffic Engineer. (TR-73)

117. Prior to issuance of a building permit for a structure that requires roadway improvements, all traffic signal, pavement marking and sign plans shall be prepared by a registered California traffic engineer and approved by the City Engineer prior to issuance of a grading, site improvement or a building permit. (TR-74)
118. Prior to issuance of an encroachment permit, Developer's shall obtain City's approval of a contractor qualified to install traffic signals, pavement markings and signs. (TR-76)
119. Prior to issuance of first Building Permit for new building or replacement buildings, Developer shall include a transportation information center stocked with bus schedules, rideshare information, and related information on alternative methods of transportation. Developer or owner shall update such information at least once a month. (TR-77)

STORMWATER QUALITY CONDITIONS

120. Developer shall construct double-bin trash enclosures (one bin for recycle use) with a solid non-combustible roof (8-foot minimum clearance) that prevents stormwater from entering the refuse bins. Developer shall construct all other components of the trash enclosure in accordance with the approved City Standard Plan on file with the Development Services Division. Developer shall finish the trash enclosure to match the major design elements of the main structure. The finish and roof appearance shall be indicated on the building plans and are subject to approval by the Planning Division . The location and configuration of trash enclosures shall be reviewed and approved by the Environmental Resources Division. All refuse bins on the site shall be stored in an approved trash enclosure. No objects other than refuse bins may be stored in the trash enclosure without the written permission of the Environmental Resources Division. (DS-79)
121. Developer shall design parking lot and other drive areas to minimize degradation of stormwater quality. Using Best Management Practices (BMPs), such as oil and water separators, sand filters, landscaped areas for infiltration, basins or approved equals, Developer shall intercept and effectively prevent pollutants from discharging to the storm drain system. The stormwater quality system design shall be approved by the City Engineer prior to the issuance of a site improvement permit. (DS-81)
122. Developer shall maintain parking lots free of litter and debris. Developer shall sweep sidewalks, drive aisles, and parking lots regularly to prevent the accumulation of litter and debris. When swept or cleaned, debris must be trapped and collected to prevent entry into the storm drain system. Developer may not discharge any cleaning agent into the storm drain system. (DS-84)

DEVELOPMENT SERVICES DIVISION SPECIAL CONDITIONS

123. Developer shall construct proposed pedestrian circulation walks within the Master Plan Area associated with construction, that cross vehicular drive aisles of colored enhanced concrete. The concrete color shall contrast with the parking lot asphalt to clearly identify pedestrian

areas. This condition is not intended to address paths exclusively providing access to disabled parking spaces. (DS)

124. Developer shall locate onsite walks to minimize impacts on landscape finger planters. Where practical, Developer shall avoid locating walks in a manner that results in splitting landscape finger planters into two smaller planters. Final walk locations shall be as directed by the Development Services Manager. (DS)
125. Developer shall construct a concrete apron along the length of the trash enclosure opening that extends a minimum of 15 feet from the face of the enclosure. (DS)
126. Developer shall construct a minimum 7-foot wide sidewalk at all locations within the Master Plan Area associated with development, where the sidewalk is constructed adjacent to the front of parking spaces. (DS)
127. Developer shall place existing overhead utility lines on and adjacent to the project underground except any electrical lines in excess of 33KV. Placement of utilities underground may be phased in conformance with the overall phasing plan. Before issuance of a site improvement permit, Developer shall post security satisfactory to the Finance Director guaranteeing utility relocation associated with that phase. (DS)
128. Developer shall provide stormwater treatment improvements and associated stormwater infiltration and conveyance improvements to serve a project phase concurrent with construction of that phase. (DS)
129. As each phase is constructed, Developer may be required to provide temporary improvements that result in appropriate transitions between existing improvements and proposed improvements. Temporary improvements may include, but are not limited to, temporary striping, curb and pavement transitions, waterline blowoffs, etc as determined by the Development Services Manager. (DS)
130. Developer shall provide calculations for each phase of construction that demonstrates that onsite fire hydrant system meets Fire Department flow requirements for that phase of construction and does not negatively affect existing fire protection. (DS)

Storm Water Quality

131. Developer shall comply with National Pollutant Discharge Elimination System (NPDES) permit Best Management Practice (BMP) requirements in effect at the time of grading or building permit issuance. Requirements shall include, but not be limited to, compliance with the then current Ventura Countywide Stormwater Quality Urban Impact Mitigation Plan (SQUIMP) and shall be applicable to stormwater runoff from the entire site as a redevelopment project. (DS)

132. Developer shall design grass filter swales in accordance with the Technical Guidance Manual for Stormwater Quality Control Measures. Design calculations shall be included in the project drainage report. (DS)
133. Developer shall construct/install all NPDES improvements on private property. No treatment or infiltration features shall be located within or affect public right-of-way. (DS)
134. Developer's engineer shall provide City with written confirmation that they have reviewed the landscape construction drawings within the NPDES grass filter swale areas and that the proposed landscaping conforms to SQUIMP standards for grass filter swales. (DS)
135. Developer shall provide a 6 inch minimum vertical drop between the flow line of the parking lot concrete gutter and the flow line of the grass filter swale at each location where stormwater enters the filter swale. The transition between gutter flow line and swale flow line shall be constructed similar to a concrete ribbon gutter. (DS)
136. Developer shall install a perforated underdrain below all grass filter swales constructed with a longitudinal slope of less than 1%. Underdrain shall connect to a point of safe discharge as approved by the Development Services Manager. (DS)
137. Developer shall require all tenants that perform food preparation activities to provide and maintain an area within the building for the washing/steam cleaning of equipment, floor mats and accessories. This area shall be self-contained and connected to the project grease interceptor. (DS)

Water

138. Developer shall provide onsite fire hydrants for each Master Plan Area associated with development, such that all points of all structures within the designated Master Plan Area are within one hundred fifty (150) feet of a fire hydrant, or as otherwise approved by the Fire Department. Fire hydrant line improvements shall be designed in accordance with City standards and shown on the civil engineer's improvement plans prior to issuance of a site improvement/grading permit. Developer shall provide the City with an easement over the waterline using standard City format. (DS)

Streets

139. Developer shall construct a minimum 7 foot wide sidewalk along the portion of Laurel Street being reconstructed as a part of this Master Plan. (DS)
140. Developer may construct a minimum 8 foot wide sidewalk along Saviers Road in locations where the sidewalk is separated from the curb by a landscaped parkway. (DS)

141. Prior to issuance of a site improvement permit for each phase of work, Developer shall dedicate right-of-way to the City that encompasses all portions of the proposed or existing street improvements not within an existing City easement. (DS)
142. Developer shall replace all broken, uplifted or missing sidewalk along the project's street frontages for each Master Plan Area associated with development. Required repairs shall be shown on the grading/site improvement plan. (DS)
143. Developer shall construct driveways of standard non-colored concrete within the right-of-way portion of the drive aisle resulting in a clear demarcation between the back of sidewalk and private onsite improvements for each Master Plan Area associated with development. (DS)

PASSED AND ADOPTED by the Planning Commission of the City of Oxnard on this 20th day of August, 2009, by the following vote:

AYES: Commissioners

NOES: Commissioners

ABSENT: Commissioners

Deirdre Frank, Chair

ATTEST: _____
Susan L. Martin, Secretary