



**Planning Division**

**PLANNING COMMISSION  
STAFF REPORT**

**TO:** Planning Commission

**FROM:** Brian Foote, AICP, Associate Planner

**DATE:** August 5, 2010

**SUBJECT:** Anacapa Townhomes Development – Planning and Zoning Permit Nos. 08-400-04 (Coastal Development Permit), 09-300-01 (Tentative Subdivision Map), and 09-590-01 (Coastal Zone Variance). Located at 5001 W. Wooley Road, at the northeast corner of Harbor Boulevard & Wooley Road.

- 1) **Recommendation:** That the Planning Commission:
  - a) Adopt a resolution approving Planning and Zoning Permit No. 08-400-04 for a Coastal Development Permit, subject to certain findings and conditions.
  - b) Adopt a resolution approving Planning and Zoning Permit No. 09-590-01 for a Coastal Zone Variance, subject to certain findings.
  - c) Adopt a resolution recommending that the City Council approve Planning and Zoning Permit No. 09-300-01 for a Tentative Subdivision Map, subject to certain findings and conditions.
  
- 2) **Project Description and Applicant:** A request to construct the “Anacapa Townhomes” on a 3.96-acre vacant property located at the northeast corner of Harbor Boulevard and Wooley Road, in the Oxnard Dunes neighborhood. A tentative subdivision map proposes creating five parcels and subdividing for 70 air-rights condominium units; a Coastal Development Permit to construct five, multifamily, three-story structures with a total of 70 condominium dwelling units, parking garages, pool and recreation building, amenities, and associated site improvements; and a Coastal Zone Variance to modify the development standards for specified front, side, and rear yard setbacks to permit architectural projections and stairways. Filed by Mike Marlow, Oxnard Shores Development Co. LLC, 1015 S. Harbor Boulevard, Oxnard, CA 93035.
  
- 3) **Existing & Surrounding Land Uses:** The project site is vacant and undeveloped. Harbor Boulevard is adjacent to the site on the west side, Wooley Road is adjacent to the site on the south side, and Canal Street bisects the easterly half of the site. The existing Oxnard Dunes subdivision is adjacent to the site on the north side. The Edison canal is adjacent to the site on the east side.

LOCATION	ZONING	GENERAL PLAN	EXISTING LAND USE
Project Site	R-3-C	Coastal Zone	Vacant
North	R-2-C	Coastal Zone	Oxnard Dunes neighborhood
South	RP	Coastal Zone	Open Space
East	CR	Edison Canal	Edison canal, agriculture beyond
West	CNC	Coastal Zone	Retail Center

**4) Background Information:** The City Council adopted Ordinance No. 2159 on April 19, 1988, approving a Development Agreement between the City of Oxnard and the Oxnard Shores Company for the subject property on Wooley Road. The ordinance approved a transfer of development rights from 22 beachfront parcels (the sending area) to the subject property (the receiving area) as prelude to the Settlement Agreement. The Settlement Agreement transferred ownership of numerous beachfront parcels to the State of California (acting through the State Lands Commission and California Coastal Commission), avoided potentially significant environmental impacts that may have otherwise resulted from future development, preserved coastal resources and recreation areas, and formally established several public access areas. The Development Agreement allows up to 70 dwelling units on the subject property, and expires in the year 2015 or at the completion of development.

The City Council adopted Ordinance No. 2169 on July 5, 1988, changing the zones for the properties included in the Development Agreement and Settlement Agreement. The subject property changed from Coastal Multiple-Family (R-2-C) to Coastal Medium Density Multiple-Family (R-3-C) in order to accommodate the density of development envisioned in the Development Agreement. The beachfront properties changed from Single-Family Beach (R-B-1) to Coastal Recreation (RC) affirming their use as public beach and recreation areas, precluding future development, and establishing conformity with the Development Agreement.

**5) Environmental Determination:** The proposed development is subject to review in accordance with the California Environmental Quality Act (CEQA). An Initial Study was prepared to analyze the potential for any significant adverse environmental effects of the proposed project. The draft Mitigated Negative Declaration (MND No. 09-03), consisting of the Initial Study and mitigation measures, was circulated for public review and comment from January 25, 2010, through February 23, 2010. The State Clearinghouse assigned number 2010011069 for circulation to State agencies. The topics analyzed in the Initial Study identified the levels of impact as one of the following: a) no impact; or b) less than significant impact; or c) less than significant impact after mitigation. None of the topics were identified as having potentially significant impacts, and an Environmental Impact Report is not required. Therefore, staff recommends that the Planning Commission adopt a Mitigated Negative Declaration (see Attachment C) for the proposed project.

### *Impacts Less Than Significant After Mitigation*

Ten areas of concern were identified as potentially being affected, and mitigation measures have been included to reduce or avoid the potentially significant impacts. The following sections in the Initial Study include mitigation measures: air quality, biological resources, cultural resources, geology/soils, hydrology, noise, public services, recreation, transportation/traffic, and utilities/service systems. The short-term impacts (i.e. construction activities) that are less than significant after mitigation are air quality, biological resources, cultural resources, geology/soils, hydrology, and noise. For example, the mitigation measure requiring pad foundations will avoid all noise and soils impacts that would otherwise result if the developer were to install pile foundations. The mitigation measures for biological resources will require pre-construction surveys in order to detect and relocate any sensitive species that might be present on the site when grading and construction begins. The long-term impacts that are less than significant after mitigation are public services, recreation, traffic, and utilities/service systems. The proposed Mitigated Negative Declaration (Attachment C) lists all mitigation measures. On September 8, 2009, the applicant agreed to the proposed mitigation measures recommended by staff.

### *Public Review & Comments*

Comments on the draft environmental document were submitted by the California Department of Fish & Game, U.S. Fish & Wildlife Service, Ventura County Air Pollution Control District, Ventura County Public Works Agency, Ventura County Watershed Protection District, and two local residents. The Ventura County Air Pollution Control District recommended a mitigation measure adding language emphasizing four standard regulations, which has been added to the MND. The Ventura County Department of Airports recommended one additional mitigation measure for a standard avigation easement, which has been added to the MND. The Ventura County Public Works Agency and Watershed Protection District submitted letters indicating agreement with the Initial Study and mitigation measures.

The City received comments from the California Department of Fish & Game and the U.S. Fish & Wildlife Service regarding the Biological Resources section, specifically, the potential for the presence of threatened or endangered species on the project site such as Silvery Legless Lizard, Least Bell's Vireo, and Ventura Marsh Milkvetch. In response to the public comments, the applicant's consultant PCR Services Corporation conducted further investigations of the project site in April 2010, and submitted an addendum to the Biological Resources Assessment report. In addition, the City retained Rincon Consultants to independently review and analyze the Biological Resources Assessment report, its findings and recommendations, the addendum and the proposed mitigation measures. Two additional mitigation measures are now included, and specify that pre-construction surveys will be conducted to verify that no sensitive species will be present when construction begins, as well as monitor and protect any sensitive species living in nearby habitat areas during construction. The applicant has agreed to the additional mitigation measures. City staff has revised the Biological Resources section of the Initial Study to incorporate the additional analysis and mitigation measures, and will prepare a response letter to CDFG indicating that their comments have been incorporated into the project.

The City also received written comments from two City residents, which were concerned with the proposed density of development and related issues such as traffic, noise, parking, and structure height. Staff has analyzed the proposed project for consistency with the Coastal Land Use Plan and Coastal Zoning Ordinance, and staff believes the project has been designed and conditioned in such a way that addresses the concerns. For a description of staff's analysis, see Section 6 of this Staff Report. All comment letters are included with the environmental document.

**6) Analysis:**

**a) General Discussion:** According to the terms of the Settlement Agreement, the subject property is an in-fill parcel allowing up to 70 condominium units, in exchange for the 23 beachfront parcels that were precluded from future development and transferred to the State Lands Commission to remain in perpetuity as public beach areas. The proposed project is consistent with the allowable density of the Coastal Land Use Plan, the R-3-C zone, the Development Agreement, and the Transfer of Development Rights program.

Ordinance No. 2006 established the Transfer of Development Rights program in 1984 and specifically designated the Oxnard Shores as a "sending area" from which development credits could be transferred to inland "receiving areas." The current project proposes development of the subject property that was designated as a receiving area. The TDR program limits the maximum number of transfer credits to 6 units per acre, in addition to the baseline number of units allowed by the zoning. At the time the Development Agreement was executed in 1988, the original zoning of the subject property was R-2-C and permitted a maximum of 47 dwelling units (in 1988 the parcel was estimated to be 3.91 gross acres). The TDR program permits up to 23 (6 x 3.91) dwelling units in addition to the base density of 47 units. Therefore, the current proposal for 70 units is consistent with the exchange rate defined in the TDR program, and is consistent with the maximum density of 18 units per acre allowed in the R-3-C zone.

Description	Density Allowed	Maximum # of Dwelling Units
R-2-C Zone.....	7 to 12 D.U. per acre	47
TDR Program...	Up to 6 additional D.U. per acre	23
Max. Potential Development.....	Equivalent to 18 D.U. per acre	70
<i>Proposed Development.....</i>	<i>Equivalent to 17.9 D.U. per acre</i>	<i>70</i>

On October 31, 1989, the Development Agreement recorded as Instrument No. 89-174751 of official records of Ventura County. The current application for a Coastal Development Permit is an opportunity for the Planning Commission to determine that the proposed project is consistent and implement the Development Agreement.

**b) Coastal Land Use Plan Consistency:** The project is consistent with a number of policies contained in the Coastal Land Use Plan, as discussed in the following table.

POLICY	DISCUSSION
<p><b>Coastal Policy #8</b> (page III-13) states that the City may permit an increase in the allowable density of non-resource areas in return for the preservation of the resource area.</p>	<p>Approval of this project will ensure consistency with this policy. The Wooley Road property is the designated Receiving Area under the terms of the Settlement Agreement and Development Agreement, and therefore, preserves the resource area consisting of public beach and recreation uses.</p>
<p><b>Coastal Policy #23</b> (page III-20) states that new multi-family residential development shall be limited to a density of no more than 18 units per acre.</p>	<p>The project proposes a residential density of 17.9 units per acre, based on the original calculation of 3.91 gross acres (<math>70 / 3.91 = 17.9</math>). Therefore, the project will be consistent with this policy.</p>
<p><b>Coastal Policy #37</b> (page III-23) states that all new development in the coastal zone shall be designed to minimize impacts on the visual resources in the area.</p>	<p>Approval of this project will ensure consistency with this policy. The Wooley Road property is the designated Receiving Area under the terms of the Settlement Agreement and Development Agreement, and therefore, preserves the visual resources of the beachfront parcels that are utilized as public beach and recreation areas by the public.</p>
<p><b>Coastal Policy #38</b> (page III-23) states that height restrictions as defined by the City Zoning Ordinance shall be used to avoid blocking views.</p>	<p>The project proposes a maximum height of 34.5 feet, which is less than the maximum allowable height of 35 feet in the R-3-C zone of the Coastal Zoning Ordinance. Therefore, the project will be consistent with this policy.</p>
<p><b>Coastal Policy #39</b> (page III-24) states that all applications for subdivisions shall be reviewed for threats from hazards such as seismic activity, liquefaction, tsunami, beach erosion, flood, storm wave runup, and expansive soils.</p>	<p>Approval of this project, as mitigated, will ensure consistency with this policy. The project Initial Study documents the environmental review, hazard analysis, and mitigation measures to be incorporated into the development. In addition, the Wooley Road property is the designated Receiving Area under the terms of the Settlement Agreement and Development Agreement, and therefore, avoids all environmental impacts that would have otherwise resulted if the beachfront parcels had been developed with residential uses.</p>
<p><b>Coastal Policy #40</b> (page III-25) states that any person developing property within the 100-year flood line shall agree to indemnify and hold the City harmless from any</p>	<p>Approval of this project, as mitigated, will ensure consistency with this policy. The project Initial Study documents the environmental review (including 100-year flood hazard), and mitigation measures have been included to minimize the risk.</p>

<p>liability or damages resulting from the construction of his development.</p>	<p>In addition, the Wooley Road property is the designated Receiving Area under the terms of the Settlement Agreement and Development Agreement, and therefore, avoids all potential flood hazards that might have otherwise resulted if the beachfront parcels had been developed with residences.</p>
<p><b>Coastal Policy #41</b> (page III-28) states that all new development shall employ the most recent water conservation methods and devices.</p>	<p>The project will be designed and constructed to comply with the water conservation standards contained in the California Building Code (e.g. low-flow fixtures and appliances) and the City's landscape design guidelines. The project will be consistent with this policy.</p>
<p><b>Coastal Policy #42</b> (page III-25) states that proposed projects shall include consideration of the remaining water and sewer service capacities, and projects may be approved only when sufficient water/sewer services are available.</p>	<p>Approval of this project, as mitigated, would be consistent with this policy. The project Initial Study documents the environmental review (including water and sewer service capacities based on current data and information), and mitigation measures have been included to ensure sufficient levels of service.</p>
<p><b>Coastal Policy #47</b> (page III-34) states that all new development shall occur in a manner consistent with the Ventura County AQMP.</p>	<p>Approval of this project, as mitigated, will ensure consistency with this policy. The project Initial Study documents the environmental review (including air quality analysis), and mitigation measures have been included to ensure compliance with the Ventura County AQMP.</p>
<p><b>Coastal Policy #71</b> (page III-53) states that the City shall ensure public use of the vacant oceanfront lots in the Oxnard Shores Neighborhood, including use of the Transfer of Development Rights program.</p>	<p>The Wooley Road property is the designated Receiving Area according to the Transfer of Development Rights program, and ensures public use of specified vacant oceanfront lots in the Oxnard Shores Neighborhood under the terms of the Settlement Agreement and Development Agreement. Therefore, the project will implement this policy.</p>
<p><b>Coastal Policy #92</b> (page III-62) states that transfer of development rights may be permitted to provide a procedure for relocating development that would otherwise adversely affect coastal access and recreation.</p>	<p>The Wooley Road property is the designated Receiving Area according to the TDR program, and relocates future development from specified vacant oceanfront lots to the Wooley Road property under the terms of the Settlement Agreement and Development Agreement. Therefore, project will implement this policy.</p>

- c) **Conformance with Coastal Zoning:** The proposed development is located in the Coastal Medium Density Multiple-Family (R-3-C) zoning district, and the proposed development may be permitted with a Coastal Development Permit and Tentative Subdivision Map. The project complies with the applicable development standards of the R-3-C zone as listed in the following table.

DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Building Height §17-14(D)(1)	Not to exceed 3 stories or 35 feet	3 stories & 34.9 ft.	Yes
Maximum Density §17-14(D)(2)	1 unit per 2,400 sq.ft. (min.) of lot area. Equivalent to 18 units per acre.	1 unit per 2,433 sq.ft. of lot area; 17.9 units / acre.	Yes
Minimum lot width §17-14(D)(3)	60 feet minimum	Approx. 200 feet each lot	Yes
Front yard setback §17-14(D)(5)	15 feet if all vehicle access is off alley	15 feet (avg.) & 0 feet (min.)	Yes, if Variance approved
Side yard setbacks §17-14(D)(7)	10 feet for three-story structures	10 feet (avg.) & 4 feet (min.)	Yes, if Variance approved
Rear yard setback §17-14(D)(6)	25 feet	25 feet (avg.) & 5 feet (min.)	Yes, if Variance approved
Extension of Open Stairways §16-306	Open, unenclosed stairways not covered by a roof may extend into a required front yard not more than 30 inches.	Extend 15 feet to front property lines	Yes, if Variance approved
Interior Yard Space §17-14(D)(4) & §17-14(E)	25% of lot area as IYS; and 25% of required IYS may be on building decks having min. dimension of 10 feet and min. area of 200 sq.ft.	42,574 sq.ft. (27%) as common IYS; 6,000 sq.ft. (4%) as private IYS.	Yes
Fences, walls, hedges – front yard §16-308	Must be at least 50% open, and may not exceed 42 inches. If a solid wall, then may not exceed 18 inches in height.	Garden walls: Greater than 18 inches high. Pilasters: vary, 2.5 – 5 ft. high. Screen wall at transformers:	Yes, if Variance approved

DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
		4 to 5 feet max.	
Fences, walls, hedges – side & rear yards §16-309(A)	Not to exceed 7 ft. in height, and not located within required setback areas. If located within the required front or side yard, see §16-308.	6 feet (max.)	Yes
Building Separation §16-362(B)	Equal to the height of the taller structure (35 ft. minimum).	30 feet (not including stairs)	Yes, if Variance approved

Source: Coastal Zoning Ordinance Section 17-14.

In addition to the R-3-C development standards, City Council Resolution No. 7658 provides advisory standards that are intended to encourage amenities that enhance the livability of condominiums. The proposed project complies with several of the advisory standards as listed in the following table.

ADVISORY STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Number of Bedrooms	Each unit shall have no fewer than 2 separate bedrooms	10 units w/ 3BR, 51 units w/ 2 BR, 9 units w/ 1 BR.	No
Dwelling Size	Each unit shall not be smaller in area than 1,000 sq.ft.	1,006 sq.ft. minimum	Yes
Distance to Parking Spaces	– Resident Parking: 50 ft. max. distance to spaces from the dwelling units they serve. – Visitor Parking: 100 ft. max. distance to spaces from the dwelling units they serve.	Resident parking spaces more than 50 feet to the units they serve; spaces are within same building as the unit served. Visitor spaces are within 100 ft.	No  Yes
Private Patios	– Minimum outdoor patio area not less than 20% of GFA of the unit it serves. – Min. dimensions 10 ft. by 10 ft. – Immediately adjacent and direct access to the unit served. – Minimize overview to adjacent patios. – Lights not to exceed 6 ft. height. – Patios adjacent to common open areas used by residents.	– Varies between 6.5% to 27% per unit – Min. 10' x 10' – Adjacent and accessible to each unit  – Patios adjacent to common open space area	No

ADVISORY STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Common Areas	<ul style="list-style-type: none"> <li>- Formal: Jacuzzi, spa, clubhouse, barbecues, picnic tables, etc.</li> <li>- Informal: Lawn area with min. dimensions of 45 ft. and area not less than 3,500 sq.ft.</li> <li>- Children: Tot lot, jungle gym.</li> </ul>	Jacuzzi / spa, pool, clubhouse, tot lot, and barbecue areas.	Yes
Storage	<ul style="list-style-type: none"> <li>- 1 private storage area per unit.</li> <li>- Minimum area: 225 cubic ft.(ft.<sup>3</sup>).</li> <li>- Minimum width: 4 ft.</li> </ul>	<ul style="list-style-type: none"> <li>- All units have storage areas</li> <li>- Min. 225 ft.<sup>3</sup></li> <li>- Min. width 4 ft.</li> </ul>	Yes, as conditioned
Security	<ul style="list-style-type: none"> <li>- Lockable entrances</li> <li>- Electric garage door openers</li> <li>- Similar features</li> </ul>	<ul style="list-style-type: none"> <li>- Gates &amp; locks at entrances</li> <li>- Electric garage gate openers</li> </ul>	Yes
Handicap Accessibility	Elevators shall be provided in structures of 3 or more stories	1 per building (up to second floor)	Yes, as conditioned

Source: City Council Resolution No. 7658.

d) **Site Design:** The site layouts on Lots 1 – 4 were influenced by the shape of the property and means of access from the alley. Each building will face the street frontages on Wooley Rd, Harbor Blvd, or Canal Street. Parking areas will be completely screened from view from Wooley Rd and Harbor Blvd by structure placement and walls. By utilizing the alley for all vehicle access, the minimum building front setback may be achieved at 15 feet. Lot 5 will take access directly from Canal Street. The minimum requirements for the side, street-side, and rear setbacks are also provided, except for the specified architectural projections and stairways subject to the Variance. Common open space areas and amenities are provided throughout the site for easy access by the residents, and will be maintained by the Homeowner’s Association. The proposed lot sizes are listed in the following table.

Lot Number	Proposed Land Use	Number of Units	Lot Size
1	Residential Condominiums	14	39,640 sq.ft. / 0.91 acre
2	Residential Condominiums	14	27,007 sq.ft. / 0.62 acre
3	Residential Condominiums	14	27,878 sq.ft. / 0.64 acre
4	Residential Condominiums	14	27,007 sq.ft. / 0.62 acre
5	Residential Condominiums	14	32,670 sq.ft. / 0.75 acre
-	Wooley Rd. dedication	-	8,704 sq.ft. / 0.20 acre
-	Canal St. dedication	-	9,012 sq.ft. / 0.22 acre
	<i>Total</i>	70	3.96 acres (gross) 3.54 acres (net after dedications)

e) **Circulation and Parking:** On-site circulation for Lots 1 – 4 is provided by the alley between Harbor Boulevard and Canal Street. Resident parking will be provided within enclosed garages located on the first floor of each structure, access will be provided from the alley, and spaces will be designated for each unit. The parking area for Lot 5 has a driveway to Canal Street without an alley. Visitor parking will be open and uncovered on all lots. The project meets the requirements for all parking standards as listed in the following table.

PARKING STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Number of Resident Spaces §16-622	For units with 2 or more bedrooms: 2 garage spaces per unit, and assigned to a specific unit. <i>Minimum: 131</i>	150 spaces	Yes
Number of Visitor Spaces §16-622	1 space per unit for the first 30 units, and 0.5 space per unit thereafter. <i>Minimum: 50</i>	50 spaces	Yes
Residential Subterranean Garages §16-626	Permitted for residential developments with minimum lot size of 25,000 sq.ft. All visitor parking must be provided at grade level.	None – garages on first floor.	N/A
Dimensions §16-638	Standard: 9 ft. wide & 19 ft. long.	9 ft. by 19 ft.	Yes
Landscaping §16-641	Minimum of 5% of the parking area shall be landscaped.	5,633 sq.ft. (22%)	Yes

A traffic study was prepared for the project and approved by the City’s Traffic Engineer, and no significant impacts will occur after implementation of mitigation measures. A number of off-site improvements will be constructed with the project in order to improve traffic circulation in the vicinity, including the following: dedication and construction of a right-turn lane on westbound Wooley Road; a new 6-foot wide Class II bike lane on both sides of Wooley Road; modification of traffic signals at the intersection of Harbor Boulevard and Wooley Road; new sidewalks, ramps, curb/gutter, storm drains, and street repaving adjacent to the project site; and placing utility lines underground along the north side of Wooley Road adjacent to the project site.

f) **Building Design:** A total of five structures are proposed with a Spanish Mediterranean style, and will be compatible with the retail center across Harbor Boulevard. The proposed height, at three stories and 35 feet maximum, complies with the requirements of the R-3-C zone. Adjacent multi-family dwellings in the Dunes Neighborhood in the R-2-C zone are one and two stories high, and the placement of the proposed structures will not have a towering effect on adjacent residences. The density of the proposed development is similar to the

adjacent neighborhood, and the proposed façade design will help contain the activities, noise, light, parking areas, etc. The prominent entrances that will be visible from Harbor Boulevard and Wooley Road are designed as curving staircases that rise to the second floor. Doors to the individual units are provided from the interior common areas, and therefore, the design successfully conceals the entries to the units, common courtyards, recreational amenities, and resident activities. The ancillary structures such as the recreation buildings, mailbox kiosk, and screen walls are designed to match the Spanish Mediterranean theme.

There will be a total of 11 floor plans that provide variation to the square-footage and interior layouts. Six of the 11 floor plans have the same layout, except with minor variations such as the placement of stairways or orientation of rooms. The alternates are provided for models A/A1, B/B1, C/C1, H/H2, J/J1, and L/L1 (see pages 24 and 25 of the full-size plans for illustrations).

Unit Type	Total Floor Area (Square Feet)	Number of Beds / Baths	Unit Quantity
A	1,849	2 / 3	3
A1	1,858	2 / 3	1
B	1,818	2 / 3	5
B1	1,848	2 / 3	1
C	1,742	2 / 2	10
C1	1,672	2 / 2	1
D	1,551	1 / 2	1
E	1,006	1 / 1	8
F	1,410	2 / 2	8
G	1,433	2 / 2	2
H	1,561	2 / 2½	2
H2	1,562	2 / 2½	8
J	1,546	2 / 2½	6
J1	1,550	2 / 2½	4
K	1,951	3 / 2½	5
L	1,939	3 / 2½	1
L1	1,987	3 / 2½	4
<i>Total</i>			70

Development Services Division staff has reviewed the conceptual drawings and noted that the third floor of each structure may not comply with requirements for handicap accessibility and elevators. A licensed architect will be required to prepare and submit construction drawings for building permits. It has not yet been determined if the potential need for elevators up to the third floors may affect the proposed designs of the interior courtyards, exteriors of the structures, or rooflines.

- g) Landscaping and Open Space:** The development standard for Interior Yard Area requires a minimum of 25% (38,732 square-feet) of the site area and minimum dimensions of 15 feet, and is satisfied by providing approximately 26% (40,492 square-feet). The common open space includes on-site amenities such as a pool, spa, tot lot, and recreation room. The full-size plans (page 2.2) include a full-color site plan illustrating the interior yard areas. Private open space, in the form of a private patio and/or balcony, is provided for every unit and varies between 102 and 271 square-feet each. The patio/balcony areas totaling approximately 8,307 square-feet are in addition to the common open space areas. A minimum of 5% of the parking lot area must be landscaped (not included in the IYS area calculation) and 22% will be provided. Although the project is not required to construct a public park or dedicate land for a public park, the developer will be required to pay the applicable Quimby Fee prior to issuance of any building permits.
- h) Tentative Subdivision Map:** The proposed subdivision is consistent with the Subdivision Map Act, City Code (Chapter 15 – Subdivisions), Coastal Land Use Plan, Coastal Zoning Ordinance and the Development Agreement. The Development Agreement allows up to 70 dwelling units on the property, specifically, condominiums or apartments. The proposal complies with the requirements for lot area, density, improvement and design, floodwater drainage control, public roads and improvements, water and sewer facilities, and environmental protection.
- i) Coastal Development Permit Request:** The proposed use is consistent with R-3-C zoning as defined in the Coastal Zoning Ordinance, as well as the policies and objectives of Coastal Land Use Plan. The project will not impair the integrity and character of the zone or general area, and will be compatible with existing and future land uses in the vicinity. The project site is physically suitable for the proposed development, and adequate public services are available to serve the project. Approval of the project will implement the Development Agreement, will protect and maintain coastal resources, will ensure public access to the coast and beaches, and will avoid or reduce cumulative environmental impacts.
- j) Coastal Variance Request:** A variance is requested for: a) five stairways (one per building) within the 15-foot front setback, and b) designated architectural projections into the street-side, interior side, and rear setback areas due to the shape of the property. The average distances for exterior walls will comply with the setback requirements, however, certain architectural projections and wall sections do not comply. The specific areas are listed in the following table, and include seven staircases, two sets of vertical columns, portions of two rear walls, and a portion of one side wall. The full-size plans (page 2.1) include a full-color site plan illustrating locations of the Variance numbers.

Variance Number	Description	Minimum Setback (ft)	Proposed Setback (ft)	Variance Request (ft)
1	Stairway	10	5	5
2	Rear Wall	25	5	20
3	Stairway	15	3	12
4	Rear Wall	25	19	6
5	Stairway	15	0	15
6	Column	25	23	2
7	Column	25	23	2
8	Stairway	15	0	15
9	Column	25	23	2
10	Column	25	23	2
11	Column	10	3	7
12	Stairway	10	4	6
13	Stairway	15	0	15
14	Column	10	8.5	1.5
15	Side Wall	10	4	6
16	Column	10	6	4

**k) Coastal Commission Jurisdiction:** According to the *Post-LCP Permit and Appeal Jurisdiction Map No. 131 (Oxnard Quadrangle)* adopted on November 20, 1985, a portion of the project site is within the permit/appeal jurisdiction of the California Coastal Commission. The project may be appealed to the Coastal Commission, and the Coastal Commission also has the discretion to require that the developer obtain a coastal permit prior to construction.

Coastal Commission staff is currently in the process of updating the map, and is considering a proposal to remove the subject property from the permit jurisdiction area (i.e. the developer would not need to obtain a permit directly from the Coastal Commission). The subject property would remain within the appeal jurisdiction area, similar to the majority of other properties in the vicinity. Coastal Commission staff expects the map update to be presented at a Coastal Commission hearing in December 2010.

**l) In-Lieu Affordable Housing Fee:** The City Council approved Pre-Application No. 09-600-1 on April 21, 2009, authorizing the developer to submit an affordable housing payment (estimated to be approximately \$350,000) in lieu of providing affordable units within the proposed project. There will be no dwelling units that will be specifically designated for households defined as very low, low, or moderate income.

**7) Development Advisory Committee:** The Development Advisory Committee (DAC) reviewed this project on March 25, 2009. No significant changes to the project design or site layout were necessary as a result of the DAC review. Requirements and conditions of the DAC are included in the attached resolutions.

**8) Community Workshop:** The Community Workshop was conducted on August 17, 2009. The applicant mailed notices of the Community Workshop meeting to all property owners within the Oxnard Dunes and Oxnard Shores neighborhoods, and two signs were posted on the project site 10 days prior to the meeting. Approximately 15 members of the public attended the meeting, most of whom were residents of the Oxnard Dunes or Oxnard Shores neighborhoods. The comments and concerns focused on the proposed number of units (i.e. density), structure height, potential traffic impacts, parking, proposed roadway improvements, and potential impacts to public parks. Subsequently, staff revised the project Initial Study in order to provide additional information and address environmental concerns.

**Appeal Procedure:** The Planning Commission's action is a recommendation, and the applications will be considered by the City Council in a noticed public hearing at a later date.

**Attachments:**

- A. Maps (Vicinity, Zoning, Aerial)
- B. Reduced Project Plans
- C. Mitigated Negative Declaration No. 09-03, with comments
- D. Resolutions

Prepared by: <u></u> BF
Approved by: <u></u> SM

# Vicinity Map



## Vicinity Map



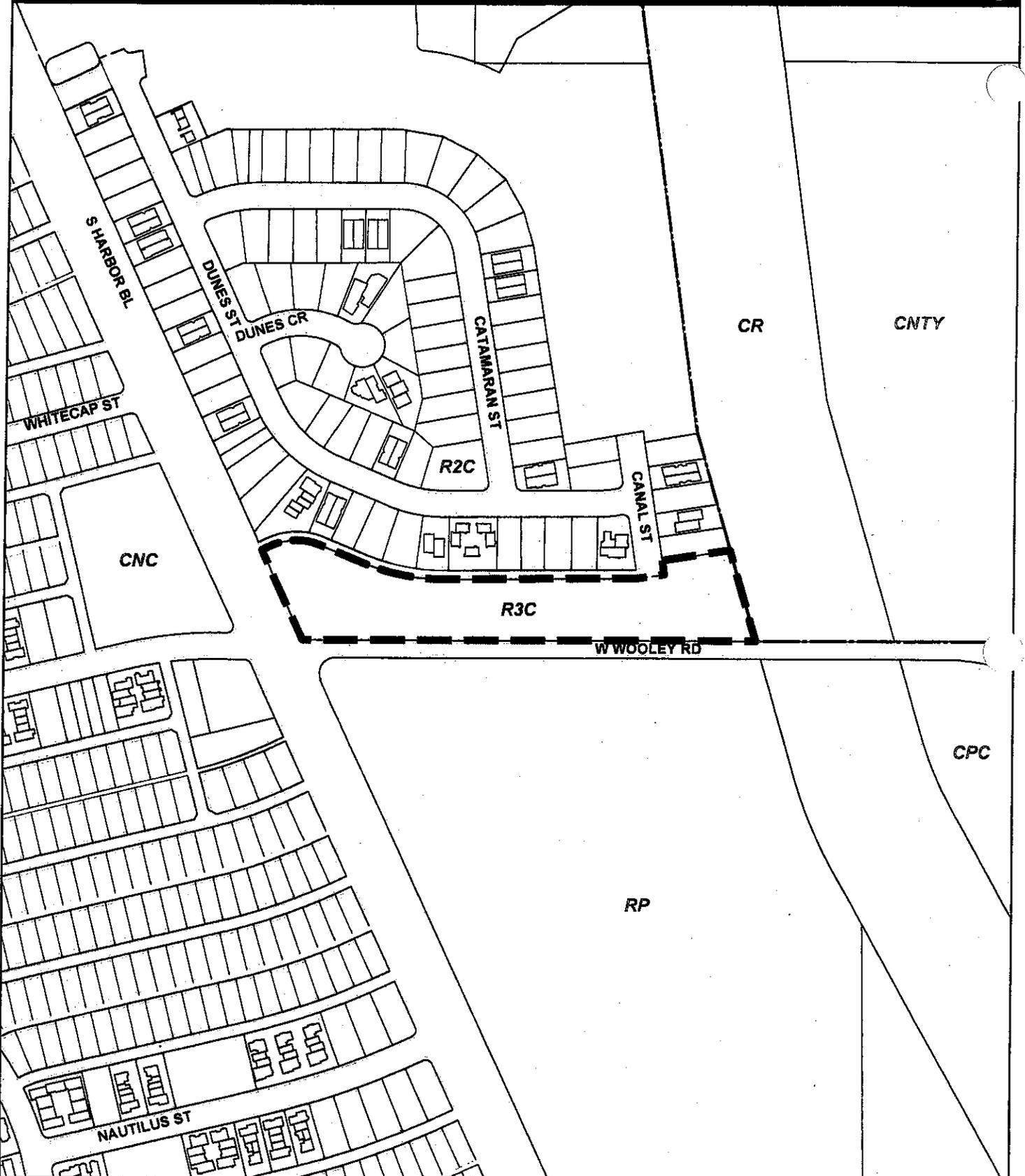
0 0.25 0.5 1 1.5 2 Miles



Oxnard Planning  
October 6, 2008

PZ 08-400-04  
Location: NE Harbor Bl & Wooley Rd.  
APN: 196003329  
The Shores Townhouses

ATTACHMENT 8  
PAGE 15 OF 185



Oxnard Planning  
October 6, 2008

PZ 08-400-04  
Location: NE Harbor Bl & Wooley Rd.  
APN: 196003329  
The Shores Townhouses



Zone Map

ATTACHMENT 8

PAGE 16 OF 185

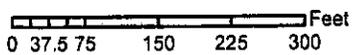


1:3,498



October 6, 2008

PZ 08-400-04  
Location: NE Harbor Bl & Wooley Rd.  
APN: 196003329  
The Shores Townhouses



Aerial Map

ATTACHMENT 8

PAGE 17 OF 185

2007 Aerial



1:2,239





**VARIANCES REQUESTED**

**BUILDING 1**

- ① STAIRWAY PROJECTS INTO 15' FRONT YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 5'-4" FOR A DISTANCE OF 29 FEET.
- ② NORTHERN CORNER OF BUILDING PROJECTS INTO 15' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 15'-11" FOR A DISTANCE OF 50 FEET.

**BUILDING 2**

- ③ STAIRWAY PROJECTS INTO 15' FRONT YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 4'-2" TO 14'-4" FOR A DISTANCE OF 29 FEET.
- ④ BUILDING PROJECTS INTO 15' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 9'-4" FOR A DISTANCE OF 18 FEET.

**BUILDING 3**

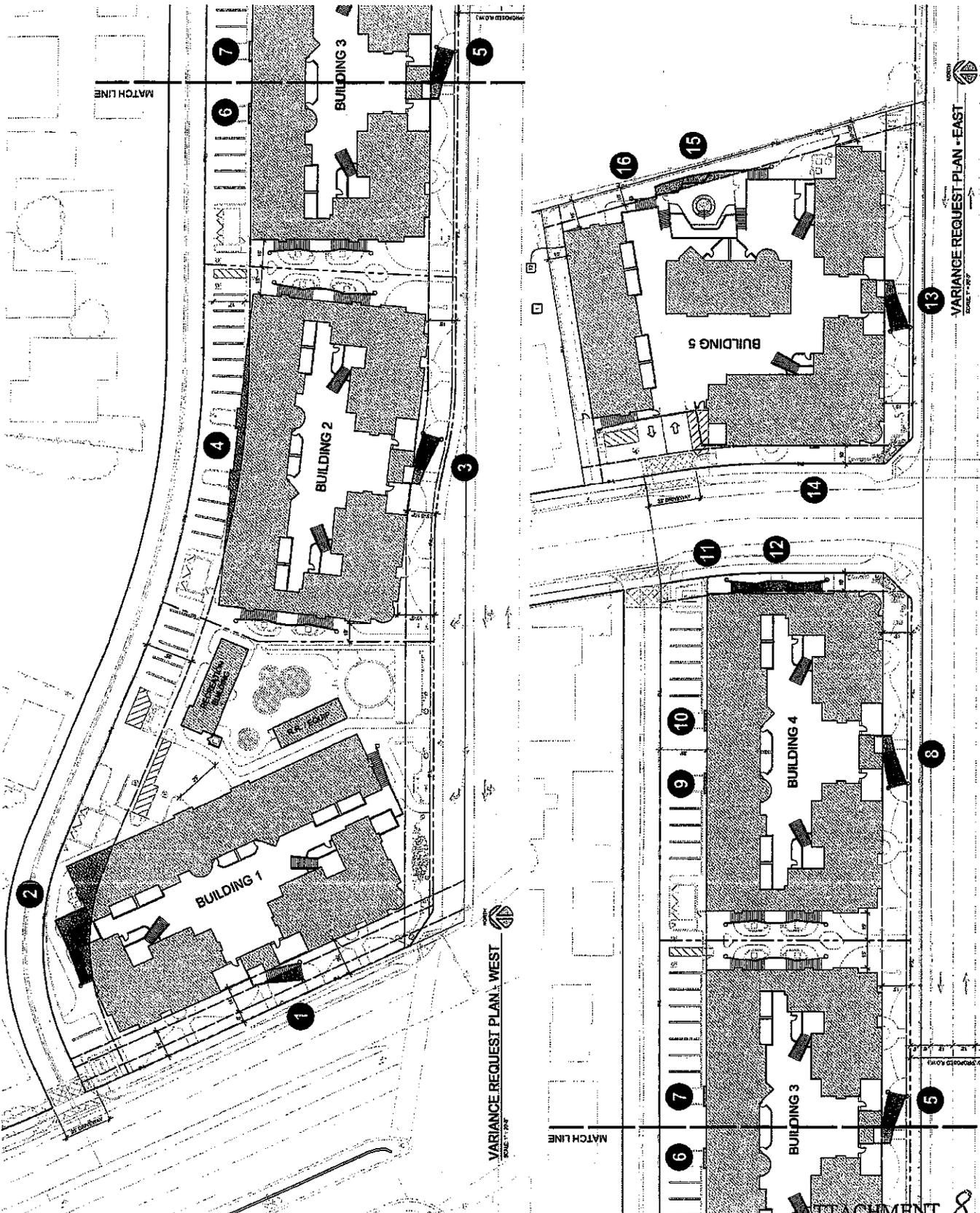
- ⑤ STAIRWAY PROJECTS INTO 15' FRONT YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 4'-2" TO 14'-4" FOR A DISTANCE OF 29 FEET.
- ⑥ BUILDING PROJECTS INTO 25' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 11 FEET TO 11 FEET.
- ⑦ BUILDING PROJECTS INTO 25' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 11 FEET TO 11 FEET.

**BUILDING 4**

- ⑧ STAIRWAY PROJECTS INTO 15' FRONT YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 4'-2" TO 14'-4" FOR A DISTANCE OF 29 FEET.
- ⑨ BUILDING PROJECTS INTO 15' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 11 FEET.
- ⑩ BUILDING PROJECTS INTO 25' REAR YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 11 FEET TO 11 FEET.

**BUILDING 5**

- ⑪ STAIRWAY PROJECTS INTO 15' FRONT YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM 4'-2" TO 14'-4" FOR A DISTANCE OF 29 FEET.
- ⑫ BUILDING PROJECTS INTO 15' STREET SIDE YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 1'-3" FOR A DISTANCE OF 5 FEET.
- ⑬ COURTYARD WALL PROJECT INTO 15' SIDE YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 3'-0" FOR A DISTANCE OF 51 FEET.
- ⑭ STAIR PROJECTS INTO 15' SIDE YARD SETBACK (OWNARD CITY CODE SEC. 17-4.0.2) FROM ZERO TO 5'-0" FOR A DISTANCE OF 11 FEET.



OXNARD SHORES COMPANY, LLC

10000 OXNARD ROAD, SUITE 100  
OXNARD, CA 93030

**DRAPHS**

3000 OXNARD ROAD, SUITE 100  
OXNARD, CA 93030

PROPOSED 70 UNIT TOWNHOME COMPLEX

**THE ANACAPA TOWNHOMES**

WOOLEY ROAD, OXNARD, CA

P-2.1

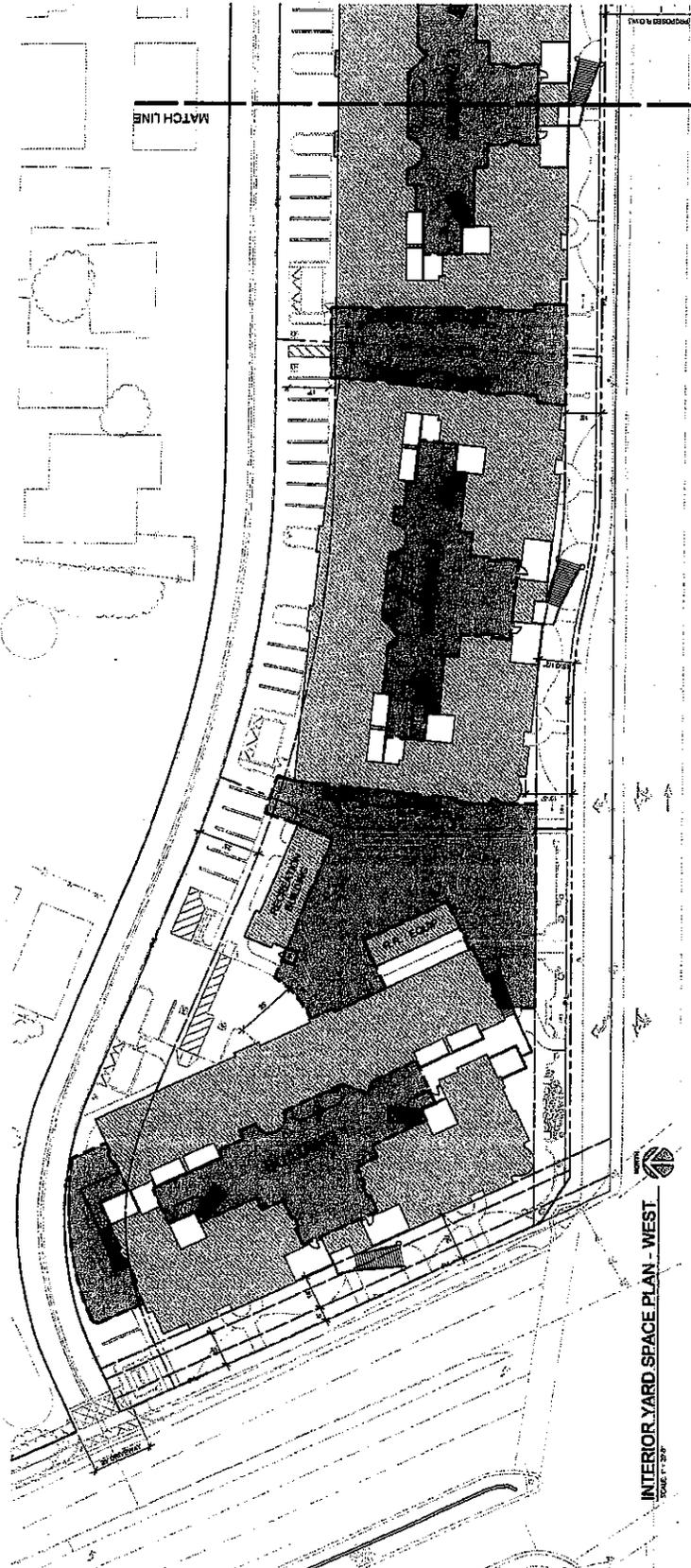
DATE: \_\_\_\_\_

APPROVED: \_\_\_\_\_

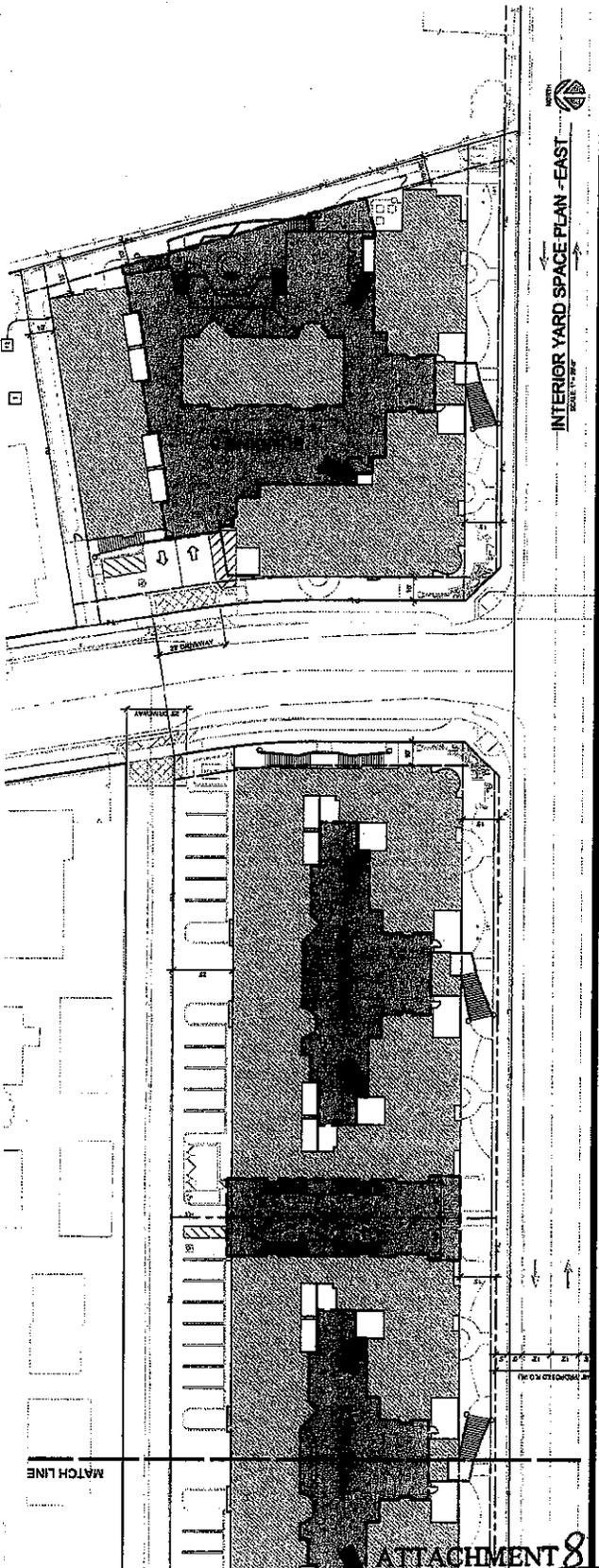
DATE: \_\_\_\_\_

**INTERIOR YARD SPACE**

COLORED AREA INDICATES INTERIOR YARD SPACE PROVIDED PER OXNARD CITY CODE SECTION 16.04.010. THIS SPACE IS TO BE PROVIDED WITHIN THE SETBACK LINE TO REAR PROPERTY LINE, INCLUDING PORTIONS OF FOOTING COURTYARDS (MIN. 10' DIMENSION).  
 SITE AREA = 154,827 S.F.  
 25% OF SITE AREA REQUIRED = 38,707 S.F.  
 INTERIOR YARD SPACE PROVIDED = 40,462 S.F.



**INTERIOR YARD SPACE PLAN - WEST**  
 SCALE: 1/8" = 1'-0"



**INTERIOR YARD SPACE PLAN - EAST**  
 SCALE: 1/8" = 1'-0"

DATE:	10/15/2010
BY:	ASD
CHECKED BY:	ASD
DESIGNED BY:	ASD
SCALE:	AS SHOWN
PROJECT:	OXNARD SHORES
DRAWN BY:	ASD

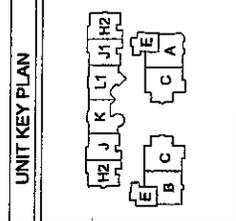
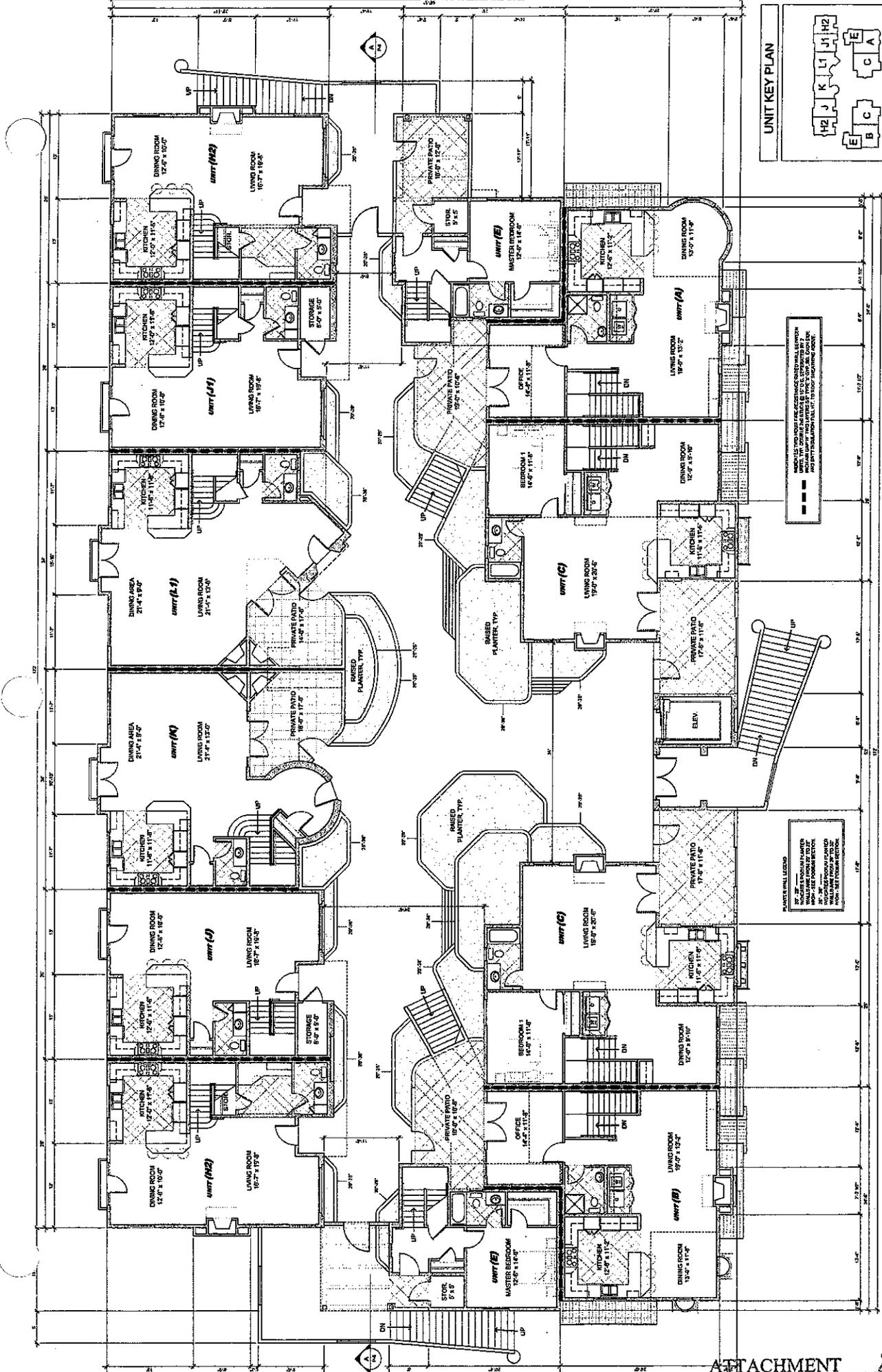
**OXNARD SHORES COMPANY, LLC**  
 9115 S. HARBOR BLVD.  
 OXNARD, CA 93025  
 TEL: (805) 306-0200  
 FAX: (805) 306-0200



**GRAPHICS**  
 2010 State Home Center, Inc. 10/10/10  
 info@statehomecenter.com  
 1-800-888-8888 (TOLL FREE)  
 2010 PRESERVATION - REPAIRS - GREEN

**PROPOSED 70 UNIT TOWNHOME COMPLEX**  
**THE ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA





INDICATES VOID ROOMS AND EXISTENCE OF WALL BETWEEN UNITS. WALLS ARE SHOWN WITH DASHES. ALL WALLS ARE PARTITION WALLS UNLESS NOTED OTHERWISE. ALL PARTITION WALLS ARE 1/2\"/>

PLASTER WALL LEGEND  
 1/2\"/>

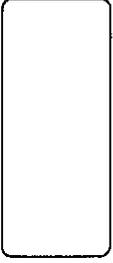


PROPOSED BUILDING ONE (1) - PODIUM "COURTYARD" LEVEL  
 SCALE: 3/8" = 1'-0"

P-4  
 SHEET 1 OF 24

PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOOLEY ROAD, OXNARD, CA

Architecture: David Brown  
 Steven B. Walker  
**DR P H I C S**  
 15115 Shady Grove Court, Suite 115, Dallas, TX 75244  
 972-443-8888  
 www.drpgraphics.com  
 3-D PRESENTATION - DRAFTING - RENDER



**OXNARD SHORES COMPANY, LLC**  
 11555 WASHINGTON AVE  
 OXNARD, CA 93030  
 805-328-1122  
 www.oxnardshores.com

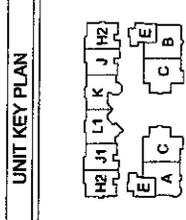
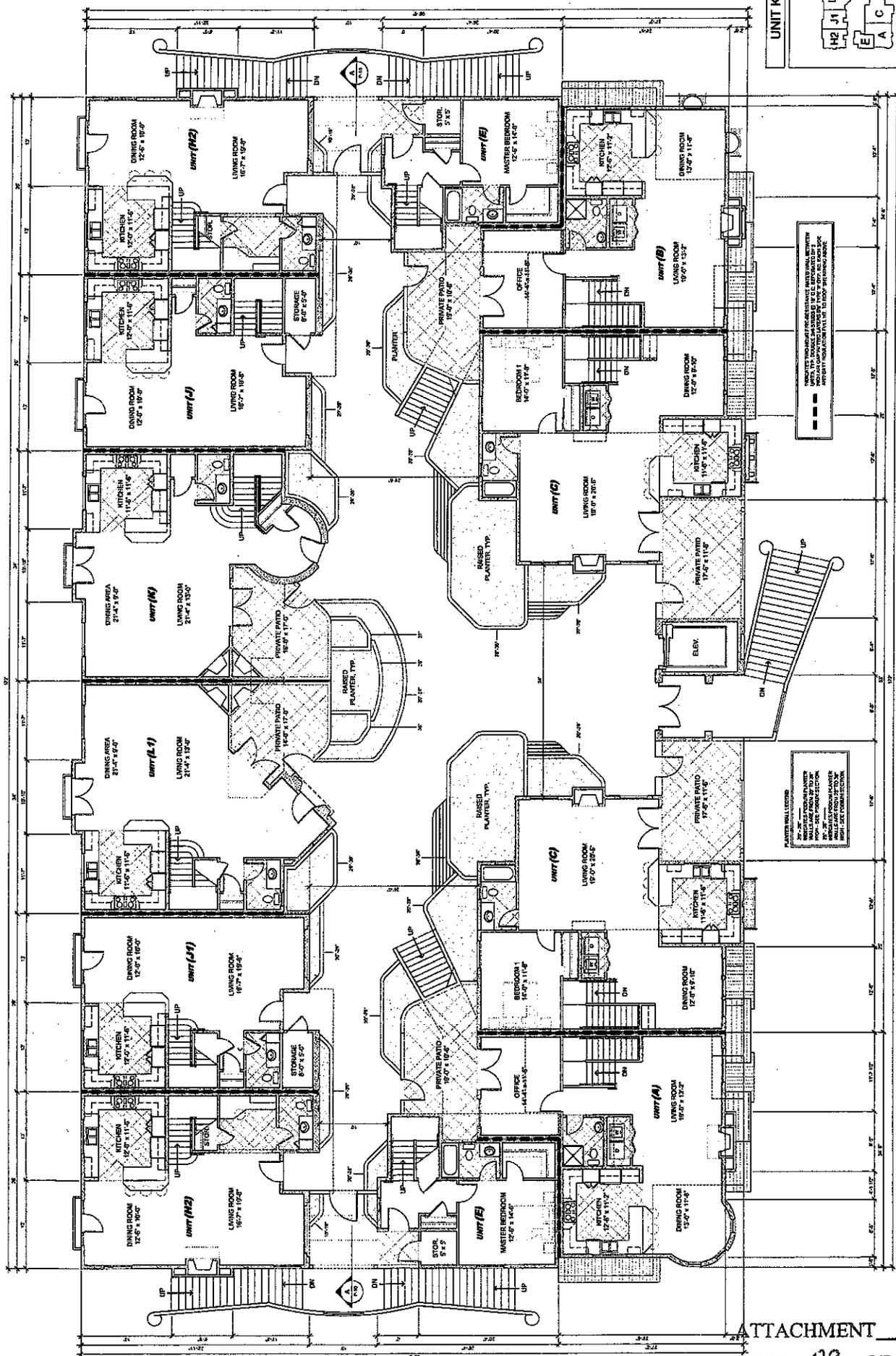
DATE	DESCRIPTION	BY	CHKD
08/11/11	ISSUED FOR PERMITS	DAVID BROWN	STEVEN B. WALKER
08/11/11	ISSUED FOR PERMITS	DAVID BROWN	STEVEN B. WALKER
08/11/11	ISSUED FOR PERMITS	DAVID BROWN	STEVEN B. WALKER











UNIT KEY PLAN

UNIT (A) THROUGH (M) ARE SHOWN WITH THE LIVING ROOM, DINING ROOM, KITCHEN, AND PRIVATE PATIO. THE MASTER BEDROOM IS SHOWN WITH THE BATH AND WALK-IN CLOSET. THE OFFICE IS SHOWN WITH THE STORAGE. THE PRIVATE PATIO IS SHOWN WITH THE PLANTER. THE BASED PLANTER IS SHOWN WITH THE PLANTER WALL LECTURE.

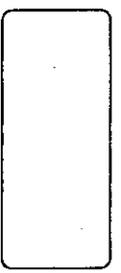
PLANTER WALL LECTURE  
 2'-0\"/>

PROPOSED BUILDING TWO (2) - PODIUM "COURTYARD" LEVEL  
 DATE: 08-17-16

P-8  
 11.0' x 11.0'

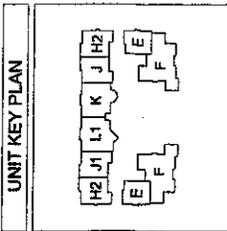
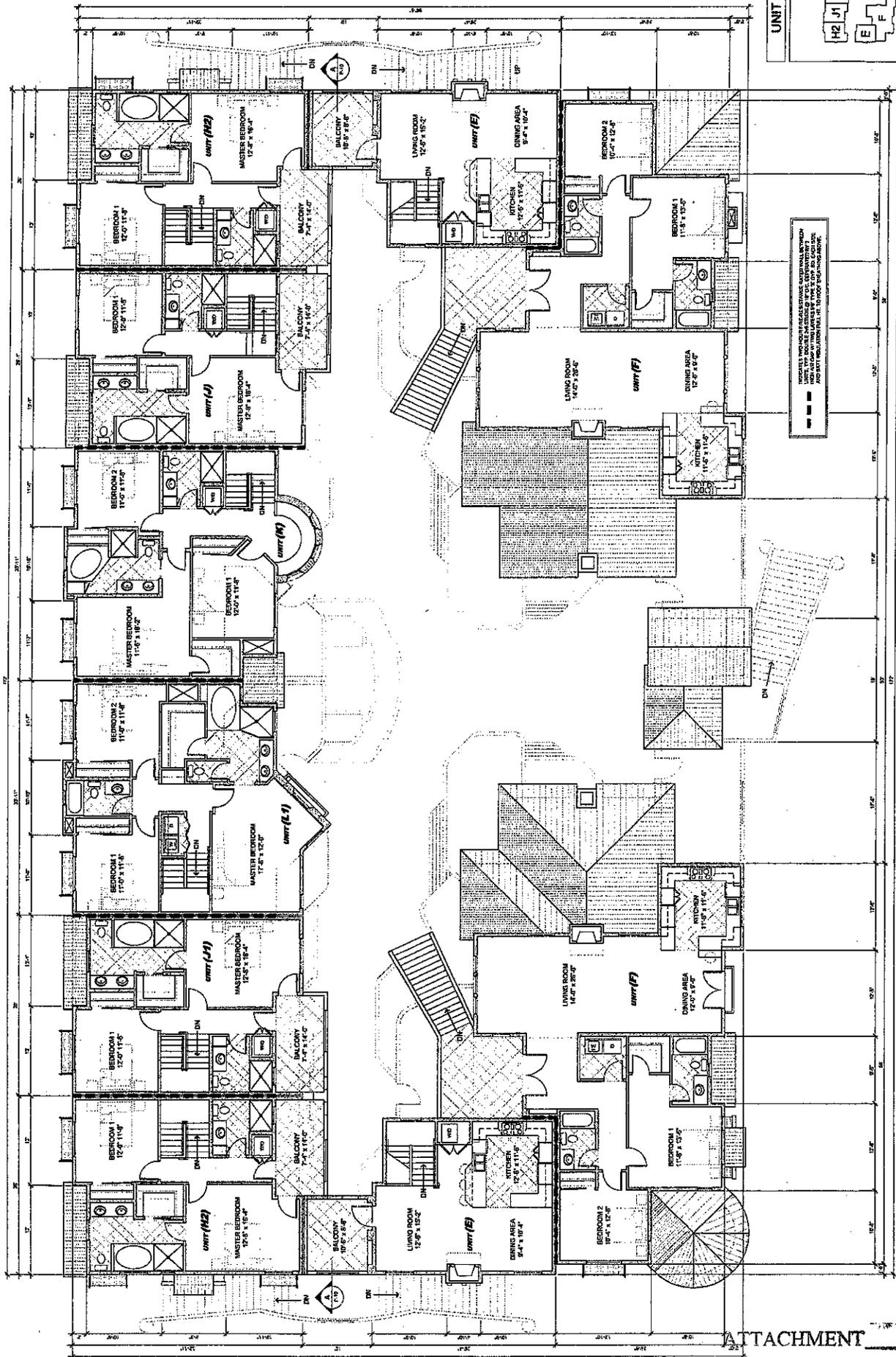
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA

Andrew Blvd Beer  
 Steven H. Oshover  
**DRAPHTICS**  
 877 Thorne Ave.  
 Oxnard, CA 93020  
 805-488-8888  
 www.draphtics.com  
 CA 93020  
 11.0' x 11.0'



**OXNARD SHORES COMPANY, LLC**  
 11.0' x 11.0'

DATE:	08-17-16
BY:	AS/ML
CHECKED:	AS/ML
APPROVED:	AS/ML
DATE:	08-17-16
BY:	AS/ML
CHECKED:	AS/ML
APPROVED:	AS/ML



P-9

# PROPOSED 70 UNIT TOWNHOME COMPLEX

## THE ANACAPA TOWNHOMES

WIDLEY ROAD, OXNARD, CA

Architect: David Brizer  
Shawn B. Olinde  
**DR A P H I C S**  
200 Thorne Avenue, Oxnard, CA 93030  
and/or fax to (805) 481-1111 www.dapgraphics.com  
P L P P R E S E N T A T I O N - S E R V I C E S - S E R V I C E S



**OXNARD SHORES COMPANY, LLC**  
7400 150th Street  
Oxnard, CA 93030

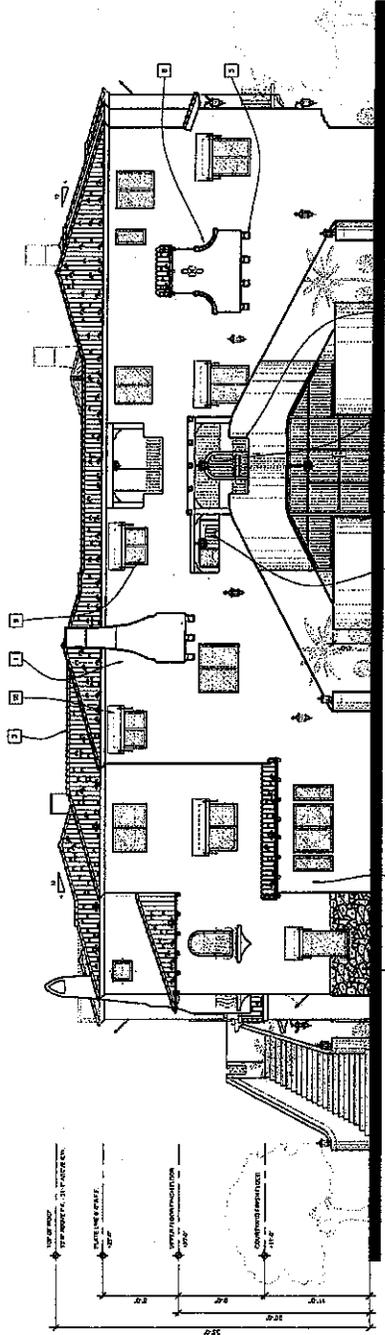
PROPOSED BUILDING TWO (2) - UPPER "BIRDS EYE" LEVEL  
SCALE: 1/4" = 1'-0"

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMITS	08/11/11
2	ISSUED FOR PERMITS	08/11/11
3	ISSUED FOR PERMITS	08/11/11
4	ISSUED FOR PERMITS	08/11/11
5	ISSUED FOR PERMITS	08/11/11
6	ISSUED FOR PERMITS	08/11/11
7	ISSUED FOR PERMITS	08/11/11
8	ISSUED FOR PERMITS	08/11/11
9	ISSUED FOR PERMITS	08/11/11
10	ISSUED FOR PERMITS	08/11/11

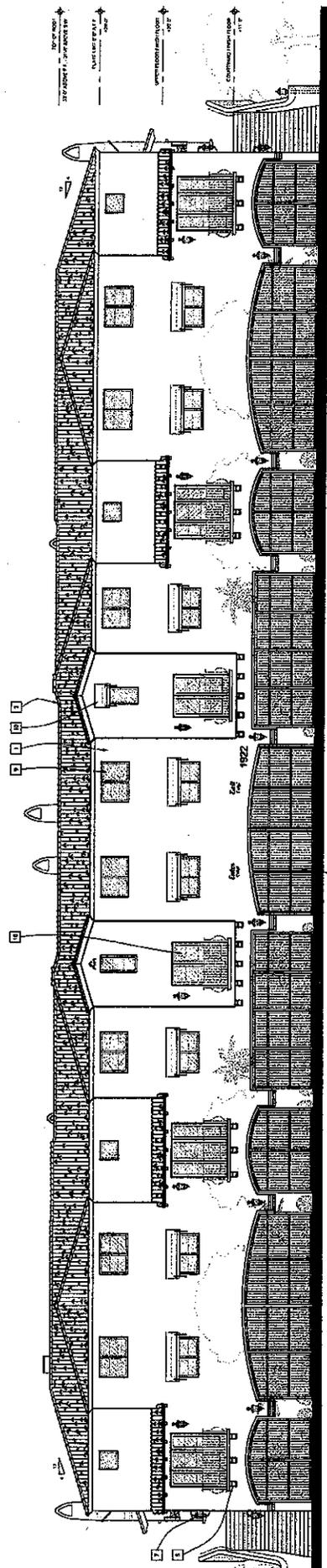


**KEY NOTES:**

- 1 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 2 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 3 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 4 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 5 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 6 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 7 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 8 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 9 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 10 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 11 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 12 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 13 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 14 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 15 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 16 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 17 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 18 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 19 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 20 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 21 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 22 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 23 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 24 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 25 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 26 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 27 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 28 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 29 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 30 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 31 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 32 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 33 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 34 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 35 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 36 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 37 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 38 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 39 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 40 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 41 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 42 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 43 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 44 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 45 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 46 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 47 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 48 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 49 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 50 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 51 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 52 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 53 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 54 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 55 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 56 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 57 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 58 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 59 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 60 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 61 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 62 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 63 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 64 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 65 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 66 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 67 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 68 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 69 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 70 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 71 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 72 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 73 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 74 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 75 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 76 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 77 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 78 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 79 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 80 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 81 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 82 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 83 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 84 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 85 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 86 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 87 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 88 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 89 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 90 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 91 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 92 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 93 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 94 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 95 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 96 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 97 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 98 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 99 EXTERIOR STUCCO FINISH BY WALKER STUCCO.
- 100 EXTERIOR STUCCO FINISH BY WALKER STUCCO.



PROPOSED BUILDING TWO EAST "SIDEYARD" ELEVATION  
SCALE: 3/8" = 1'-0"



PROPOSED BUILDING TWO NORTH "ALLEY" ELEVATION  
SCALE: 3/8" = 1'-0"

**OXNARD SHORES COMPANY, LLC**  
1110 S. WATSON BLVD.  
OAKLAND, CA 94612  
TEL: (925) 361-1500  
FAX: (925) 361-1501

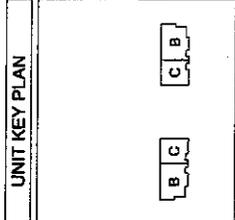
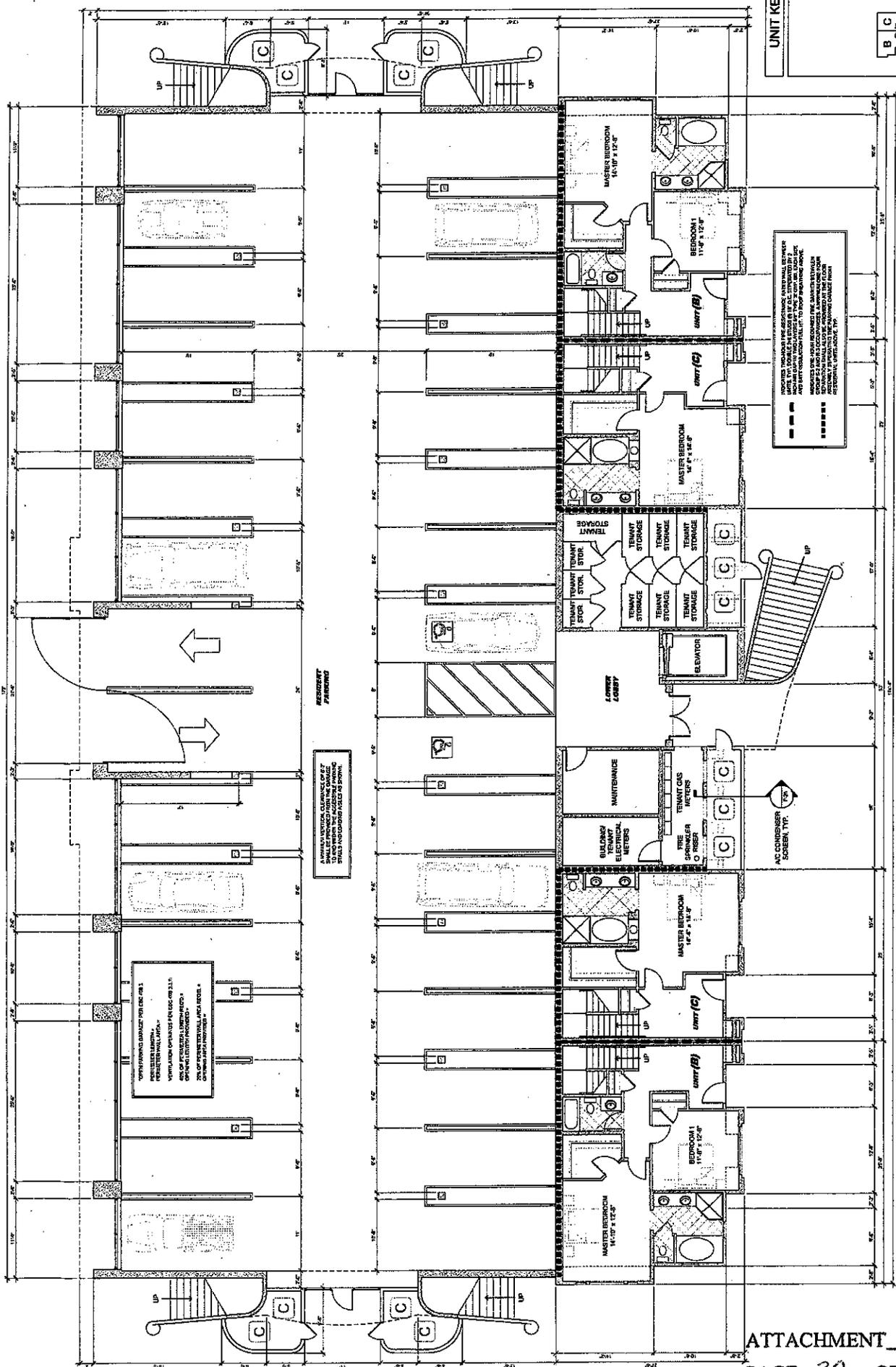
**DRAPHICS**  
1813 TOWN AND COUNTRY, CA 94618  
TEL: (925) 361-1500  
FAX: (925) 361-1501

PROPOSED 70 UNIT TOWNHOME COMPLEX

# THE ANACAPA TOWNHOMES

11000 RIVER ROAD, OAKLAND, CA

**P-10.1**  
SHEET NO. 0 0 1



--- DASHED LINE INDICATES WALL LOCATION BETWEEN UNITS  
 --- DASHED LINE WITH CIRCLES INDICATES WALL LOCATION BETWEEN UNITS AND COMMON AREAS  
 --- DASHED LINE WITH SQUARES INDICATES WALL LOCATION BETWEEN UNITS AND COMMON AREAS  
 --- DASHED LINE WITH TRIANGLES INDICATES WALL LOCATION BETWEEN UNITS AND COMMON AREAS  
 --- DASHED LINE WITH DIAMONDS INDICATES WALL LOCATION BETWEEN UNITS AND COMMON AREAS  
 --- DASHED LINE WITH STARS INDICATES WALL LOCATION BETWEEN UNITS AND COMMON AREAS

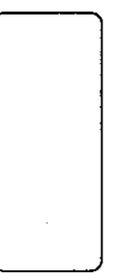
REINFORCED CONCRETE  
 WALLS AND PARTITION WALLS  
 SHALL BE CONCRETE WITH REINFORCING BARS  
 AND SHALL BE FINISHED WITH GYPSONUM BOARD  
 AND PAINTED TO MATCH ADJACENT WALLS

OPENING DIMENSIONS FOR CASES  
 SHALL BE AS SHOWN UNLESS OTHERWISE NOTED  
 DIMENSIONS FOR CASES SHALL BE AS SHOWN  
 UNLESS OTHERWISE NOTED  
 DIMENSIONS FOR CASES SHALL BE AS SHOWN  
 UNLESS OTHERWISE NOTED

PROPOSED BUILDING THREE (3) - GROUND "PARKING" LEVEL  
 SCALE: 1/8" = 1'-0"

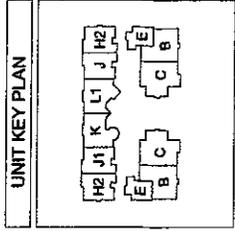
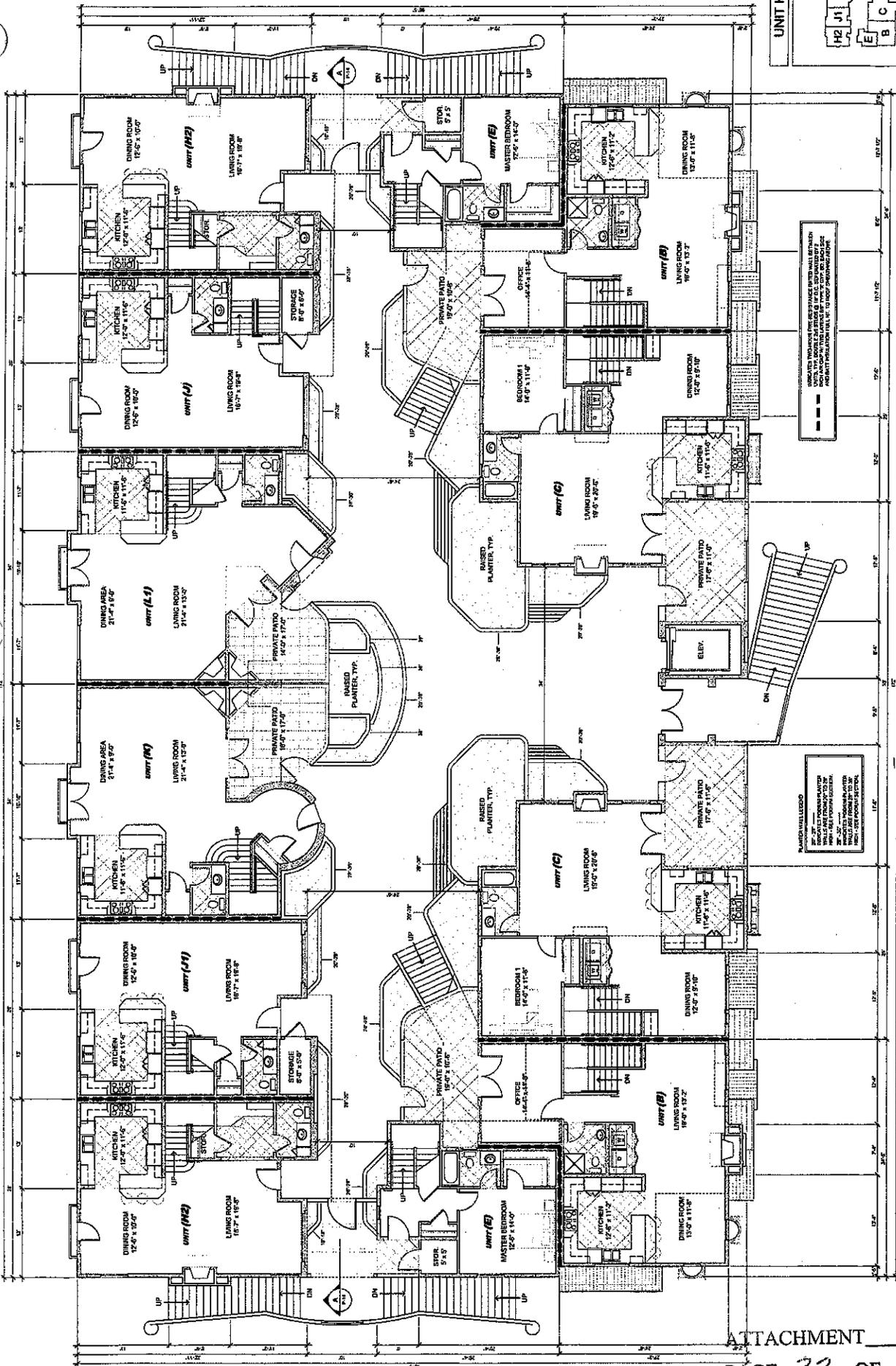
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA

ARCHITECT: David R. Burt  
 ARCHITECT: Steven T. Umiker  
**DRAPHTICS**  
 8000 Ventura Ave.  
 Van Nuys, CA 91411  
 TEL: 818-708-1111  
 FAX: 818-708-1112  
 WWW: WWW.DRAPHTICS.COM  
 U.S. REGISTERED - ARCHITECT



**OXNARD SHORES COMPANY, LLC**  
 1400 OXNARD AVENUE  
 OXNARD, CA 93030  
 TEL: 805-325-1111  
 FAX: 805-325-1112  
 WWW: WWW.OXNARDSHORES.COM

DATE	10/10/10
BY	DAVID R. BURT
CHECKED BY	STEVEN T. UMIKER
DATE	10/10/10
BY	DAVID R. BURT
CHECKED BY	STEVEN T. UMIKER
DATE	10/10/10
BY	DAVID R. BURT
CHECKED BY	STEVEN T. UMIKER



--- UNIT WALLS  
 --- UNIT WALLS WITH  
 OPENING TO ADJACENT UNITS  
 --- UNIT WALLS WITH  
 OPENING TO ADJACENT UNITS  
 --- UNIT WALLS WITH  
 OPENING TO ADJACENT UNITS

RAISED PLANTER  
 --- RAISED PLANTER  
 --- RAISED PLANTER  
 --- RAISED PLANTER



PROPOSED BUILDING THREE (3) - PODIUM "COURTYARD" LEVEL  
 SCALE: 3/8" = 1'-0"

P-12

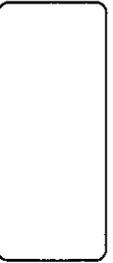
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOOLEY ROAD, OXNARD, CA

OXNARD SHORES COMPANY, LLC  
 11555 LAKESHORE BLVD  
 OXNARD, CA 93025

DATE: 07/20/18  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 PROJECT: [Name]

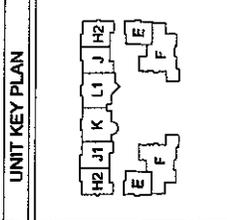
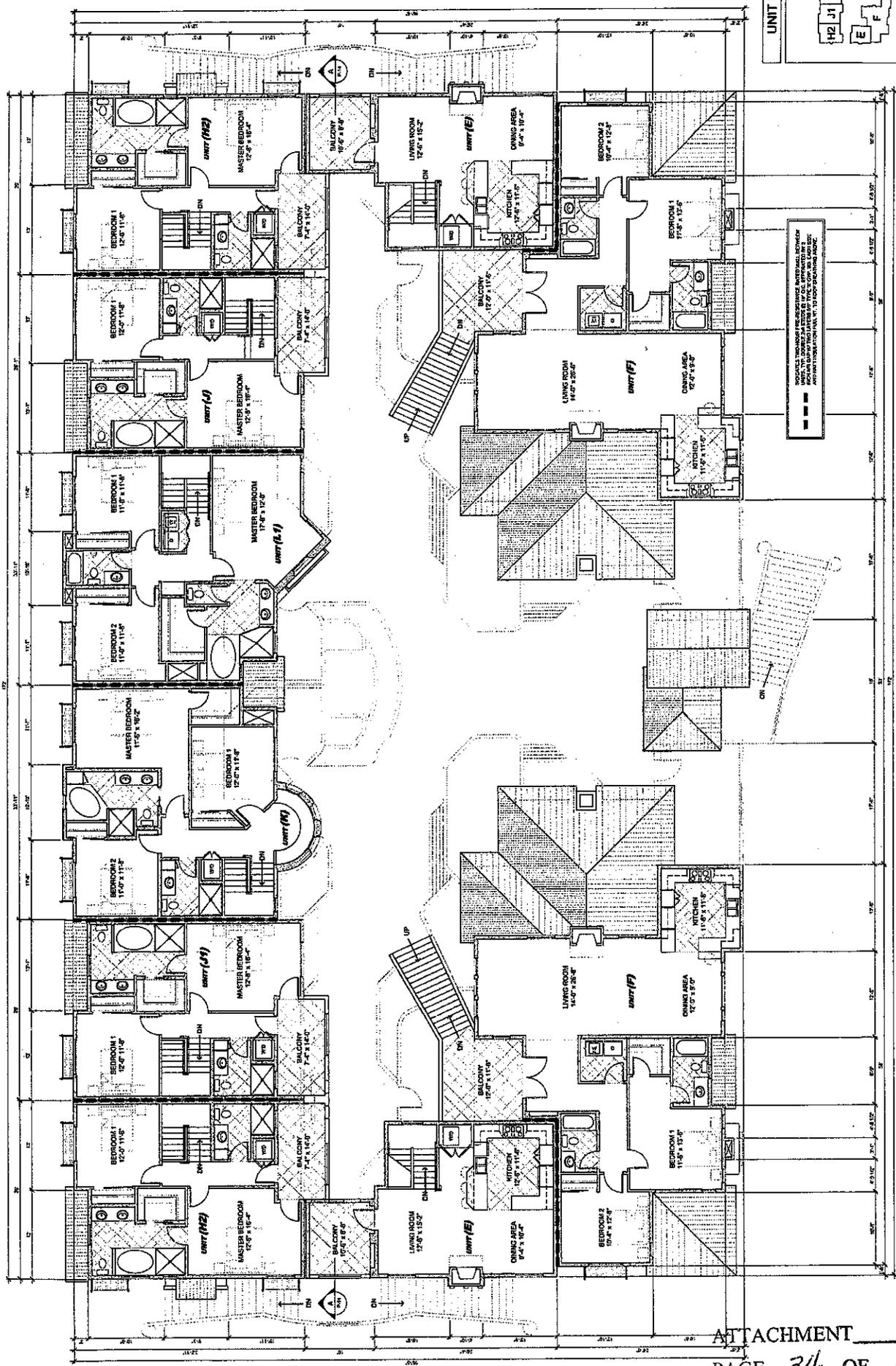
OXNARD SHORES COMPANY, LLC  
 897 TORRANCE AVENUE, SUITE 100, OXNARD, CA 93025  
 (805) 325-1111  
 WWW.OXNARDSHORES.COM

GRAPHICS  
 3-D RENDERING - EXTERIOR - VIEW



OXNARD SHORES COMPANY, LLC  
 11555 LAKESHORE BLVD  
 OXNARD, CA 93025

DATE: 07/20/18  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 PROJECT: [Name]



UNIT KEY PLAN

PROPOSED BUILDING THREE (3) - UPPER "BIRDS EYE" LEVEL

SCALE: 1/8" = 1'-0"

P-13

PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOOLEY ROAD, ONWARD, CA

Native Digital Group  
 Steven B. Cooney  
**DRAPHTICS**  
 8777 Tamien Ave  
 San Diego, CA 92123  
 619.451.8800  
 www.nativegroup.com

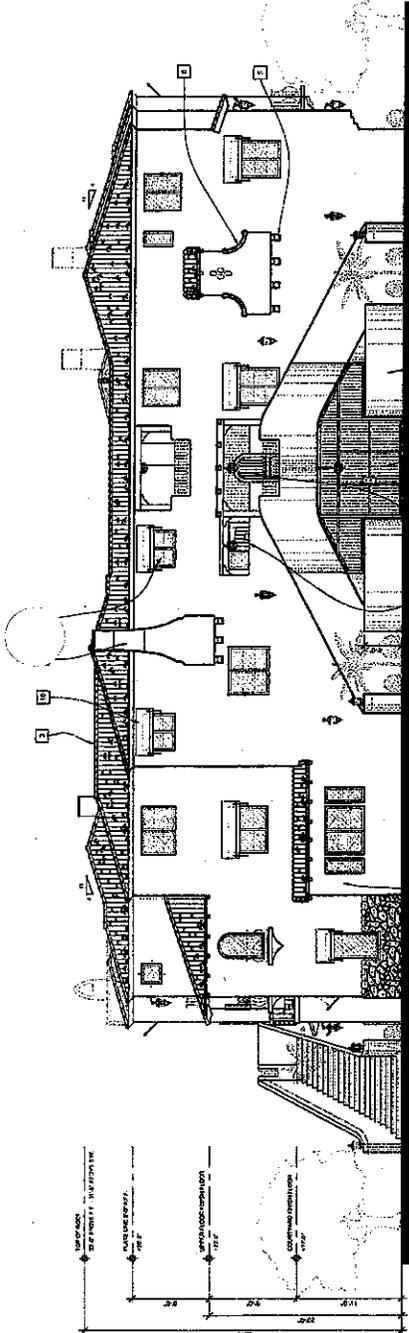


**OXNARD SHORES COMPANY, LLC**  
 24000 BIRCHWOOD  
 VAN NUYS, CA 91411  
 818.708.1111  
 www.oxnardshores.com

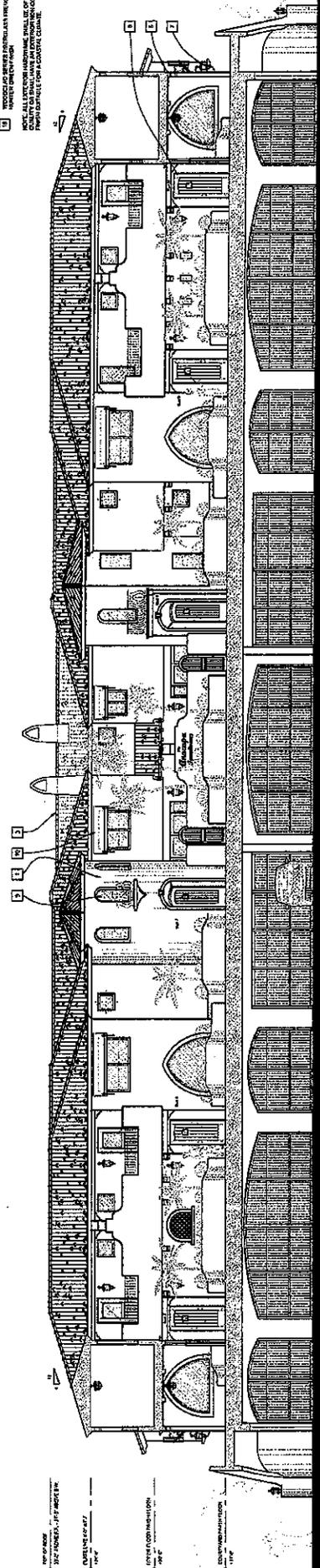
DATE	BY	CHKD
01/11/17	SS	SS

**KEY**

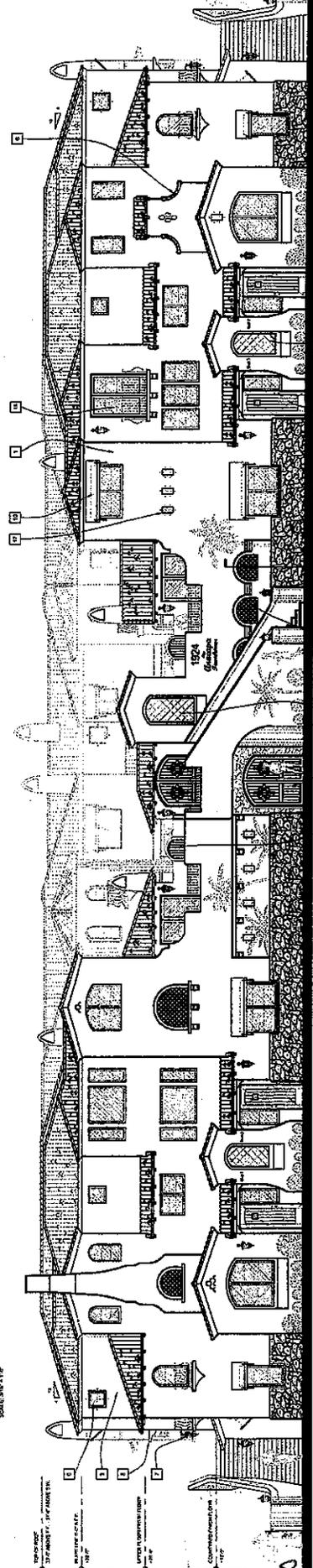
- 1. EXISTING CONCRETE FOUNDATION
- 2. EXISTING CONCRETE SLAB ON GRADE
- 3. EXISTING CONCRETE WALLS
- 4. EXISTING CONCRETE ROOF
- 5. EXISTING CONCRETE STAIRS
- 6. EXISTING CONCRETE BALCONY
- 7. EXISTING CONCRETE DRIVEWAY
- 8. EXISTING CONCRETE DRIVEWAY
- 9. EXISTING CONCRETE DRIVEWAY
- 10. EXISTING CONCRETE DRIVEWAY
- 11. EXISTING CONCRETE DRIVEWAY
- 12. EXISTING CONCRETE DRIVEWAY
- 13. EXISTING CONCRETE DRIVEWAY
- 14. EXISTING CONCRETE DRIVEWAY
- 15. EXISTING CONCRETE DRIVEWAY
- 16. EXISTING CONCRETE DRIVEWAY
- 17. EXISTING CONCRETE DRIVEWAY
- 18. EXISTING CONCRETE DRIVEWAY
- 19. EXISTING CONCRETE DRIVEWAY
- 20. EXISTING CONCRETE DRIVEWAY
- 21. EXISTING CONCRETE DRIVEWAY
- 22. EXISTING CONCRETE DRIVEWAY
- 23. EXISTING CONCRETE DRIVEWAY
- 24. EXISTING CONCRETE DRIVEWAY
- 25. EXISTING CONCRETE DRIVEWAY
- 26. EXISTING CONCRETE DRIVEWAY
- 27. EXISTING CONCRETE DRIVEWAY
- 28. EXISTING CONCRETE DRIVEWAY
- 29. EXISTING CONCRETE DRIVEWAY
- 30. EXISTING CONCRETE DRIVEWAY
- 31. EXISTING CONCRETE DRIVEWAY
- 32. EXISTING CONCRETE DRIVEWAY
- 33. EXISTING CONCRETE DRIVEWAY
- 34. EXISTING CONCRETE DRIVEWAY
- 35. EXISTING CONCRETE DRIVEWAY
- 36. EXISTING CONCRETE DRIVEWAY
- 37. EXISTING CONCRETE DRIVEWAY
- 38. EXISTING CONCRETE DRIVEWAY
- 39. EXISTING CONCRETE DRIVEWAY
- 40. EXISTING CONCRETE DRIVEWAY
- 41. EXISTING CONCRETE DRIVEWAY
- 42. EXISTING CONCRETE DRIVEWAY
- 43. EXISTING CONCRETE DRIVEWAY
- 44. EXISTING CONCRETE DRIVEWAY
- 45. EXISTING CONCRETE DRIVEWAY
- 46. EXISTING CONCRETE DRIVEWAY
- 47. EXISTING CONCRETE DRIVEWAY
- 48. EXISTING CONCRETE DRIVEWAY
- 49. EXISTING CONCRETE DRIVEWAY
- 50. EXISTING CONCRETE DRIVEWAY
- 51. EXISTING CONCRETE DRIVEWAY
- 52. EXISTING CONCRETE DRIVEWAY
- 53. EXISTING CONCRETE DRIVEWAY
- 54. EXISTING CONCRETE DRIVEWAY
- 55. EXISTING CONCRETE DRIVEWAY
- 56. EXISTING CONCRETE DRIVEWAY
- 57. EXISTING CONCRETE DRIVEWAY
- 58. EXISTING CONCRETE DRIVEWAY
- 59. EXISTING CONCRETE DRIVEWAY
- 60. EXISTING CONCRETE DRIVEWAY
- 61. EXISTING CONCRETE DRIVEWAY
- 62. EXISTING CONCRETE DRIVEWAY
- 63. EXISTING CONCRETE DRIVEWAY
- 64. EXISTING CONCRETE DRIVEWAY
- 65. EXISTING CONCRETE DRIVEWAY
- 66. EXISTING CONCRETE DRIVEWAY
- 67. EXISTING CONCRETE DRIVEWAY
- 68. EXISTING CONCRETE DRIVEWAY
- 69. EXISTING CONCRETE DRIVEWAY
- 70. EXISTING CONCRETE DRIVEWAY
- 71. EXISTING CONCRETE DRIVEWAY
- 72. EXISTING CONCRETE DRIVEWAY
- 73. EXISTING CONCRETE DRIVEWAY
- 74. EXISTING CONCRETE DRIVEWAY
- 75. EXISTING CONCRETE DRIVEWAY
- 76. EXISTING CONCRETE DRIVEWAY
- 77. EXISTING CONCRETE DRIVEWAY
- 78. EXISTING CONCRETE DRIVEWAY
- 79. EXISTING CONCRETE DRIVEWAY
- 80. EXISTING CONCRETE DRIVEWAY
- 81. EXISTING CONCRETE DRIVEWAY
- 82. EXISTING CONCRETE DRIVEWAY
- 83. EXISTING CONCRETE DRIVEWAY
- 84. EXISTING CONCRETE DRIVEWAY
- 85. EXISTING CONCRETE DRIVEWAY
- 86. EXISTING CONCRETE DRIVEWAY
- 87. EXISTING CONCRETE DRIVEWAY
- 88. EXISTING CONCRETE DRIVEWAY
- 89. EXISTING CONCRETE DRIVEWAY
- 90. EXISTING CONCRETE DRIVEWAY
- 91. EXISTING CONCRETE DRIVEWAY
- 92. EXISTING CONCRETE DRIVEWAY
- 93. EXISTING CONCRETE DRIVEWAY
- 94. EXISTING CONCRETE DRIVEWAY
- 95. EXISTING CONCRETE DRIVEWAY
- 96. EXISTING CONCRETE DRIVEWAY
- 97. EXISTING CONCRETE DRIVEWAY
- 98. EXISTING CONCRETE DRIVEWAY
- 99. EXISTING CONCRETE DRIVEWAY
- 100. EXISTING CONCRETE DRIVEWAY



PROPOSED BUILDING THREE EAST "SIDE YARD" ELEVATION  
SCALE 1/8" = 1'-0"



PROPOSED BUILDING THREE "COURTYARD" SECTION 'A'  
SCALE 1/8" = 1'-0"



PROPOSED BUILDING THREE SOUTH "WOOLEY" ELEVATION  
SCALE 1/8" = 1'-0"

**OXNARD SHORES COMPANY, LLC**  
115 S. WINDY BLVD  
Oxnard, CA 93030  
Tel: 805-321-1977  
Fax: 805-321-1522

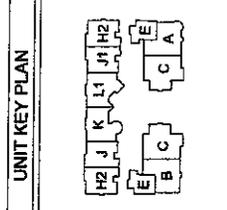
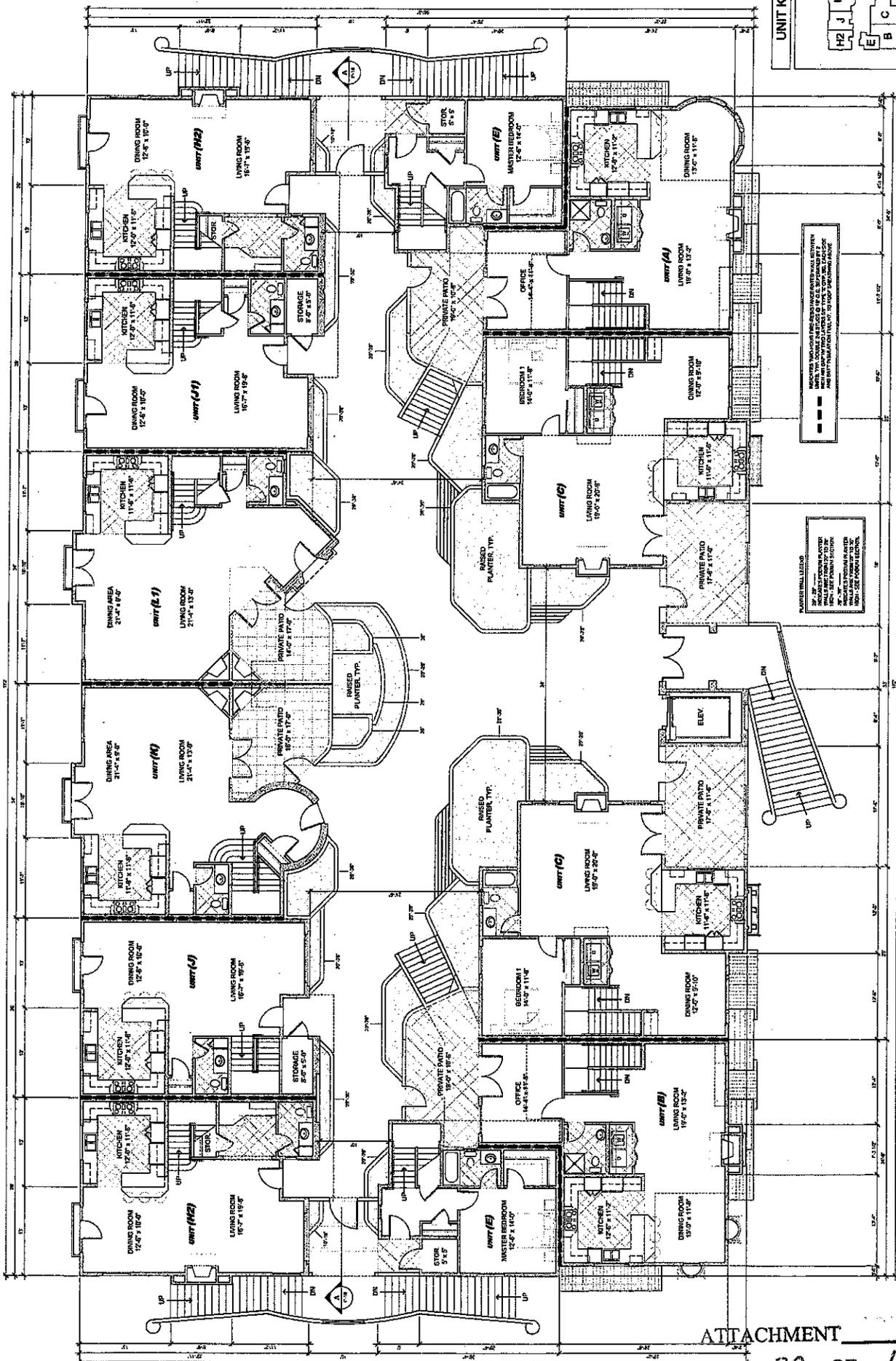
**DRAPHTICS**  
180 TOWN ARCADE, CARLSBAD, CA 92008  
www.draphtics.com  
Tel: 760-439-1111  
Fax: 760-439-1112

**PROPOSED 70 UNIT TOWNHOME COMPLEX**  
**THE ANACAPA TOWNHOMES**  
WOODLEY ROAD, OXNARD, CA

**P-14**  
SHEET NO. 14







P-16

# PROPOSED 70 UNIT TOWNHOME COMPLEX THE ANACAPA TOWNHOMES

WOODLEY ROAD, OXNARD, CA

PROPOSED BUILDING FOUR (4) - PODIUM "COURTYARD" LEVEL

Architect: **Shoreline Builders**  
 Architect: **Shoreline Builders**  
 300 Thruway Ave., Suite 100  
 Oxnard, CA 93030  
 Phone: (805) 481-1111  
 Fax: (805) 481-1112  
 Email: info@shorelinebuilders.com

**GRAPHICS**

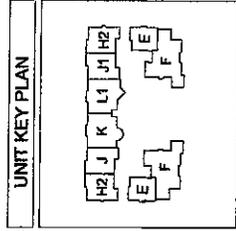
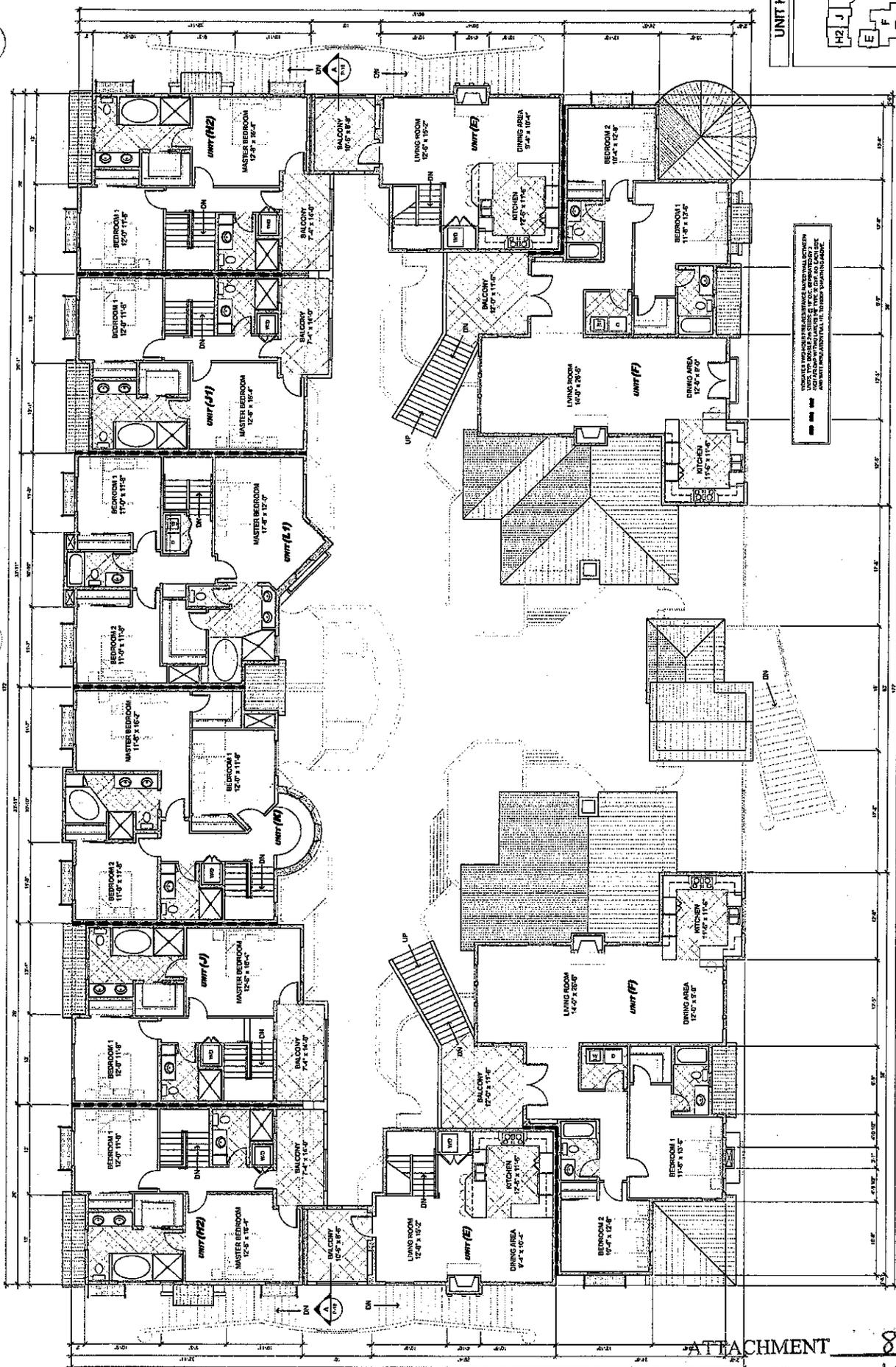
300 Thruway Ave., Suite 100  
 Oxnard, CA 93030  
 Phone: (805) 481-1111  
 Fax: (805) 481-1112  
 Email: info@shorelinebuilders.com

P.O. PRESBYTERIAN - BERRATI

**OXNARD SHORES COMPANY, LLC**

1000 W. Oxnard Blvd., Suite 100  
 Oxnard, CA 93030  
 Phone: (805) 481-1111  
 Fax: (805) 481-1112  
 Email: info@oxnardshores.com

DATE:	08/15/12
SCALE:	AS SHOWN
DESIGNER:	SHORELINE BUILDERS
DATE:	08/15/12
PROJECT:	OXNARD SHORES



P-17

PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 HODLEY ROAD, OXNARD, CA

Author: David Fisher  
 Steven H. Olinari  
**GRAPHICS**  
 833 Tacoma Avenue, Capetown, CA 91302  
 www.davidfisher.com (805) 316-8973 www.swhp.com  
 PLO PRESENTATION - DRAFTER - DESIGN



**OXNARD SHORES COMPANY, LLC**  
 6915 HANFORD BLVD  
 OXNARD, CA 91320  
 (805) 316-8973  
 www.oxshores.com

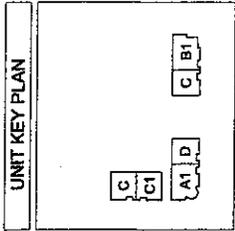
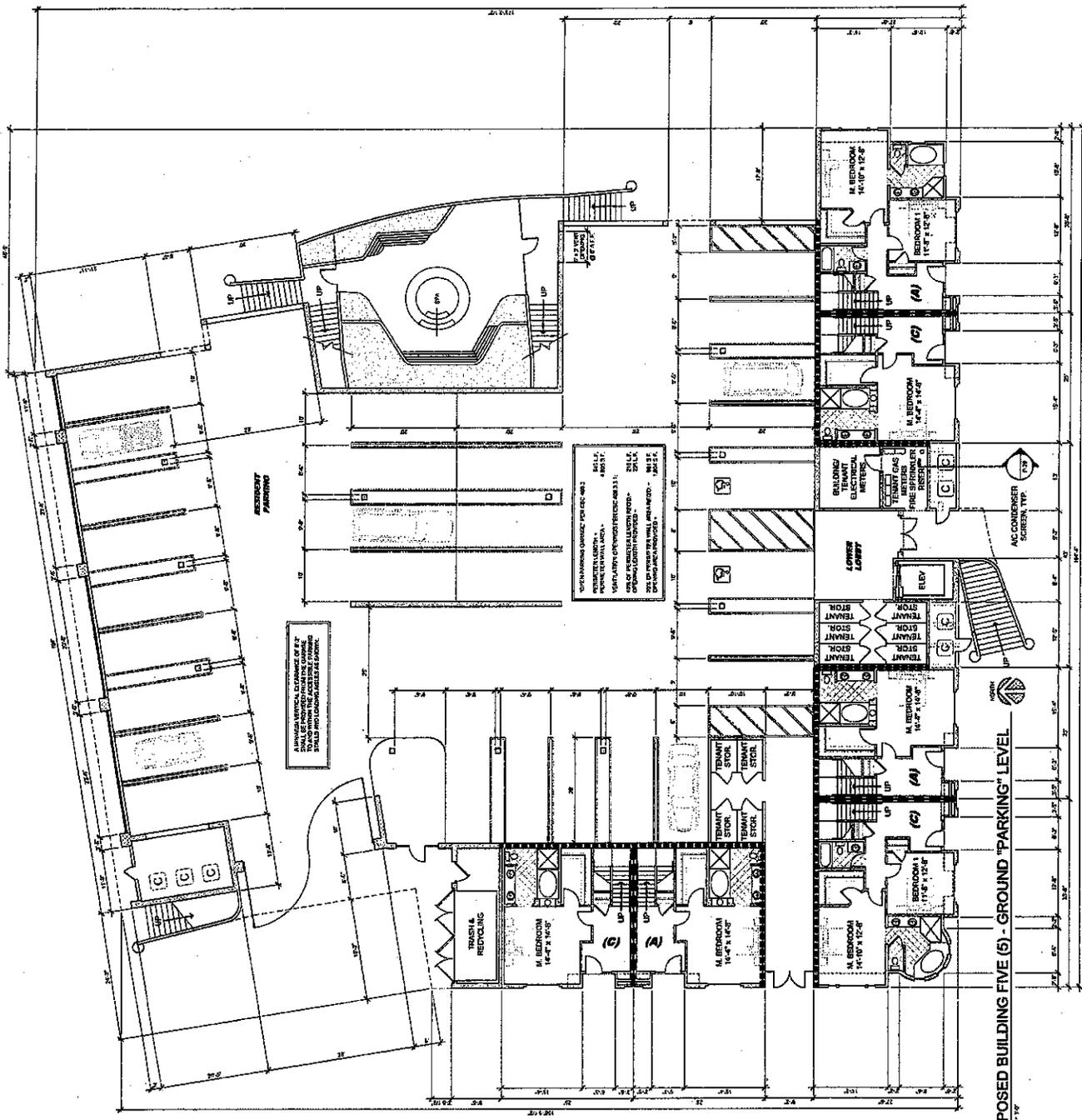
DATE:	07/18/2014
BY:	DAVID FISHER
CHECKED BY:	STEVEN H. OLINARI
SCALE:	AS SHOWN
PROJECT:	THE ANACAPA TOWNHOMES
NO.:	017-0000
DATE:	07/18/2014
BY:	DAVID FISHER
CHECKED BY:	STEVEN H. OLINARI
SCALE:	AS SHOWN
PROJECT:	THE ANACAPA TOWNHOMES
NO.:	017-0000

PROPOSED BUILDING FOUR (4) - UPPER "BIRDS EYE" LEVEL  
 SHEET 201-117





NOTES:  
 1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 2. ALL WALLS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 3. ALL FLOORS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 4. ALL CEILING ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 5. ALL ROOFS ARE TO BE FLAT UNLESS OTHERWISE NOTED.  
 6. ALL STAIRS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 7. ALL ELEVATORS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 8. ALL MECHANICAL ROOMS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 9. ALL ELECTRICAL ROOMS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 10. ALL PLUMBING ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 11. ALL FINISHES ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 12. ALL PARTITIONS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 13. ALL DOORS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 14. ALL WINDOWS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 15. ALL GLAZING ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 16. ALL ROOFING ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 17. ALL EXTERIOR WALLS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 18. ALL EXTERIOR FLOORS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 19. ALL EXTERIOR ROOFS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.  
 20. ALL EXTERIOR STAIRS ARE TO BE CONCRETE UNLESS OTHERWISE NOTED.



PROPOSED BUILDING FIVE (5) - GROUND "PARKING" LEVEL  
 SCALE: 1/8" = 1'-0"

P-19  
 SHEET NO. 21 OF 22

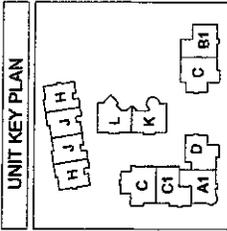
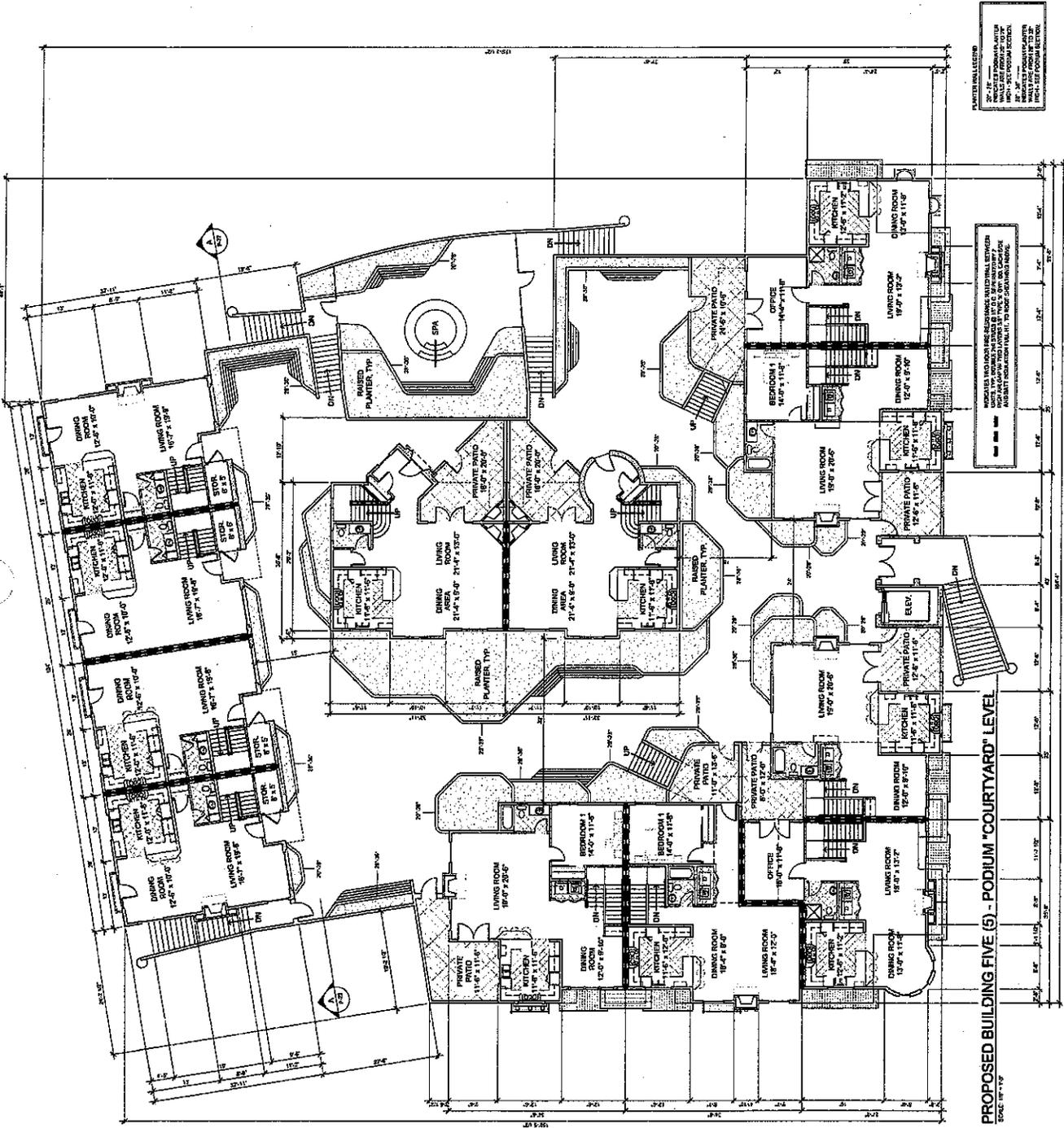
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOOLEY ROAD, OMMARD, CA

ARCHITECT  
 Oxnard Shores Company, LLC  
 833 South Ave  
 Oxnard, CA 93025  
 805.481.1111  
 www.oxnardshores.com

GRAPHICS  
 833 South Ave  
 Oxnard, CA 93025  
 805.481.1111  
 www.oxnardshores.com

OXNARD SHORES COMPANY, LLC  
 833 South Ave  
 Oxnard, CA 93025  
 805.481.1111  
 www.oxnardshores.com

OXNARD SHORES COMPANY, LLC  
 833 South Ave  
 Oxnard, CA 93025  
 805.481.1111  
 www.oxnardshores.com



P-20

# PROPOSED 70 UNIT TOWNHOME COMPLEX THE ANACAPA TOWNHOMES

WOCLEY ROAD, OXNARD, CA

WATER COLLECTION  
20'-0" DIA.  
INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS  
INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS  
INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS

INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS  
INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS  
INDICATES TYPICAL LOCATION OF  
INDIVIDUAL ROOMS AND SECTIONS

PROPOSED BUILDING FIVE (5) - PODIUM "COURTYARD" LEVEL  
SCALE 1/8" = 1'-0"

Author: David Ross  
Senior: October

## DRAPINGS

8000 Via Arroyo, Oxnard, CA 93025  
www.drapings.com  
(805) 388-0070

1-800-REPRESENTATIVE - DRAPINGS - 8000

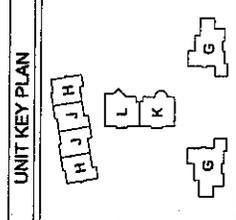


**OXNARD SHORES COMPANY, LLC**

10000 Oxnard Blvd  
Oxnard, CA 93025

DATE:	08/11/11
BY:	DAVID ROSS
CHECKED BY:	DAVID ROSS
DATE:	08/11/11
BY:	DAVID ROSS
CHECKED BY:	DAVID ROSS

INDICATED DIMENSIONS ARE MEASURED BETWEEN WALLS UNLESS OTHERWISE NOTED. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED. DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.



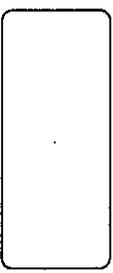
P-21  
DATE: 07.09.18

PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA



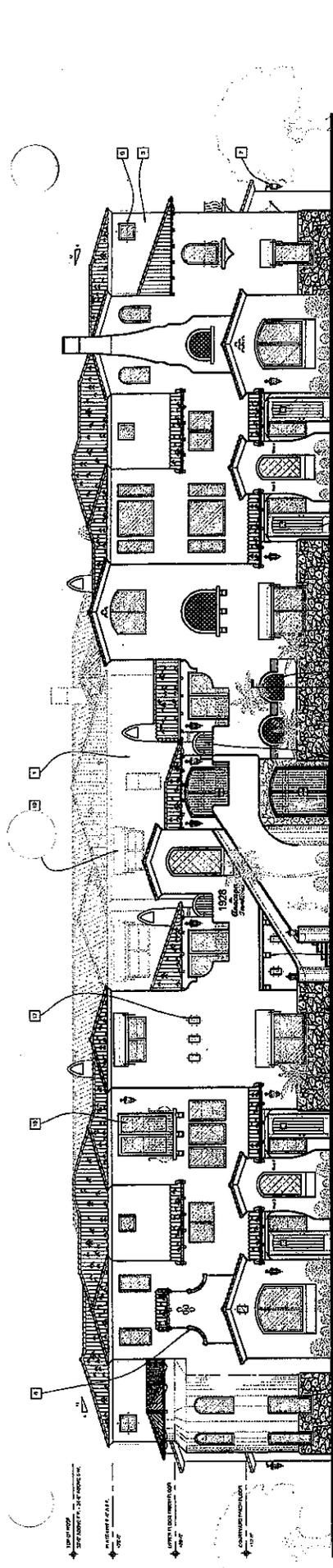
PROPOSED BUILDING FIVE (5) - UPPER "BIRDS EYE" LEVEL  
 SCALE: 1/8" = 1'-0"

GRAPHICS  
 3030 Sherman Street, Suite 100  
 Oxnard, CA 93025  
 (805) 466-1111  
 www.oxnardgraphics.com

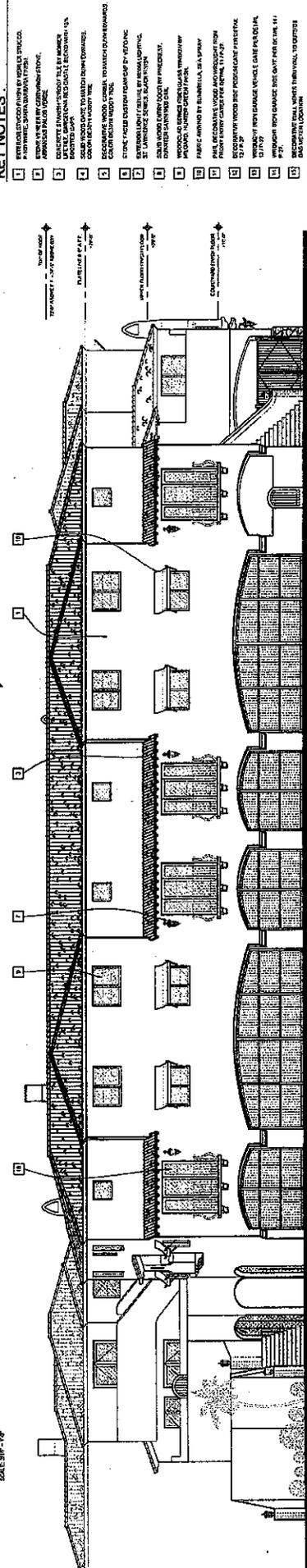


OXNARD SHORES COMPANY, LLC  
 18 N.W. 25th Ave.  
 Ft. Lauderdale, FL 33309  
 (954) 576-1111  
 www.oxnardshores.com

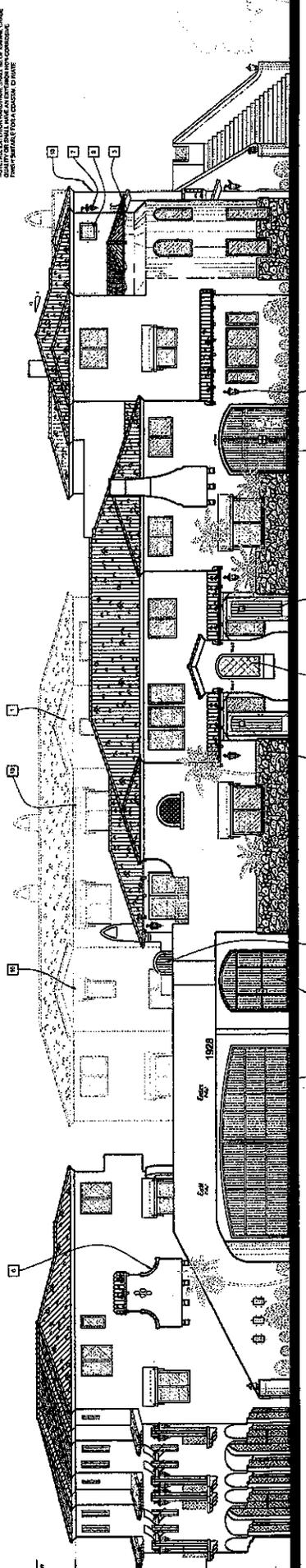
NO.	DATE	DESCRIPTION
1	07.09.18	ISSUED FOR PERMITS
2	07.09.18	ISSUED FOR PERMITS
3	07.09.18	ISSUED FOR PERMITS
4	07.09.18	ISSUED FOR PERMITS
5	07.09.18	ISSUED FOR PERMITS
6	07.09.18	ISSUED FOR PERMITS
7	07.09.18	ISSUED FOR PERMITS
8	07.09.18	ISSUED FOR PERMITS
9	07.09.18	ISSUED FOR PERMITS
10	07.09.18	ISSUED FOR PERMITS



PROPOSED BUILDING FIVE SOUTH "WOOLEY" ELEVATION  
SCALE 3/8" = 1'-0"



PROPOSED BUILDING FIVE NORTH ELEVATION  
SCALE 3/8" = 1'-0"



PROPOSED BUILDING FIVE WEST "CANAL STREET" ELEVATION  
SCALE 3/8" = 1'-0"

KEY NOTES:

1. REFER TO SECTION FOR WINDOW SCHEDULE.
2. REFER TO SECTION FOR ROOFING SCHEDULE.
3. REFER TO SECTION FOR EXTERIOR FINISH SCHEDULE.
4. REFER TO SECTION FOR EXTERIOR LIGHTING SCHEDULE.
5. REFER TO SECTION FOR EXTERIOR FURNITURE SCHEDULE.
6. REFER TO SECTION FOR EXTERIOR PLANTING SCHEDULE.
7. REFER TO SECTION FOR EXTERIOR MATERIAL SCHEDULE.
8. REFER TO SECTION FOR EXTERIOR PAINT SCHEDULE.
9. REFER TO SECTION FOR EXTERIOR SIGNAGE SCHEDULE.
10. REFER TO SECTION FOR EXTERIOR ACCESSORIES SCHEDULE.
11. REFER TO SECTION FOR EXTERIOR UTILITIES SCHEDULE.
12. REFER TO SECTION FOR EXTERIOR STRUCTURE SCHEDULE.
13. REFER TO SECTION FOR EXTERIOR MECHANICAL SCHEDULE.
14. REFER TO SECTION FOR EXTERIOR ELECTRICAL SCHEDULE.
15. REFER TO SECTION FOR EXTERIOR PLUMBING SCHEDULE.
16. REFER TO SECTION FOR EXTERIOR HEATING SCHEDULE.
17. REFER TO SECTION FOR EXTERIOR COOLING SCHEDULE.
18. REFER TO SECTION FOR EXTERIOR INSULATION SCHEDULE.
19. REFER TO SECTION FOR EXTERIOR SOUNDING SCHEDULE.
20. REFER TO SECTION FOR EXTERIOR VENTILATION SCHEDULE.
21. REFER TO SECTION FOR EXTERIOR SECURITY SCHEDULE.
22. REFER TO SECTION FOR EXTERIOR MAINTENANCE SCHEDULE.
23. REFER TO SECTION FOR EXTERIOR SAFETY SCHEDULE.
24. REFER TO SECTION FOR EXTERIOR COMPLIANCE SCHEDULE.
25. REFER TO SECTION FOR EXTERIOR DOCUMENTATION SCHEDULE.

PROPOSED 70 UNIT TOWNHOME COMPLEX

# THE ANACAPA TOWNHOMES

WOODLEY ROAD, OXNARD, CA

P-22

DATE: 08/15/2018

PROJECT: THE ANACAPA TOWNHOMES

SCALE: 3/8" = 1'-0"

DATE: 08/15/2018

PROJECT: THE ANACAPA TOWNHOMES

**OXNARD SHORES COMPANY, LLC**

11515 WOODLEY ROAD  
OXNARD, CA 93025

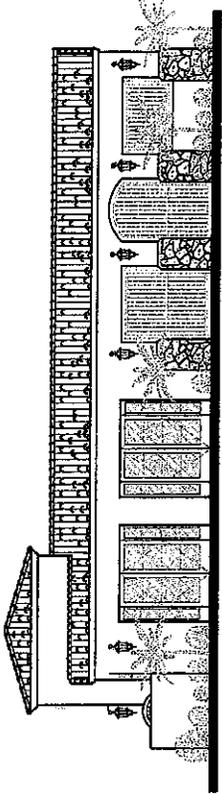
**DRAPHTICS**

492 TOWN AND COUNTRY, CA 93010  
www.draphtics.com

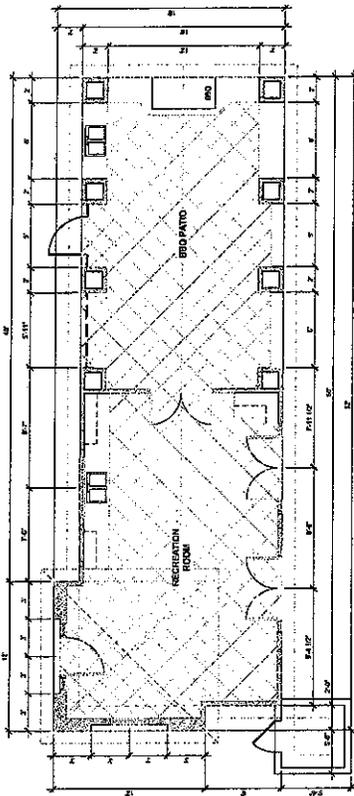
ATTACHMENT 8

PAGE 45 OF 185

8



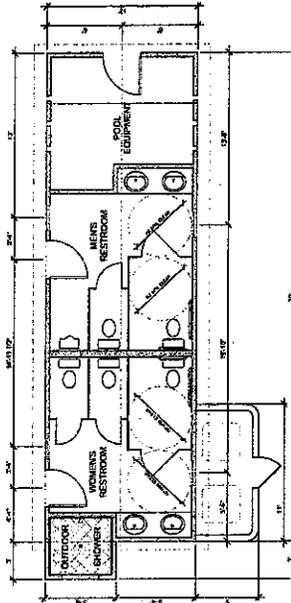
PROPOSED RECREATION BUILDING SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"



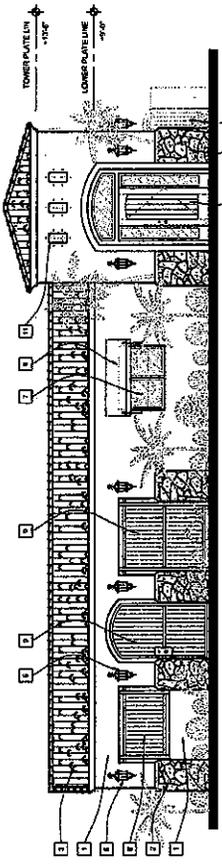
PROPOSED RECREATION BUILDING PLAN  
SCALE: 1/8" = 1'-0"

KEY NOTES:

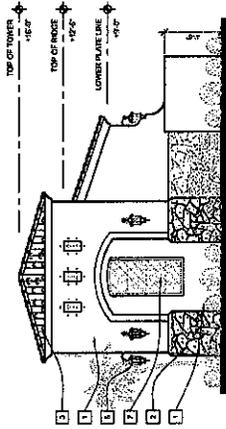
1. EXTERIOR FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
2. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
3. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
4. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
5. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
6. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
7. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
8. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
9. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
10. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.
11. FINISHES TO BE AS SHOWN UNLESS NOTED OTHERWISE.



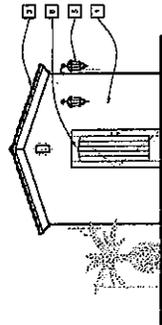
PROPOSED RESTROOM / EQUIPMENT BUILDING FLOOR PLAN  
SCALE: 1/8" = 1'-0"



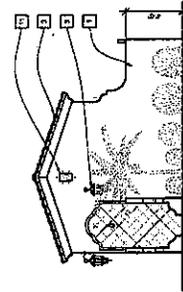
PROPOSED RECREATION BUILDING NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



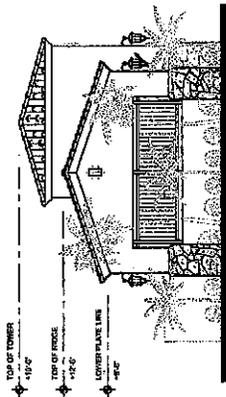
PROPOSED REC. BDLG. WEST ELEV.  
SCALE: 1/8" = 1'-0"



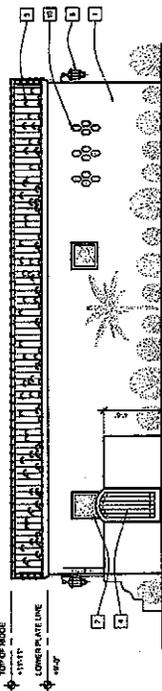
PROP. R.R. / EQUIP. BLDG. NORTH ELEV.  
SCALE: 1/8" = 1'-0"



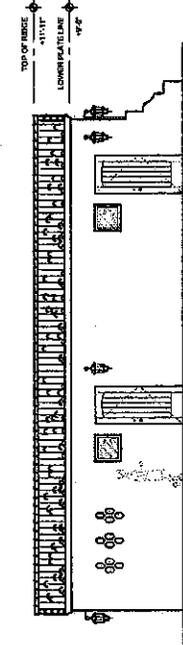
PROP. R.R. / EQUIP. BLDG. SOUTH ELEV.  
SCALE: 1/8" = 1'-0"



PROPOSED REC. BDLG. EAST ELEV.  
SCALE: 1/8" = 1'-0"



PROPOSED R.R. / EQUIP. BUILDING EAST ELEVATION  
SCALE: 1/8" = 1'-0"



PROPOSED R.R. / EQUIP. BUILDING WEST ELEVATION  
SCALE: 1/8" = 1'-0"

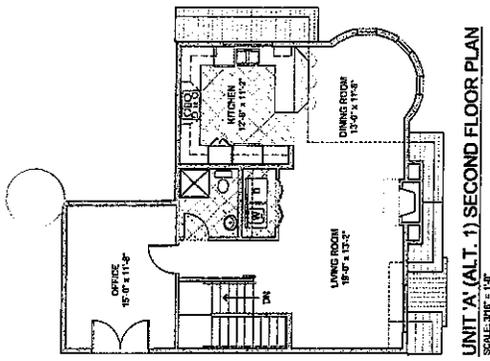
OXNARD SHORES COMPANY, LLC  
THE OXNARD SHORE COMPANY  
401 OXNARD AVENUE  
OXNARD, CA 93030  
TEL: 805.321.1111  
WWW.OXNARDSHORES.COM

THE ANACAPA TOWNHOMES  
WOODLEY ROAD, OXNARD, CA

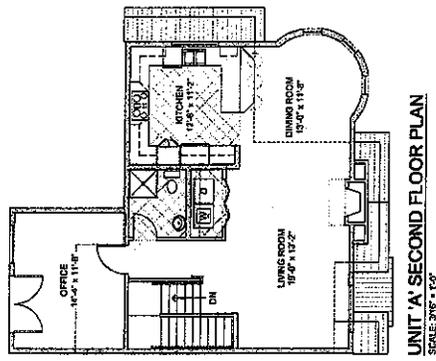
GRAPHICS  
817 TOWNLEY AVENUE  
OXNARD, CA 93030  
TEL: 805.321.1111  
WWW.OXNARDSHORES.COM

OXNARD SHORES COMPANY, LLC  
THE OXNARD SHORE COMPANY  
401 OXNARD AVENUE  
OXNARD, CA 93030  
TEL: 805.321.1111  
WWW.OXNARDSHORES.COM

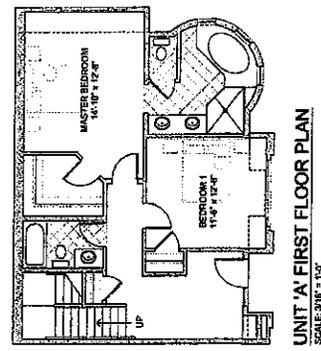
P-23  
SHEET NO. 2 OF 3



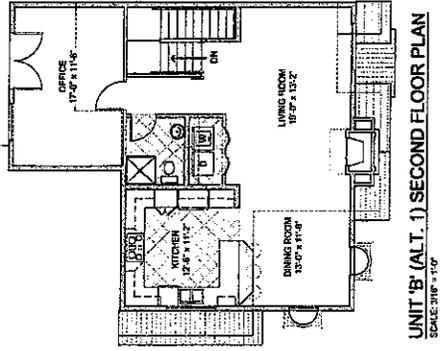
UNIT 'A' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



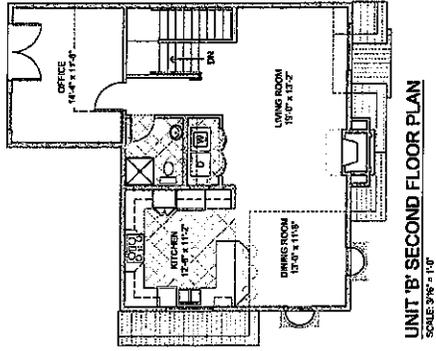
UNIT 'A' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



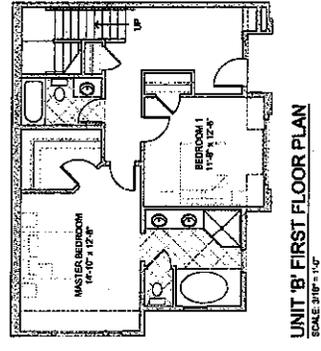
UNIT 'A' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



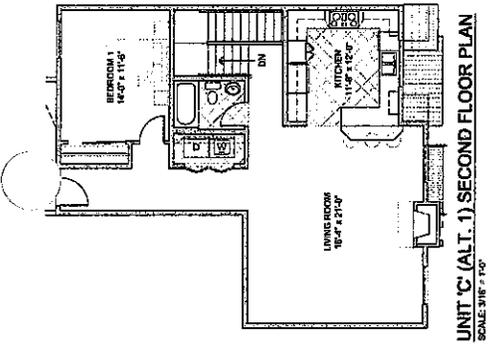
UNIT 'B' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



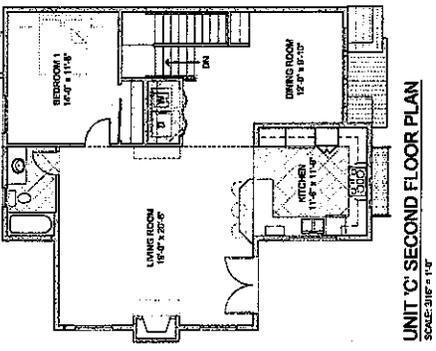
UNIT 'B' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



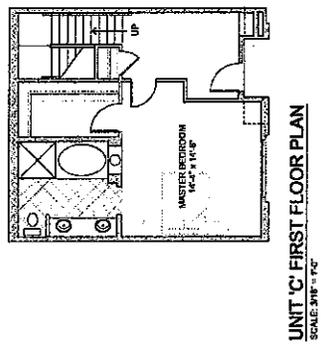
UNIT 'B' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



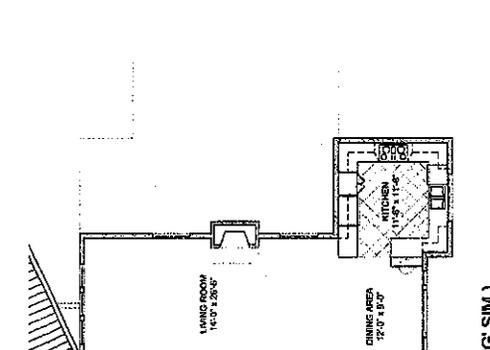
UNIT 'C' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



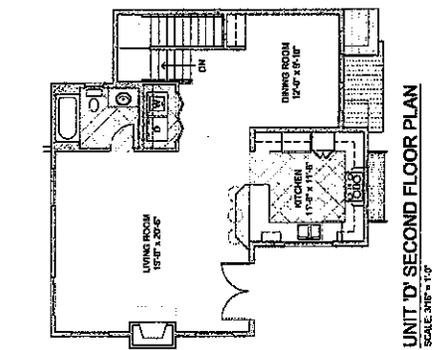
UNIT 'C' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



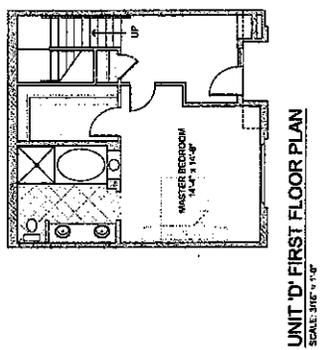
UNIT 'C' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



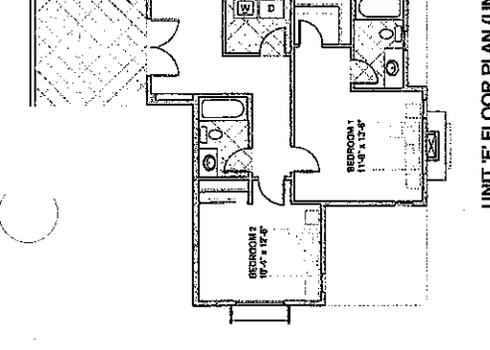
UNIT 'F' FLOOR PLAN (UNIT 'G' SIM.)  
SCALE: 3/16" = 1'-0"



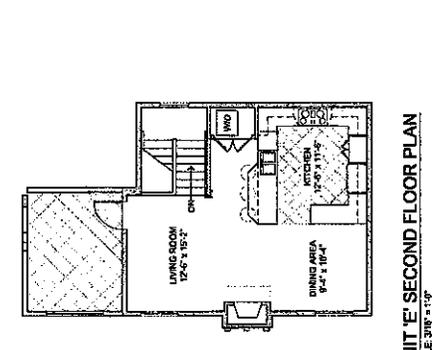
UNIT 'D' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



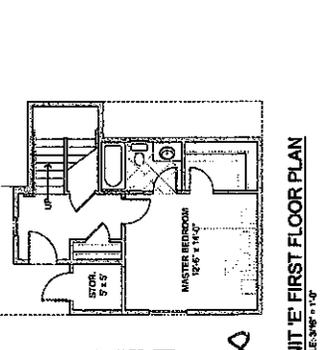
UNIT 'D' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



UNIT 'E' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



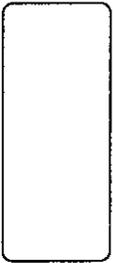
UNIT 'D' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



UNIT 'E' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"

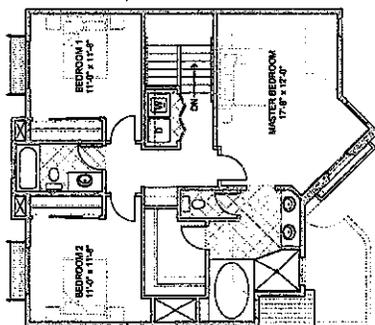
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
WOODLEY ROAD, OXNARD, CA

Author: David Ripps  
Sketch by: Christian  
**DRAPHS**  
10777 Torres Avenue, Chatsworth, CA 91311  
www.draphs.com  
1-800-338-3333

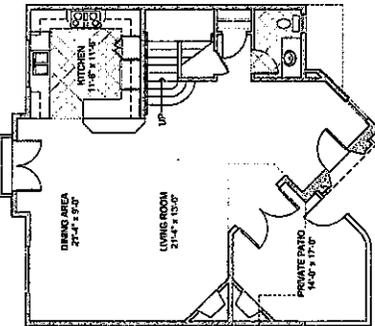


**OXNARD SHORES COMPANY, LLC**  
10111 OXNARD BLVD  
OXNARD, CA 91320

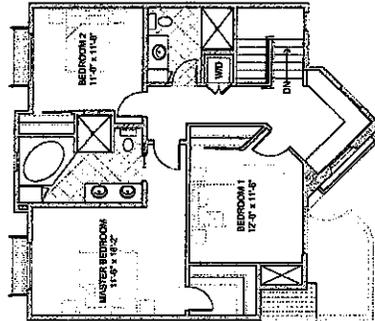
DATE:	08/11/10
BY:	DRAPHS
PROJECT:	OXNARD SHORES
NO.:	08/11/10
SCALE:	AS SHOWN
DATE:	08/11/10
BY:	DRAPHS
PROJECT:	OXNARD SHORES
NO.:	08/11/10
SCALE:	AS SHOWN



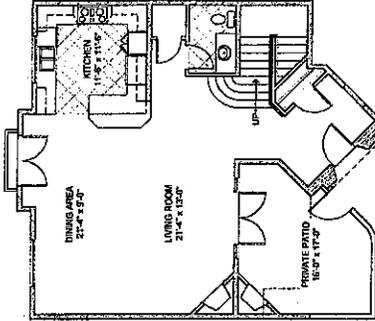
UNIT L' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



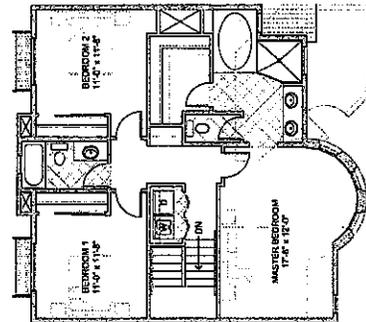
UNIT L' (ALT. 1) FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



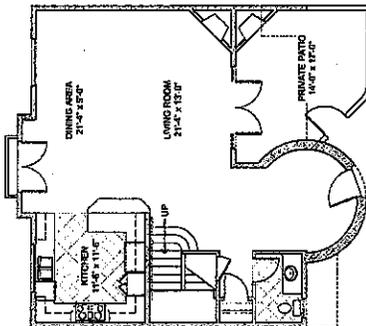
UNIT L' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



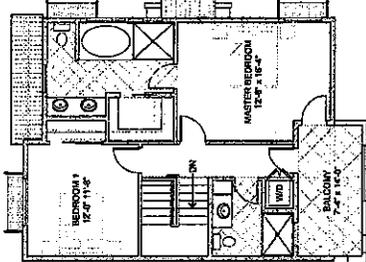
UNIT L' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



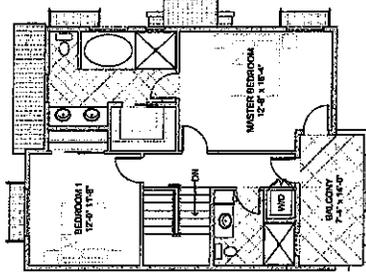
UNIT K' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



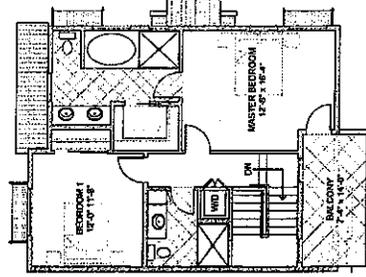
UNIT K' (ALT. 1) FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



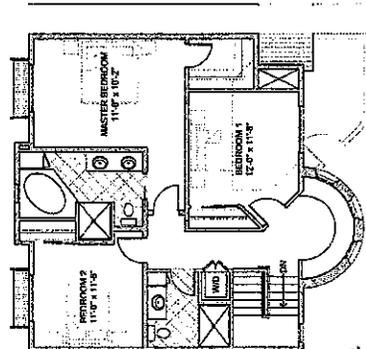
UNIT H' (ALT. 2) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



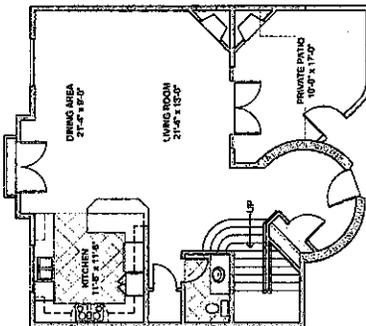
UNIT H' (ALT. 1) SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



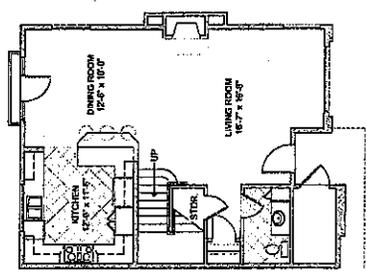
UNIT H' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



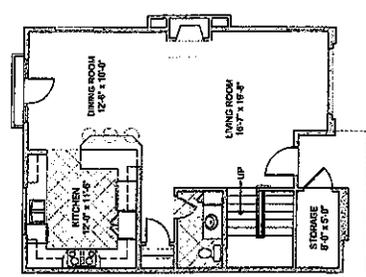
UNIT K' SECOND FLOOR PLAN  
SCALE: 3/16" = 1'-0"



UNIT K' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



UNIT H' (ALT. 1) FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"



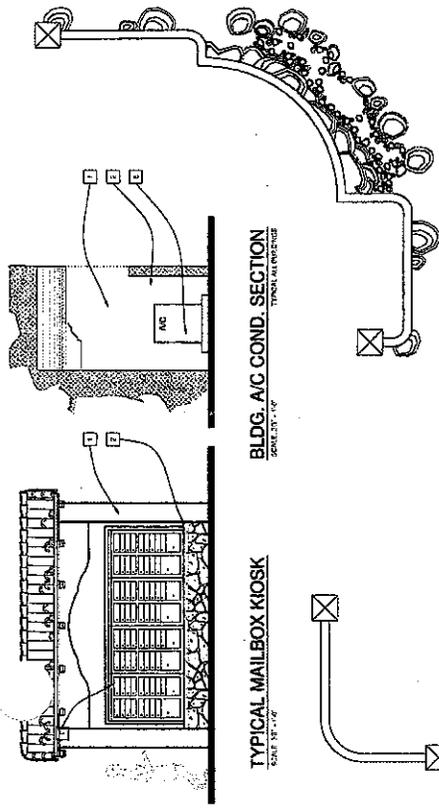
UNIT H' FIRST FLOOR PLAN  
SCALE: 3/16" = 1'-0"

DATE	DESCRIPTION

**OXNARD SHORES COMPANY, LLC**  
 14000 OXNARD BLVD  
 OXNARD, CA 94951  
 (805) 313-3333

**GRAPHICS**  
 301 DOWNEY AVE  
 OXNARD, CA 94951  
 (805) 313-3333

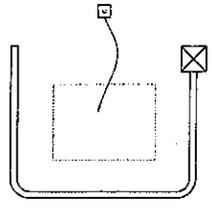
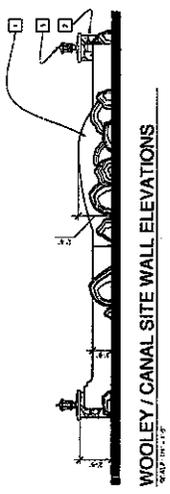
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**THE ANACAPA TOWNHOMES**  
 WOOLEY ROAD, OXNARD, CA



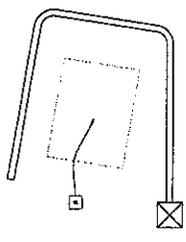
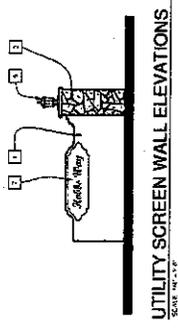
**SIDEYARD SITE WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"



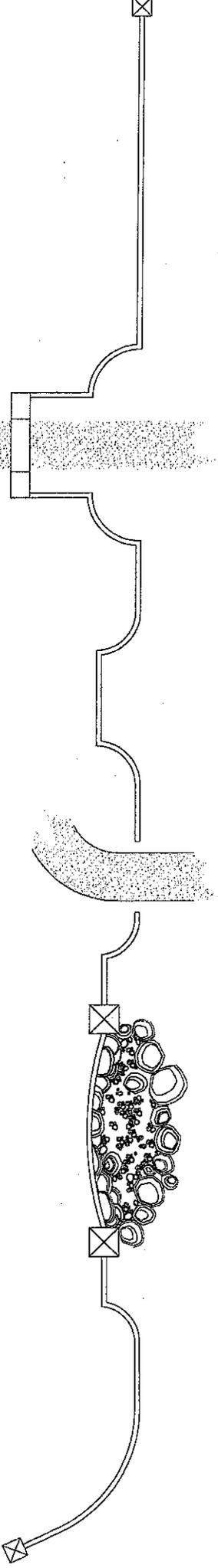
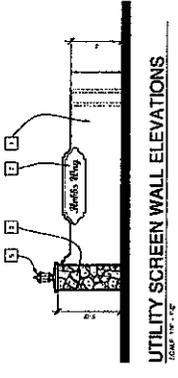
**WOOLEY / CANAL SITE WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"



**UTILITY SCREEN WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"



**UTILITY SCREEN WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"



**PROPOSED WOOLEY / HARBOR SITE WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"

**PROPOSED WOOLEY SITE WALL FLOOR PLAN**  
SCALE: 1/4" = 1'-0"

**PROPOSED WOOLEY / HARBOR SITE WALL ELEVATION**  
SCALE: 1/4" = 1'-0"

**PROPOSED WOOLEY SITE WALL ELEVATION**  
SCALE: 1/4" = 1'-0"

**KEY NOTES:**

- 1. ALL WALLS SHALL BE CONCRETE FINISH.
- 2. ALL WALLS SHALL BE 12" MIN. THICK.
- 3. ALL WALLS SHALL BE 4" MIN. THICK.
- 4. ALL WALLS SHALL BE 4" MIN. THICK.
- 5. ALL WALLS SHALL BE 4" MIN. THICK.
- 6. ALL WALLS SHALL BE 4" MIN. THICK.
- 7. ALL WALLS SHALL BE 4" MIN. THICK.
- 8. ALL WALLS SHALL BE 4" MIN. THICK.
- 9. ALL WALLS SHALL BE 4" MIN. THICK.
- 10. ALL WALLS SHALL BE 4" MIN. THICK.

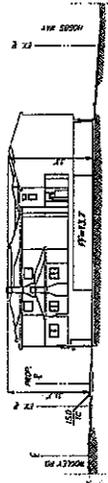
DATE:	08/11/17
DESIGNED BY:	JOHN
CHECKED BY:	JOHN
DATE PLOTTED:	08/11/17
SCALE:	AS SHOWN
PROJECT:	74-30208-15P
DATE:	08/11/17

**OXNARD SHORES COMPANY, LLC**  
1155 HARBOR BLVD.  
OXNARD, CA 93025  
TEL: 805.308.1157  
FAX: 805.308.1153

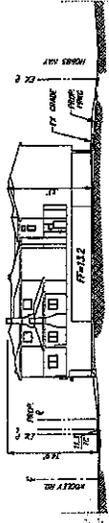
**DRAPPHICS**  
1000 S. OXNARD BLVD.  
OXNARD, CA 93025  
TEL: 805.308.1157  
FAX: 805.308.1153

**PROPOSED 70 UNIT TOWNHOME COMPLEX**  
**THE ANACAPA TOWNHOMES**  
WOOLEY ROAD, OXNARD, CA  
P-26  
SECTION 71 OF 78

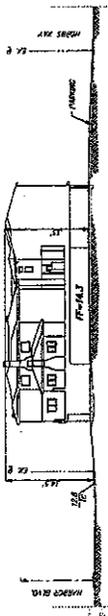




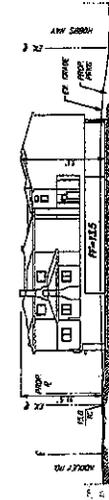
SECTION 2  
SCALE: 1/8" = 1'-0"



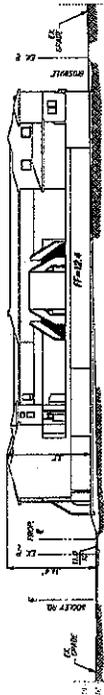
SECTION 4  
SCALE: 1/8" = 1'-0"



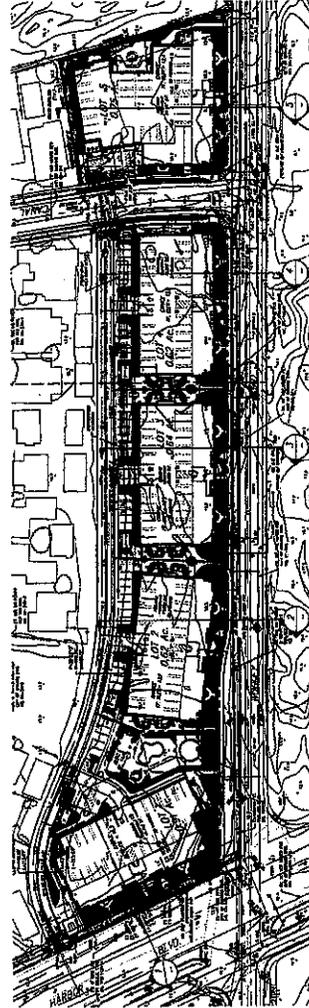
SECTION 1  
SCALE: 1/8" = 1'-0"



SECTION 3  
SCALE: 1/8" = 1'-0"



SECTION 5  
SCALE: 1/8" = 1'-0"



PROPOSED 70 UNIT TOWNHOME COMPLEX

# THE ANACAPA TOWNHOMES

WOODLEY ROAD, CONNARD, CA

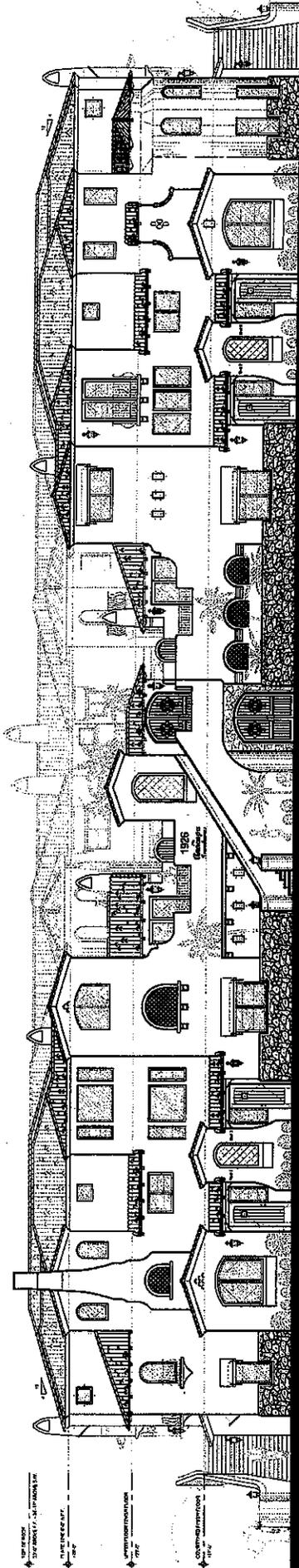
P-28

Atkins David Brown  
Shawn B. Umiker  
**DR A P H I C S**  
800 Tenth Avenue, Suite 100, San Francisco, CA 94103  
info@atkinsdavidbrown.com  
415.774.1100

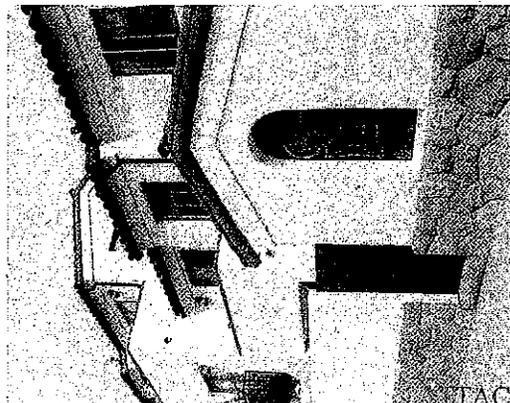
**OXNARD SHORES COMPANY, LLC**  
1155 S. OXNARD BLVD  
OXNARD, CA 93028

**OXNARD SHORES COMPANY, LLC**  
1155 S. OXNARD BLVD  
OXNARD, CA 93028

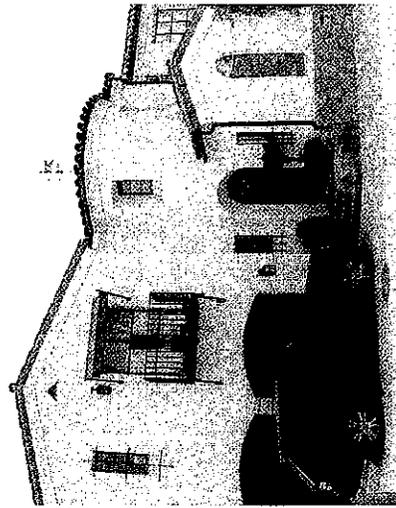
DATE:	2020
DESIGNED BY:	AD
CHECKED BY:	AD
DATE:	2020
DESIGNED BY:	AD
CHECKED BY:	AD



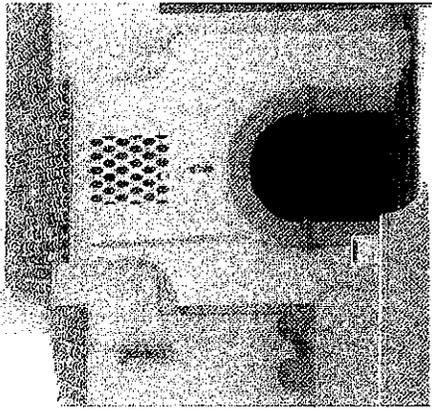
PROPOSED BUILDING FOUR SOUTH "WOOLEY" ELEVATION  
SCALE 3/8" = 1'-0"



EXAMPLE MOSAIC TILE DETAIL, GABLE ROOF DETAIL & STONE



EXAMPLE OF EXTERIOR MASSING, ROOF, ENTRY DETAILING



EXAMPLE OF EXTERIOR FIREPLACE VENTING AND CORBEL



EXAMPLE OF EXTERIOR MASSING, WINDOWS, AWNING DETAIL



EXAMPLE OF EXTERIOR BARREL ARCH DESIGN DETAILING

PROJECT DESIGN EXAMPLES

DATE:	2/17/2016
BY:	ADP/STP
CHECKED BY:	ADP/STP
SCALE:	AS SHOWN

**OXNARD SHORES COMPANY, LLC**  
 1400 OXNARD AVENUE  
 OXNARD, CA 93030  
 TEL: 805.321.1000  
 FAX: 805.321.1001



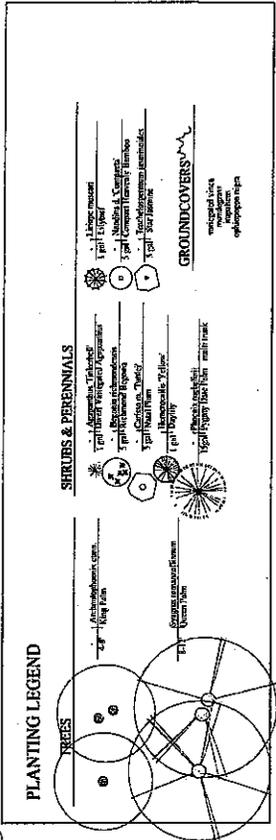
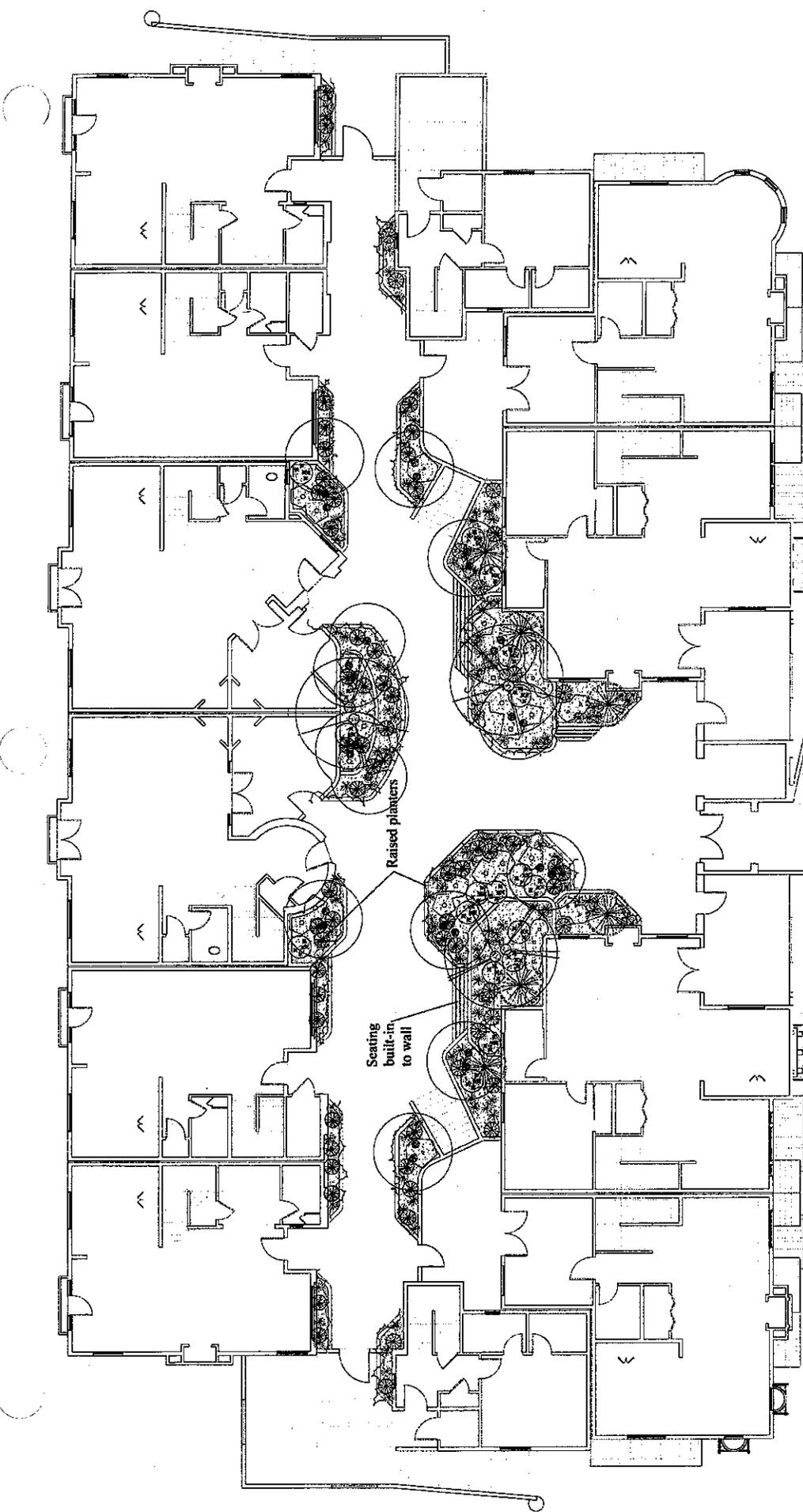
**GRAPHICS**  
 1400 OXNARD AVENUE  
 OXNARD, CA 93030  
 TEL: 805.321.1000  
 FAX: 805.321.1001

**PROPOSED 70 UNIT TOWNHOME COMPLEX**  
**THE ANACAPA TOWNHOMES**  
 HODDLEY ROAD, OXNARD, CA

**P-29**  
 SHEET NO. 29 OF 30







**Philip S. Miller & Assoc.**  
 Site Planning and Landscape Architecture  
 620 Cole Budge Court, CA 92012 (657) 383-7391  
 Email: philipmiller@verizon.net

**OXNARD SHORES COMPANY, LLC**  
 1155 SANDHURST ROAD  
 OXNARD, CA 93025  
 TEL: (805) 345-5577  
 FAX: (805) 345-5578

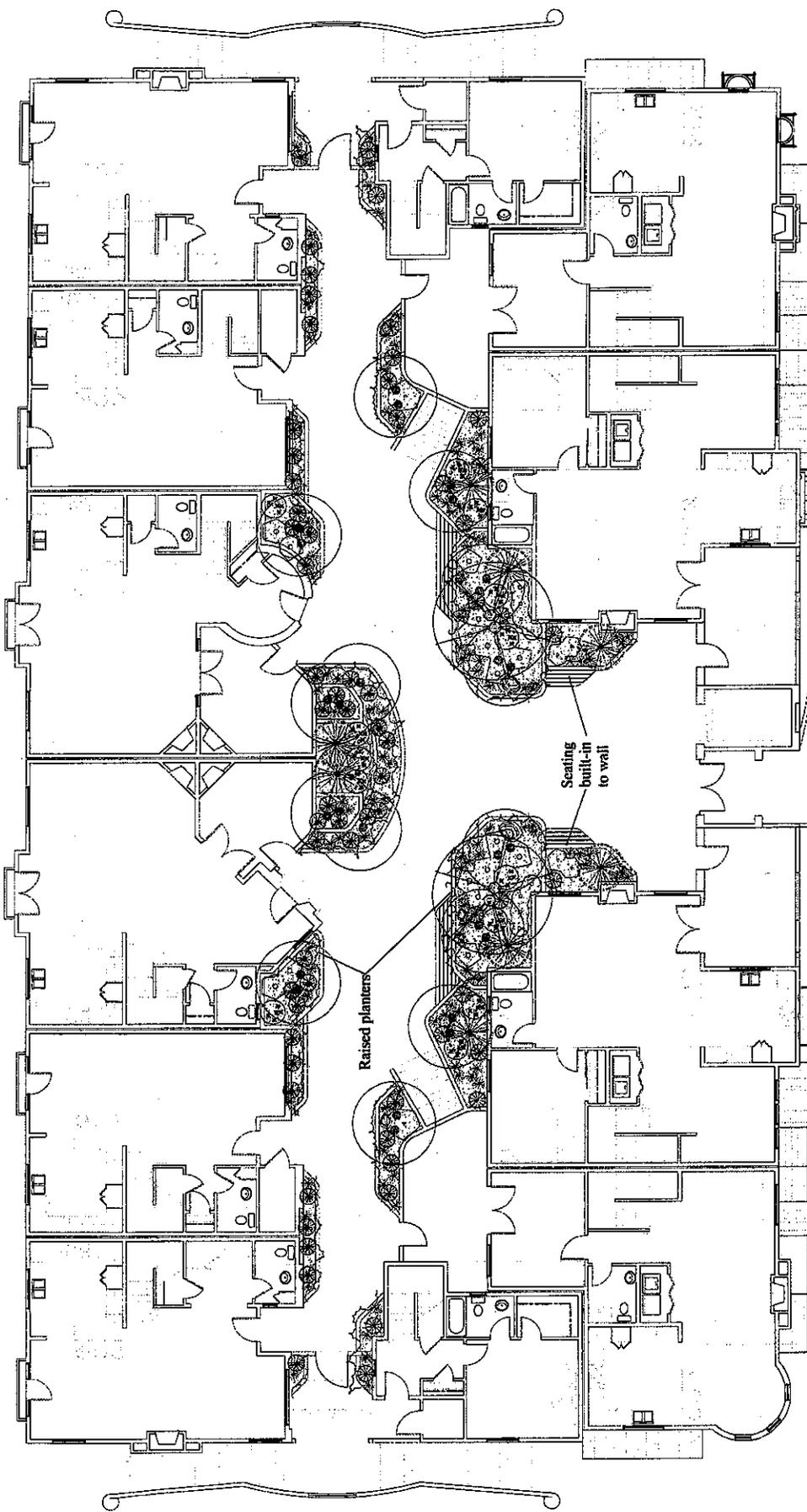
**BUILDING 1  
 PODIUM LEVEL**

**DRAPHTICS**  
 Andrew David Brown  
 Steven H. Olschky  
 8317 Thomas Avenue, Carlsbad, CA 92009  
 (619) 434-9400  
 WWW.DRAUGHTICS.COM  
 PLO PRESENTATION - DRAFTING - DESIGN

**PROPOSED 70 UNIT TOWNHOME COMPLEX**  
**ANACAPA TOWNHOMES**  
 1100LEY ROAD, OXNARD, CA

**L-3**  
 SHEET NO. OF 3

DATE:	10/20/2010
BY:	PHILIP MILLER
CHECKED BY:	ANDREW BROWN
DATE:	10/20/2010
BY:	PHILIP MILLER
CHECKED BY:	ANDREW BROWN
DATE:	10/20/2010
BY:	PHILIP MILLER
CHECKED BY:	ANDREW BROWN



**PLANTING LEGEND**

**TREES**

- 1" - 1/2" Calceolarius
- 2" - 1/2" Calceolarius
- 3" - 1/2" Calceolarius
- 4" - 1/2" Calceolarius
- 5" - 1/2" Calceolarius
- 6" - 1/2" Calceolarius
- 7" - 1/2" Calceolarius
- 8" - 1/2" Calceolarius
- 9" - 1/2" Calceolarius
- 10" - 1/2" Calceolarius

**SHRUBS & PERENNIALS**

- 1" - 1/2" Calceolarius
- 2" - 1/2" Calceolarius
- 3" - 1/2" Calceolarius
- 4" - 1/2" Calceolarius
- 5" - 1/2" Calceolarius
- 6" - 1/2" Calceolarius
- 7" - 1/2" Calceolarius
- 8" - 1/2" Calceolarius
- 9" - 1/2" Calceolarius
- 10" - 1/2" Calceolarius

**GROUNDCOVERS**

- 1" - 1/2" Calceolarius
- 2" - 1/2" Calceolarius
- 3" - 1/2" Calceolarius
- 4" - 1/2" Calceolarius
- 5" - 1/2" Calceolarius
- 6" - 1/2" Calceolarius
- 7" - 1/2" Calceolarius
- 8" - 1/2" Calceolarius
- 9" - 1/2" Calceolarius
- 10" - 1/2" Calceolarius

PROPOSED 70 UNIT TOWNHOME COMPLEX  
**ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA

**DRAPHTICS**  
 1070 Yuma Ave  
 Oxnard, CA 93030  
 805.461.1111  
 www.draphtics.com

**BUILDINGS 2,3&4  
 PODIUM LEVEL**

**Philip S. Miller & Assoc.**  
 Site Planning and Landscape Architecture  
 6300 Oak Ridge Canyon, CA 93012 (805) 343-3791  
 E-mail: philipmiller@verizon.net

**OXNARD SHORES COMPANY, LLC**  
 1810 20th St  
 Oxnard, CA 93030

DATE	BY
APPROVED	DATE
DATE	BY
DATE	BY



Scale: 3/16" = 1'-0"

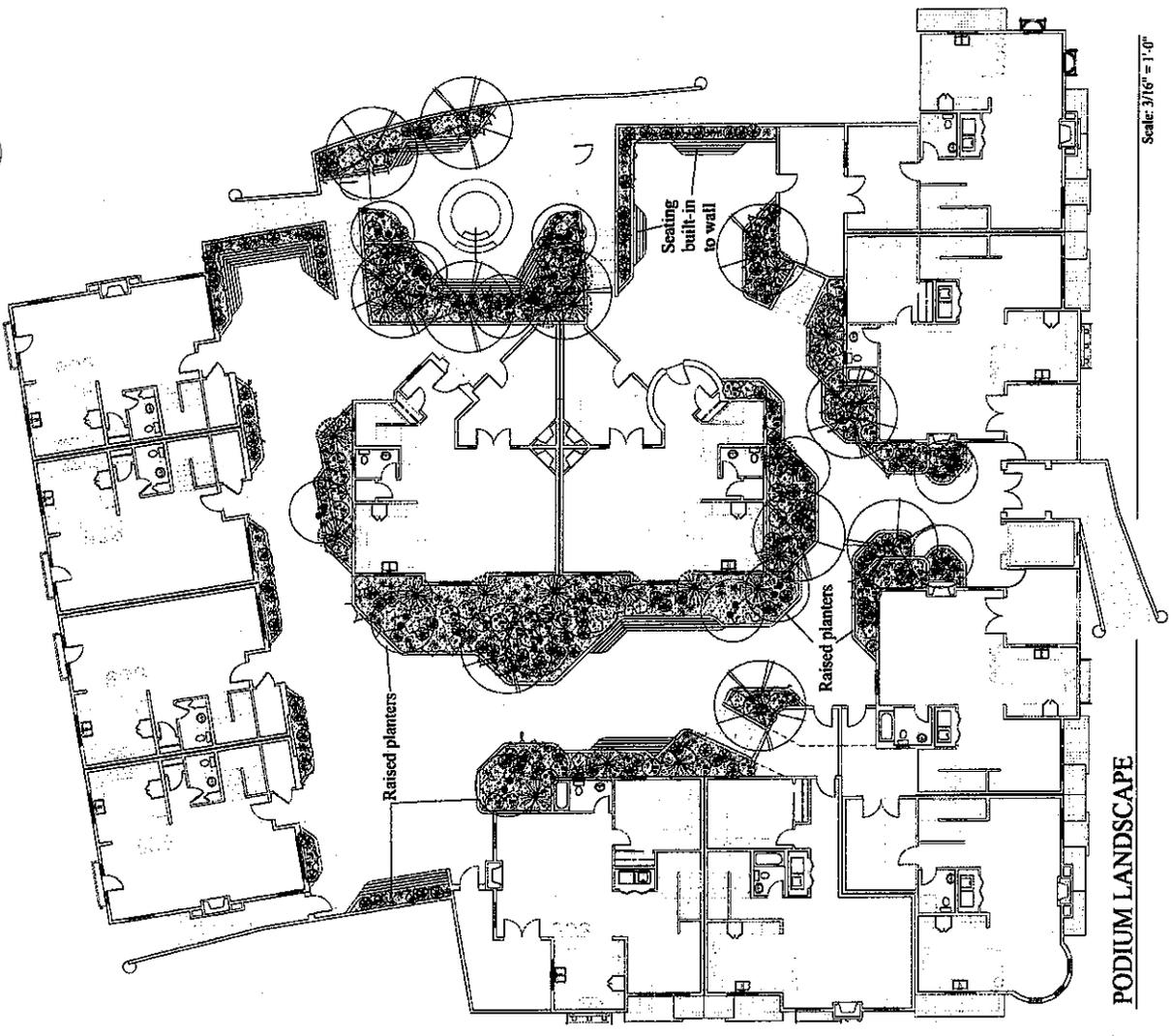
PROPOSED 70 UNIT TOWNHOME COMPLEX  
**ANACAPA TOWNHOMES**  
 WOODLEY ROAD, OXNARD, CA

Author David Brown  
 Steven B. Oberdorfer  
**DR A P H I C S**  
 833 Torres Avenue, Carlsbad, CA 92008  
 info@drgraphics.com (619) 435-1977 www.drgraphics.com  
 L-D PREPARATION - GRAYSCALE - REVISION

**BUILDING 5  
 PODIUM LEVEL  
 LANDSCAPE & SPA**

**OXNARD SHORES COMPANY, LLC**  
 14400 155TH AVE  
 FARMERSVILLE  
 OXNARD, CA 93021

DATE:	11/11/11
BY:	DAVID BROWN
CHECKED BY:	STEVEN OBERDORFER
SCALE:	AS SHOWN
PROJECT:	OXNARD SHORES
DATE:	11/11/11



PODIUM LANDSCAPE

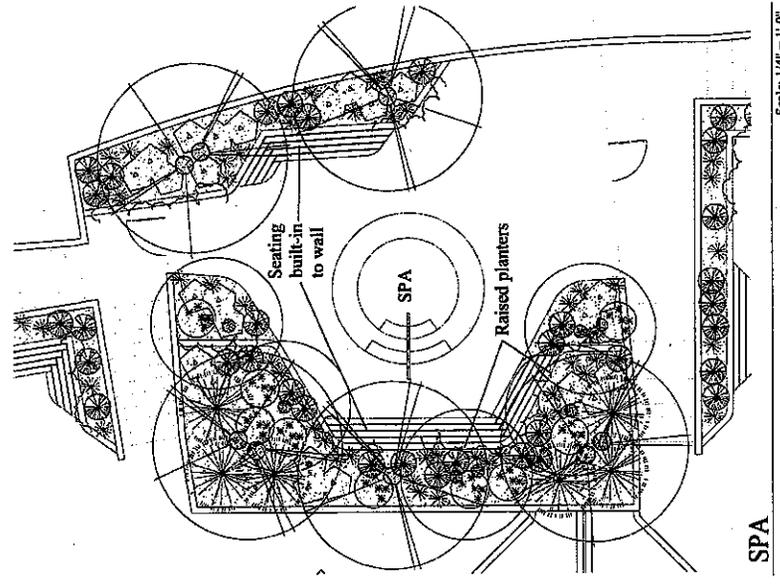
**PLANTING LEGEND**

**SHRUBS & PERENNIALS**

- 1" - 1" (1/2" - 1" High) - 1" (1/2" - 1" High)
- 2" - 2" (1" - 2" High) - 2" (1" - 2" High)
- 3" - 3" (2" - 3" High) - 3" (2" - 3" High)
- 4" - 4" (3" - 4" High) - 4" (3" - 4" High)
- 5" - 5" (4" - 5" High) - 5" (4" - 5" High)
- 6" - 6" (5" - 6" High) - 6" (5" - 6" High)
- 7" - 7" (6" - 7" High) - 7" (6" - 7" High)
- 8" - 8" (7" - 8" High) - 8" (7" - 8" High)
- 9" - 9" (8" - 9" High) - 9" (8" - 9" High)
- 10" - 10" (9" - 10" High) - 10" (9" - 10" High)

**GROUND COVERS**

- 1" - 1" (1/2" - 1" High) - 1" (1/2" - 1" High)
- 2" - 2" (1" - 2" High) - 2" (1" - 2" High)
- 3" - 3" (2" - 3" High) - 3" (2" - 3" High)
- 4" - 4" (3" - 4" High) - 4" (3" - 4" High)
- 5" - 5" (4" - 5" High) - 5" (4" - 5" High)
- 6" - 6" (5" - 6" High) - 6" (5" - 6" High)
- 7" - 7" (6" - 7" High) - 7" (6" - 7" High)
- 8" - 8" (7" - 8" High) - 8" (7" - 8" High)
- 9" - 9" (8" - 9" High) - 9" (8" - 9" High)
- 10" - 10" (9" - 10" High) - 10" (9" - 10" High)



SPA



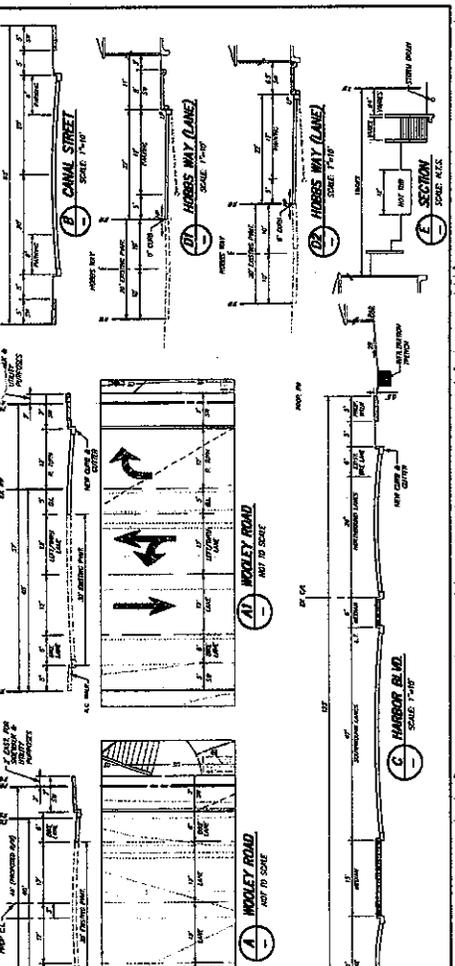
VARIES



**LEGEND**

	STREET RIGHT-OF-WAY
	CATCH BASIN
	SEWER
	STORM DRAIN
	WATER CATCH BASIN
	GAS
	TOP OF STREET
	EASEMENT
	EASEMENT LINE
	EASEMENT AREA
	EASEMENT WALL
	STREET LIGHT
	FIRE HYDRANT
	SEWER MANHOLE
	STORM DRAIN MANHOLE
	UTILITY POLE
	UTILITY LINE
	UTILITY TUNNEL
	UTILITY VAULT
	UTILITY CHAMBER
	UTILITY VAULT CHAMBER
	UTILITY VAULT CHAMBER WALL
	UTILITY VAULT CHAMBER FLOOR
	UTILITY VAULT CHAMBER CEILING
	UTILITY VAULT CHAMBER WALL AND FLOOR
	UTILITY VAULT CHAMBER WALL AND CEILING
	UTILITY VAULT CHAMBER FLOOR AND CEILING
	UTILITY VAULT CHAMBER WALL, FLOOR AND CEILING

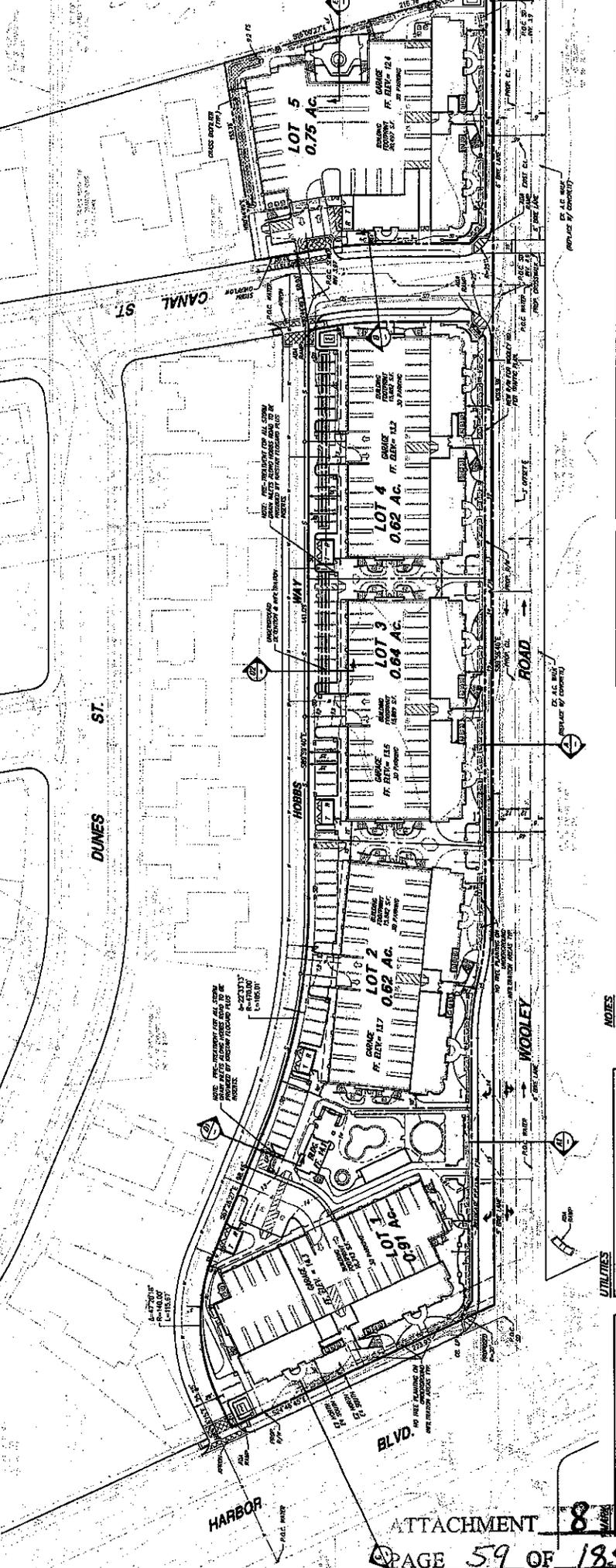
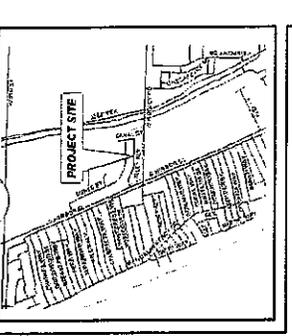
**SECTIONS**



**LAND USE DATA**

TOTAL PROJECT AREA:	136 AC
RESIDENTIAL TOWNHOUSE UNITS:	70 UNITS
PROJECT DENSITY:	70/136 AC
MOOREY ROAD WIDENING:	12.57 AC
CANAL STREET WIDENING:	0.50 AC
A. AREA - LOT 1:	0.91 AC
B. AREA - LOT 2:	0.62 AC
C. AREA - LOT 3:	0.62 AC
D. AREA - LOT 4:	0.62 AC
E. AREA - LOT 5:	0.75 AC
TOTAL NET AREA:	134 AC
C. AREA DATA:	8400 SF. = 5.8 E.P. SITE
D. AREA DATA:	8400 SF. = 4.8 E.P. SITE
D. PARKING:	120 STALLS
RESIDENT PARKING REQUIRED:	120 STALLS
1. PER UNIT @ 1.7 UNITS:	120 STALLS
2. PER UNIT @ 1.7 UNITS:	120 STALLS
3. PER UNIT @ 1.7 UNITS:	120 STALLS
4. PER UNIT @ 1.7 UNITS:	120 STALLS
5. PER UNIT @ 1.7 UNITS:	120 STALLS
6. PER UNIT @ 1.7 UNITS:	120 STALLS
7. PER UNIT @ 1.7 UNITS:	120 STALLS
8. PER UNIT @ 1.7 UNITS:	120 STALLS
9. PER UNIT @ 1.7 UNITS:	120 STALLS
10. PER UNIT @ 1.7 UNITS:	120 STALLS

**VICINITY MAP**



**ANACAPA TOWNHOMES  
TENTATIVE TRACT NO. 5843  
FOR CONDOMINIUM PURPOSES**

CITY OF ORANGE COUNTY OF CALIFORNIA  
DATE: APR 1, 2009  
DRAWN: J. J. JONES  
SCALE: 1"=30'

NO.	NAME	ADDRESS	PHONE
1	ARCHITECT	JENSEN ARCHITECTURE, INC.	(714) 961-1177
2	ENGINEER	WILLIAMS ENGINEERING, INC.	(714) 961-1177
3	PLANNING	PLANNING	
4	LANDSCAPE ARCHITECT	LANDSCAPE ARCHITECT	
5	CONTRACTOR	CONTRACTOR	
6	INSURANCE	INSURANCE	
7	LEGAL COUNSEL	LEGAL COUNSEL	
8	FINANCIAL ADVISOR	FINANCIAL ADVISOR	
9	MARKETING	MARKETING	
10	SALES	SALES	

**SEE SHEET 2 FOR TRANSITION  
MOOREY ROAD**

**NOTES**

- SEE SHEET 2 FOR TRANSITION MOOREY ROAD

**REFERENCES**

ASBESTOS: CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS (CDIR) - 800-999-9434  
 AIR QUALITY: CALIFORNIA AIR RESOURCES BOARD (CARB) - 916-227-2000  
 ARCHITECTURE: JENSEN ARCHITECTURE, INC. - 714-961-1177  
 ENGINEERING: WILLIAMS ENGINEERING, INC. - 714-961-1177  
 LANDSCAPE ARCHITECTURE: LANDSCAPE ARCHITECT - 714-961-1177  
 PLANNING: PLANNING - 714-961-1177  
 CONSTRUCTION: CONSTRUCTION - 714-961-1177  
 INSURANCE: INSURANCE - 714-961-1177  
 LEGAL COUNSEL: LEGAL COUNSEL - 714-961-1177  
 FINANCIAL ADVISOR: FINANCIAL ADVISOR - 714-961-1177  
 MARKETING: MARKETING - 714-961-1177  
 SALES: SALES - 714-961-1177

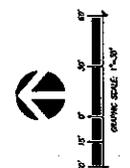
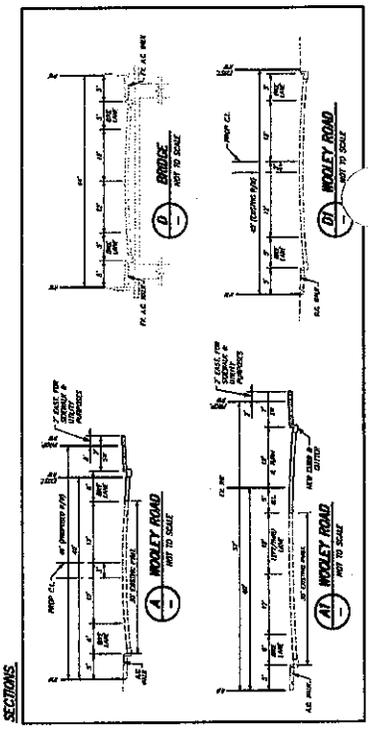
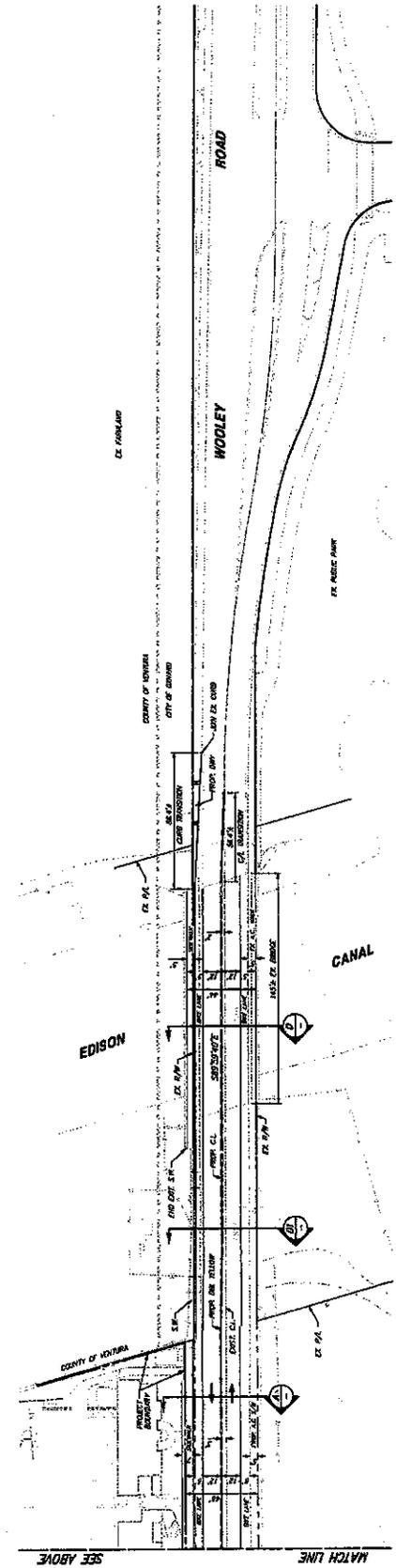
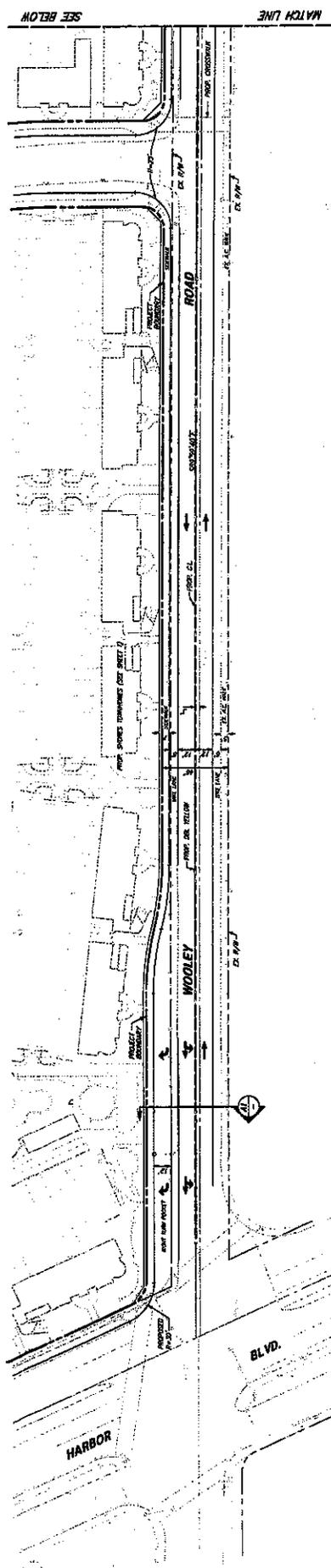
**UTILITIES**

ALL UTILITIES SHOWN ARE BASED ON RECORD DRAWINGS AND FIELD SURVEY. THE LOCATION AND DEPTH OF UTILITIES IS NOT GUARANTEED BY THE ENGINEER. THE USER OF THIS PLAN SHALL VERIFY THE LOCATION AND DEPTH OF UTILITIES PRIOR TO CONSTRUCTION.

**EASEMENT SCHEDULE**

FOR LAND ADJACENT TO THE TRACT SHOWN HEREON, THE FOLLOWING EASEMENTS ARE GRANTED TO THE CITY OF ORANGE:

NO.	DESCRIPTION	DATE
1	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
2	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
3	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
4	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
5	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
6	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
7	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
8	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
9	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009
10	EASEMENT FOR THE CITY OF ORANGE	APR 1, 2009



CITY OF KANSAS  
 COUNTY OF OSAGE  
 ANACAPA TOWNHOMES  
 TENTATIVE TRACT No. 5843  
 FOR CONDOMINIUM PURPOSES

SHEET 2 OF 2  
 DATE: JAN 28, 2011  
 SCALE: 1"=30'  
 PROJECT: ANACAPA TOWNHOMES

DESIGNER: JENSEN PERINI INC.  
 10700 DANFORTH STREET  
 OVERLAND PARK, KS 66214-9979  
 PHONE: 913.241.1000  
 FAX: 913.241.1001  
 WWW: JENSENPERINI.COM

CHECKED BY: [Signature]  
 DATE: [Date]

ALL INFORMATION CONTAINED HEREIN IS THE PROPERTY OF JENSEN PERINI INC. AND IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. NO PART OF THIS DOCUMENT IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF JENSEN PERINI INC.





Planning Division  
214 South 'C' Street  
Oxnard, CA. 93030  
Phone (805) 385-7858  
Fax (805) 385-7417

**INITIAL STUDY  
MITIGATED NEGATIVE DECLARATION NO. 09-03**

Coastal Development Permit (PZ No. 08-400-04)  
Tentative Subdivision Map No. 5843 (PZ No. 09-300-01)  
Coastal Zone Variance (PZ No. 09-590-01)

4781 and 5001 West Wooley Road  
Northeast Corner of Harbor Boulevard and Wooley Road

January 15, 2010

**Introduction**

This Initial Study/Mitigated Negative Declaration has been prepared in accordance with relevant provisions of the *California Environmental Quality Act (CEQA) of 1970*, as amended, *CEQA Guidelines* as revised, and the City of Oxnard Threshold Guidelines, as revised. Section 15063(c) of the *CEQA Guidelines* states that the purposes of an Initial Study are to:

1. Provide the Lead Agency (i.e. City of Oxnard) with information to use as the basis for deciding whether to prepare a Program, Supplemental, Subsequent, or Project Environmental Impact Report (EIR); a Negative Declaration, or a Mitigated Negative Declaration;
2. Enable an applicant and/or Lead Agency to modify a project to mitigate adverse impacts, thereby enabling the project to qualify for a Negative Declaration;
3. Assist the preparation of an EIR, if one is required, by:
  - Focusing the EIR on the effects determined to be significant;
  - Identifying the effects determined not to be significant;
  - Explaining the reasons why potentially significant effects would not be significant; and
4. Facilitate environmental assessment early in the design of a project;
5. Provide documentation of the factual basis for the finding in a Negative Declaration that a project will not have a significant effect on the environment;
6. Eliminate unnecessary environmental review ; and
7. Determine whether a previously prepared EIR could be used with the project.

The purpose of the City of Oxnard *Threshold Guidelines* is to inform the public, project applicants, consultants, and City staff of the threshold criteria and standard methodology used in determining whether or not a project (individually or cumulatively) could have a significant effect on the environment. Furthermore, the *Threshold Guidelines* provide instructions for completing the Initial Study and determining the type of environmental document required for individual projects.

An EIR is a detailed statement that describes and analyzes the significant environmental impacts of a proposed project, discusses ways to reduce or avoid them, and suggests alternatives to the project, as proposed. Determining the significance of environmental impacts is a critical and possibly controversial aspect of the environmental review process. A determination of significance may require that the project be substantially altered, or that mitigation measures be employed to avoid the impact or reduce it below the level of significance. If the significant adverse impact cannot be reduced or avoided, an EIR must be prepared to allow decision makers to consider adopting overriding considerations.

Determining the significance of impacts is often controversial because the decision requires staff to use their judgment regarding a topic that may not be clearly defined by an objective scientific standard or the law. The State CEQA Guidelines define the term “significant impact on the environment” as a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project. However, there are topics for which there is no clear definition of what constitutes a substantial change because the significance of an activity may vary according to location, context, and/or local community standards.

To help clarify and standardize decision-making in the environmental review process, Oxnard has adopted thresholds of environmental significance in several topical areas. Thresholds are measures of environmental change that are either quantitative for topics like noise, air quality, and traffic; or qualitative for topics like aesthetics, land use compatibility, and biology. For some projects special studies and/or outside professional judgment may enter into the decision-making process. Therefore, Oxnard’s thresholds are intended to supplement CEQA provisions governing the definition of significance.

The City’s 1995 environmental thresholds are being updated as part of the 2030 General Plan environmental review and certification process. New thresholds are anticipated by the end of 2009 or early 2010. In the interim, thresholds used in recent large environmental impact reports (The Village Specific Plan, Ormond Beach) and the 2030 General Plan EIR are considered more current than the 1995 *Thresholds Guidelines* where they may conflict.

When other agencies have some jurisdiction or discretionary action over a project, the project proponent will have to meet the thresholds, design, mitigation, and monitoring requirements imposed by those agencies, as well as those established by the City of Oxnard.

**CITY OF OXNARD  
 INITIAL STUDY ENVIRONMENTAL CHECKLIST FORM**

1. Project Title: Anacapa Townhomes Condominium Development
2. Lead Agency Name and Address: City of Oxnard, Planning Division, 214 South 'C' Street, Oxnard, CA 93030
3. Contact Person and Phone Number: Brian Foote, A.I.C.P., Associate Planner (805) 385-8312
4. Project Location: 4781 and 5001 W. Wooley Road (northeast corner of Harbor Boulevard and Wooley Road). APN: 196-003-329. See Figure 1 (Vicinity Map) and Figure 2 (Aerial Photo).
5. Applicant Name and Address: Oxnard Shores Co. LLC, 1015 S. Harbor Boulevard, Oxnard, CA. 93035
6. General Plan Designation: Residential Low Medium (RLM)
7. Zoning: Garden Apartment Coastal (R-3-C, allows up to 18 units per net acre)
8. Description of Project: The proposal is for development of a 3.91 acre vacant property, to include:
  - a) Tentative Subdivision Map for Tract No. 5843 (PZ No. 09-300-01) to subdivide the property into five residential parcels and create a total of 70 air-rights condominium units;
  - b) A Coastal Development Permit (PZ No. 08-400-04) to construct five, multifamily, three-story structures with a combined total of 70 condominium dwelling units, parking garages, recreation facilities, and associated landscaping and driveways (see Figures 3a & 3b); and,
  - c) A Coastal Zone Variance (PZ No. 09-590-01) to modify development standards for front, side, and rear yard setbacks to allow architectural projections (e.g. staircases, vertical columns, portions of two rear walls, and a portion of one side wall), and distance between parking spaces and dwelling units. See Figure 4.

*a.) Tentative Tract Map*

The City Council adopted Ordinance No. 2159 on April 19, 1988, approving the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property (Instrument No. 88-059788 recorded on May 2, 1988, and Instrument No. 89-174751 recorded on October 31, 1989). The agreement established the R-3-C zoning which allows up to 70 units based on the estimated gross acreage of 3.91 acres (i.e. 17.9 units per acre).

**Table 1. Proposed Lot Sizes**

Lot Number	Proposed Land Use	Number of Condo Units	Lot Size (Approximate)
1	Residential Condominiums	14	39,640 SF (0.91 acre)
2	Residential Condominiums	14	27,007 SF (0.62 acre)
3	Residential Condominiums	14	27,878 SF (0.64 acre)
4	Residential Condominiums	14	27,007 SF (0.62 acre)
5	Residential Condominiums	14	32,670 SF (0.75 acre)
<i>Total</i>		70	3.54 acres <i>(net after street dedications)</i>

Access to the development will be provided by extending Canal Street by approximately 150 feet to connect to Wooley Road and utilizing the existing alley between Canal Street and Harbor Boulevard. The developer will dedicate 9,012 square-feet to widen Canal Street and construct an intersection at Wooley Road. Also, the developer will dedicate a 8,704 square-foot portion at the southwest corner of the property in order to construct a 250-foot long right-turn lane on westbound Wooley Road. The existing alley between Harbor Boulevard and Canal Street will be widened, resurfaced, and reconstructed with full improvements.

*b.) Coastal Development Permit*

The proposed project includes five multi-family structures with a combined total of 70 townhouse-style units. The exterior architectural design is in the Spanish Mediterranean style. There will be a total of 11 floor plans (see Table 2) that vary the unit sizes and interior layouts. The proposed Site Plan and Elevations are shown in Figures 3a and 3b. The applicant has received approval from the City Council to pay the in-lieu affordable housing fee rather than provide affordable units within the project for low-income and very low-income households.

**Table 2. Plan Type, Size, and Quantity**

Plan Type	Total Floor Area (Square Feet)	Bed/Bath	Unit Quantity
A	1,849	2 / 3	3
A1	1,858	2 / 3	1
B	1,818	2 / 3	5
B1	1,848	2 / 3	1
C	1,742	2 / 2	10
C1	1,672	2 / 2	1
D	1,551	1 / 2	1
E	1,006	1 / 1	8
F	1,410	2 / 2	8
G	1,433	2 / 2	2
H	1,561	2 / 2½	2
H2	1,562	2 / 2½	8
J	1,546	2 / 2½	6
J1	1,550	2 / 2½	4
K	1,951	3 / 2½	5
L	1,939	3 / 2½	1
L1	1,987	3 / 2½	4
<i>Total</i>			<i>70</i>

The development standard for Interior Yard Area requires a minimum of 25% of the site area (or 38,732 square-feet) and is satisfied with approximately 26% (or 40,492 square-feet) provided on-site. Additional private open space, either a private patio or balcony, is also provided for each unit and varies in size between 102 and 271 square-feet (providing a total of approximately 8,307 square-feet of private open space in addition to the common open space areas). A minimum of 181 on-site parking spaces are required, and a total of 200 spaces will be provided in accordance with the City's Zoning Ordinance. The proposed project otherwise meets all current development standards, and would be constructed according to current energy and building codes.

c.) Coastal Zone Variance

A Coastal Zone Variance request was submitted to modify the development standards for front, side, and rear setbacks. The specific variances are listed in Table 3 by number, and Figure 4 illustrates the locations of each corresponding number. The architectural projections include seven staircases, two sets of vertical columns, portions of two rear walls, and a portion of one side wall. Due to the shape of the property and narrow property boundaries, the resulting design and layout does not allow the project to comply with some setback areas.

**Table 3. Variance for Front/Side/Rear Setbacks (see Figure 4)**

Variance Number	Description	Minimum Setback (ft)	Proposed Setback (ft)	Variance Request (ft)
1	Stairway	10	5	5
2	Rear Wall	25	5	20
3	Stairway	15	3	12
4	Rear Wall	25	19	6
5	Stairway	15	0	15
6	Column	25	23	2
7	Column	25	23	2
8	Stairway	15	0	15
9	Column	25	23	2
10	Column	25	23	2
11	Column	10	3	7
12	Stairway	10	4	6
13	Stairway	15	0	15
14	Column	10	8.5	1.5
15	Side Wall	10	4	6
16	Column	10	6	4

A Variance approval may also be required for the advisory standards pertaining to distances between parking spaces and dwelling units they serve. The maximum distances from parking spaces to the units they serve are: a) 50 feet for resident parking, and b) 100 feet for visitor parking. Resident parking in all five structures will not comply with the 50 feet maximum, and visitor parking for Buildings 1 and 5 will not comply with the 100 feet maximum. Due to the shape of the property and its affect on the proposed site design, the parking areas do not allow the developer to comply with the maximum distances. The proposed project otherwise meets all current development standards.



Figure 2: Aerial Photo

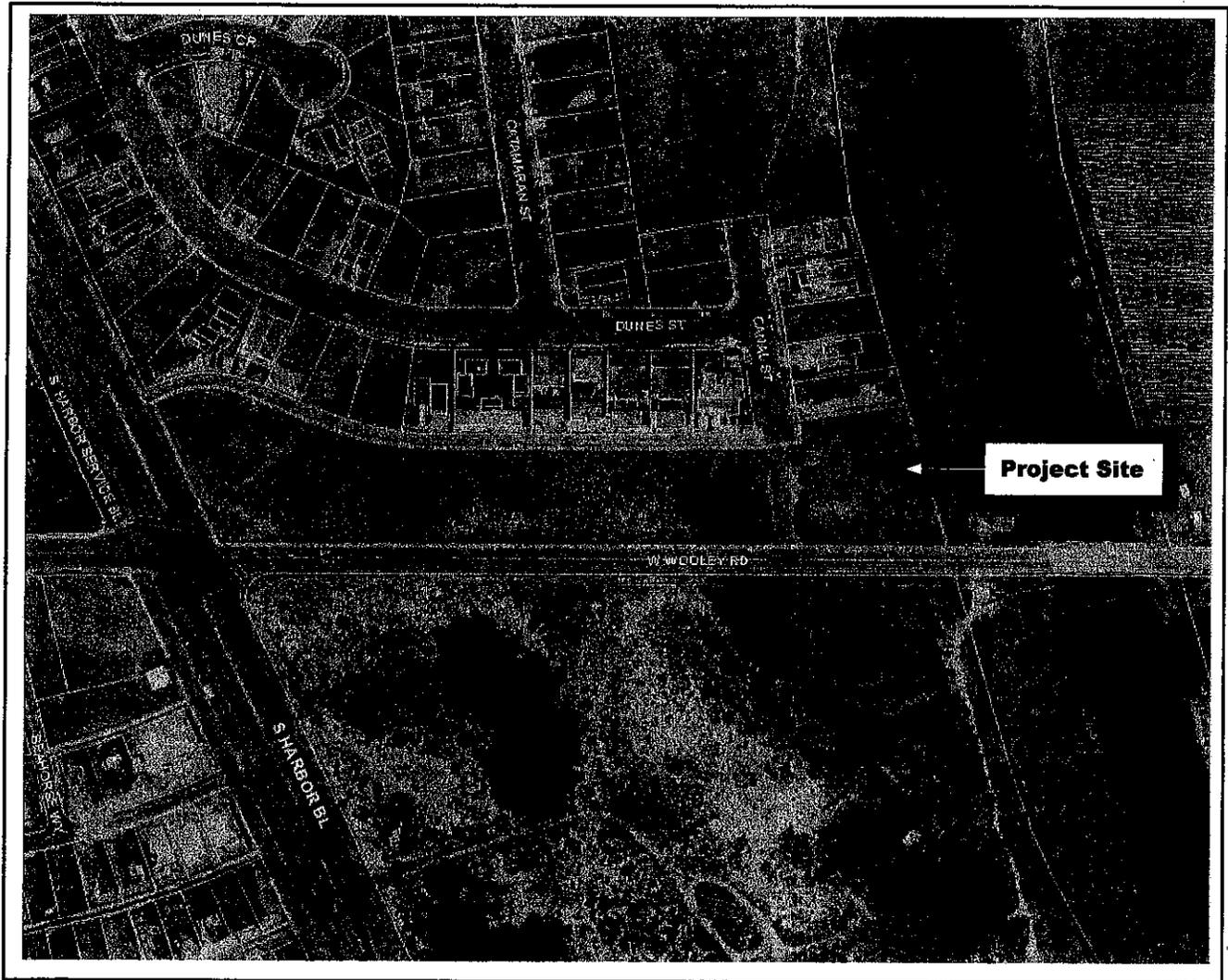


Figure 3a: Conceptual Site Plan & Elevations



**Figure 3b: Conceptual Site Plan & Elevations**

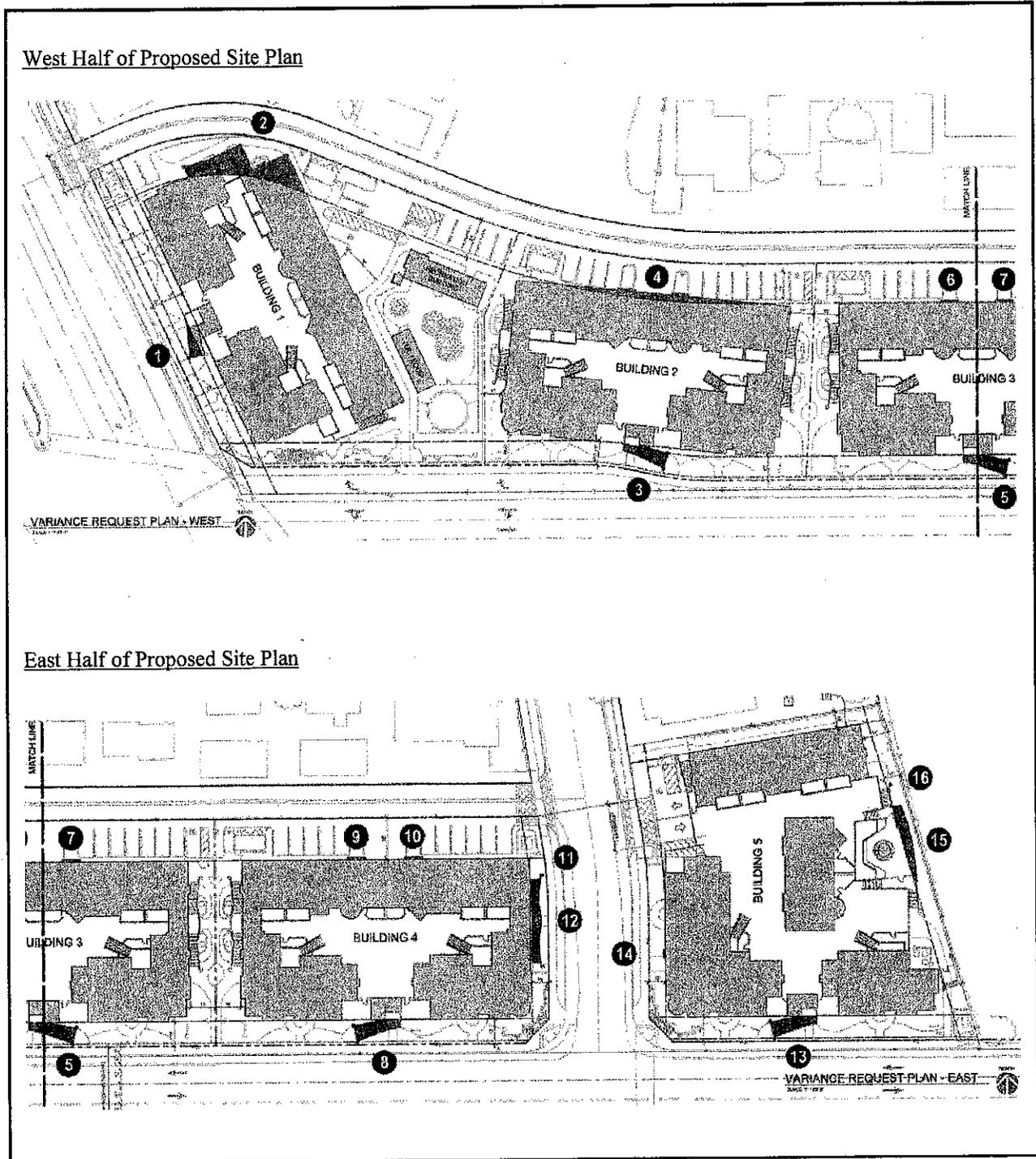
Proposed Structures 1 – 4, West Half (Street-Side View, Typical)



Proposed Structures 1 – 4, East Half (Street-Side View, Typical)



Figure 4: Variance Request



9. **Surrounding Land Uses and Setting:** The project site is vacant with non-irrigated natural vegetation. To the south is Wooley Road abutting the site and open space in the Coastal Resource Protection (RP) zone. To the west is Harbor Boulevard abutting the site, and a retail center at the northwest corner of Harbor Boulevard and Wooley Road in the Coastal Neighborhood Commercial (CNC) zone. Further to the west on Wooley Road are multi-family apartments in the R-3-C zone, and a neighborhood with single-family detached homes in the Single-Family Beach (R-B-1) zone. To the north are single and duplex residences in the Coastal Low-Density Multiple Family (R-2-C) zone. To the immediate east is the Reliant/Edison canal that provides cooling water to the Reliant Mandalay power plant. Beyond the canal is agricultural land in unincorporated County of Ventura. See Figure 5, Coastal Zoning Map.

**Table 4. General Plan, Zoning Designations, and Land Use**

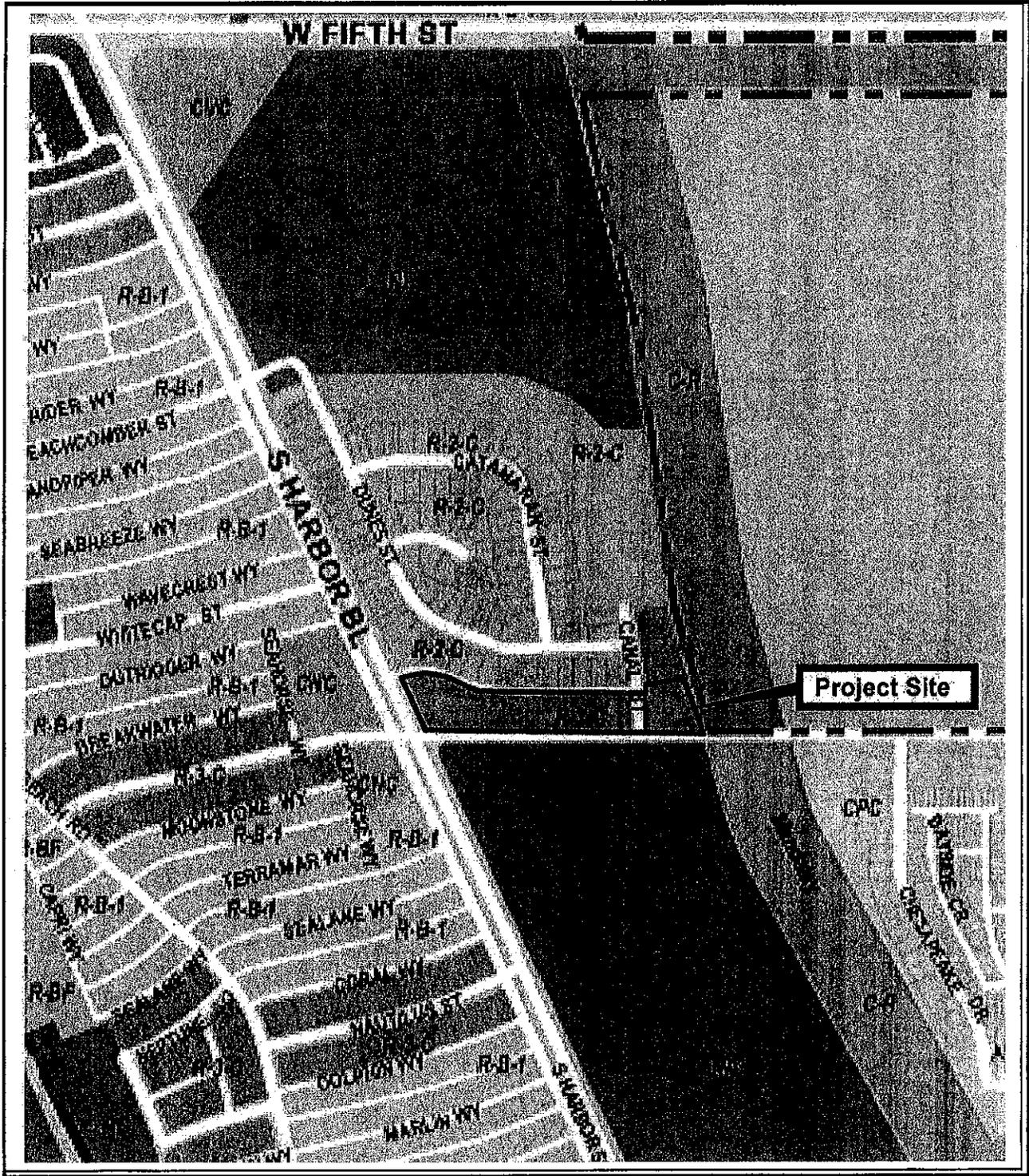
Location	General Plan Designation	Zoning Designation	Neighborhood Name	Existing Land Use
Project site	Coastal Zone	R-3-C	Oxnard Dunes	Vacant
North	Coastal Zone	R-2-C	Oxnard Dunes	Multi-Family & Single-Family Attached Homes
South	Coastal Zone	RP	Channel Islands	Open Space
East	EC	CR	–	Edison Canal & Agriculture
West	Coastal Zone	CNC, R-3-C, R-B-1	Oxnard Shores	Retail Shopping Center, Multi-Family Attached Units, Single-Family Detached Homes

*Legend:* R-3-C – Garden Apartment Coastal      RP – Coastal Resource Protection  
 R-2-C – Multiple-Family Coastal              CR – Community Reserve  
 R-B-1 – Single-Family Beach                  CNC – Coastal Neighborhood Commercial  
 EC – Edison Canal

10. Other agencies whose approval is required (e.g., permits, financing approval, or participation agreement): Coastal Permit from the California Coastal Commission (according to the *Post-LCP Permit and Appeal Jurisdiction Map No. 131 (Oxnard Quadrangle)* adopted on November 20, 1985).
11. Coastal Zone and California Coastal Commission: The project site is located within the Oxnard Coastal Zone. Oxnard has a certified Coastal Land Use Plan and Program. This project, if approved by the Planning Commission and City Council of the City of Oxnard, would not be appealable to the Coastal Commission per Section 17-58(K) of the Oxnard City Code.
12. The following reports and studies have been prepared by independent consultants to analyze the proposed development, and are hereby incorporated by reference. These reports and studies are available for review at the City of Oxnard Service Center, located at 214 South ‘C’ Street in downtown Oxnard, during the hours of 8:00 a.m. to 6:00 p.m. Monday through Thursday, and 9:00 a.m. to 5:00 p.m. on alternating Fridays.
  - *Site Specific Noise Study, Oxnard Shores LLC, Shores Townhomes, Oxnard, CA.* November 5, 2008. Prepared by Acentech, Inc., Westlake Village CA.
  - *Traffic and Circulation Study (ATE Project #08116).* December 4, 2008. Updated June 10, 2009. Prepared by Associated Transportation Engineers, Santa Barbara CA.

- *A Phase One Site Survey Cultural Resources Investigation of a Tract of Land Located at the Intersection of Wooley Road and Harbor Boulevard, City of Oxnard, California.* October 27, 2008. Prepared by Environmental Research Archaeologists, Los Angeles CA.
  - *Preliminary Hydrology Report, The Shores Townhouse Project.* December 16, 2008. Prepared by Jensen Design & Survey, Inc., Ventura CA.
  - *Biological Resources Assessment: Anacapa Townhomes.* December 2009. Prepared by PCR Services Corporation, Irvine CA.
  - *Update to 2005 Biological Resources Assessment.* November 10, 2008. Prepared by PCR Services Corporation, Irvine CA.
  - *Biological Resources Assessment.* February 2005. Prepared by PCR Services Corporation, Irvine CA.
  - *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch. NE Corner of Harbor Boulevard and Wooley Road, Oxnard, California.* November 28, 2008. Prepared by Subsurface Designs, Inc., Sylmar CA.
  - *Phase II Sample Results, The Dunes Property Oxnard, California.* October 9, 2008. Prepared by Waterstone Environmental, Inc., Anaheim CA.
13. This Mitigated Negative Declaration (MND) incorporates by reference the Oxnard Village Specific Plan (Wagon Wheel) Final and Draft Environmental Impact Report (EIR No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. The Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). The cumulative impact analyses in the Wagon Wheel EIR for the western portion of Oxnard are incorporated by reference with regards to topics addressed by this MND unless other cumulative analyses are specifically presented. Reference is also made to technical studies completed as part of the 2030 General Plan and are individually included by reference and available from the City's Planning Division Internet site, at Oxnard libraries, and from the Planning Division office at the above location.

Figure 5: Coastal Zoning Map



Source: City of Oxnard Zoning Map, current edition.

**ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED**

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" or as indicated by the checklist on the following pages.

- |  |   |   |
|--|---|---|
| <input type="checkbox"/> Aesthetics                    | <input type="checkbox"/> Agricultural Resources             | <input type="checkbox"/> Air Quality            |
| <input type="checkbox"/> Biological Resources          | <input type="checkbox"/> Cultural Resources                 | <input type="checkbox"/> Geology/Soils          |
| <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology/Water Quality            | <input type="checkbox"/> Land Use/Planning      |
| <input type="checkbox"/> Mineral Resources             | <input type="checkbox"/> Noise                              | <input type="checkbox"/> Population/Housing     |
| <input type="checkbox"/> Public Services               | <input type="checkbox"/> Recreation                         | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities/Service Systems     | <input type="checkbox"/> Mandatory Findings of Significance |   |

**DETERMINATION:** (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find the proposed project COULD NOT have a significant effect on the environment and a NEGATIVE DECLARATION will be prepared.
- I find that although the project could have a significant effect on the environment there will not be a significant effect in this case because revisions in the project have been made or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signature

BRIAN FOOTE, A.I.C.P.

Print Name

Date

Associate Planner

Title

## EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analyses,” cited in support of conclusions reached in other sections may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
  - a. Earlier Analysis Used – Identify and state where they are available for review.
  - b. Impacts Adequately Addressed – Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
  - c. Mitigation Measures – For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. The explanation of each issue should identify: a) The significance criteria or threshold, if any, used to evaluate each question; and b) The mitigation measure identified, if any, to reduce the impact to less than significance.

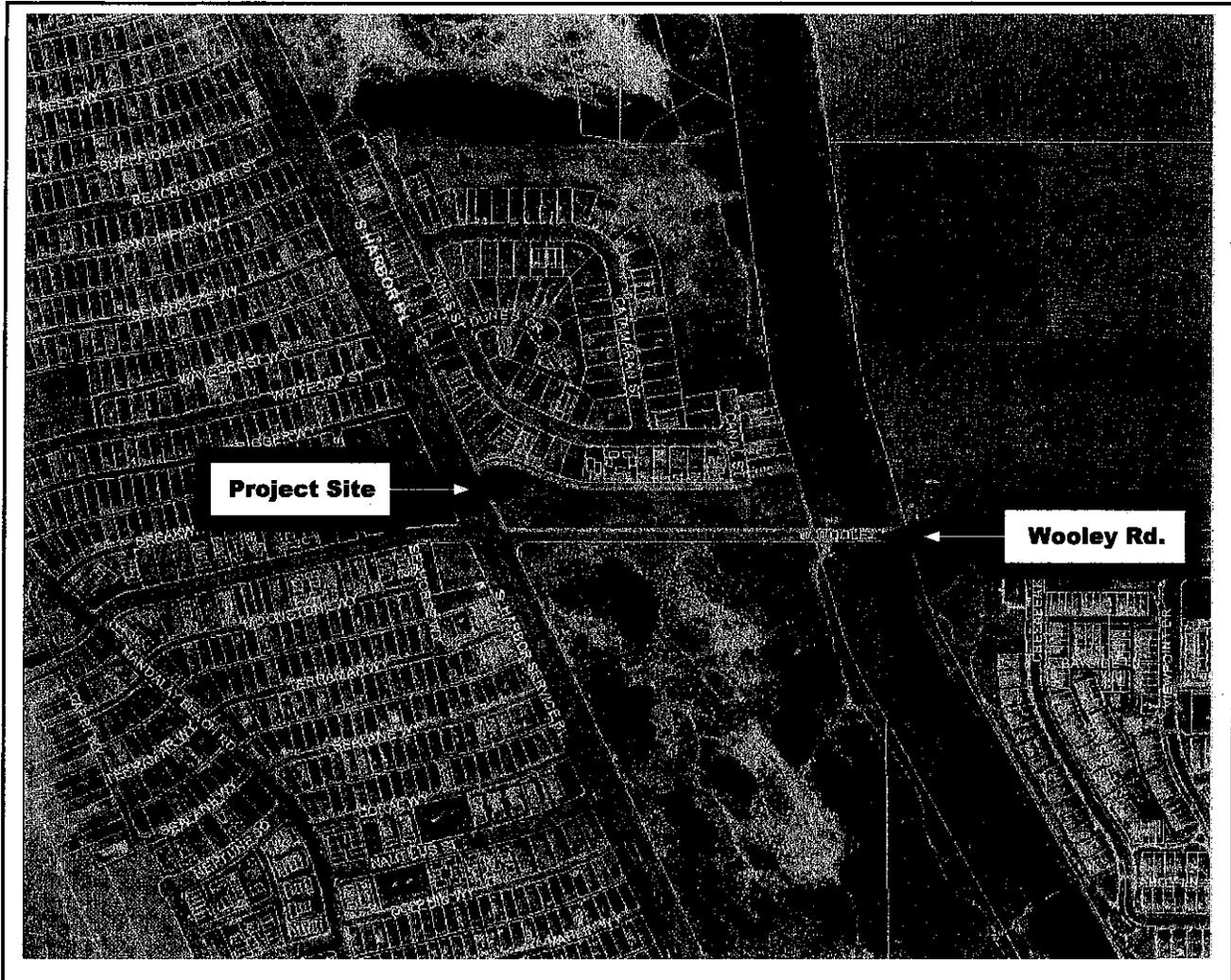
**A. AESTHETICS**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect on a scenic vista? (2020 General Plan, VIII - Open Space/ Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? (2020 General Plan, VIII - Open Space/ Conservation Element; XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Substantially degrade the existing visual character or quality of the site and its surroundings? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Create a source of substantial light or glare, which would adversely affect day or nighttime views in the area? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-2) The project proposal includes the development of three-story buildings that will not exceed the height limitations of the R-3-C zone as listed in the Coastal Zoning Ordinance. Natural scenic resources within the City of Oxnard include the beaches, sand dunes, coastline agricultural areas, and parks. Manmade aesthetic resources in the City of Oxnard include structures of historic significance and areas with special architectural design themes. The project site does not contain any historic resources. Harbor Boulevard and Wooley Road are designated as Scenic Highways in the General Plan (see Figure 6; Open Space/Conservation Element, Figure VIII-9); however, the Coastal Land Use Plan (Section 3.2.5 – Visual Resources) specifically states that the ocean is not visible from Harbor Boulevard. The majority of Harbor Boulevard south of 5<sup>th</sup> Street is enclosed by concrete block walls on both sides and residential dwellings in the Oxnard Shores and Oxnard Dunes neighborhoods, and as a result there are no views to the ocean from either Harbor Boulevard or Wooley Road. After development, the project will not block or interfere with views from Harbor Boulevard or Wooley Road toward beaches, sand dunes, or other scenic resources. As a part of the project, the existing overhead utility lines and poles adjacent to the project site (i.e. the north side of Wooley Road) will be removed and placed underground, thereby improving the aesthetic quality of the area as well as the scenic routes. Views of the Los Padres mountains from Wooley Road and, to a lesser extent, Harbor Blvd. will be blocked by the proposed development. The mountain views remain at locations just east and north of the project site. Since the project site has been previously designated for development and the proposed project conforms to the height limit, the impact on mountain views is considered less than significant. Impacts will be less than significant.

Figure 6: Scenic Routes



Source: City of Oxnard, Geographic Information Systems (ESRI ArcMap, Version 9.3).

The proposed project is consistent with the City's current Coastal Land Use Plan and the Coastal Zoning Ordinance, as well as the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property (Instrument No. 88-059788 recorded on May 2, 1988, and Instrument No. 89-174751 recorded on October 31, 1989). The proposed project is consistent with Policy 8 of the Coastal Land Use Plan, in that the City Council approved a Transfer of Development Rights from several beachfront parcels to the inland property on Wooley Road (see Figure 14 – Transfer of Development Rights) for the purpose of avoiding significant environmental impacts and preserving coastal resources and beach areas, including substantial scenic vistas and scenic resources along the beachfront. The Settlement Agreement resulted in the transfer of ownership of numerous properties to the State Lands Commission of the State of California for the purpose of creating additional public beaches, including 14 large beach areas and 9 public access areas measuring approximately 1,438 lineal feet for public access. A complete analysis with background information is provided in the *Final Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation* (ENVICOM Corp., March 18, 1988), and is hereby incorporated by reference. Overall, impacts to aesthetic resources are less than significant, and no mitigation measures are necessary.

3) The proposal is an in-fill project that is surrounded by urban development, on property with a coastal zoning designation as Garden Apartment Coastal (R-3-C). The project site is vacant and views will be improved by providing new residential development that is designed to be consistent with existing surrounding residential neighborhoods. As a part of the project, the existing overhead utility lines and poles along the south side of the project site (i.e. the north side of Wooley Road) will be removed and placed underground, thereby improving the aesthetic quality of the area and scenic routes. By developing a vacant property, the project will be consistent with the Community Design Element of the General Plan, including Objective 4 that states, "Revitalize areas of the City that are currently deteriorated or detract from the visual quality of the City." The proposed building heights will be slightly taller than adjacent residences and buildings, but height will be within the limitations of the Coastal Zoning Ordinance and may be permitted with approval of the Coastal Development Permit. This project will not have any detrimental impact to the existing visual character or quality of the site and its surroundings. There will be no impact, and no mitigation is necessary.

4) The proposed project would result in the construction of 70 condominium units within several buildings, on property with a coastal zoning designation as Garden Apartment Coastal (R-3-C). The project will generate an amount of light typical for residential development as envisioned in the Coastal Land Use Plan and Coastal Zoning Ordinance. Light and glare can be expected from new street lights, exterior light fixtures, parking lot lighting, and vehicles. As a result, there will be additional sources of light or glare since the project site is currently vacant and unimproved. However, the Design Standards of the Coastal Zoning Ordinance require that light sources be stationary and deflected away from adjacent properties. The proposal is an in-fill project surrounded by urban development, and the intensity and amount of light will be no more than that being generated from the structures in the adjacent retail center and the Oxnard Shores and Oxnard Dunes neighborhoods. The Building Division will review construction drawings to ensure compliance with the California Building Code as it applies to residential construction and lighting. Any impacts to aesthetic resources as a result of light or glare will be less than significant, and no mitigation is necessary.

Cumulative Development:

1-4) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03 ) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse aesthetic cumulative impacts will occur as a result of cumulative projects.

Mitigation Measure(s): None Required.

Monitoring: None Required.

Result After Mitigation: Not Applicable.

**B. AGRICULTURAL RESOURCES\***

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Conflict with existing zoning for agricultural use, or a Williamson Act contract? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

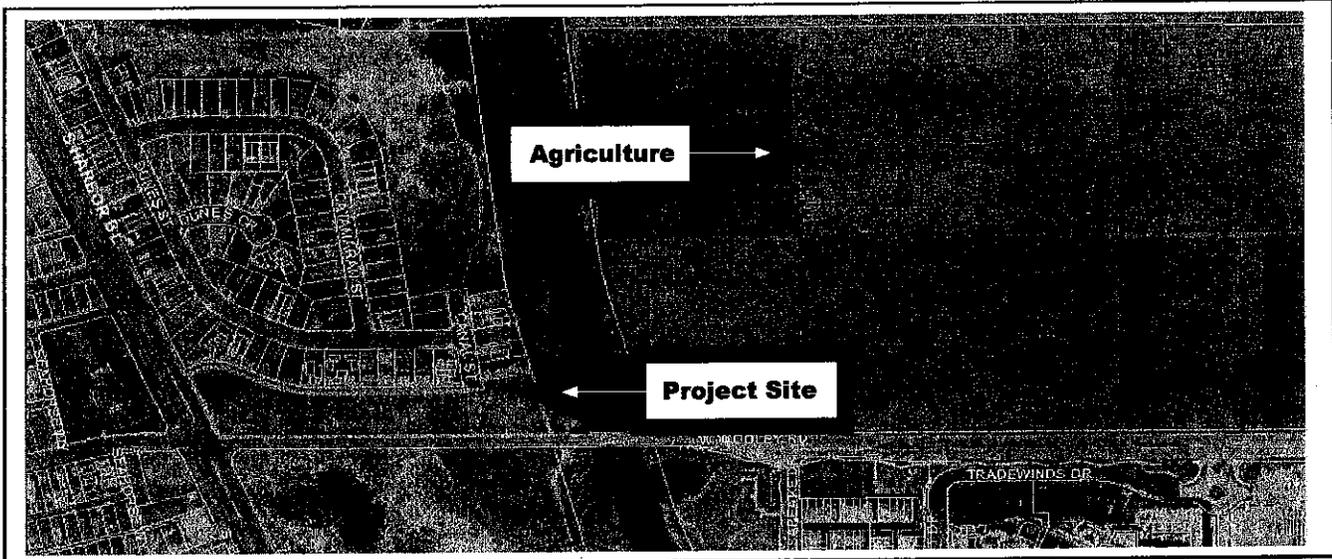
\* In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agricultural and farmland.

Discussion:

1 & 3) The project site is located in an urbanized developed area largely surrounded by urban development, and is not used or zoned for agricultural use. No impacts are anticipated related to agricultural resources.

2) The Edison Canal is located adjacent to the easterly boundary of the project site. The waterway portion of the canal is approximately 100 feet wide, and the total right-of-way is approximately 300 feet wide. The property on the east side of the Edison Canal has an agricultural land use, and is located in an agricultural zone within the jurisdiction of County of Ventura (see Figure 7 – Agricultural Land Use). The result is a minimum of 300 feet of separation between the project site and the nearest agricultural land use. According to the 2020 General Plan EIR (page 4.7-18), there should be a minimum of 100 feet of buffer to avoid conflicts between agricultural and residential land uses, and such buffers may be infrastructure easements, roadways, open space, etc. According to the Agricultural/Urban Buffer Policy of Ventura County, there should be a minimum of 300 feet of separation between active agricultural land uses and new development. The existing 300-foot wide Edison Canal property between the project site and nearest agriculture land use is an adequate buffer. Therefore, there will be no impact, and no mitigation measures are required.

Figure 7 – Agricultural Land Use



Source: City of Oxnard, Geographic Information Systems (ESRI ArcMap, Version 9.3).

Cumulative Development:

1-3) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse agricultural impacts will occur on the west side of Oxnard as a result of cumulative projects.

**C. AIR QUALITY\***

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Conflict with or obstruct implementation of the applicable air quality plan? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Violate any air quality standard or contribute substantially to an existing or projected air quality violation? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2002 Computer Program)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is nonattainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2007 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Expose sensitive receptors to substantial pollutant concentrations? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2007 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Create objectionable odors affecting a substantial number of people? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; Urbemis 2007 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

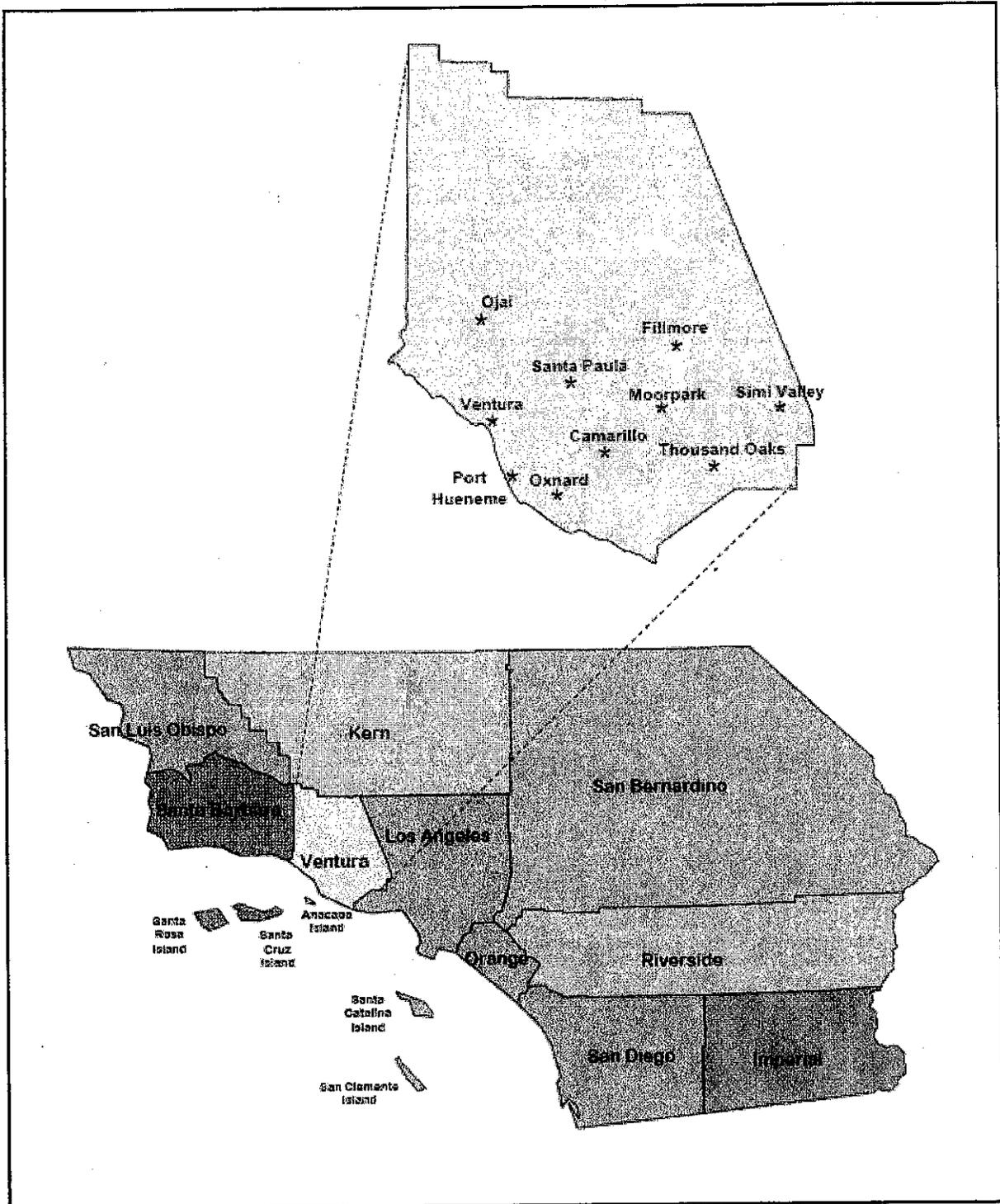
\* Where available, the significant criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.

Discussion:

The City of Oxnard and the proposed project are located within the jurisdiction of the Ventura County Air Pollution Control District (APCD), see Figure 8. *It is the applicant's responsibility to contact the APCD to verify compliance with any permitting needs of the APCD.*

1-3) Short-term impacts to air quality are the result of grading and other construction activities associated with the project, such as earth-moving and heavy equipment vehicle operations. The Air Pollution Control District considers short-term activities related to construction to be less than significant impacts. Standard APCD mitigations will be required in order to minimize on-site construction emissions and maximize dust suppression. Also, the developer will not be installing a pile foundation for the buildings, and therefore, pile-driving equipment will not be operated and there will not be any associated emissions from the equipment. By requiring compliance with standard mitigation measures identified in the City's *Threshold Guideline—Initial Study Assessment* (February 1995) and compliance with APCD permitting requirements, short-term air quality impacts would be considered less than significant. Mitigation measures C-1 through C-6 will reduce emissions to the extent that is practically feasible during construction.

Figure 8 – Ventura County Air Pollution Control District



Source: *Ventura County Air Quality Assessment Guidelines*, Ventura County APCD.

Long-term impacts of the proposed project (e.g. residential activities and vehicle trips) will result in an increase in vehicle trips to and from the site. Potential long-term air quality impacts associated with the proposed project can be contributed primarily to vehicular emissions and residential appliances. The project has been designed to reduce long-term emissions (e.g. Energy Star appliances, insulation beyond Title 24 CCR requirements, passive cooling and ventilation, etc.). The City's adopted thresholds of significance for Reactive Organic Compounds (ROC) and Nitrogen Oxide (NOx) emissions is 25 pounds per day. Project emissions were calculated utilizing the Urban Emissions (URBEMIS 2007, Version 9.2.4) computer modeling program. The results from the model simulation are summarized in Table 5 (see also Appendix 1). The URBEMIS model estimates that the proposed project would not generate vehicle emissions or other long-term emissions that might exceed the City's air quality thresholds. With implementation of mitigation measures C-7 and C-8, potential long-term impacts will be reduced to the extent feasible.

**Table 5. Area Source & Operational Emissions (in Pounds Per Day)**

Emission Source	Amount of Emissions, Unmitigated		Amount of Emissions, Mitigated	
	ROC	NO <sub>x</sub>	ROC	NO <sub>x</sub>
Operational (Vehicles)	3.8	3.3	3.7 <sup>1</sup>	3.2 <sup>1</sup>
Area Source (Stationary)	3.9	0.5	3.9	0.5
Total Emissions	7.7	3.8	7.6	3.7
APCD Threshold Amount	25.0	25.0	25.0	25.0
Exceeds Threshold?	No	No	No	No
Significant Impact?	No	No	No	No

Source: URBEMIS 2007, Version 9.2.4

<sup>1</sup> The URBEMIS model assigns a 2% trip reduction value if local-serving retail uses are located within 1/2 mile of the project site. The measure assumes that the presence of retail will reduce a small percentage of the total trips associated with residential land uses.

Consistency with the 1997 Air Quality Management Plan (AQMP). The Ventura County Air basin is currently a non-attainment area for both the Federal and State standards for ozone and the state standards for PM10. Exceeding the air quality standards is the result of past and ongoing urban and rural development that has caused emissions to exceed the air basin's capacity for dispersal and removal of air pollutants. It should be noted, however, that the goal of the Ventura County Air Quality Management Plan (AQMP), which was most recently revised in 1997, is to reduce pollutant concentrations below National Ambient Air Quality Standards (NAAQS) through the implementation of air pollutant emissions controls. The plan predicts attainment of the Federal ozone standards by the year 2005. To achieve full compliance, the Federal 1-hour ozone standard cannot be exceeded more than one day in any year for three consecutive years. Although there were no events exceeding the Federal 1-hour ozone standard throughout the South Central Coast Air Basin in 2004 and 2005, there were two episodes in 2003. Attainment, therefore, has not yet been achieved.

The project is consistent with the site's land use designation in the City's Coastal Land Use Plan. In addition, the population increase upon occupancy is within the current and future demographic projections for the City. According to the APCD Guidelines, the consistency of a project with the current Ventura County Air Quality Management Plan is assessed based on whether the project is consistent with the local land use designation and current population projections. As the current project is consistent with the site's land use designation and within the adopted VCOG demographic projections for the area, the project is considered to be consistent with the Ventura County Air Quality Management Plan.

## Greenhouse Gases.

*Background.* In response to growing scientific and political concern with global climate change, California has recently adopted a series of laws to reduce emissions of GHGs to the atmosphere from commercial and private activities within the State. In September 2002, then-Governor Gray Davis signed Assembly Bill (AB) 1493, requiring the development and adoption of regulations to achieve “the maximum feasible reduction of greenhouse gases” emitted by noncommercial passenger vehicles, light-duty trucks, and other vehicles used primarily for personal transportation in the State. In September 2006, Governor Arnold Schwarzenegger signed the California Global Warming Solutions Act of 2006, also known as AB32, into law. AB 32 commits the State to achieving 1990 levels of GHGs by 2020. To achieve this goal, AB32 mandates that the ARB establish a quantified emissions cap, institute a schedule to meet the cap, implement regulations to reduce statewide GHG emissions from stationary sources, and develop tracking, reporting, and enforcement mechanisms to ensure that reductions are achieved. Senate Bill (SB) 1368, a companion bill to AB 32, requires the California Public Utilities Commission (PUC) and CEC to establish GHG emission performance standards for the generation of electricity. These standards will also apply to power that is generated outside of California and imported into the State.

In October 2006, Governor Schwarzenegger issued an Executive Order in which he designated the Cal/EPA Secretary with the primary responsibility for implementing AB 32 (rather than providing the ARB with unfettered discretion as the law required). In late December, the Governor announced the members of a blue-ribbon Market Advisory Committee board to devise approaches to develop a market for carbon trading. More developments are likely as the Governor and the Legislature determine who has primary responsibility for implementation and the relationship between regulations and market-based mechanisms. Because, the intent of AB 32 is to limit 2020 emissions to the equivalent of 1990, and the present year (2007) is near the midpoint of this timeframe, it is expected that the regulations would affect many existing sources of greenhouse and not just new general development projects.

In response to the Executive Order, the Secretary of Cal/EPA created the Climate Action Team (CAT), which, in March 2006, published the *Climate Action Team Report to Governor Schwarzenegger and the Legislature* (the “2006 CAT Report”). The 2006 CAT Report identifies a recommended list of strategies that the State could pursue to reduce climate change greenhouse gas emissions. These are strategies that could be implemented by various State agencies to ensure that the Governor’s targets are met and can be met with existing authority of the State agencies.

*Setting – Existing State-Wide Greenhouse Gas Emissions.* In December 2006, the California Energy Commission published the *Inventory of California Greenhouse Gas Emissions and Sinks: 1990 to 2004*. This report indicates that California is the second largest emitter of greenhouse gasses in the United States next to Texas. This is largely a result of the number of people living in a large state, as opposed to a small state such as Rhode Island. California generates about half as much CO<sub>2</sub> emissions as Texas. When considering fossil fuel emissions at the individual person level, California is second lowest in the nation in per capita CO<sub>2</sub> emissions with only the District of Columbia lower. Between 1990 and 2000, California’s population grew by 4.1 million people and during the 1990 to 2003 period, California’s gross state product grew by 83 percent (in dollars, not adjusted for inflation). However, California’s greenhouse gas emissions grew by only 12 percent between 1990 and 2003. The report concludes that California’s ability to slow the rate of growth of greenhouse gas emissions is largely due to the success of its energy efficiency, renewable energy programs, and commitment to clean air and clean energy. In fact, the State’s programs and commitments lowered its greenhouse gas emissions rate of growth by more than half of what it would have been otherwise.

*Impacts – Greenhouse Gas Emissions.* No air agency, including the VCAPCD, or municipality, including the City of Oxnard, has yet established project-level significance thresholds for GHGs emissions. Thus, emissions of GHGs can be quantified, but should not be used to determine significance under CEQA. Furthermore, the regulations required to

meet the goal under AB 32 of reducing emissions to 1990 levels by 2010 are still under development, expected to be finalized by January 1, 2008, and implemented no later than January 1, 2010. The list of discrete early action measures that can be adopted and implemented before January 1, 2010, was adopted by the ARB in June, 2007. The three early action measures focus on major State-wide contributing sources and industries, not on individual development projects or practices. These three measures are: 1) a low-carbon fuel standard; 2) reduction of refrigerant losses from motor vehicle air conditioning system maintenance; and 3) increased methane capture from landfills. At this time, there is no single criterion by which the implementation of a project can be judged to support or hinder attainment of the State's goals. The project's emissions are below the adopted thresholds for NOx and ROC, and a possible GHG threshold being considered would use the same threshold, resulting in no significant impact, if that GHG threshold is adopted. In the absence of an adopted GHG threshold, no impact determination is made for the project.

*Compliance with Strategies.* The consistency of the proposed project with the strategies from the 2006 CAT Report is evaluated in Table 6. As shown, the project would be consistent with all feasible and applicable strategies to reduce greenhouse gas emissions in California.

<b>Table 6</b>	
<b>Project Consistency with 2006 CAT Report Greenhouse Gas Emission Reduction Strategies</b>	
Strategy	Project Consistency
<i>California Air Resources Board</i>	
<u>Vehicle Climate Change Standards</u> AB 1493 (Pavley) required the state to develop and adopt regulations that achieve the maximum feasible and cost-effective reduction of climate change emissions emitted by passenger vehicles and light duty trucks. Regulations were adopted by the ARB I September 2004.	Consistent. The vehicles that travel to and from the Project site on public roadways would be in compliance with ARB vehicle standards that are in effect at the time of vehicle purchase.
<u>Diesel Anti-Idling</u> In July 2004, the ARB adopted a measure to limit diesel-fueled commercial motor vehicle idling.	Consistent. Current State law restricts diesel truck idling to five minutes or less. Diesel trucks making deliveries to the project site are subject to this state-wide law.
<u>Hydrofluorocarbon Reduction</u> 1) Ban retail sale of HFC in small cans. 2) Require that only low GWP refrigerants be used in new vehicular systems. 3) Adopt specifications for new commercial refrigeration. 4) Add refrigerant leak-tightness to the pass criteria for vehicular inspection and maintenance programs. 5) Enforce federal ban on releasing HFCs.	Consistent. This strategy applies to consumer products. All applicable products would comply with the regulations that are in effect at the time of manufacture.
<u>Transportation Refrigeration Units, Off-Road Electrification, Port Electrification (ship to shore)</u> Require all new transportation refrigeration units (TRU) to be equipped with electric standby. Require cold storage facilities to install electric infrastructure to support electric standby TRUs. Off-road Electrification Port Electrification	Not applicable. Not applicable. Not applicable. Not applicable.

<p><u>Manure Management</u>          Improved management practices, manure handling practices, and lagoon/liquid waste control options.</p>	<p>Not applicable.</p>
<p><u>Semi Conductor Industry Targets</u>          Emission reduction rules for semiconductor operations.</p>	<p>Not applicable.</p>
<p><u>Alternative Fuels: Biodiesel Blends</u>          ARB would develop regulations to require the use of 1 to 4 percent biodiesel displacement of California diesel fuel.</p>	<p>Consistent.          The diesel vehicles that travel to and from the project site on public roadways could utilize this fuel once it is commercially available.</p>
<p><u>Alternative Fuels: Ethanol</u>          Increased use of E-85 fuel.</p>	<p>Consistent          Future residents of the project site could purchase flex-fuel vehicles and utilize this fuel once it is commercially available in the region and local vicinity.</p>
<p><u>Heavy-Duty Vehicle Emission Reduction Measures</u>          Increased efficiency in the design of heavy duty vehicles and an education program for the heavy duty vehicle sector.</p>	<p>Consistent.          The heavy-duty vehicles that travel to and from the project site on public roadways would be subject to all applicable ARB efficiency standards that are in effect at the time of vehicle manufacture.</p>
<p><u>Reduced Venting and Leaks on Oil and Gas Systems</u>          Improved management practices in the production, processing, transport, and distribution of oil and natural gas.</p>	<p>Not applicable.</p>
<p><u>Hydrogen Highway</u>          The California Hydrogen Highway Network (CA H2 Net) is a State initiative to promote the use of hydrogen as a means of diversifying the sources of transportation energy.</p>	<p>Not applicable.</p>
<p><u>Achieve 50% Statewide Recycling Goal</u>          Achieving the State's 50 percent waste diversion mandate as established by the Integrated Waste Management Act of 1989, (AB 939, Sher, Chapter 1095, Statutes of 1989), will reduce climate change emissions associated with energy intensive material extraction and production as well as methane emission from landfills. A diversion rate of 48% has been achieved on a statewide basis. Therefore, a 2% additional reduction is needed.</p>	<p>Consistent.          As discussed in Section IV.A, Impacts Found to be Less Than Significant, Solid Waste, the project would divert at least 50 percent of its solid waste after the recyclable content is diverted. Mitigation measure UTIL-7 provides recycling bins at the project site to promote recycling of paper, metal, glass, and other recyclable material.</p>
<p><u>Landfill Methane Capture</u>          Install direct gas use or electricity projects at landfills to capture and use emitted methane.</p>	<p>Not applicable.</p>
<p><u>Zero Waste – High Recycling</u>          Efforts to exceed the 50 percent goal would allow for additional reductions in climate change emissions.</p>	<p>Consistent.          As discussed in Section P (Utilities and Service Systems), Impacts Found to be Less Than Significant, the project would divert at least 50 percent of its solid waste after the recyclable content is diverted. Recycling bins will be provided at the project site to promote recycling of paper, metal, glass, and other recyclable material. The project would also be subject to all applicable State and City requirements for solid waste reduction as they change in the future.</p>

<i>Department of Forestry</i>	
<u>Forest Management</u> Increasing the growth of individual forest trees, the overall age of trees prior to harvest, or dedicating land to older aged trees.	Not applicable.
<u>Forest Conservation</u> Provide incentives to maintain an undeveloped forest landscape.	Not applicable.
<u>Fuels Management/Biomass</u> Reduce the risk of wildland fire through fuel reduction and biomass development.	Not applicable.
<u>Urban Forestry</u> A new statewide goal of planting 5 million trees in urban areas by 2020 would be achieved through the expansion of local urban forestry programs.	Consistent. The landscaping proposed for the project would include new trees.
<u>Afforestation/Reforestation</u> Reforestation projects focus on restoring native tree cover on lands that were previously forested and are now covered with other vegetative types.	Not applicable.
<i>Department of Water Resources</i>	
<u>Water Use Efficiency</u> Approximately 19% of all electricity, 30% of all natural gas, and 88 million gallons of diesel are used to convey, treat, distribute and use water and wastewater. Increasing the efficiency of water transport and reducing water use would reduce greenhouse gas emissions.	Consistent. The project would be required to be constructed in compliance with the standards of Title 24 that are in effect at the time of development.
<i>Energy Commission (CEC)</i>	
<u>Building Energy Efficiency Standards in Place and in Progress</u> Public Resources Code 25402 authorizes the CEC to adopt and periodically update its building energy efficiency standards (that apply to newly constructed buildings and additions to and alterations to existing buildings).	Consistent. The project would be required to be constructed in compliance with the standards of Title 24 that are in effect at the time of development.
<u>Appliance Energy Efficiency Standards in Place and in Progress</u> Public Resources Code 25402 authorizes the Energy Commission to adopt and periodically update its appliance energy efficiency standards (that apply to devices and equipment using energy that are sold or offered for sale in California).	Consistent. Under State law, appliances that are purchased for the project – both pre- and post-development – would be consistent with energy efficiency standards that are in effect at the time of manufacture.
<u>Fuel-Efficient Replacement Tires &amp; Inflation Programs</u> State legislation established a statewide program to encourage the production and use of more efficient tires.	Consistent. Future residents of the project site could purchase tires for their vehicles that comply with state programs for increased fuel efficiency.
<u>Cement Manufacturing</u> Cost-effective reductions to reduce energy consumption and to lower carbon dioxide emissions in the cement industry.	Not applicable.
<u>Municipal Utility Energy Efficiency Programs/Demand Response</u> Includes energy efficiency programs, renewable portfolio standard, combined heat and power, and transitioning away from carbon-intensive generation.	Not applicable.

<p><u>Municipal Utility Renewable Portfolio Standard</u>          California’s Renewable Portfolio Standard (RPS), established in 2002, requires that all load serving entities achieve a goal of 20 percent of retail electricity sales from renewable energy sources by 2017, within certain cost constraints.</p>	<p>Not applicable.</p>
<p><u>Municipal Utility Combined Heat and Power</u>          Cost effective reduction from fossil fuel consumption in the commercial and industrial sector through the application of on-site power production to meet both heat and electricity loads.</p>	<p>Not applicable.</p>
<p><u>Municipal Utility Electricity Sector Carbon Policy</u>          State agencies to address ways to transition investor-owned utilities away from carbon-intensive electricity sources.</p>	<p>Not applicable.</p>
<p><u>Alternative Fuels: Non-Petroleum Fuels</u>          Increasing the use of non-petroleum fuels in California’s transportation sector, as recommended as recommended in the CEC’s 2003 and 2005 Integrated Energy Policy Reports.</p>	<p>Not applicable.</p>
<p><i>Business, Transportation and Housing</i></p>	
<p><u>Measures to Improve Transportation Energy Efficiency</u>          Builds on current efforts to provide a framework for expanded and new initiatives including incentives, tools, information that advance cleaner transportation and reduce climate change emissions.</p>	<p>Not applicable.</p>
<p><u>Smart Land Use and Intelligent Transportation Systems (ITS)</u>          Smart land use strategies encourage jobs/housing proximity, promote transit-oriented development, and encourage high-density residential/commercial development along transit corridors. ITS is the application of advanced technology systems and management strategies to improve operational efficiency of transportation systems and movement of people, goods and services. The Governor is finalizing a comprehensive 10-year strategic growth plan with the intent of developing ways to promote, through state investments, incentives and technical assistance, land use, and technology strategies that provide for a prosperous economy, social equity and a quality environment.</p> <p>Smart land use, demand management, ITS, and value pricing are critical elements in this plan for improving mobility and transportation efficiency. Specific strategies include: promoting jobs/housing proximity and transit-oriented development; encouraging high density residential/commercial development along transit/rail corridor; valuing and congestion pricing; implementing intelligent transportation systems, traveler information/traffic control, incident management; accelerating the development of broadband infrastructure; and comprehensive, integrated, multimodal/intermodal transportation planning.</p>	<p>Consistent.</p> <p>The project is an infill development at the corner of two arterials along which transit service would likely occur in the future, assuming that funding and ridership demand were adequate.</p>
<p><i>Department of Food and Agriculture</i></p>	
<p><u>Conservation Tillage/Cover Crops</u>          Conservation tillage and cover crops practices are used to improve soil tilth and water use efficiency, and to reduce tillage requirements, labor, fuel, and fertilizer requirements.</p>	<p>Not applicable.</p>

<u>Enteric Fermentation</u> Cattle emit methane from digestion processes. Changes in diet could result in a reduction in emissions.	Not applicable.
<i>State and Consumer Services Agency</i>	
<u>Green Buildings Initiative</u> Green Building Executive Order, S-20-04 (CA 2004), sets a goal of reducing energy use in public and private buildings by 20 percent by the year 2015, as compared with 2003 levels. The Executive Order and related action plan spell out specific actions state agencies are to take with state-owned and –leased buildings. The order and plan also discuss various strategies and incentives to encourage private building owners and operators to achieve the 20 percent target.	Consistent. The project would be required to be constructed in compliance with the standards of Title 24 that are in effect at the time of development. The current 2005 Title 24 standards are approximately 8.5 percent more efficient than those of the 2001 standards.
<i>Public Utilities Commission (PUC)</i>	
<u>Accelerated Renewable Portfolio Standard</u> The Governor has set a goal of achieving 33 percent renewable in the State’s resource mix by 2020. The joint PUC/Energy Commission September 2005 Energy Action Plan II (EAP II) adopts the 33 percent goal.	Not applicable.
<u>California Solar Initiative</u> The solar initiative includes installation of 1 million solar roofs or an equivalent 3,000 MW by 2017 on homes and businesses, increased use of solar thermal systems to offset the increasing demand for natural gas, use of advanced metering in solar applications, and creation of a funding source that can provide rebates over 10 years through a declining incentive schedule.	Inconsistent. Solar panels are not proposed as part of the project. At a minimum, the swimming pool building shall be redesigned to accommodate solar panels for heating the pool water to the maximum extent feasible to reduce natural gas demand. (Mitigation C-8)
<u>Investor-Owned Utility Programs</u> These strategies include energy efficiency programs, combined heat and power initiative, and electricity sector carbon policy for investor owned utilities.	Not applicable.
Sources: Climate Action Team, 2006 and Christopher A. Joseph & Associates, 2007.	

4) Carbon Monoxide (CO) Hot Spots and Sensitive Receptors. Projects generating traffic impacts may result in the formation of Carbon Monoxide (CO) hot spots. Although the Ventura County Air Basin is currently an attainment area for CO, exhaust emissions can potentially cause a direct, localized “hotspot” impact at or near the proposed development. CO is a product of incomplete combustion of fossil fuel; unlike ozone, CO is emitted directly out of a vehicle exhaust pipe and is heavier than air. The optimum conditions for a CO hotspot is cool and calm weather (a stable and reduced air mixing layer) at a congested major roadway intersection with sensitive receptors nearby, and where vehicles are either idling or moving at a stop-and-go pace. The URBEMIS model estimates that the project may generate approximately 13 pounds per day of CO gases during construction. To analyze the project for operational and vehicle impacts, information from the project traffic study (*Traffic and Circulation Study, ATE Project #08116*, December 4, 2008, by Associated Transportation Engineers) was used to determine if any intersections would have a significant decrease in the level of service affected by the project. No intersections or locations have been identified as having the potential for CO hot spot impacts. Therefore, no impact is expected.

Sensitive receptors are defined as young children, ill persons, elderly persons, hospitals, etc. Sensitive receptors may include some residents in the Oxnard Dunes neighborhood to the north, and possibly the Oxnard Shores neighborhood to the west. Although incident(s) of minor and temporary exposure might occur, it is not considered exposure to a substantial level of pollutant concentration. Therefore, any impacts would be considered less than significant.

5) Odors are typically associated with industrial type land uses (e.g. manufacturing, chemical production or processing, energy production, livestock, etc.). Noxious odors are not normally associated with residential land uses. There will be no impacts from this project.

Cumulative Development:

1-5) Although the Ventura County Air Basin is currently a non-attainment area for federal and state standards for ozone and state standard for PM10, the APCD predicts attainment through implementation of the control mechanisms in its Air Quality Management Plan. The proposed project is consistent with population projections in the AQMP, will be consistent with the AQMP, and therefore cumulative impacts are considered to be less than significant. Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse air quality and related cumulative impacts will occur as a result of cumulative projects.

Mitigation Measure(s): The following mitigation measures shall be applicable to the project.

C-1 The developer shall prepare and submit an Air Emissions Mitigation Plan for Dust Control. This Plan shall be included as part of the construction contract and submitted to the City of Oxnard for review and approval prior to the issuance of grading permits. This plan shall include the following elements:

- Fugitive dust throughout the construction site shall be controlled by the use of a watering truck or equivalent means, generally at least three times a day (except during and immediately after rainfall). Water shall be applied to all unpaved roads, unpaved parking areas or staging areas, and active portions of the construction site. Environmentally-safe dust control agents may be used in lieu of watering.
- Revegetate or apply APCD-approved chemical soil stabilizers to all inactive portions of the construction site that are inactive for four or more days.
- Suspend or curtail all excavation, earth moving, and grading operations during episodes of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties) to prevent fugitive dust from being a nuisance or hazard.
- Material transported in trucks off site shall comply with State Vehicle Code Section 23114, with special attention to Sections 23114(b)(F), (e)(2), and (e)(4) as amended. Material transported on site shall be sufficiently watered or secured to prevent fugitive dust.
- Inform all employees involved in grading operations on the project to wear face masks during dry periods to reduce inhalation of dust.
- Signs shall be posted on-site requiring traffic speeds to not exceed 15 miles per hour.
- Sweep streets at the end of the day if visible soil material is carried over to adjacent streets and roads.
- At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.

- C-2 Maintain equipment engines in good condition and in proper tune as per manufacturer's specifications. Minimize idling time. Prohibit the use of on-site electric generators, and connect to utility lines adjacent to the project site.
- C-3 If feasible, use alternatively fueled construction equipment, such as compressed natural gas (CNG), liquefied natural gas (LNG), or electric.
- C-4 During construction, contractors shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
- C-5 A temporary fence around the project site shall include an opaque visual barrier up to 6 feet high. The fence shall include signs identifying the name, telephone number, and emergency contact information for the contractor(s) responsible for the site, construction activities, and rectifying any nuisance conditions.
- C-6 Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements.
- C-7 As stated in the APCD Guidelines, applicants are required to mitigate environmental impacts associated with their projects to the greatest extent feasible. The following are those measures that could be feasibly implemented to mitigate operational phase air quality impacts of the proposed project:
- Lighting for public streets, parking areas, and recreational areas shall utilize energy-efficient mechanical, computerized, or photo cell switching devices to reduce energy usage.
  - Energy-efficient, automated controls for air conditioners shall be installed into proposed buildings to reduce energy consumption and emissions.
  - Automatic lighting on/off controls and energy-efficient lighting shall be installed into proposed buildings to reduce electricity consumption and associated emissions.
  - Light-colored roofing materials as opposed to dark roofing materials shall be used on proposed buildings. Light colored materials reflect sunlight and minimize heat gains in buildings. This measure would lessen the overall demand for mechanical air conditioning systems.
  - Wall and attic insulation shall be provided in proposed buildings beyond the requirements of Title 24, *California Code of Regulations*.
  - Built-in energy-efficient appliances shall be installed into proposed buildings.
  - Special sunlight filtering window coatings or double-paned windows shall be installed into proposed buildings to reduce thermal gain in hot weather and loss in the cold weather, thus reducing emissions associated with heaters and air conditioners.
- C-8 The design of the swimming pool building shall incorporate solar panels to the maximum extent feasible for pool water heating.
- C-9 All project construction and site preparation operations shall be conducted in compliance with all applicable Ventura County APCD Rules and Regulations with emphasis on Rule 50 (Opacity), Rule 51 (Nuisance), Rule 55 (Fugitive dust), and Rule 10 (Permits Required).

Required Monitoring: Building Division staff and Public Works Division staff shall review grading and construction plans to ensure compliance. Building Division and Public Works Division inspectors shall field monitor all applicable measures during grading activities and during construction.

Result After Mitigation: Less than significant.

**D. BIOLOGICAL RESOURCES**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1) The proposed infill project would result in the construction of 70 condominium units on approximately 3.54 acres of land largely surrounded by urban development. **The project site is not designated nor determined to be an Environmentally Sensitive Habitat Area (ESHA).** PCR Services Corporation prepared and submitted the following reports for this project: *Biological Resources Assessment* (February 2005), *Update to 2005 Biological Resources Assessment* (November 2008), and *Anacapa Townhomes Biological Resources Assessment* (December 2009). All reports have been independently reviewed and verified by Rincon Consultants, Inc., on behalf of the City. Site conditions are now best described as ornamental and ruderal (see Figure 9 – Biological Resources). The investigations of the project site included several inspections by qualified biologists, and reviews of literature and archives including lists maintained by the U.S. Fish & Wildlife Service and the California Department of Fish & Game. The results of all reports indicate that no plant or wildlife species listed as Threatened or Endangered were observed on the project site (see Appendix 2) and no sensitive habitat areas were present on the project site. The Least Bell's Vireo, a State-listed and federally-listed endangered species, has been observed ¼ mile north of the project site near Fifth Street, and mitigation measure D-2 will ensure that no potentially significant impacts will occur. The potential exists for the Silvery Legless Lizard to be in the vicinity of the project site, and mitigation measure D-3 will ensure that no potentially significant impacts will occur. If there are any impacts, the level will be less than significant with implementation of the mitigation measures listed below. Rincon Consultants, Inc., has independently verified that the proposed mitigations will be adequate, and CDFG and USFWS has no further comments or concerns.

2) The project will not have any adverse effect on any riparian habitat or other sensitive natural community as identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. PCR Services Corporation prepared and submitted the following reports for this project: *Biological Resources Assessment* (February 2005), *Update to 2005 Biological Resources Assessment* (November 2008), and *Anacapa Townhomes Biological Resources Assessment* (December 2009). All reports have been independently reviewed and verified by Rincon Consultants, Inc., on behalf of the City. The site is not identified in the 2020 General Plan conservation element, and is not a part of a larger component of habitat area. No potentially significant impacts are anticipated to occur.

3) There are no federally protected wetlands that occur within the project area. There will be no impacts.

4) PCR Services Corporation prepared and submitted the following reports for this project: *Biological Resources Assessment* (February 2005), *Update to 2005 Biological Resources Assessment* (November 2008), and *Anacapa Townhomes Biological Resources Assessment* (December 2009). All reports have been independently reviewed and verified by Rincon Consultants, Inc., on behalf of the City. **The site is not an Environmentally Sensitive Habitat Area (ESHA).** The results indicated that there is no habitat present on the Wooley Road site that could support migratory birds, there were no migratory birds observed on or near the site, and therefore no impacts are anticipated. Mitigation measure D-1 will ensure that any birds protected by the Migratory Bird Treaty Act will not be potentially affected by the project. The project will not interfere with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, and there will be no impact. The project will not impede the use of native wildlife nursery sites, and there will be no impact.

5 & 6) The proposal will not conflict with any local policies or ordinances that protect biological resources, or a Habitat Conservation Plan or Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. The project is consistent with Policy 8 of the Coastal Land Use Plan, in that the City Council adopted Ordinance No. 2159 on April 19, 1988, approving the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property, in order to transfer the development rights

from several beachfront parcels (i.e. the resource area) to the inland property on Wooley Road for the purpose of preserving beach access and open space, in addition to avoiding other adverse environmental impacts.

The Coastal Zoning Ordinance created the Resource Protection (RP) zoning district specifically to protect, preserve and restore environmentally sensitive habitat areas. The project site is not located within or adjacent to a Resource Protection (RP) zone. **The project site is not designated nor determined to be an Environmentally Sensitive Habitat Area (ESHA).** The nearest RP zone is on the parcel to the south of Wooley Road. Since Wooley Road currently separates the project site from the open space and dunes on the south side of Wooley Road, the existing road serves as a buffer between the project site and the RP zone. Before the project is completed, Wooley Road will be fully improved to a width of at least 46 feet, and up to 58 feet wide near the intersection with Harbor Boulevard. The width of the existing road together with the setbacks on the project site will provide at least 50 feet of buffer area. Therefore, there will be no impacts.

Cumulative Development:

1-4) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse biological cumulative impacts will occur as a result of cumulative projects.

Mitigation Measure(s):

- D-1 The developer shall avoid grubbing or grading the site during the breeding season (i.e. February 15 through August 31) as the preferred mitigation. If avoidance is not feasible and grubbing/grading will occur during the breeding season, then the project site and adjacent open space areas shall be surveyed by a qualified biologist for the presence of nesting birds prior to removal of vegetation. The developer shall be required to submit a report documenting the findings of the site survey to the Planning Division for review and approval. The site survey shall be conducted within 30 days of the beginning of any grubbing/grading activity. If any active nests are detected, then a 300-foot buffer (or as otherwise deemed appropriate by the biological monitor in consultation with the City and CDFG) shall be placed around the nest site until the nestlings have successfully fledged.
- D-2 Focused pre-construction surveys for the Least Bell's Vireo (LBV) will be conducted in suitable habitat within 500 linear feet of construction activities. The surveys will follow agency protocol as to survey methodology (i.e. time of day and climatic conditions). However, a modified survey schedule is appropriate given the site-specific circumstances. Therefore, surveys of the area within 500 feet of construction activities and the area where LBV has been reported will be initiated if commencement of construction is to occur during the breeding season for the species (April 10 to July 31). If no LBV are detected within 500 feet of the construction activities during any of the three survey bouts, no further action is required. If, however, LBV are detected within 500 feet of construction activities, CDFG and the U.S. Fish & Wildlife Service (USFWS) will be contacted to determine what, if any, actions need to be taken to avoid adverse effects on the species. Such actions, if necessary, may include one or more of the following: phasing the construction activities to avoid certain construction activities

within 500 feet of occupied LBV habitat until after the breeding season has ended; noise monitoring; and nest monitoring. If LBV is detected in suitable habitat farther away than 500 feet, these areas will be monitored during each of the three survey bouts so as to gain assurances that occupied habitat remains more than 500 feet from construction activities. The findings of the surveys will be documented and submitted to the City within 10 days of completion, as well as CDFG and USFWS as applicable.

- D-3 Prior to the initiation of construction activities, a temporary silt fence will be installed along the project site's easterly boundary to prevent ingress of Silvery Legless Lizards (SLL) onto the site for the duration of construction activities. The fencing will be keyed into the ground to a depth of at least three inches. Preconstruction surveys for SLL will then be conducted within areas of the site that could be used by the species. The surveys will be conducted according to the following protocol provided by CDFG.

Surveys will be conducted on a warm day (70 to 80 degrees Fahrenheit) in the mid-morning and no longer than two weeks prior to the commencement of soil disturbance activities. Relative soil moisture should be approximately 65% to 80% with soil temperature being 60 to 70 degrees Fahrenheit. Two person teams will work together with one person using a hand rake to gently rake the loose litter and soil down to substrate. The other person will search the areas being raked for SLL.

Surveys will employ a "three-pass" methodology to locate as many SLL as possible. If SLL are found during the first pass, an overnight period of no disturbance will occur before the second pass. Similarly, if SLL are found during the second pass, another overnight period of no disturbance will occur before the third pass. If during any pass no SLL are found, the survey will be concluded.

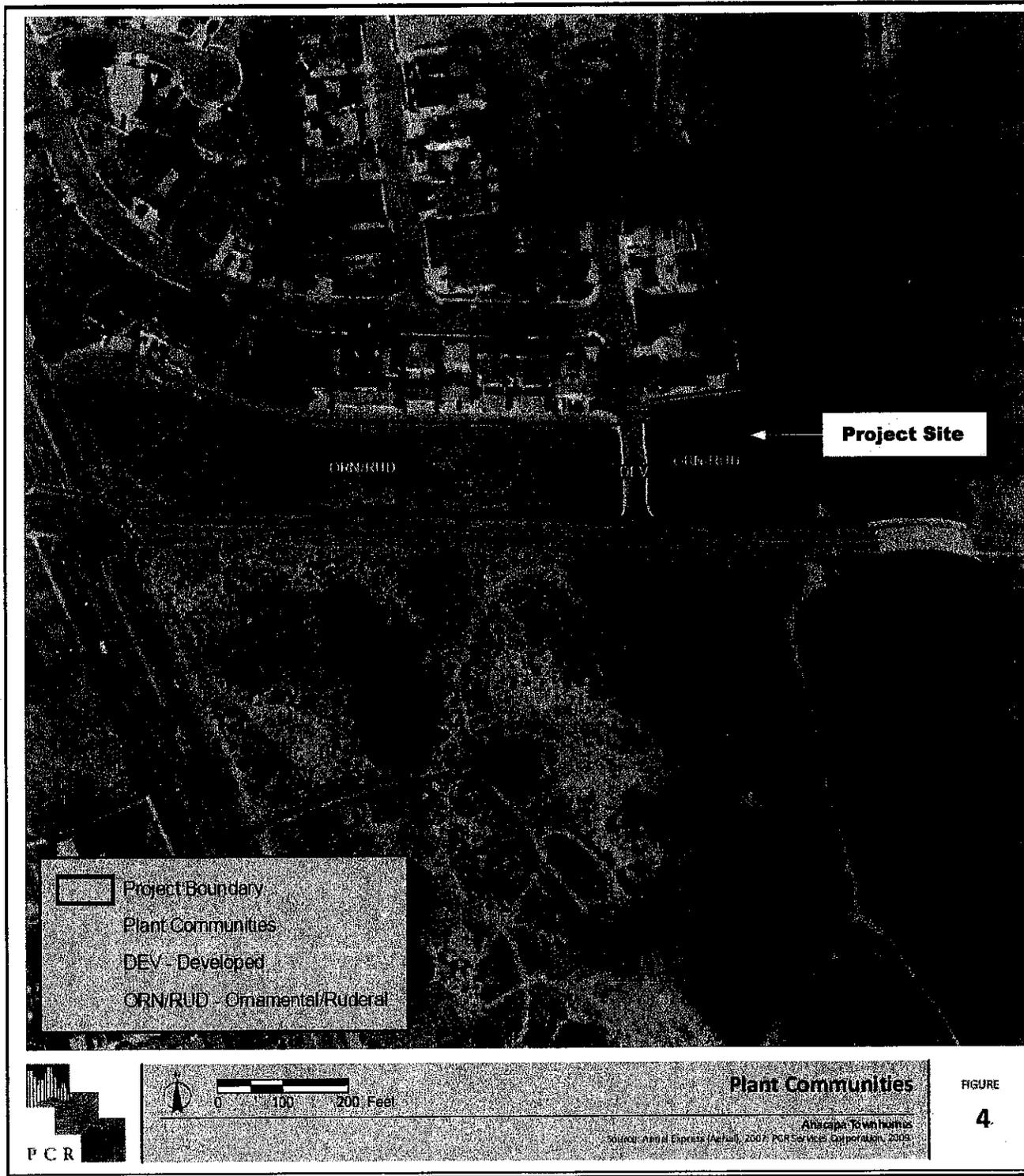
Any SLL found will be placed in a lidded, vented box containing clean sand. Areas of moist and dry sand will be present in the box. Boxes will be kept out of direct sunlight and protected from temperatures over 72 degrees Fahrenheit. The temperatures of the sand in the box will be kept under 66 degrees Fahrenheit. Information on each lizard captured will be recorded, including date of capture, location, length, color, age, and tail condition. All lizards will be relocated to suitable off-site habitat the same day as capture. GPS location of released lizards will be recorded. CDFG will be notified about any injured SLL.

All work will be performed by personnel who hold active permits from CDFG in order to conduct the work. At the conclusion of the surveys, documentation of survey results and associated records will be submitted to the City within 10 days of completion, as well as to CDFG.

Monitoring: Planning staff will review and approve the site survey reports prior to issuance of any grading or building permits. Development Services inspectors will monitor the on-site grading and construction activities.

Result after Mitigation: With implementation of the above mitigation measure, the project will not result in any potentially significant adverse effects on biological resources.

Figure 9 – Biological Resources



Source: PCR Services Corp., *Biological Resources Assessment* (2009), Figure 4.

**E. CULTURAL RESOURCES**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Directly or indirectly destroy a unique paleontological resource or site or unique geological feature? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Disturb any human remains, including those interred outside of formal cemeteries? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1) The project site does not contain any historical structures or resources. According to the 2020 General Plan EIR (page 4.11-3), historical structures in the City of Oxnard are generally located within the Cultural Heritage District and Heritage Square in the downtown area. No impact is expected to historical resources.

2, 4) A report *A Phase One Site Survey Cultural Resources Investigation* (October 27, 2008) was prepared for this project by Environmental Research Archaeologists, and the investigation included a site inspection, archive search, and review of literature. The results of the investigation indicate that the area is not considered a likely source of historic or archaeological artifacts. The project site is within the area of the Chumash Tribal Territory and the literature search indicated a potential for the occurrence of cultural resources (e.g. sites and artifacts), however, the site inspection did not reveal any evidence of artifacts, sites, or other cultural resources. Based on the information provided in the Phase I study, a Phase II Archaeological Survey is not warranted. No evidence is available to suggest the project site has been used for ancient or pre-California human burials. The California Health and Safety Code (Section 7050.5) states that if human remains are discovered on-site, no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. As compliance with State law is a standard requirement on all development, no mitigation is required for the unlikely event that human remains are discovered on-site. The development of the project will require subsurface excavation, re-compaction, and grading of the site, and therefore the potential exists that previously unknown subsurface artifacts or deposits might exist on-site that could be disturbed by grading and other subsurface activities. Although it is not expected that subsurface artifacts will be present, Mitigation Measure E-1 is included as a precaution in the event that any subsurface discoveries are made, and will ensure that any impact would be less than significant.

3) Regarding paleontological resources, the 2020 General Plan EIR (page 4.11-2) indicates that the Oxnard Plain Basin as a whole is comprised of recent alluvial deposits which due to their geologic youth do not contain fossils. Therefore, paleontological resources are not expected to occur on the project site. No impact is expected to paleontological resources, and no mitigation measures are necessary.

Cumulative Development:

1-4) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-04) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional cultural resource or related cumulative impacts will occur as a result of cumulative projects.

Mitigation Measure(s):

E-1 Based on the recognized sensitivity for the occurrence of buried sites and artifacts and as mandated by the City of Oxnard archaeological guidelines, Developer shall pay for monitoring by a qualified Archaeologist and Native American Monitor (specifically a qualified Ventureño Chumash descendant). A contract with the Archaeologist shall be presented to the City prior to issuance of grading permits. Monitoring shall be required during all activity involving soil disturbance including grading (cut and fill) or other excavation (e.g., trenching). Should movement of soils during grading for re-compaction activities show no evidence of an archaeological site or artifacts, and with the concurrence of the City of Oxnard Planning and Environmental Services Division and the on-site Native American Monitor, further monitoring at this location by the Archaeologist may no longer be required. In the event that a prehistoric site or historic remains older than 50 years is identified during monitoring, the Archaeologist and/or Native American Monitor shall be empowered to stop all construction activities in the vicinity of the find. The Archaeologist shall document, identify, and evaluate the potential significance of any find. Such evaluation may require a Phase II site subsurface excavation and evaluation program. Should any remains prove to be significant, avoidance of the resource is the preferred mitigation. If avoidance through project redesign is not feasible, further investigations in the form of a Phase III data recovery program will be implemented to mitigate impacts to the identified resource. The Native American Monitor shall remain on-site throughout any necessary site documentation, evaluation, and mitigation processes.

Contracts shall include weekly reports from the archaeological monitor to the Planning Division summarizing the Archaeologist's activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning and Environmental Services Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building occupancy for each building.

Monitoring: Planning staff will review the Archaeological / Native American monitoring contract(s) prior to issuance of any grading permits. Planning staff will ensure the monitoring reports are received prior to Planning Division inspection for final building permit sign-off. Development Services staff will monitor on-site construction activities.

Result after Mitigation: With implementation of the above mitigation measures, the project will not result in any potentially significant adverse effects on the environment related to cultural resources.

**F. GEOLOGY AND SOILS**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
a. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of known fault? Refer to Division of Mines and Geology Special Pub. 42. (2020 General Plan, IX-Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Strong seismic ground shaking? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Seismic-related ground failure, including liquefaction? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Landslides? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Result in substantial soil erosion, or the loss of topsoil? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-3) The City of Oxnard is located in an area that has a high potential for seismic ground shaking (2020 General Plan, Figure IX-2; Coastal Land Use Plan, Section 3.3). The 2020 General Plan Safety Element, Table IX-1 and Figure IX-2, lists fault systems that are located in the vicinity of the City of Oxnard (see Figure 10 – Geologic Hazards). There are no known active faults within the City limits, and the Subsurface Designs report states that the property is not in an Earthquake Fault Zone. The 2020 General Plan (Figure IX-2) identifies the project site as being within an area that is marked by a High Potential for Liquefaction. Although the *Preliminary Soils Engineering Investigation* report determined that the site does not have historically high levels of groundwater, the report confirmed that there is a potential for liquefaction to occur on the site (see Figure 11 – Liquefaction Potential). There is no potential for landslides on or near the site due to the level topography. Impacts will be less than significant.

The *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch* (November 28, 2008) prepared by Subsurface Designs, Inc., recommends that a mat foundation be constructed, plus removing and compacting the upper 6 feet of topsoil on the project site. Development Services Department staff will review the final design during the Plan Check process prior to issuing any building or grading permits, and will ensure that the plans satisfy the requirements of the 2007 California Building Code for Seismic Zone 4 applicable to Oxnard. With implementation of the recommended design features to mitigate potential structural problems associated with liquefaction and other geologic conditions, the impacts would be reduced to a level that is considered less than significant. Mitigation Measure F-1 will also result in any impacts being less than significant.

The current project is consistent with Policy 39 of the Coastal Land Use Plan that requires review of any seismic, liquefaction, or other geologic hazards. Also, the City Council adopted Ordinance No. 2159 on April 19, 1988, approving the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property, in order to transfer the development rights from several beachfront parcels (i.e. the relatively higher-risk area) to the inland property on Wooley Road for the purpose of avoiding and/or reducing significant environmental impacts from natural hazards such as tsunamis, storms, wave action, beach erosion, etc.

4) The Subsurface Designs report indicates that the expansion potential is very low for the soils beneath the surface of the project site. No mitigation measures are required or recommended. Impacts, if any, will be less than significant.

Cumulative Development:

1-4) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse geology and soils impacts will occur as a result of cumulative projects.

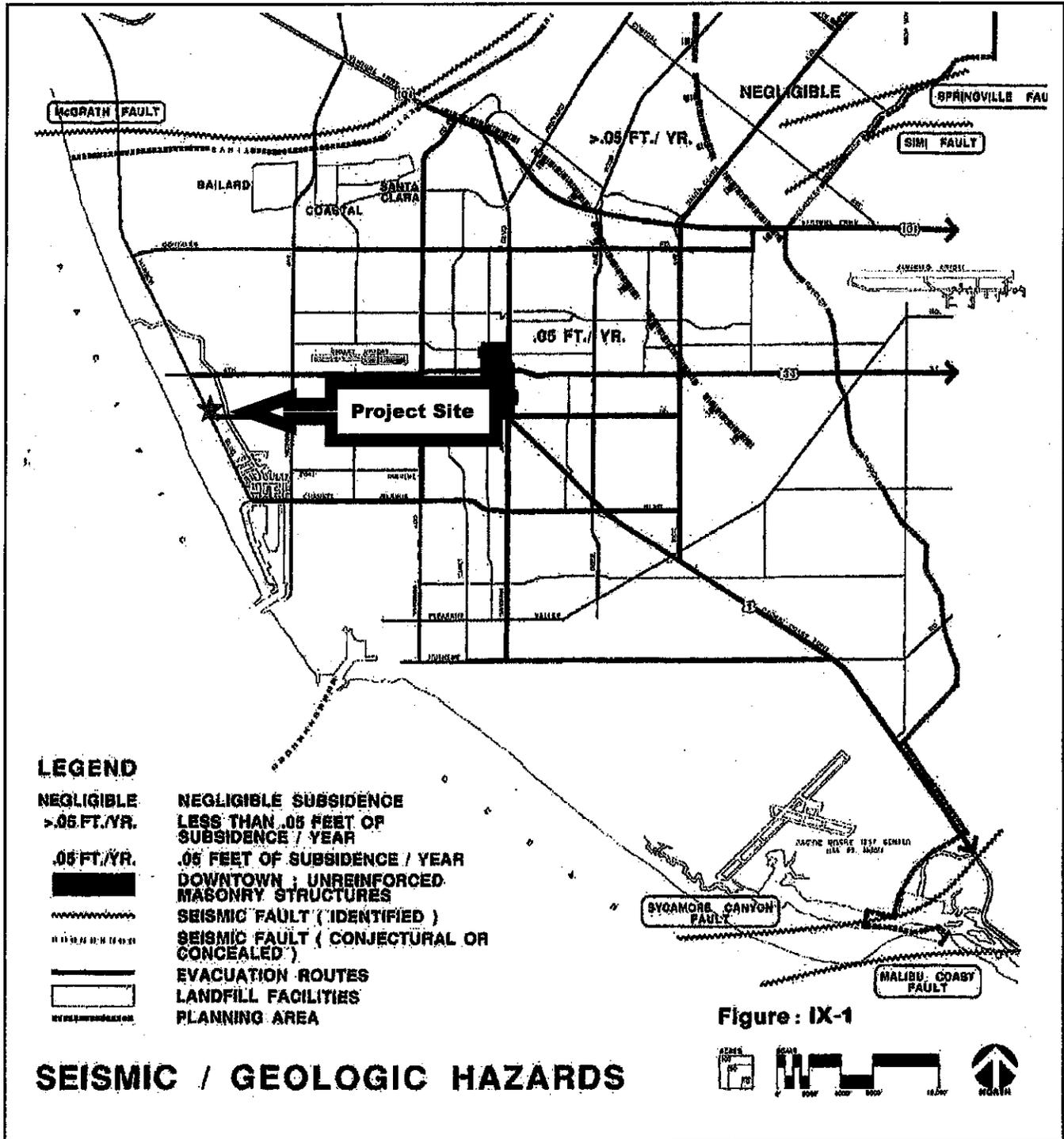
Mitigation Measure(s):

F-1 The developer shall be required to incorporate a mat foundation into each building according to the recommendations of the *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch* (by Subsurface Designs, Inc. November 28, 2008). A pile foundation shall be prohibited.

Required Monitoring: Building Division staff and Public Works Division staff shall review grading and construction plans to ensure compliance. Building Division and Public Works Division inspectors shall field monitor all applicable measures during grading activities and during construction.

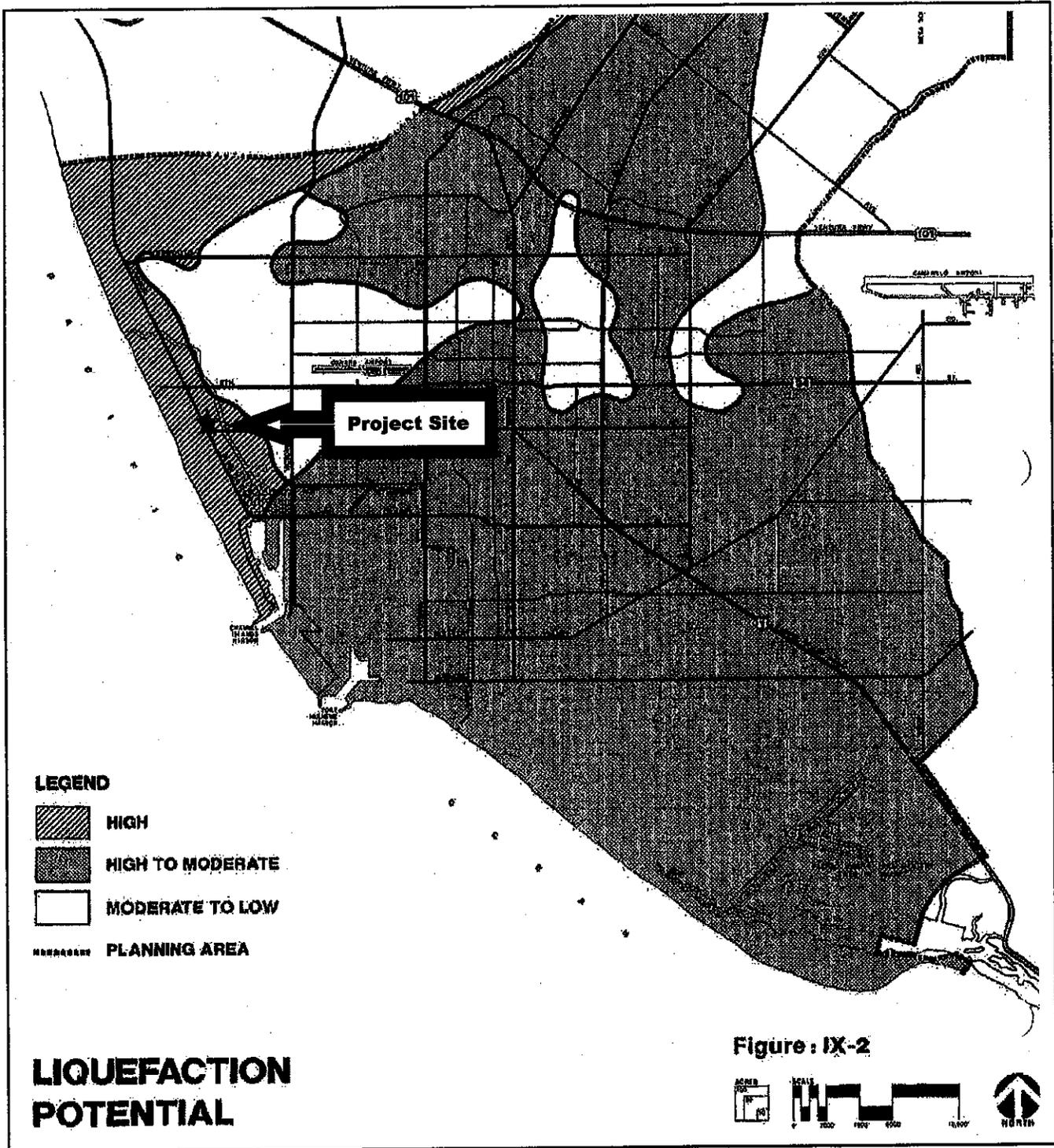
Result After Mitigation: Less than significant.

Figure 10 – Geologic Hazards



Source: City of Oxnard 2020 General Plan, Figure IX-1.

Figure 11 – Liquefaction Potential



Source: City of Oxnard 2020 General Plan, Figure IX-2.

**G. HAZARDS AND HAZARDOUS MATERIALS**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? (2020 General Plan, IX - Safety Element; City of Oxnard Emergency Preparedness Plan and Response Manual)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The information in this section is based on the *Phase II Sample Results, The Dunes Property, Oxnard California* (October 9, 2008) prepared by Waterstone Environmental, Inc., Anaheim CA.

1 & 2) The proposed project will not create any significant hazards to the public or the environment through the routine transport, use or disposal of hazardous materials. There will be storage and use of small domestic quantities of oil, gasoline, household solvents and cleaners, and other household chemicals that would be associated with the project. The storage and use of such small quantities of household materials would not result in any health hazards. The proposed project will not create any significant hazards through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment. There will be no impacts.

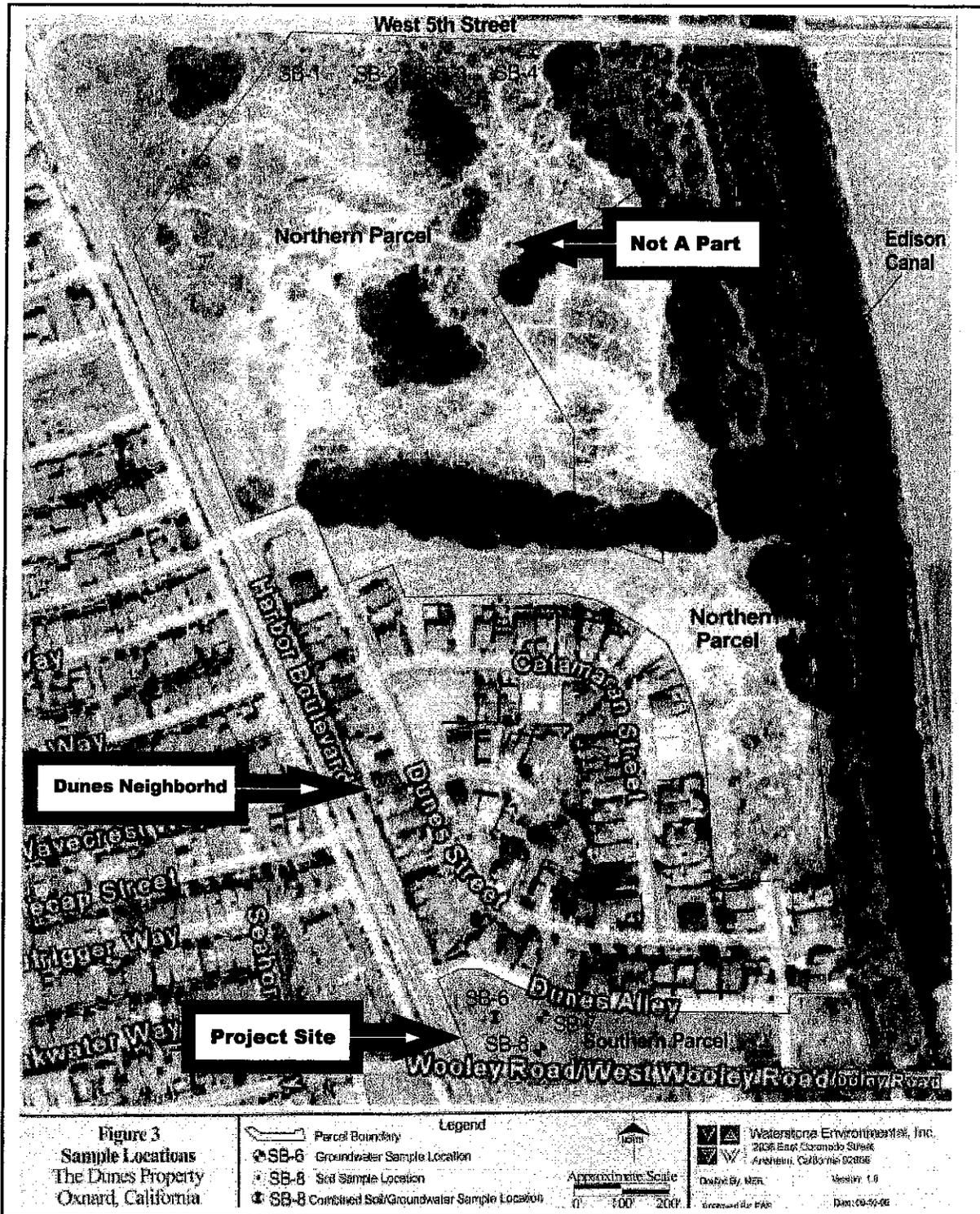
3) The project will not emit or contain any known hazardous materials during or after construction. The project is not located within one-quarter mile of a school. It is anticipated that the storage of small quantities of oil, gasoline, and household chemicals would be present for domestic household uses, and the storage and use of such small quantities of household materials would not result in a potentially significant health hazards. There will be no impacts.

4) The project site is not on the list of hazardous materials and cleanup sites compiled pursuant to Government Code Section 65962.5. A parcel to the north of the project site (see Figure 12) was formerly on the Department of Toxic Substances Control (DTSC) list in the 1980's, the entire site was cleaned to the satisfaction of DTSC, a sign-off letter from DTSC was issued on June 19, 1991, and the property was removed from the DTSC list on July 29, 1991. A portion of the northerly property has previously been developed with duplexes and single-family residences (a total of 148 dwelling units) and is now known as the Dunes Neighborhood (Figure 12). Due to the proximity of the current project to the northerly parcel at 5<sup>th</sup> Street, as well as the former activities that historically occurred on and near the project site, the developer retained an independent consultant to investigate the site for the presence of any hazardous substances. Results do not exceed any regulatory standards (see Appendix 3). **No potentially significant levels of hazardous substances, materials, or other conditions were found to be present in the soil or groundwater.** The results and recommendations of the *Phase II Sample Results* include the following:

- No Volatile Organic Compound (VOC) was detected in the groundwater sample collected in the southwestern portion of the subject property (i.e. the 4-acre parcel located at the northeast corner of Wooley Road and Harbor Blvd.).
- Total Petroleum Hydrocarbons (TPH) was detected in one of six soil samples collected and analyzed from the southern portion of the subject property (i.e. the 4 acres located at the northeast corner of Wooley Road and Harbor Blvd.). The detectable concentrations of petroleum hydrocarbons are below action levels for both diesel and oil range TPH.
- Toulene was the only detectable Volatile Organic Compound (VOC) detected in the soil samples collected and analyzed from the southern portion of the subject property. A trace concentration (significantly below any type of regulatory level or guideline) of toluene was detected in one of six samples orders of magnitude below residential and industrial Preliminary Remediation Goals (PRG's).
- Metals were detected in all of the six soil samples collected and analyzed from the southern portion of the subject property. All metals detected at concentrations consistent with published values of naturally-occurring background levels of metals in the area, with the exception of Barium in one of the samples.
- One soil sample exhibited an elevated concentration of Barium to warrant further Toxicity Characteristic Leaching Procedure (TCLP) analysis for hazardous determination. The subsequent analysis found that the concentration of Barium in the soil was at a non-hazardous level, based on established standards of the State of California and Federal Government.
- Waterstone recommends no further investigation in the area of the former oil field "petroleum waste pit" in the southern portion of the subject property.

No mitigation measures are recommended or required. The project site was determined to be suitable for residential development, and will not create any significant hazards to the public or the environment. There will be no impacts.

Figure 12 – Sample Locations



Source: Phase II Sample Results, The Dunes Property, Figure 3, by Waterstone Environmental, Inc. (October 9, 2008).

- 5) The project site is approximately 6,000 feet (more than 1 mile) southwest of the runway at Oxnard Airport, is within the Oxnard Airport Sphere of Influence, and is therefore also covered by the Ventura County Comprehensive Land Use Plan. Although the site is not under a direct flight pattern for the airport, it is subject to overflight of aircraft arriving and departing the Oxnard Airport (see Figure 17). The project will not result in a safety hazard for people residing in the project area. No potentially significant impacts are anticipated, and no mitigation is required or recommended. Any impacts will be less than significant.
- 6) The project site is not located in the vicinity or sphere of influence of any private airstrip. There will be no impact.
- 7) The project will not interfere with an adopted emergency response plan or emergency evacuation plan. At the time of construction, the developer will be required to widen and/or rehabilitate portions of Wooley Road and Harbor Boulevard, which would facilitate orderly evacuation of the project site and adjacent neighborhoods in the event of an emergency. The City's Fire Department maintains the tsunami evacuation plans and other emergency preparedness plans, and has reviewed and accepted the proposal. Therefore, there will be no impacts.
- 8) The project site is located within an urban area and surrounded by urban development. No wildlands exist in the vicinity of the project site, and the development of the site will not result in any hazards related to wildland fires. Therefore, there will be no impacts.

Cumulative Development:

- 1-8) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse hazards and hazardous materials impacts will occur as a result of cumulative projects.

Mitigation Measure(s): None Required.

Monitoring: None Required.

Result After Mitigation: Not Applicable.

**H. HYDROLOGY AND WATER QUALITY**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Violate any water quality standards or waste discharge requirements? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff, in a manner which would result in substantial erosion or siltation on- or off-site? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Create or contribute runoff water, which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Otherwise substantially degrade water quality? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**H. HYDROLOGY AND WATER QUALITY**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
7. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
8. Place within a 100-year flood hazard area structures which would impede or redirect flood flows? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
10. Inundation by seiche, tsunami, or mudflow? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1, 3, 4, 5, 6) Water quality impacts associated with the proposed use would primarily be those from motor vehicles and landscape maintenance. The primary source of contaminants would be oil, grease, and particulates emitted by motor vehicles that are transported by runoff drainage. However, the project site has been designed to incorporate grassy bioswales and detention basins in order to filter the runoff and allow recharge of the groundwater, prior to discharge to the stormwater drainage system. There are no surface water bodies or wetlands within the vicinity of the proposed project; however, existing absorption rates, drainage patterns, and runoff rates of the subject site would be affected by an increase in impervious surfaces on-site. The *Preliminary Hydrology Report* states that runoff will be directed towards several grass swale filter areas along the south perimeter of the site. The parking areas will have several CONTECH Vortech Units to store and treat runoff, and will be equipped with insert filters to purify the water. This runoff water will be directed to existing catch basins in Wooley Road, then into the existing stormwater collector line to be conveyed to the canal for discharge. These City facilities have the capacity to accept drainage from the site.

The proposed project will be required to comply with the National Pollutant Discharge Elimination System (NPDES) program, which will result in cleaner water being directed into the City’s storm drain system. In order to comply with the NPDES requirements for a permit to discharge storm water, a project that disturbs five acres or more must prepare a Storm Water Pollution Prevention Plan or “SWPPP” (see mitigation H-1). A SWPPP outlines both a plan to control storm water pollution during construction and after construction is complete. A SWPPP will be required for this project, and is subject to review and approval by the City of Oxnard in order to verify compliance with applicable NPDES requirements. Impacts are expected to be less than significant.

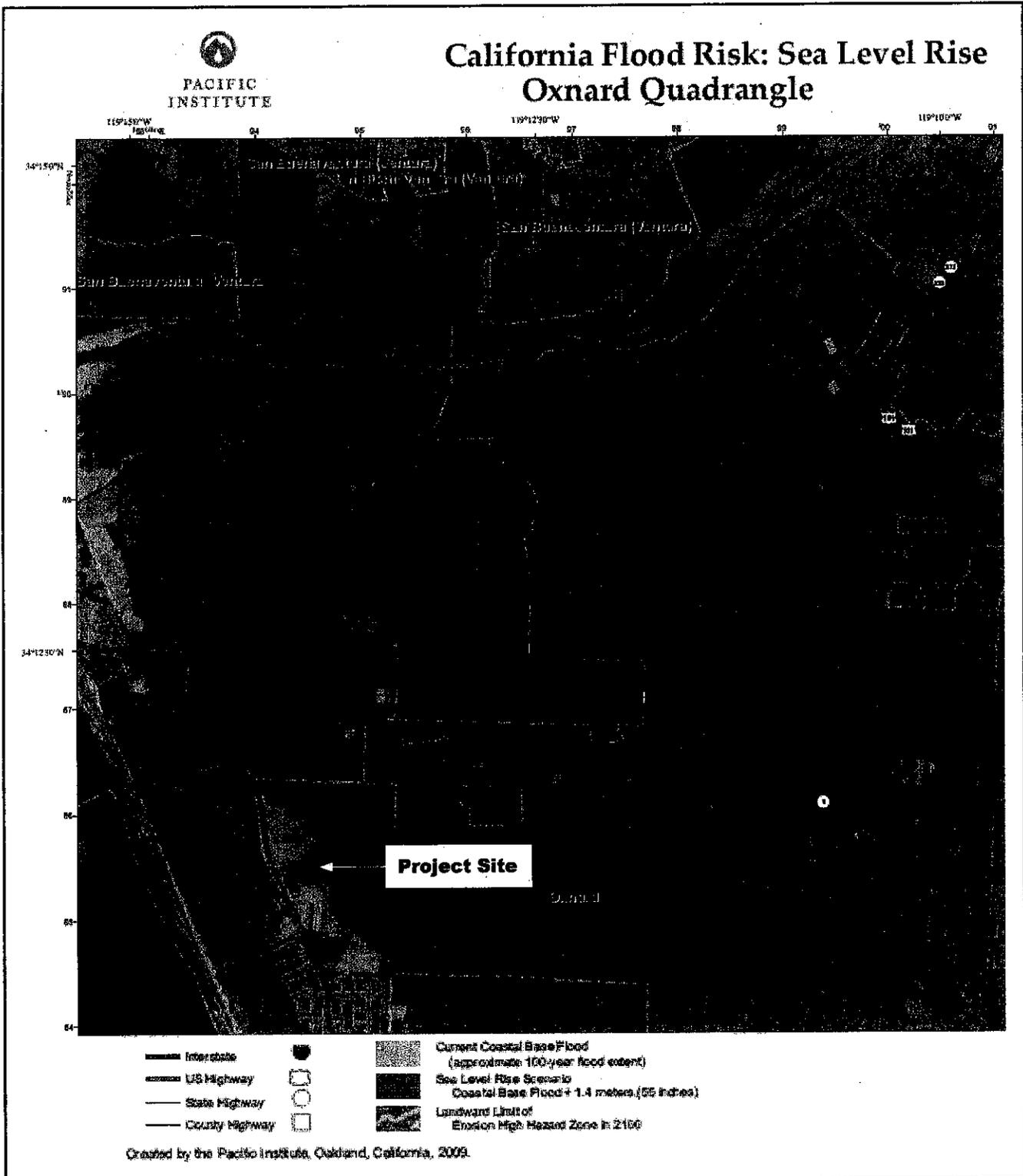
2) The project area would be served by City municipal water; the City obtains most of its water from the Calleguas Water District, which in turn purchases most its water from the Metropolitan Water District of Southern California. Other sources of water include local well water from United Water Conservation District and City wells. In order to address water supply needs at a regional level, representatives of the City of Oxnard, the Port Hueneme Water Agency (PHWA), the United Water Conservation District (UWCD), and the Calleguas Municipal Water District (CMWD) meet regularly. A collective effort to ensure continued delivery of high-quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program; a new, regional groundwater desalination facility is associated with this program and is intended to serve the Cities of Oxnard and Port Hueneme. The proposed project will result in approximately 200 additional residents in the City than that which exists today. Based on an average per capita water demand rate of 155 gallons per day (GPD) per person, the anticipated increase in water demand would be approximately 31,000 GPD, or approximately 35 Acre-Feet per Year (AFY). The water demand required by the proposed development is included in the 2005 *Urban Water Management Plan* which documents adequate long-term supply. The City's projected water supplies will meet the City's projected demand during normal, single dry, and multiple dry years through Year 2030. This includes the proposed project as well as the anticipated cumulative development expected to occur during that time frame. For a complete discussion, refer to section 4.14.1 & 4.14.2 of the Oxnard Village (Wagon Wheel) Specific Plan EIR. Impacts will be less than significant.

7 & 8) The proposed project site is located outside of the 100-year flood plain, according to Figure IX-3 in the 2020 General Plan. The project site is within Flood Zone B of the Flood Insurance Rate Maps (FIRM), and is an area between the 100-year and 500-year flood and some areas may be subject to 100-year (Q100) flooding. To protect the proposed buildings, the standard engineering requirement is to elevate the finish floor of each building approximately 1 foot above the on-site Q100 depths. The proposed project will be consistent with Coastal Policy 40 that requires the developer to agree to indemnify and hold the City harmless from any liability or damages resulting from the construction of his development (see mitigation measure H-2). Therefore, impacts will be less than significant.

9 & 10) The project site is located approximately 2,000 feet from the Pacific Ocean and adjacent to the Edison Canal, and located in an area that may be subject to inundation by a tsunami (2020 General Plan, Figure IX-3). The California Emergency Management Agency (Cal EMA) reviewed and updated the tsunami map in August 2009, and the City's tsunami evacuation plans and other emergency preparedness plans are adequate and will continue to serve the City well. The projected tsunami recurrence interval is hundreds to thousands of years along the Southern California coast (Legg et al., 2003). The California coastline has a tsunami warning system that will help ensure timely evacuation of the residents in affected areas. The risk exists, but is not considered potentially significant; because the risk cannot be completely eliminated, mitigation measure H-2 is included that requires the Developer to acknowledge the risks of development and indemnify and hold the City of Oxnard harmless from any liability or damages resulting from the construction of the project. Therefore, any impacts are considered less than significant.

Global climate change will have increasingly significant impacts on California and its coastal environments and communities. The state Coastal Act, through the Local Coastal Plans, mandates the protection, conservation, restoration, and enhancement of the state's coastal resources. As a result, the City must consider climate change, including global warming and potential sea level rise, through its planning and regulatory activities, and work to reduce greenhouse gas emissions and the detrimental impacts of global warming on the coast. While the amount of future sea level rise cannot be predicted with certainty, global climate change and 100-year flood events will certainly continue. Figure 13 – Sea Level Rise & Flood Risk (next page) illustrates one possible scenario for inundation areas during a 100-year flood, according to a recent report by the Pacific Institute. The map shows the areas of the city that may experience temporary flooding during 100-year flood events, if the average height of sea level were to rise approximately 4 feet over an unknown period that would be, at a minimum, 50 years. If sea level rise was less than the 4 feet assumed in the model simulation, then the inundation areas would not be as extensive throughout the City.

Figure 13 – Sea Level Rise & Flood Risk



As discussed in paragraph 7 & 8 above, the project site is located outside of the 100-year flood plain as identified in current Flood Insurance Rate Maps. The site is within Flood Zone B and is an area between the 100-year and 500-year flood and some areas may be subject to 100-year (Q100) flooding. To protect the proposed buildings, the finish floor of each structure will be elevated approximately 1 foot above the on-site Q100 depths. In addition, the project site on Wooley Road has a higher elevation than the beachfront parcels subject to the Development Agreement between the City of Oxnard and Oxnard Shores Development Company (Instrument No. 88-059788 and Instrument No. 89-174751), and therefore, the Wooley Road property is exposed to less relative risk from flooding or tsunami events compared to the beachfront parcels. In the event of long-term sea level rise, a worst-case scenario for this project would be approximately 4 feet of floodwater around the first floor and parking garages. However, because the risk cannot be completely eliminated, a mitigation measure is included that requires the Developer to acknowledge the risks of development and indemnify and hold the City of Oxnard harmless from any liability or damages. The proposed project will be consistent with Coastal Policy 40 that requires the developer to agree to indemnify and hold the City harmless from any liability or damages resulting from the construction of his development. By proceeding to obtain building and grading permits, the Developer acknowledges and assumes any potential risks associated with the project location and proximity to the coast. Any impacts are considered to be less than significant.

Cumulative Development:

1-5) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse cumulative impacts to hydrology and water quality will occur as a result of cumulative projects.

Mitigation Measure(s):

- H-1 Developer shall submit a Storm Water Pollution Prevention Plan (SWPPP) to demonstrate compliance with National Pollutant Discharge Elimination System (NPDES) requirements prior to issuance of building permits.
- H-2 Prior to issuance of any permits, the Developer shall submit documentation agreeing to indemnify and hold the City of Oxnard harmless from any liability or damages resulting from the construction of the project.

Monitoring:

- H-1 Public Works Staff shall review and approve the SWPPP prior to issuance of a Building Permit.
- H-2 Development Services Department staff and the City Attorney shall review and approve the Agreement prior to issuance of any permits.

Result After Mitigation: Less than significant.

**I. LAND USE AND PLANNING**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Physically divide an established community? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? (2020 General Plan; City adopted Specific Plans; Local Coastal Program; and Zoning Ordinance; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Conflict with any applicable habitat conservation plan or natural community conservation plan? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1) The project site is in the Garden Apartment Coastal (R-3-C) zone of the Coastal Zoning Map (see Figure 5). The site is surrounded by residential, retail and open space uses. The project will not change any of the land use designations as prescribed by the Coastal Zoning Ordinance. The proposed project will not physically divide an established community. There will be no impact.

2) The proposed project will not conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project. The project is consistent with the policies of the state Coastal Act in that the new residential development is concentrated in an existing developed area and consistent with the City's service capacities. The proposed project is consistent with Coastal Act Policy No. 30250, in that the development is located contiguous with existing developed areas with adequate public services, and where it will not have significant adverse effects on coastal resources. The project is designed and conditioned to be consistent with the City's current Coastal Land Use Plan and the Coastal Zoning Ordinance, as well as the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property (Instrument No. 88-059788 recorded on May 2, 1988, and Instrument No. 89-174751 recorded on October 31, 1989). A complete analysis with background information is provided in the *Final Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation* (ENVICOM Corp., March 18, 1988), and is hereby incorporated by reference. The proposed project is consistent with Policy 8 of the Coastal Land Use Plan, in that the City Council approved a Transfer of Development Rights from several beachfront parcels to the inland property on Wooley Road (see Figure 14 – Transfer of Development Rights) for the purpose of avoiding significant environmental impacts and preserving coastal resources and beach areas. Therefore, there will be no impacts.

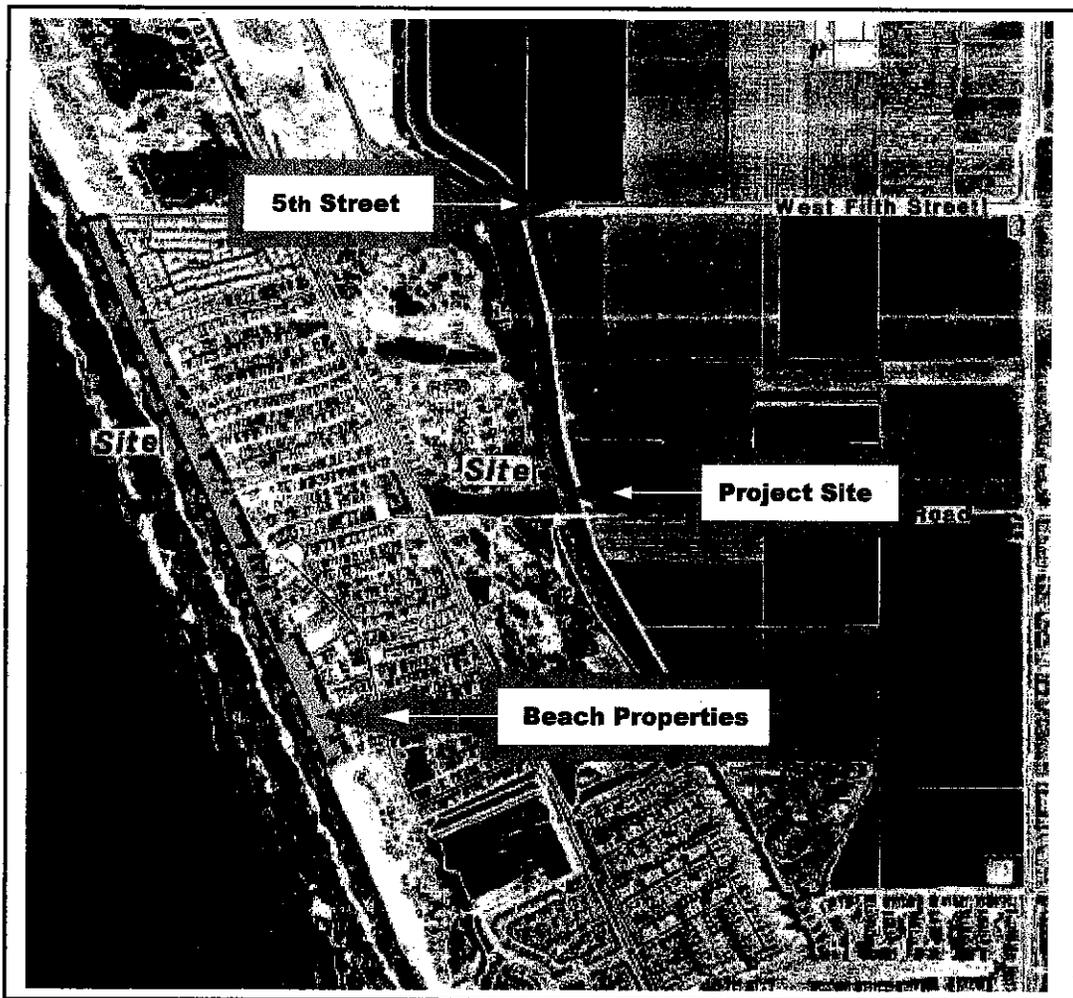
3) No habitat conservation plan or natural community conservation plan exist for the project site. Therefore, no impacts to land use and planning are expected.

Cumulative Development:

1-3) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse land use impacts will occur as a result of cumulative projects.

Mitigation Measure(s): None required.  
Monitoring: None required.  
Result After Mitigation: Not Applicable.

**Figure 14 – Transfer of Development Rights**



Source: Draft EIR for Oxnard Shores Agreement for Settlement of Litigation, Plate 1 (page 4-2).

**J. MINERAL RESOURCES**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1 & 2) According to the Open Space/Conservation Element of the 2020 General Plan (Figure VIII-7), the project will not result in the loss of availability of mineral resources, since the project site is not located near an area of importance for mineral deposits. In the City of Oxnard, the deposits of minerals, sand, and gravel occur predominantly along the Santa Clara River Channel, along the 101 Freeway corridor, and along the eastern edge of the City extending east from Oxnard Boulevard. The project does not fall within any of the areas listed as having significant mineral deposits. Therefore, no impacts to natural and mineral resources are expected.

Development of the subject site would increase the rate of consumption of fuel and other energy sources. During construction, energy resources would be necessary for on-site building activities, equipment operations, and transport vehicles bringing supplies to the site and hauling away debris. After construction, necessary energy resources might include gasoline for project vehicle trips, natural gas for heating and cooling, and electrical service for lighting. The proposal does not create any unique demand on the resources described above. Mitigation measure C-8 (requiring photovoltaic solar panels to be included in the design of the swimming pool building to heat pool water) will also reduce the long-term use of electricity, thereby resulting in less demand on energy-production facilities and less consumption of fossil fuels. Therefore, no impacts are expected on natural and mineral resources.

Cumulative Development:

1 & 2) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts. Therefore, no additional adverse mineral resource impacts will occur as a result of cumulative projects.

Mitigation Measure(s): None required.

Monitoring: None required.

Result After Mitigation: Not Applicable.

**K. NOISE**

Would the project result in:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 - 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. For a project located within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The information in this section is based on the *Site Specific Noise Study, Oxnard Shores LLC, The Shores Townhomes* (November 5, 2008) prepared by Acentech, Inc., and the *Traffic and Circulation Study* (December 4, 2008) prepared by Associated Transportation Engineers.

1 & 2) Operational & Traffic Noise - The City has established noise guidelines in the Noise Element of the City's General Plan. These guidelines identify compatible exterior noise levels for various types of land uses, and the maximum allowable noise levels vary depending on the land use. According to the City's Threshold Guidelines, noise levels exceeding 65 dB CNEL in outdoor living areas for residential land uses are usually considered significant (see Figure 15 – Noise Compatibility Matrix; and Figure 16 – Typical Decibel Level of Common Sounds). Noise levels in interior areas for residences should be 45 dB CNEL or less, and are usually considered significant if exceeding 50 dB.

Operational noises from the project will be typical for residential development (e.g. doors and windows opening/closing, electrical devices, motors, ventilation, etc.) and is not expected to exceed 65 dB CNEL outdoors or 45 dB CNEL indoors. The project will be affected by noise from traffic along Harbor Blvd. and Wooley Rd. According to the *Site Specific Noise Study* prepared by Acentech, Inc., the future noise levels will not exceed the thresholds for exterior or interior noise. The design of the site and construction materials will substantially reduce noise levels for the project (both interior and exterior), and the building design does not allow outdoor living areas to face the adjacent streets. The structures will be constructed according to current California Building Code standards, including noise reduction in building materials. Impacts will be less than significant, and mitigation is not required.

Construction Noise - The City limits the hours of construction activities to Monday through Saturday from 7:00 a.m. to 6:00 p.m., and the City's noise ordinance regulates the volume and intensity of noise. Construction would involve clearing, grading, foundation construction and finish construction. The noise levels generated by construction equipment would vary greatly depending upon factors such as the type and specific model of the equipment, the operation being performed and the condition of the equipment. Based on typical construction operations, it is expected that during the clearing and grading activities the equipment will include a scraper, dozer, blade, loader, and water truck. When the grading and construction equipment are operating, some residences north of the project site could be disturbed by the noise from engines, mufflers, tools, etc. However, the average noise level is not anticipated to exceed 65 dB CNEL in outdoor areas for residential land uses. The developer will not be allowed to install structural foundations based on piles (see Mitigation K-1), and therefore, all impacts will be avoided in terms of the noise and vibration associated with pile-driving equipment and activities related to pile foundations. Because of the short-term duration of grading and construction activities, plus the City's existing noise ordinance, the potential noise impacts to adjacent residences are less than significant. No mitigation is required.

3 & 4) Since the project site is currently vacant, the construction of 70 condominium dwelling units will increase the ambient noise levels in the vicinity from sources such as motor vehicles (in addition to the typical household noises to be expected from a residential land use). An April 2008 noise study that was prepared for the 104-unit Decksides Villas planned unit development on the southeast corner of Wooley Road and Victoria Avenue recorded ambient traffic exterior noise levels in excess of 65 dB Leq, which is an existing condition that exceeds the City of Oxnard's noise threshold of significance. The Decksides Villas project, developed in the early 1980's, is the only housing development on Wooley Road without soundwalls constructed between the development and the adjacent Primary Arterial streets. The proposed project will generate additional trips that will exacerbate the existing traffic-generated ambient noise levels around the Decksides Villas.

According to the project traffic study updated on June 10, 2009, the existing traffic volume is a total of 1,218 trips traveling to or from Wooley Road at the intersection of Victoria Avenue during the AM/PM peak hours (see Figure 3 in the *Traffic and Circulation Study*). The project would add 40 trips ( $67 \times (0.20 + 0.15 + 0.20 + 0.05)$ ) along Wooley Road that would pass the Decksides Villas during peak hours (see Table 4 and Figure 5 in the *Traffic and Circulation Study*). The future traffic volume for Existing + Project + Pending Projects during the AM/PM peak hours is estimated to be 2,243 trips (see Figure 6 in the *Traffic and Circulation Study*). Therefore, the project's contribution to the estimated 1,025 (2,243-1,218) new trips on Wooley Road during peak hours will be 3.9% ( $40/1,025 = 0.039$ ). The Decksides Home Owner's Association (HOA) obtained an estimate of \$100,000 to construct a 1,500-foot length of a perimeter Proto II soundwall to reduce ambient noise from traffic. Permit fees, engineering, demolition, and landscape reconstruction would add approximately 25%, for a total cost of \$125,000. In order to reduce the future traffic noise to a level less than 65 dB Leq (i.e. less than significant impact) at the Decksides Villas development, mitigation measure K-2 requires the applicant to pay into a fund for soundwall construction an amount proportional to the project's impact, or a total of \$4,875.00 ( $0.039 \times 125,000$ ). With payment of the project's proportional share of the soundwall to mitigate the noise from the increased traffic that will result from the project, there would be no significant impact by this project to ambient traffic-generated noise.

Figure 15 – Noise Compatibility Matrix

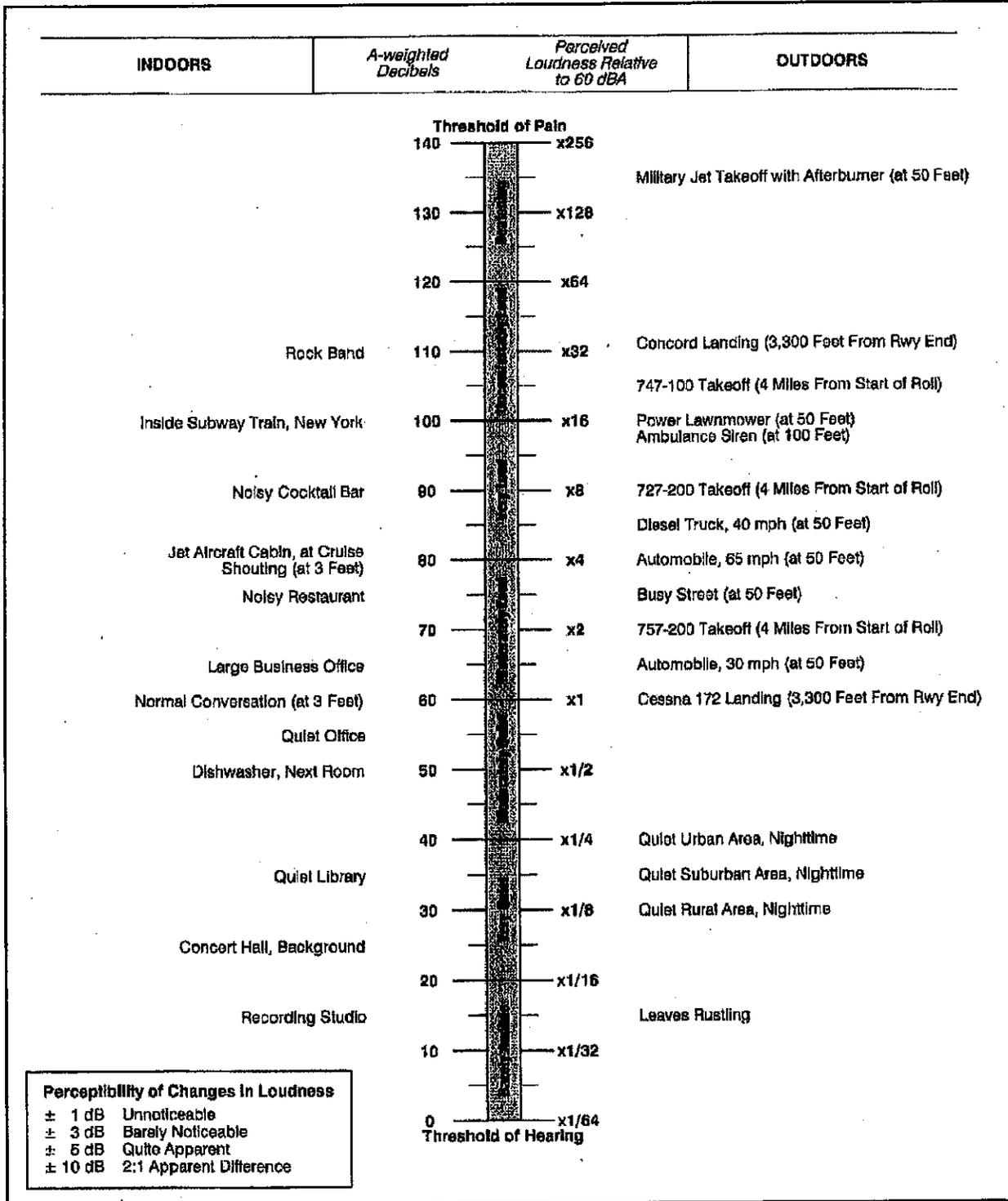
LAND USE CATEGORY	COMMUNITY NOISE EXPOSURE						
	Ldn or CNEL, dBA						
	55	60	65	70	75	80	85
RESIDENTIAL - LOW DENSITY SINGLE FAMILY, DUPLEX, MOBILE HOMES		Normally Acceptable	Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
RESIDENTIAL - MULTI-FAMILY		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
TRANSIENT LODGING - MOTELS, HOTELS		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
SCHOOLS, LIBRARIES, CHURCHES, HOSPITALS, NURSING HOMES		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
AUDITORIUMS, CONCERT HALLS, AMPHITHEATRES	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
SPORTS ARENA, OUTDOOR SPECTATOR SPORTS	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
PLAYGROUNDS, NEIGHBORHOOD PARKS		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
GOLF COURSES, RIDING STABLES, WATER RECREATION, CEMETERIES		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
OFFICE BUILDINGS, BUSINESS COMMERCIAL AND PROFESSIONAL		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
INDUSTRIAL, MANUFACTURING, UTILITIES, AGRICULTURE		Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable

<p> <b>NORMALLY ACCEPTABLE</b>          Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction, without any special noise insulation requirements.</p> <p> <b>CONDITIONALLY ACCEPTABLE</b>          New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice.</p>	<p> <b>NORMALLY UNACCEPTABLE</b>          New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.</p> <p> <b>CLEARLY UNACCEPTABLE</b>          New construction or development should generally not be undertaken.</p>
--	--

Source: Guidelines for the Preparation and Content of Noise Elements of the General Plan, California Office of Planning and Research, 1998.

Figure 16 – Typical Decibel Level of Common Sounds



Source: California Dept. of Transportation, *California Airport Land Use Planning Handbook*, Figure 6A.

5) The project site is located within the Oxnard Airport Sphere of Influence and is therefore also covered by the Ventura County Comprehensive Land Use Plan. Although the site is not under a direct flight pattern and is not located within a noise contour for the Oxnard Airport, it is subject to overflight of aircraft arriving and departing the Oxnard Airport (see Figure 17 – Oxnard Airport Noise Contours). Aircraft noise due to overflights may be present from time to time, but the project falls outside of the 65 CNEL noise contour for the Oxnard Airport. Future residents in the project area will not experience excessive aircraft noise or vibration, or any increase in the average level of ambient noise, or other effects. No impact is anticipated, and no mitigation is required. Mitigation measure K-3 is included as a standard requirement for projects located within the Oxnard Airport Sphere of Influence. There will be no impact.

6) The project site is not located in the vicinity or influence area of any private airstrip. There will be no impact.

Cumulative Development:

1-6) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative noise impacts other than the impact on the Decksides Villas project discussed above.

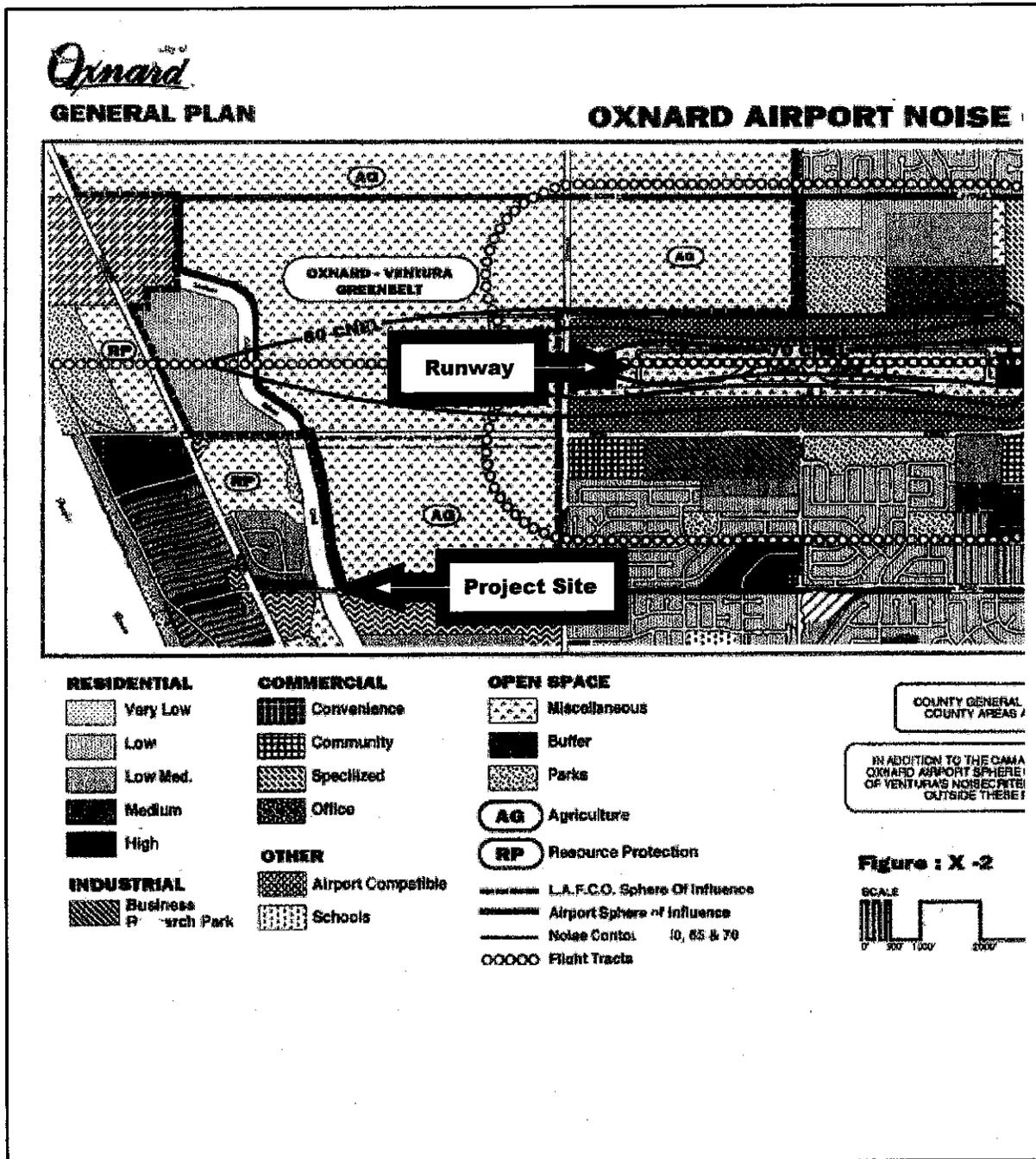
Mitigation Measure(s):

- K-1 The developer shall be required to incorporate a mat foundation into each building according to the recommendations of the *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch* (by Subsurface Designs, Inc. November 28, 2008) A pile foundation shall be prohibited.
- K-2 Prior to issuance of the first Certificate of Occupancy, the applicant shall contribute \$4,875 to a designated City fund for the Decksides Villas that is to be used only for soundwall-related expenses.
- K-3 Prior to issuance of the first Certificate of Occupancy, the developer shall record on all properties an Avigation Easement granting the County of Ventura an Avigation Easement for the operation of the Oxnard Airport. The Avigation Easement shall be consistent with the FAA model Avigation Easement.

Required Monitoring: Development Services and Public Works Division staff shall review grading and construction plans to ensure compliance, and inspectors shall field monitor all grading activities and foundation construction. Prior to issuance of the first Certificate of Occupancy, the Planning Manager shall confirm that the noise mitigation payment has been deposited with the Decksides HOA.

Result After Mitigation: All impacts associated with pile foundations will be avoided, and there will be no impact. Impacts related to traffic-generated noise will be mitigated to a level that is less than significant.

Figure 17 – Oxnard Airport Noise Contours



Source: City of Oxnard 2020 General Plan, Figure X-2.

**L. POPULATION AND HOUSING**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through an extension of roads or other infra-structure)? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1) The Coastal Zoning Ordinance envisioned development of the project site with a medium-density residential land use, and designated the property as Garden Apartment Coastal (R-3-C). The project will provide 70 residential condominium units within five buildings. As an in-fill residential project surrounded by urban development, the project will not induce substantial population growth either directly or indirectly. The proposed 70 new dwelling units were previously considered and approved when the City Council adopted Ordinance No. 2159 on April 19, 1988, approving the Development Agreement between the City of Oxnard and Oxnard Shores Development Company for the Wooley Road Property (Instrument No. 88-059788 recorded on May 2, 1988, and Instrument No. 89-174751 recorded on October 31, 1989).

The anticipated resident population associated with 70 condominium units would be approximately 200 persons (or an average of 2.85 persons per unit which reflects the average persons per unit in the Oxnard coastal area), and is population growth anticipated in the 2020 and draft 2030 General Plans. The intent of the Development Agreement was to prevent development on several vacant parcels along the beachfront, in order to preserve the beach resource and avoid overcrowding and significant impacts, among other purposes (see Figure 14 – Transfer of Development Rights). The City Council approved the developer’s payment of the In-Lieu Affordable Housing Fee, which contributes to the City’s various programs to increase the affordability of housing for all economic segments of the population. Therefore, no impact is anticipated.

2 & 3) The property is currently vacant, and no housing or people will be displaced. There will be no impacts.

Anacapa Townhomes – MND No. 09-03  
PZ Nos. 08-400-4, 09-300-1, 09-590-1  
January 15, 2010

Cumulative Development:

1-3) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts to population and housing.

Mitigation Measure(s): None required.

Monitoring: None required.

Result After Mitigation: Not Applicable.

**M. PUBLIC SERVICES\***

Would the project result in substantial adverse physical impacts to the following:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Fire protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Police protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Schools? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Parks? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Other public facilities? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

\* Include potential effects associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services.

Discussion:

1) Fire. The project has been designed to include adequate fire hydrants, vehicle and pedestrian access (see discussion in Section O, paragraph 5), signs, smoke detectors, and all requirements of the Uniform Fire Code in order to minimize any potential impacts to fire services. In addition, Fire Department conditions regarding access, monitoring, and other design features (see Section O, paragraph 5) will be incorporated into the proposed project. By complying with the California Fire Code, standard requirements and conditions of the Fire Department, the potential impacts to fire services is considered to be less than significant.

2) Police. According to the City’s 2020 General Plan, the current staffing ratio of City police officers to population should be maintained to provide adequate police services as the City’s population increases. The City monitors the need for additional equipment, facilities, and/or personnel as part of the Five-Year Development Plan. Through this action, the City ensures that police services are available to serve new development, including the proposed project and cumulative development in the City. The increase in tax base generated by the project and cumulative projects would help fund the project’s share of necessary police services within the City. In addition, the project must incorporate any mandatory Police Department design requirements (such as those pertaining to site access, site security, lighting, etc.) which will increase the safety of the public. Therefore, any potential impact on police services is considered to be less than significant.

3) Schools. School age residents in the project would attend either the Oxnard School District (K through 8) or the Oxnard Union High School District (OUHSD). The Oxnard School District uses a student generation factor for K-8 grade students of 0.110 for single-family attached units. For the project, the 70 condominium townhomes could potentially generate a total of eight K-8 grade students. The OUHSD uses a student generation factor of 0.1914 students per unit. The 70-unit development is calculated to generate 13 high school students. It is a mandatory requirement of all new residential development that prior to issuance of building permits, the applicant pays the applicable school impact fees to the school district in order to mitigate impacts to schools. By requiring the developer to pay the applicable fees to the school district, any potential impacts are considered to be less than significant.

4) Parks. The project provides on-site amenities for its residents, but future residents will place additional demands on community and regional parks and recreational facilities. It is a mandatory requirement of all residential development that prior to issuance of building permits, the applicant is required to pay the applicable Quimby impact fees in order to mitigate the effects of these additional demands. The proposed project will not be required to dedicate land or construct a public park on the site. It should be noted that the passive recreational opportunities nearby, notably the City's public beaches that are State resources, are not included in the inventory of parkland within the City. For a complete discussion, refer to section 4.12.1 of the Oxnard Village Specific Plan (Wagon Wheel) EIR, which is hereby incorporated by reference. However, a new public park was constructed with the Seabridge project (at the southwest corner of Wooley Road and Chesapeake Drive) and is located less than 1,000 feet east of the proposed project. Therefore, by payment of the applicable Quimby fee to mitigate the near-term as well as cumulative impacts, the impacts to parks are considered to be less than significant.

A major component of the Agreement for Settlement of Litigation (included as Appendix C of the *Final Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation* by ENVICOM Corp., March 18, 1988) resulted in the transfer of ownership of numerous properties to the State Lands Commission of the State of California for the purpose of creating additional public beaches (see Figure 14 – Transfer of Development Rights). A total of 14 large beach areas were created for use by general public, and 9 public access areas measuring approximately 1,438 lineal feet were created for public access. Also, under the terms of the Development Agreement between the City of Oxnard and Oxnard Shores Development Company, the City can impose reasonable conditions, mitigation measures, and other requirements as would ordinarily be imposed on projects of this nature or which may be required through normal CEQA review. The applicant will be required to pay the usual and ordinary Quimby impact fee (rather than a dedication of land or the construction of a public park) in order to mitigate the effects of additional demands on existing park facilities. Therefore, any potential impacts to parks are considered to be less than significant.

5) Public Facilities. All required infrastructure currently exists around the site and can serve the proposed project. Water will be provided to the development by the City of Oxnard's Public Works Water Division (within the Calleguas Water District), and wastewater disposal will be provided by the City's Public Works Wastewater Division. To address the project's share of demands on public infrastructure, the City requires developers of new projects to pay the following development fees: *Planned Traffic Circulation System Facilities Fees* (Traffic Impact); *Planned Water Facilities Fee*; *Planned Wastewater Facilities Fee*; *Planned Drainage Facilities Fee*; and *Growth Requirement Capital Fee*. The developer must also pay the Calleguas Capital Construction fee (see Mitigation Measure P-1). Community center facilities and the existing Central Library will be sufficient to meet the future needs of the City as identified in the 2020 General Plan. Therefore, with payment of applicable fees, impacts will be less than significant.

The project area would be served by City municipal water; the City obtains water from three sources: the Calleguas Water District, local well water from United Water Conservation District, and City wells. The proposed project was included in the 2005 Urban Water Management Plan. In order to address water supply needs at a regional level, representatives of the City of Oxnard, the Port Hueneme Water Agency (PHWA), the United Water Conservation District (UWCD), and the Calleguas Municipal Water District (CMWD) meet regularly. A collective effort to ensure continued delivery of high-quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program; a new, regional groundwater desalination facility is associated with this program and is intended to serve the Cities of Oxnard and Port Hueneme. A detailed water demand and supply study to the year 2030 prepared by the Water Division for the 2030 General Plan indicates the City will have adequate water supplies under normal year conditions for this project and the 2030 General Plan buildout. The City has banked groundwater pumping credits with the United Water Conservation District approximately equivalent to a one year's demand to make up for temporary supply shortages and/or dry year scenarios. The Draft 2030 General Plan EIR Water Supply impact analysis, pages 4-26 to 4-31, is hereby included by reference.

In terms of climate change effects on water supplies, numerous studies have been conducted including studies by the California Department of Water Resources, and the studies recognize that future impacts depend on the degree of global warming and that there is substantial uncertainty regarding its effect on local and regional climates. Current models being utilized are imperfect and imprecise, and there is no way to predict future temperature rise. However, global warming trends will likely have a detrimental impact on water supplies throughout California and the western United States, and California's existing infrastructure systems have not been designed for the likely future hydrologic conditions. Water suppliers in California (including the Metropolitan Water District) are addressing climate change impacts and developing policies to prevent water shortages, including new strategies that promote conservation, groundwater recharge, and water recycling.

Cumulative Development:

1-5) Except for water supply, cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts to public services.

Mitigation Measure(s):

- M-1 Prior to issuance of any building permits, the developer shall pay the mandatory impact fees to the school district and present evidence of payment.
- M-2 Prior to issuance of any building permits, the developer shall pay the mandatory Quimby Fee.
- M-3 Prior to issuance of any building permits, the Developer shall pay the following mandatory development impact fees: *Planned Traffic Circulation System Facilities Fees (Traffic Impact); Planned Water Facilities Fee; Planned Wastewater Facilities Fee; Planned Drainage Facilities Fee; and Growth Requirement Capital Fee.*

Monitoring: Development Services Department staff shall collect impact fees prior to issuance of any building or engineering permits.

Result After Mitigation: Less than significant impacts.

**N. RECREATION**

	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1 & 2) The project provides on-site amenities for its residents, but future residents will place additional demands on community and regional parks and recreational facilities. It is a mandatory requirement of all residential development that prior to issuance of building permits, the applicant is required to pay the applicable Quimby impact fees in order to mitigate the effects of these additional demands. The proposed project will not be required to dedicate land or construct a public park on the site. It should be noted that the passive recreational opportunities nearby, notably the City’s public beaches that are State resources, are not included in the inventory of parkland within the City. For a complete discussion, refer to section 4.12.1 of the Oxnard Village Specific Plan (Wagon Wheel) EIR, hereby incorporated by reference. However, a new public park was constructed with the Seabridge project (at the southwest corner of Wooley Road and Chesapeake Drive) and is located less than 1,000 feet east of the proposed project. Therefore, by payment of the applicable Quimby fee to mitigate the near-term as well as cumulative impacts, the impacts to parks are considered to be less than significant.

A major component of the Agreement for Settlement of Litigation (see Appendix C of the *Final Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation* by ENVICOM Corp., March 18, 1988) resulted in the transfer of ownership of numerous properties to the State Lands Commission of the State of California for the purpose of creating additional public beaches (see Figure 14 – Transfer of Development Rights). A total of 14 large beach areas were created for use by general public, and 9 public access areas measuring approximately 1,438 linear feet were created for public access. Also, under the terms of the Development Agreement between the City of Oxnard and Oxnard Shores Development Company (Instrument Nos. 88-059788 and 89-174751), the City can impose reasonable conditions, mitigation measures, and other requirements as would ordinarily be imposed on projects of this nature or which may be required through normal CEQA review. The applicant will be required to pay the usual and ordinary Quimby impact fee (rather than a dedication of land or construction of a park) in order to mitigate the effects of additional demands on existing park facilities. Any impacts to parks are considered to be less than significant.

Mitigation Measure(s):

N-1. The developer shall be required to pay the mandatory Quimby Fee prior to issuance of any building permits.

Monitoring: Planning Staff shall assess and collect the Quimby Fee prior to issuance of any building permits.

Result After Mitigation: Less than significant impact.

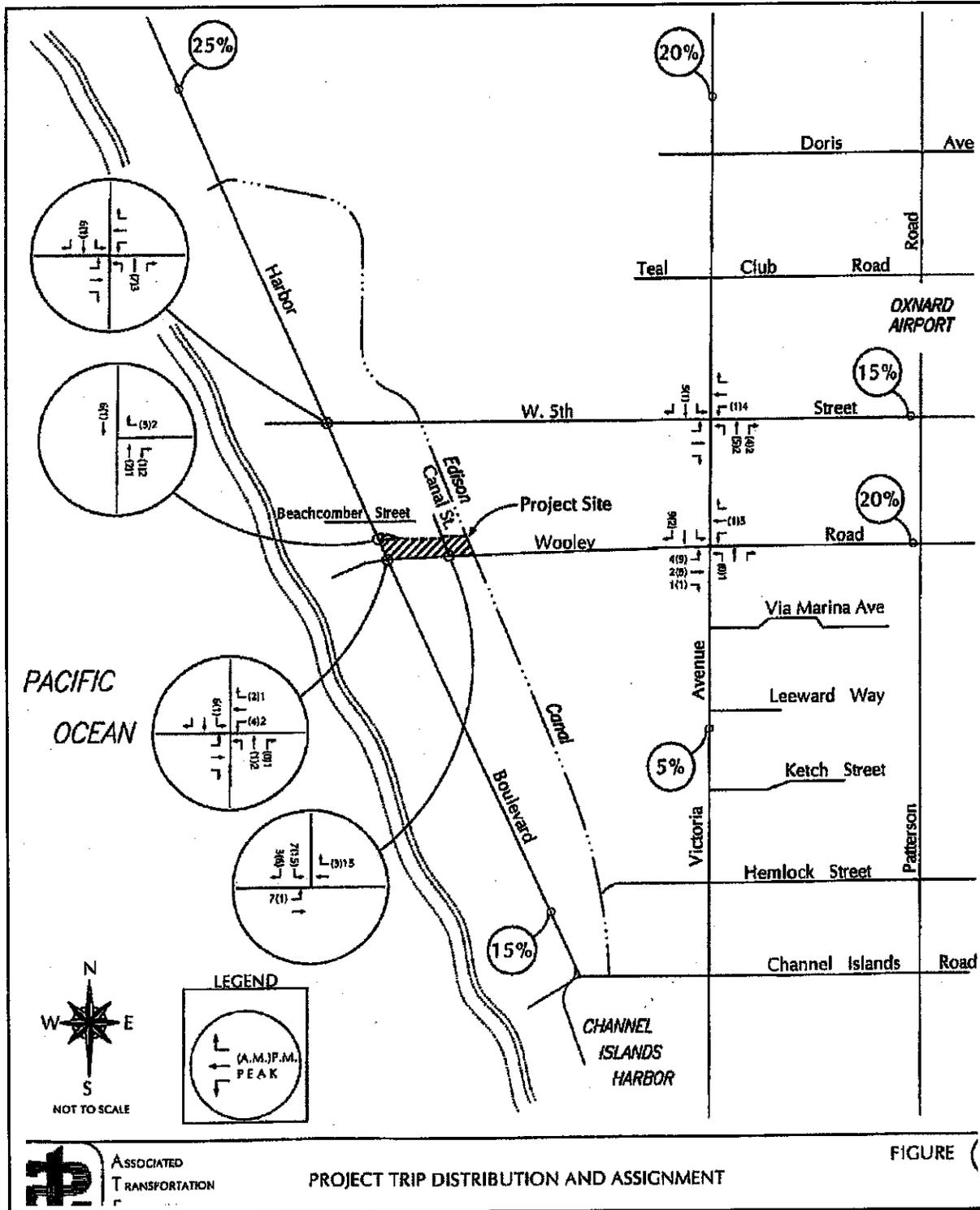
**O. TRANSPORTATION/TRAFFIC**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Cause an increase in traffic, which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Exceed, either individually or cumulatively, a level of service standard established by the County congestion management agency for designated roads or highways? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Result in a change in traffic patterns, including either an increase in traffic levels or a change in location, that results in substantial safety risks? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/ Circulation)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Result in inadequate emergency access? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. Result in inadequate parking capacity? (Zone Ordinance - Parking Regulations and Parking Lot Design Standards)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. Conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Bicycle Facilities Master Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

The following sections are based on the conclusions and recommendations in the *Traffic and Circulation Study, ATE Project #08116* (December 4, 2008), prepared by Associated Transportation Engineers. The study was prepared under the supervision of the City's Traffic Engineering staff, and reviewed and accepted by the Traffic Engineer. Five intersections were included in the study (see Figure 18 – Project Trip Distribution): Harbor Blvd. & Wooley Rd.; Harbor Blvd. & 5<sup>th</sup> Street; Wooley Rd. & Canal Street; Wooley Rd. & Victoria Ave.; Victoria Ave. & 5<sup>th</sup> Street. The methodology, analysis, and results are explained in detail on pages 1 – 14 in the *Traffic and Circulation Study*.

Figure 18 – Project Trip Distribution



Source: Traffic and Circulation Study, Figure 5, prepared by Associated Transportation Engineers (December 4, 2008).

1 & 2) An analysis of potential traffic and circulation impacts associated with the proposed residential development was conducted for this specific project. The analysis included the identification of existing and projected traffic volumes, distribution of project-related traffic, and capacity analysis at several key intersections in the project vicinity during a.m. and p.m. peak hours. The analysis was conducted to identify the proposed project’s potential traffic-related impacts on specified intersections and roadways under opening year (2010) and future year (2020) conditions. A full explanation of the methodology and threshold criteria are included in the traffic study on pages 8 and 9.

Table 7 summarizes the project’s trip generation. Based on the generation rate published in *Trip Generation* (8<sup>th</sup> Ed., by the Institute of Transportation Engineers), a total of 407 Average Daily Trips can be expected due to the project. According to Figure 18, the traffic model estimates that 60% (20 + 15 + 20 + 5 = 60) of the trips would utilize Wooley Road traveling east, and 40% (25 + 15 = 40) would utilize Harbor Boulevard. Of the total of 407 daily trips, approximately 31 trips will occur in the morning peak-hour 7:00-9:00 a.m. and 36 trips will occur in the evening peak-hour 4:00-6:00 p.m. (see Table 7). These peak-hour trips will generally be split between the two primary access points (one being the alley to Harbor Blvd. and the second to Canal St.) distributed over a time period of two hours each in the morning and evening. Assuming a total of 31 and 36 trips in the morning and evening peak-hours, respectively; an average of 8 or 9 trips per hour would enter/exit the project at Harbor Blvd. and Canal St. The remainder of the 340 daily trips (407 – 67 = 340) will be distributed throughout the day like in other residential neighborhoods.

**Table 7 – Project Trip Generation**

Land Use	ADT	Weekday Peak Hour Trips					
		AM Peak Hour (7:00 – 9:00 a.m.)			PM Peak Hour (4:00 p.m. – 6:00 p.m.)		
		Entering	Exiting	Total	Entering	Exiting	Total
70 Condominium / Townhouse Units	407	5	26	31	24	12	36

Source: *Traffic and Circulation Study*, Table 4.

Based on the City of Oxnard criteria for evaluating project impacts at intersections, the City’s Traffic Engineer determined that the project will not exceed the threshold criteria, and would not have any potentially significant impacts. Therefore, no mitigation measures are required to compensate for the project’s traffic. However, the developer will be required to construct, at his expense, the following standard Public Works improvements:

- Dedication of land and a new right-turn only lane on westbound Wooley Rd.;
- Modify the traffic signal at the Harbor Blvd./Wooley Rd. intersection to accommodate Wooley Rd. widening;
- Construct curb/gutter/sidewalk along frontages facing Harbor Blvd., Wooley Rd., and Canal St.;
- Grind and repave the asphalt surfaces on Wooley Rd., Canal St., and Hobbs Way (alley);
- Remove poles and install underground wires along the north side of Wooley Rd. adjacent to the project site;
- Install storm drain catch basins and street lights adjacent to the project site; and,
- Install other usual and ordinary improvements (e.g. ADA compliant pathways and driveways, sidewalk ramps, street signs, etc.) to serve the development.

In addition to the Public Works standard requirements, mitigations O-1 through O-6 identify design features that the developer will construct, at his expense, as part of the project. With construction of the standard requirements and mitigation measures, the potential impacts will be less than significant.

Table 8 shows the traffic model’s estimates for future Levels of Service (LOS). Level of Service (LOS) is an index utilizing letters A through F to describe the amount of traffic congestion, with ‘A’ indicating little or no delay, and ‘F’ indicating lengthy delays or gridlock. As Table 8 indicates, the proposed project will not result in any LOS reaching ‘C’ or lower (in fact, there will be no deterioration in LOS at local intersections), and there will be no potentially significant impacts. There are no special mitigation measures that are required for traffic. However, standard off-site improvements will be required, such as a right-turn lane on westbound Wooley Road, sidewalks, ramps, etc.

**Table 8 – Existing (2008) and Opening Year (2010) Level of Service Summary**

Intersection	Existing		Year 2010 without Project <sup>1</sup>		Year 2010 with Project <sup>2</sup>		Significant Impact?
	AM	PM	AM	PM	AM	PM	
Harbor Blvd & 5 <sup>th</sup> Street	A	A	B	A	B	A	No
Harbor Blvd & Dunes Alley	B	A	B	B	B	B	No
Harbor Blvd & Wooley Rd	A	A	A	B	A	B	No
Victoria Ave & 5 <sup>th</sup> Street	A	B	A	C	A	C	No
Victoria Ave & Wooley Rd	A	A	A	A	A	A	No
Wooley Rd & Canal Street	A	B	B	B	B	B	No

1. Existing conditions plus approved/pending projects not yet built.  
 2. Includes proposed project and approved/pending projects.  
 Source: *Traffic and Circulation Study*, Tables 1, 3, 5 & 6.

Construction of the proposed project will result in increased vehicle trips, resulting in the increased use of City and County roads in the vicinity of the project site. Additional traffic on local roads will result in maintenance costs to the City and County, and therefore, it is appropriate to impose a standard traffic impact fee in proportion to the project’s expected trip generation. The City of Oxnard and Ventura County have executed a “Reciprocal Traffic Mitigation Agreement” wherein the City and County have agreed that a pro-rata share of the cost of mitigations will be collected by each agency for traffic impacts in the other jurisdiction. The project would be consistent with the Ventura County General Plan by complying with the terms of the “Reciprocal Traffic Mitigation Agreement” adopted on February 2, 1993. By paying the mandatory County/City Traffic Impact Fee, the developer will mitigate the project’s impacts on local roads and intersections. With payment of the fee, any impacts will be less than significant.

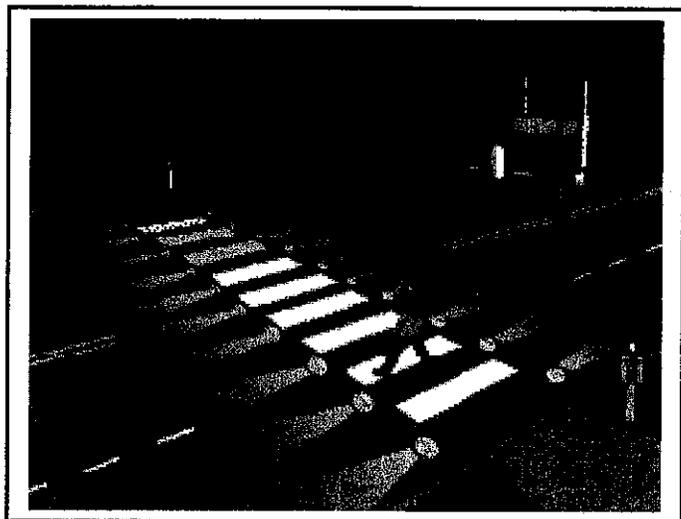
With regard to cumulative impacts (e.g. impacts in the year 2020), the County of Ventura has adopted a Traffic Fee Mitigation Ordinance. The County of Ventura allows for participation in the Traffic Fee Program as a way to comply with its Traffic Fee Mitigation Ordinance and General Plan traffic policies. By paying the mandatory County/City Traffic Impact Fee (see mitigation O-7), the developer will mitigate the project’s potential cumulative impacts. There, with payment of the County/City Traffic Impact Fee, potential impacts will be less than significant.

3) Harbor Boulevard is the principal north-south access route to the project site, and is designated a secondary arterial in the Circulation Element of the 2020 General Plan (Figure VI-5). Wooley Road is a major east-west thoroughfare that is also adjacent to the project site, and is designated a local arterial for that segment (2020 General Plan, Figure VI-5). The project proposes primary access from alley connected to Harbor Boulevard and Canal Street adjacent to the project site, and will not take direct access (i.e. no driveways) from either Harbor Blvd. or Wooley Road. Traffic patterns will remain the same, as existing intersections currently can serve the site and permit turns directly into the site (i.e. to and from Harbor Blvd. and Canal Street). The signal timing at Harbor Blvd./Wooley Rd. allows protected U-turns on Harbor Blvd. (a minimum of 35 feet is required to make a U-turn, and currently there is 38 feet of existing roadway) and most if not all of the traffic arriving via Harbor Blvd. will utilize the alley access. The City will require frontage improvements along Wooley Road to provide an additional right-turn lane at Harbor Blvd., widening and an

intersection on Canal Street, a ladder-type pedestrian crosswalk with embedded flashing LED lights (crossing Wooley Road at Canal Street), traffic advisory signs, and bike lanes on Wooley Road. All improvements will be constructed according to current design criteria and traffic safety standards. Wooley Road will be widened at Harbor Blvd. and have 12-foot wide vehicle lanes which will help reduce speeds (currently the lanes are 15 feet wide) and further improve safety in a variety of weather conditions. After the improvements are completed on Wooley Road and the project has been constructed, the Public Works Division will conduct a speed zone study to analyze the justification for reducing the speed limit to 35 m.p.h. on Wooley Road east of Harbor Blvd. Mitigation measures O-1 through O-6 list the standard design features that will be constructed as part of the project, and will result in improved safety for traffic and pedestrians. After installation of the mitigation measures, the potential impacts will be less than significant.

4) The project will not substantially increase hazards due to any design features or incompatible uses (e.g., farm equipment). Although an active agricultural land use is in the vicinity of the project site, the small amount of truck traffic is intermittent and seasonal, and access to agriculture is not located adjacent to the project site. Historical data was obtained from the City's Traffic Engineering staff for motor vehicle collisions that occurred in the time period from 10/01/03 – 09/01/09, at the intersections of Harbor Blvd./Wooley Rd. as well as at Harbor Blvd./Beachcomber Way. A total of 27 collisions occurred at Harbor Blvd./Wooley Rd. (7 of which were due to DUI), and a total of 15 collisions occurred (3 of which were due to DUI) at the Harbor Blvd./Beachcomber intersection. Traffic Engineering staff has analyzed the historical data and determined that design features have not contributed to past traffic accidents, and the proposed project will not construct any design features that might contribute to future collisions.

The City will require improvements to ensure safety, including a ladder-type pedestrian crosswalk with in-pavement flashing LED lights (on Wooley Rd. at Canal St.), and traffic control signs. The in-pavement LED lights are used at crosswalks to alert motorists to the presence of a pedestrian crossing the street. The amber lights are embedded in the pavement on both sides of the crosswalk and oriented to face oncoming traffic. When the pedestrian activates the system by using a push-button, the lights begin to flash at a constant rate, warning the motorist that a pedestrian is in the crosswalk ahead. The amber LED lights flash in unison at a rate designed for maximum motorist recognition and are visible in daylight as well as at night. The flashing lights are only activated when a pedestrian wants to cross and are automatically shut off after a set period of time (i.e., the time required to safely cross the street). Since the lights only flash when activated, the motorist receives real-time information indicating that a pedestrian is in the crosswalk or preparing to cross. In addition, warning signs will be required to advise motorists of the nearby crosswalk. In Oxnard, existing crosswalks with effective LED lights are located at Saviers Rd./Dollie Street. Other than a new right-turn pocket on westbound Wooley Rd. at Harbor Blvd., no additional turn pockets (e.g. at Canal Street) are required, based on the volume of future traffic and the requirements of the CalTrans *Highway Design Manual*. The City's Traffic Engineer has reviewed the proposed plans, and attached the necessary standard requirements in order to comply with the *Highway Design Manual*. All improvements will be constructed to comply with current design criteria and traffic safety standards. After compliance with Standard Requirements and mitigations O-5 and O-6, the impacts will be less than significant.



5) The project proposes vehicular access from Canal Street and the proposed alley at the rear of the project site. Two arterial roads (Harbor Boulevard and Wooley Road) are adjacent to the site and provide adequate access for emergency vehicles and services, and the site design has been approved by the Fire Marshal. The standard requirements prepared by the Fire Department will include the following items:

- The project shall meet the minimum requirements of the “Fire Protection Planning Guide” published by the Fire Department;
- At all times during construction, Developer shall maintain paved surfaces capable of handling loads of 46,000 pounds which will provide access for fire fighting apparatus to all parts of the project property;
- The turning radius of all project property driveways and turnaround areas used for emergency access shall be approved by the City Traffic Engineering Department;
- All emergency egress/Fire Department access windows or doors that serve any room that can be utilized for sleeping shall have access to a public right-of-way without re-entering the structure;
- Developer shall provide automatic fire sprinklers as required by the City Code;
- Developer shall install in each structure in the project an alarm system with a central station monitor that will automatically notify the Fire Department in the event of a fire in the structure;
- Before the City issues a certificate of occupancy, the Developer shall install a Knox Box key vault at a location on the building to be determined by the Fire Department; and,
- Developer shall install security devices and measures, including walkway and vehicle control gates, entrance telephones, intercoms and similar features, subject to approval of the Police Chief and the Fire Chief.

Prior to issuance of any building permits, Development Services Department and Fire Department staff will review construction plans to ensure compliance with all Standard Requirements. Therefore, there will be no impacts.

6) The proposed project will be required to comply with the parking requirements of the Zoning Code (a minimum of 181 spaces are required, and 200 spaces will be provided). The parking areas will be required for vehicle parking only, and cannot be converted to storage spaces. The Zoning Code does not regulate the number of vehicles allowed per dwelling unit (e.g. such as multiple tenants residing within one house in the adjacent neighborhood). Parking due to vehicles from this project will not have any significant effect on adjacent properties or streets. The project C.C.&R.’s will control parking by the residents and their guests. The alley and curbs will be striped and marked as a Fire Lane, and parking will not be allowed within the alley. No impacts are anticipated on the project site, and no cumulative impacts are expected to occur as a result of this project.

7) The proposed project will not conflict with any adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks). Bicycle parking and storage will be provided on-site for residents and their guests. The local transit agency does not require a bus turnout adjacent to the site. There will be no impacts.

#### Cumulative Development:

1-7) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts to traffic/transportation.

In addition to the project's traffic study, City staff has completed analyses of projected 2030 traffic as part of the environmental review of the Draft 2030 General Plan. The Citywide Oxnard Traffic Model (OTM) includes the 70 units on the project site, proposed development in the harbor under County of Ventura jurisdiction, the completion of the North Shore project, the completion of the Seabridge project (with an elementary school or with housing as permitted), and the eventual development of the tract just adjacent and north of the Dunes neighborhood. OTM results show no intersections on the west of Ventura Road operating a below LOS C under the 2030 General Plan Alternative B scenario after OTM mitigations are assumed completed.

Mitigation Measure(s):

- O-1 Developer shall dedicate to the City the Right-of-Way along Wooley Road and Harbor Blvd. associated with the road widening under this project.
- O-2 Developer shall modify the traffic signal at Wooley Rd/Harbor Blvd. associated with the proposed road widening. The signal modification shall include provision of a traffic signal battery back-up system, new service equipment enclosure and emergency vehicle detection system.
- O-3 Developer shall design and reconstruct the curb ramp at the southeast corner of Wooley Rd and Harbor Blvd. The redesign shall comply with disabled access requirements and address the sand accumulation problem at the ramp.
- O-4 Developer shall place existing overhead utility lines (except electrical lines in excess of 33KV) on and adjacent (as defined in City ordinance) to the project (including Wooley Road, Harbor Boulevard and the canal easterly of the project) underground in accordance with City ordinance. Before issuance of a site improvement permit or recordation of a final map, Developer shall post security satisfactory to the Finance Director guaranteeing utility relocation.
- O-5 Developer shall install a wireless, solar powered, in-pavement crosswalk warning light system on the easterly leg of the Wooley Road/Canal Street intersection along with associated striping and signage, including advanced warning signage and markings, to enhance the safety for pedestrian crossing of Wooley Road. Final design of the crosswalk warning light system to be approved by the City Traffic Engineer.
- O-6 Developer shall design the walls at the entry and exit gate of the Ground Parking Level of Buildings 1 and 4 to provide additional corner visibility similar to that provided on Buildings 2 and 3. Final design shall be approved by the City Traffic Engineer.
- O-7 Developer shall pay the applicable County/City Traffic Impact Fee(s) prior to issuance of a building permit.

Monitoring: The Development Services Division will ensure that all impact fees are paid. The Development Services Division shall review improvement and construction plans to ensure compliance with these mitigation measures and standard requirements.

Result After Mitigation: Less than significant impacts.

**P. UTILITIES AND SERVICE SYSTEMS**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
7. Comply with federal, state, and local statutes and regulations related to solid waste? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1, 2, 3) As an in-fill residential project surrounded by urban development, the project will create additional incremental demand on existing utilities and service systems. The project will not require or result in the construction of new water or wastewater treatment facilities, expansion of existing facilities, construction of new storm water drainage facilities, or expansion of existing storm water facilities. Existing infrastructure exists adjacent to the property, in Harbor Boulevard and Wooley Road, and can serve the project. As noted in Section H (Hydrology & Water Quality) above, the developer will be responsible for installing standard storm drain facilities to serve the project site and connect to existing infrastructure around the property, including on-site storm water treatment and reduction devices, that will meet the requirements of the City and the National Pollutant Discharge Elimination System (NPDES). Therefore, any impacts to drainage and treatment facilities are expected to be less than significant.

4) Water service to this area is currently provided by the City's Water Division and is located within the Calleguas Water District area. The Calleguas Water District purchases most its water from the Metropolitan Water District of Southern California. Other sources of water include local well water from United Water Conservation District and City wells. A collective effort to ensure continued delivery of high-quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program; a new, regional groundwater desalination facility is associated with this program and is intended to serve the Cities of Oxnard and Port Hueneme. The project will not create any unusual demands on water supplies. Based on an average per capita water demand rate of 155 gallons per day (GPD) per person, the anticipated increase in water demand would be approximately 31,000 GPD or approximately 35 Acre-Feet per Year (AFY). Water conservation and shut-off devices will be installed in the project, such as landscape irrigation systems, interior/exterior fixtures, appliances, etc. The water demand required by the proposed development is included in the *2005 Urban Water Management Plan* which documents adequate long-term supply. The City's projected water supplies will meet the City's projected demand during normal, single dry, and multiple dry years through the Year 2030. This includes the proposed project as well as the anticipated cumulative development expected to occur during that time frame. For a complete discussion, refer to section 4.14.1 and 4.14.2 of the Oxnard Village Specific Plan (Wagon Wheel) EIR, hereby incorporated by reference. The project site is currently located within Calleguas service boundary, annexation is not required, and the developer shall be required to pay the applicable Calleguas Capital Construction fee in effect at the time of building permit issuance. By paying the applicable fees, the impact will be less than significant.

In terms of climate change effects on water supplies, numerous studies have been conducted including studies by the California Department of Water Resources, and the studies recognize that future impacts depend on the degree of global warming and that there is substantial uncertainty regarding its effect on local and regional climates. Current models being utilized are imperfect and imprecise, and there is no way to predict future temperature rise. However, global warming trends will likely have a detrimental impact on water supplies throughout California and the western United States, and California's existing infrastructure systems have not been designed for the likely future hydrologic conditions. Water suppliers in California (including the Metropolitan Water District) are addressing climate change impacts and developing policies to prevent water shortages and reassess distribution of water supplies, including new strategies that promote conservation, groundwater recharge, and water recycling.

5) All required infrastructure currently exists around the site and can serve the proposed project. Wastewater disposal will be provided by the City's Public Works Wastewater Division. Currently, the sewer flow from this project site is assessed upon the Water System Master Plan, and staff from the Public Works Division has determined that the wastewater treatment facilities have adequate capacity to serve this project and existing urban development in the City. To address the project's share of demands on public infrastructure, the City requires developers of new projects to pay the development impact fees, including the *Planned Wastewater Facilities Fee* and *Growth Requirement Capital Fee*. By paying the applicable fees, impacts to wastewater services are considered to be less than significant.

6 & 7) The two primary landfills receiving solid waste from the City are the Toland Road and Simi Valley landfills, and both landfills have a predicted lifespan that exceeds 20 years. Construction of the proposed project will result in a temporary increase in the volume of waste deposited in local landfills during the construction phase, but given the temporary nature of construction activity and the availability of space at both landfills, plus the recycling of waste at the Materials Recovery Facility, there will be no adverse impacts to solid waste disposal facilities during construction of the project. Compliance with the City's Source Reduction and Recycling Element will ensure that any impacts will be less than significant. Standard requirements and conditions of approval will require compliance with the City's recycling and waste program, which is designed to manage and reduce the amount of waste being directed to landfills. The project will also be required to comply with federal, state, and local statutes and regulations related to solid waste. Any impacts will be less than significant.

Cumulative Development:

1-7) Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts to utilities and service systems.

Mitigation Measure(s):

P-1 The developer shall pay the applicable Calleguas Capital Construction charges prior to issuance of a building permit.

Monitoring: Development Services Staff shall ensure that fees are paid prior to issuance of a building permit.

Result After Mitigation: Less than significant.

**Q. MANDATORY FINDINGS OF SIGNIFICANCE**

	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Does the project have impacts that are individually limited, but cumulatively considerable? ( <i>"Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.</i> )	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1, 2, 3) The proposed infill project would result in the construction of 70 condominium units on approximately 3.54 acres of land largely surrounded by urban development. The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory. The project will not have environmental effects that cause substantial adverse effects on human beings, either directly or indirectly. Mitigation measures are included in the Mitigated Negative Declaration (described in previous sections of this Initial Study), or the project has been modified to include or omit features that will effectively avoid any potentially significant environmental impacts.

Cumulative development impacts for the west side of Oxnard are generally addressed in the Oxnard Village Specific Plan (Wagon Wheel) Final Environmental Impact Report (No. 06-03) that was certified by the Oxnard Planning Commission on November 18, 2008. These Draft and Final EIR documents are available at the City of Oxnard Planning Division office, 214 South C Street, during office hours and at the Main and South branches of the Oxnard Library (251 South A Street and 200 East Bard Street, respectively). Anticipated future cumulative projects will be constructed within the City Urban Restriction Boundary (CURB) on land already designated for development under existing General Plan and zoning designations. Development within CURB and conforming to General Plan and zoning designations would have impacts anticipated by the 2020 General Plan, reviewed by the 2020 General Plan EIR, and would not create adverse cumulative impacts.

Mitigation Measure(s): None required.

Monitoring: None required.

Result After Mitigation: Not Applicable.

## SUMMARY OF MITIGATION MEASURES INCORPORATED INTO THE PROJECT

### SECTION C – AIR QUALITY

- C-1 The developer shall prepare and submit an Air Emissions Mitigation Plan for Dust Control. This Plan shall be included as part of the construction contract and submitted to the City of Oxnard for review and approval prior to the issuance of grading permits. This plan shall include the following elements:
- Fugitive dust throughout the construction site shall be controlled by the use of a watering truck or equivalent means, generally at least three times a day (except during and immediately after rainfall). Water shall be applied to all unpaved roads, unpaved parking areas or staging areas, and active portions of the construction site. Environmentally-safe dust control agents may be used in lieu of watering.
  - Revegetate or apply APCD-approved chemical soil stabilizers to all inactive portions of the construction site that are inactive for four or more days.
  - Suspend or curtail all excavation, earth moving, and grading operations during episodes of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties) to prevent fugitive dust from being a nuisance or hazard.
  - Material transported in trucks off site shall comply with State Vehicle Code Section 23114, with special attention to Sections 23114(b)(F), (e)(2), and (e)(4) as amended. Material transported on site shall be sufficiently watered or secured to prevent fugitive dust.
  - Inform all employees involved in grading operations on the project to wear face masks during dry periods to reduce inhalation of dust.
  - Signs shall be posted on-site requiring traffic speeds to not exceed 15 miles per hour.
  - Sweep streets at the end of the day if visible soil material is carried over to adjacent streets and roads.
  - At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.
- C-2 Maintain equipment engines in good condition and in proper tune as per manufacturer's specifications. Minimize idling time. Prohibit the use of on-site electric generators, and connect to utility lines adjacent to the project site.
- C-3 If feasible, use alternatively fueled construction equipment, such as compressed natural gas (CNG), liquefied natural gas (LNG), or electric.
- C-4 During construction, contractors shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
- C-5 A temporary fence around the project site shall include an opaque visual barrier up to 6 feet high. The fence shall include signs identifying the name, telephone number, and emergency contact information for the contractor(s) responsible for the site, construction activities, and rectifying any nuisance conditions.
- C-6 Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements.

- C-7 As stated in the APCD Guidelines, applicants are required to mitigate environmental impacts associated with their projects to the greatest extent feasible. The following are those measures that could be feasibly implemented to mitigate operational phase air quality impacts of the proposed project:
- Lighting for public streets, parking areas, and recreational areas shall utilize energy-efficient mechanical, computerized, or photo cell switching devices to reduce energy usage.
  - Energy-efficient, automated controls for air conditioners shall be installed into proposed buildings to reduce energy consumption and emissions.
  - Automatic lighting on/off controls and energy-efficient lighting shall be installed into proposed buildings to reduce electricity consumption and associated emissions.
  - Light-colored roofing materials as opposed to dark roofing materials shall be used on proposed buildings. Light colored materials reflect sunlight and minimize heat gains in buildings. This measure would lessen the overall demand for mechanical air conditioning systems.
  - Wall and attic insulation shall be provided in proposed buildings beyond the requirements of Title 24, *California Code of Regulations*.
  - Built-in energy-efficient appliances shall be installed into proposed buildings.
  - Special sunlight filtering window coatings or double-paned windows shall be installed into proposed buildings to reduce thermal gain in hot weather and loss in the cold weather, thus reducing emissions associated with heaters and air conditioners.
- C-8 The design of the swimming pool building shall incorporate solar panels to the maximum extent feasible for pool water heating.
- C-9 All project construction and site preparation operations shall be conducted in compliance with all applicable Ventura County APCD Rules and Regulations with emphasis on Rule 50 (Opacity), Rule 51 (Nuisance), Rule 55 (Fugitive dust), and Rule 10 (Permits Required).

#### SECTION D – BIOLOGICAL RESOURCES

- D-1 The developer shall avoid grubbing or grading the site during the breeding season (i.e. February 15 through August 31) as the preferred mitigation. If avoidance is not feasible and grubbing/grading will occur during the breeding season, then the project site shall be surveyed by a qualified biologist for the presence of nesting birds prior to removal of vegetation. The developer shall be required to submit a report documenting the findings of the site survey to the Planning Division for review and approval. The site survey shall be conducted within 30 days of the beginning of any grubbing/grading activity. If any active nests are detected, then a 300-foot buffer (or as otherwise deemed appropriate by the biological monitor) shall be placed around the nest site until the nestlings have successfully fledged.
- D-2 Focused pre-construction surveys for the Least Bell's Vireo (LBV) will be conducted in suitable habitat within 500 linear feet of construction activities. The surveys will follow agency protocol as to survey methodology (i.e. time of day and climatic conditions). However, a modified survey schedule is appropriate given the site-specific circumstances. Therefore, surveys of the area within 500 feet of construction activities and the area where LBV has been reported will be initiated if commencement of construction is to occur during the breeding season for the species (April 10 to July 31). If no LBV are detected within 500 feet of the construction activities during any of the three survey bouts, no further action is required. If, however, LBV are detected within 500 feet of construction activities, CDFG and the U.S. Fish & Wildlife Service (USFWS) will be contacted to determine what, if any, actions need to be taken to avoid adverse effects on the species. Such actions, if necessary, may

include one or more of the following: phasing the construction activities to avoid certain construction activities within 500 feet of occupied LBV habitat until after the breeding season has ended; noise monitoring; and nest monitoring. If LBV is detected in suitable habitat farther away than 500 feet, these areas will be monitored during each of the three survey bouts so as to gain assurances that occupied habitat remains more than 500 feet from construction activities. The findings of the surveys will be documented and submitted to the City within 10 days of completion, as well as CDFG and USFWS as applicable.

- D-3 Prior to the initiation of construction activities, a temporary silt fence will be installed along the project site's easterly boundary to prevent ingress of Silvery Legless Lizards (SLL) onto the site for the duration of construction activities. The fencing will be keyed into the ground to a depth of at least three inches. Preconstruction surveys for SLL will then be conducted within areas of the site that could be used by the species. The surveys will be conducted according to the following protocol provided by CDFG.

Surveys will be conducted on a warm day (70 to 80 degrees Fahrenheit) in the mid-morning and no longer than two weeks prior to the commencement of soil disturbance activities. Relative soil moisture should be approximately 65% to 80% with soil temperature being 60 to 70 degrees Fahrenheit. Two person teams will work together with one person using a hand rake to gently rake the loose litter and soil down to substrate. The other person will search the areas being raked for SLL.

Surveys will employ a "three-pass" methodology to locate as many SLL as possible. If SLL are found during the first pass, an overnight period of no disturbance will occur before the second pass. Similarly, if SLL are found during the second pass, another overnight period of no disturbance will occur before the third pass. If during any pass no SLL are found, the survey will be concluded.

Any SLL found will be placed in a lidded, vented box containing clean sand. Areas of moist and dry sand will be present in the box. Boxes will be kept out of direct sunlight and protected from temperatures over 72 degrees Fahrenheit. The temperatures of the sand in the box will be kept under 66 degrees Fahrenheit. Information on each lizard captured will be recorded, including date of capture, location, length, color, age, and tail condition. All lizards will be relocated to suitable off-site habitat the same day as capture. GPS location of released lizards will be recorded. CDFG will be notified about any injured SLL.

All work will be performed by personnel who hold active permits from CDFG in order to conduct the work. At the conclusion of the surveys, documentation of survey results and associated records will be submitted to the City within 10 days of completion, as well as to CDFG.

## SECTION E – CULTURAL RESOURCES

- E-1 Based on the recognized sensitivity for the occurrence of buried sites and artifacts and as mandated by the City of Oxnard archaeological guidelines, Developer shall pay for monitoring by a qualified Archaeologist and Native American Monitor (specifically a qualified Ventureño Chumash descendant). A contract with the Archaeologist shall be presented to the City prior to issuance of grading permits. Monitoring shall be required during all activity involving soil disturbance including grading (cut and fill) or other excavation (e.g., trenching). Should movement of soils during grading for re-compaction activities show no evidence of an archaeological site or artifacts, and with the concurrence of the City of Oxnard Planning and Environmental Services Division and the on-site Native American Monitor, further monitoring at this location by the Archaeologist may no longer be required. In the event that a prehistoric site or historic remains older than 50 years is identified during monitoring, the Archaeologist and/or Native American Monitor shall be empowered to

stop all construction activities in the vicinity of the find. The Archaeologist shall document, identify, and evaluate the potential significance of any find. Such evaluation may require a Phase II site subsurface excavation and evaluation program. Should any remains prove to be significant, avoidance of the resource is the preferred mitigation. If avoidance through project redesign is not feasible, further investigations in the form of a Phase III data recovery program will be implemented to mitigate impacts to the identified resource. The Native American Monitor shall remain on-site throughout any necessary site documentation, evaluation, and mitigation processes.

Contracts shall include weekly reports from the archaeological monitor to the Planning Division summarizing the Archaeologist's activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning and Environmental Services Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final building occupancy for each building.

#### SECTION F – GEOLOGY & SOILS

F-1 The developer shall be required to incorporate a mat foundation into each building according to the recommendations of the *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch* (by Subsurface Designs, Inc. November 28, 2008). A pile foundation shall be prohibited.

#### SECTION H – HYDROLOGY & WATER QUALITY

H-1 Developer shall submit a Storm Water Pollution Prevention Plan (SWPPP) to demonstrate compliance with National Pollutant Discharge Elimination System (NPDES) requirements prior to issuance of building permits.

H-2 Prior to issuance of any permits, the Developer shall submit documentation agreeing to indemnify and hold the City of Oxnard harmless from any liability or damages resulting from the construction of the project.

#### SECTION K – NOISE

K-1 The developer shall be required to incorporate a mat foundation into each building according to the recommendations of the *Preliminary Soils Engineering Investigation, Portions of Lots 90, 91 and 92 of the Patterson Ranch* (by Subsurface Designs, Inc. November 28, 2008) A pile foundation shall be prohibited.

K-2 Prior to issuance of the first Certificate of Occupancy, the applicant shall contribute \$4,875 to a designated City fund for the Deckside Villas that is to be used only for soundwall-related expenses.

K-3 Prior to issuance of the first Certificate of Occupancy, the developer shall record on all properties an Avigation Easement granting the County of Ventura an Avigation Easement for the operation of the Oxnard Airport. The Avigation Easement shall be consistent with the FAA model Avigation Easement.

#### SECTION M – PUBLIC SERVICES

M-1 Prior to issuance of any building permits, the developer shall pay the mandatory impact fees to the school district and present evidence of payment.

M-2 Prior to issuance of any building permits, the developer shall pay the mandatory Quimby Fee.

M-3 Prior to issuance of any building permits, the Developer shall pay the following mandatory development impact fees: *Planned Traffic Circulation System Facilities Fees; Planned Water Facilities Fee; Planned Wastewater Facilities Fee; Planned Drainage Facilities Fee; and Growth Requirement Capital Fee.*

#### SECTION N – RECREATION

N-1 The developer shall be required to pay the mandatory Quimby Fee prior to issuance of any building permits.

#### SECTION O – TRANSPORTATION

- O-1 Developer shall dedicate to the City the right-of-way along Wooley Road and Harbor Blvd. associated with the road widening under this project.
- O-2 Developer shall modify the traffic signal at Wooley Rd/Harbor Blvd. associated with the proposed road widening. The signal modification shall include provision of a traffic signal battery back-up system, new service equipment enclosure and emergency vehicle detection system.
- O-3 Developer shall design and reconstruct the curb ramp at the southeast corner of Wooley Rd and Harbor Blvd. The redesign shall comply with disabled access requirements and address the sand accumulation problem at the ramp.
- O-4 Developer shall place existing overhead utility lines (except electrical lines in excess of 33KV) on and adjacent (as defined in City ordinance) to the project (including Wooley Road, Harbor Boulevard and the canal easterly of the project) underground in accordance with City ordinance. Before issuance of a site improvement permit or recordation of a final map, Developer shall post security satisfactory to the Finance Director guaranteeing utility relocation.
- O-5 Developer shall install a wireless, solar powered, in-pavement crosswalk warning light system on the easterly leg of the Wooley Road/Canal Street intersection along with associated striping and signage, including advanced warning signage and markings, to enhance the safety for pedestrian crossing of Wooley Road. Final design of the crosswalk warning light system to be approved by the City Traffic Engineer.
- O-6 Developer shall design the walls at the entry and exit gate of the Ground Parking Level of Buildings 1 and 4 to provide additional corner visibility similar to that provided on Buildings 2 and 3. Final design shall be approved by the City Traffic Engineer.
- O-7 Developer shall pay the applicable County/City Traffic Impact Fee(s) prior to issuance of a building permit.

#### SECTION P – UTILITIES

P-1 The developer shall pay the applicable Calleguas Capital Construction charges prior to issuance of a building permit.

## REFERENCES

1. California, State of, Air Resources Board, Urban Emissions Model (*URBEMIS 2007, Version 9.2.4*).
2. California, State of, Department of Transportation, Division of Aeronautics (January 2002). *California Airport Land Use Planning Handbook*.
3. California, State of, Department of Transportation (July 2009). *Highway Design Manual*.
4. California, State of, Governor's Office of Planning and Research, Office of Permit Assistance, *Hazardous Waste and Substances Sites - List Pursuant to AB 3750*, current edition.
5. California, State of, Governor's Office of Planning and Research (February 1, 2001). *Guidelines for Implementation of the California Environmental Quality Act*.
6. California, State of, Governor's Office of Planning and Research (1988). *Guidelines for the Preparation and Content of Noise Elements of the General Plan*.
7. City of Oxnard, *The Municipal Code of the City of Oxnard - Chapter 16 (Zoning)*, current edition.
8. City of Oxnard, Planning Division, *Coastal Zoning Ordinance and Zone Maps*, current edition.
9. City of Oxnard, Planning Division, *Coastal Land Use Plan*, current edition.
10. City of Oxnard, Planning Division, *2020 General Plan*, current edition.
11. City of Oxnard, Planning Division, *Zone Maps*, current edition.
12. City of Oxnard, Fire Department (January 1990). *Fire Protection Planning Guide*.
13. Legg, M.R. and Kamerling, M.J. (2003). Large-Scale Basement-Involved Landslides, California Continental Borderland. *Pure and Applied Geophysics (160)*, 2033-2051.
14. Pacific Institute, California Climate Change Center (May 2009). *The Impacts of Sea-Level Rise on the California Coast*.
15. Ventura County Air Pollution Control District, *Air Quality Management Plan*, current edition.
16. Ventura County Air Pollution Control District (October 2003). *Air Quality Assessment Guidelines*.
17. U.S. Federal Emergency Management Agency, National Flood Insurance Program (October 1985). *FIRM Flood Insurance Rate Maps for the City of Oxnard*.
18. City of Oxnard, Public Works Department, *Master Sewer Plan*, current edition.
19. City of Oxnard, Public Works Department, *Master Drainage Plan*, current edition.
20. City of Oxnard, Public Works Department, *Master Water Plan*, current edition.
21. City of Oxnard, Water Division (January 2006). *2005 Urban Water Management Plan*.
22. Tetra Tech, Inc. (November 1990). *Remedial Investigations Report, Volumes I & II. Dunes Subdivision Site, Oxnard, CA*. Pasadena, CA: Author.
23. Envicom Corp. (January 22, 1988). *Final Development Suitability Study, Marlow Dunes Project*. Calabasas, CA: Author.

### Environmental Impact Reports

24. Envicom Corp. (January 22, 1988). *Draft Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation*. Calabasas, CA: Author.
25. Envicom Corp. (March 18, 1988). *Final Environmental Impact Report for Oxnard Shores Agreement for Settlement of Litigation*. Calabasas, CA: Author.
26. CH2M Hill Corp. (May 2004). *Groundwater Recovery Enhancement and Treatment (GREAT) Program, Final Program Environmental Impact Report*. Los Angeles, CA: Author.
27. Rincon Consultants, Inc. (August 2008). *Oxnard Village Specific Plan Project (Wagon Wheel), Final and Draft Environmental Impact Report (SCH #2006101099)*. Ventura, CA: Author.
28. City of Oxnard (1990). *2020 General Plan Final Environmental Impact Report (SCH #881-00528)*.
29. City of Oxnard (February 2009). *Draft 2030 Oxnard General Plan Environmental Impact Report (SCH #2007041024) and Appendices*.

Anacapa Townhomes – MND No. 09-03  
PZ Nos. 08-400-4, 09-300-1, 09-590-1  
January 15, 2010

## APPENDICES

- I. Air Quality
- II. Biological Resources
- III. Hazards & Hazardous Materials

Urbemis 2007 Version 9.2.4

Summary Report for Summer Emissions (Pounds/Day)

File Name: C:\Documents and Settings\cdbrif\My Documents\URBEMIS results\Ox Shores Townhomes 70 DU\URBEMIS results 1.urb924

Project Name: Ox Shores Townhomes 70 DU

Project Location: Ventura County APCD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

CONSTRUCTION EMISSION ESTIMATES

	ROG	NOx	CO	SO2	PM10 Dust	PM10 Exhaust	PM2.5 Dust	PM2.5 Exhaust	PM2.5	CO2
2010 TOTALS (lbs/day unmitigated)	78.24	25.04	13.34	0.01	19.60	1.25	4.10	1.15	5.25	2,349.77
2010 TOTALS (lbs/day mitigated)	70.42	25.04	13.34	0.01	5.24	1.25	1.09	1.15	2.24	2,349.77

AREA SOURCE EMISSION ESTIMATES

TOTALS (lbs/day, unmitigated)	ROG	NOx	CO	SO2	PM10	PM2.5	CO2
	3.93	0.53	0.22	0.00	0.00	0.00	673.93

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

TOTALS (lbs/day, unmitigated)	ROG	NOx	CO	SO2	PM10	PM2.5	CO2
	3.80	3.25	38.62	0.03	5.26	1.00	3,026.90

Page: 2

5/5/2009 12:12:08 PM

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	7.73	3.78	38.84	0.03	5.26	1.00	3,700.83

Table 1  
Sensitive Floral Species Known to Occur in the Vicinity of the Project Site

VASCULAR PLANTS							Occurrence On-site
Scientific Name	Common Name	Flowering Period	Federal	State	CNPS List	Other	Preferred Habitat
<b>ANGIOSPERMS (DICOTYLEDONS)</b>							
<b>Asteraceae</b>	<b>Sunflower Family</b>						
<i>Lasthenia glabrata</i> ssp. <i>coulteri</i>	Coulter's goldfields	Feb.-Jun.	NONE	NONE	1B	NONE	Saline places: coastal salt marsh, playas, vernal pools in foothill/valley grassland.
Comments: Coulter's goldfields were not observed on-site and are not expected to occur due to the lack of suitable habitat.							
<i>Chaenactis glabriuscula</i> var. <i>orcuttiana</i>	Orcutt's pincushion	Jan-Aug	NONE	NONE	1B	NONE	Coastal Dunes, Coastal Bluff Scrub
Comments: Orcutt's pincushion was not observed on-site and is not expected to occur due to the lack of suitable habitat.							
<b>Chenopodiaceae</b>							
<b>Goosefoot Family</b>							
<i>Atriplex coulteri</i>	Coulter's saltbush	Mar-Oct.	NONE	NONE	1B	NONE	Coastal dunes.
Comments: Coulter's saltbush was not observed on-site and are not expected to occur due to the lack of suitable habitat.							
<i>Aphanisma blitoides</i>	Aphanisma	Mar-Jun.	NONE	NONE	1B	NONE	Coastal dunes.
Comments: aphanisma was not observed on-site and is not expected to occur due to the lack of suitable habitat.							
<i>Atriplex pacifica</i>	South Coast saltscale	Mar-Oct.	NONE	NONE	1B	NONE	Coastal dunes, Coastal scrub, Coastal bluff scrub.
Comments: South Coast saltscale was not observed on-site and is not expected to occur due to the lack of suitable habitat.							
<b>Fabaceae</b>							
<b>Legume Family</b>							
<i>Astragalus pycnostachyus</i> var. <i>lanosissimus</i>	Ventura marsh milk-vetch	Jul.-Oct.	FE	SE	1B	NONE	Coastal salt marsh.
Comments: A focused survey was performed for the Ventura marsh milk-vetch in January 2004, November 2008, October 2009, November 2009. Ventura marsh milk-vetch was not observed on-site.							
<b>Nyctaginaceae</b>							
<b>Four O'Clock Family</b>							
<i>Abronia villosa</i> var. <i>aurita</i>	chaparral sand-verbena	Jan-Sep.	NONE	NONE	1B	NONE	Coastal scrub, Desert Dunes.
Comments: chaparral sand-verbena was not observed on-site and is not expected to occur due to the lack of suitable habitat.							
<b>Scrophulariaceae</b>							
<b>Figwort Family</b>							
<i>Cordylanthus maritimus</i> ssp. <i>maritimus</i>	Salt marsh bird's-beak	May-Oct.	FE	SE	1B	NONE	Coastal dunes, salt marsh.

NE = species not expected to occur on-site due to the lack of suitable habitat

Biological Resources Assessment Corporation

Anacap

ynhomes

Table 1 (Continued)

Sensitive Floral Species Known to Occur in the Vicinity of the Project Site

VASCULAR PLANTS							Occurrence	
Scientific Name	Common Name	Flowering Period	Federal	State	CNPS List	Other	Preferred Habitat	On-site
Comments: Salt marsh bird's-beak was not observed on-site and is not expected to occur due to the lack of suitable habitat.								
<b>Key to Species Listing Status Codes</b>								
FE	Federally Listed as Endangered	SE	State Listed as Endangered		SFP	State Fully Protected		
FT	Federally Listed as Threatened	ST	State Listed as Threatened		CSC	California Special Concern Species		
FPE	Federally Proposed as Endangered	SCE	State Candidate for Endangered					
FPT	Federally Proposed as Threatened	SCT	State Candidate for Threatened					
<b>California Native Plant Society (CNPS)</b>								
List 1A:	Presumed extinct in California.							
List 1B:	Rare, threatened, or endangered throughout their range.							
List 2:	Rare, threatened, or endangered in California, but more common in other states.							
List 3:	Plant species for which additional information is needed before rarity can be determined.							
List 4:	Species of limited distribution in California (i.e., naturally rare in the wild), but whose existence does not appear to be susceptible to threat.							

NE = species not expected to occur on-site due to the lack of suitable habitat

Biological Resources Assessment

PCR Services Corporation

Anacapa Townhomes

Table 2  
Sensitive Wildlife Species Known to Occur in the Vicinity of the Project Site

INVERTEBRATES						
Scientific Name	Common Name	Federal	State	Preferred Habitat	Distribution	Occurrence On-site
INSECTA						
Lepidoptera	Butterflies and Moths					
Nymphalidae	Brush-footed butterflies					
<i>Danaus plexippus</i>	Monarch butterfly	NONE	NONE	Roosts in wind-protected tree groves with nectar sources and water nearby. Most often found roosting in <i>Eucalyptus</i> , Monterey Pine, and cypress.	Winter roost sites occur along the California coast from Mendocino south to Baja California.	OB
Comments: Individual Monarch butterflies were observed on-site during surveys in January 2004; however no potential roosting sites are present.						
Hesperiidae	Skipper Family					
<i>Panoquina errans</i>	wandering skipper	NONE	NONE	Salt marshes.	Coastal southern California, coastal Baja California, and mainland western Mexico	NE
Comments: The species was not observed on-site and is not expected due to the absence of suitable habitat.						
VERTEBRATES						
Scientific Name	Common Name	Federal	State	Preferred Habitat	Distribution	Occurrence On-site
REPTILES						
Anniellidae	American Legless Lizard Family					
<i>Anniella pulchra pulchra</i>	Silvery legless lizard	NONE	CSC	Found in sandy or loose organic soil or where there is plenty of leaf litter.	Coast Ranges from the vicinity of Antioch, Contra Costa County south to the Mexican border.	NE
Comments: The species was not observed on-site and is not expected due to the highly degraded condition of the habitat and extremely low cover of native vegetation						

OB = Observed; EX = Species expected to occur on-site; NE = Species not expected to occur on-site; P = Species has potential to occur on-site

Biological Resources Assessment Corporation

Anacay

whomes

Table 2 (Continued)

Sensitive Wildlife Species Known to Occur in the Vicinity of the Project Site

VERTEBRATES						
Scientific Name	Common Name	Federa I	State	Preferred Habitat	Distribution	Occurrence On-site
<i>Iguanidae</i> <i>Phrynosoma coronatum</i>	Iguanid Lizards Coast horned lizard	NONE	CSC	Valley-foothill hardwood, conifer, and riparian habitats, pine-cypress, juniper and annual grassland habitats below 6,000 feet, open country, especially sandy areas, washes, flood plains, and windblown deposits.	Coastal ranges from south Ventura, Los Angeles, San Bernardino counties, Orange, western Riverside and western San Diego counties.	NE
Comments: The species was not observed on-site and is not expected due to the highly degraded condition of the habitat and extremely low cover of native vegetation						
BIRDS						
<i>Alaudidae</i> <i>Eremophila alpestris actia</i>	Lark Family California horned lark	NONE	NONE	Grasslands along the coast and deserts near sea level to alpine dwarf-shrub habitat above treeline.	Throughout California.	NE
Comments: The species was not observed on-site and is not expected due to the highly degraded condition of the habitat and extremely low cover of native vegetation						
<i>Charadriidae</i> <i>Charadrius alexandrinus nivosus</i>	Lapwings and Plovers Western snowy plover	FT	CSC	Beaches and sandy flats.	Southern California.	NE
Comments: The species was not observed on-site and is not expected due to the highly degraded condition of the habitat and extremely low cover of native vegetation						
<i>Emberizidae</i> <i>Passerculus sandwichensis beldingi</i>	Emberizids Belding's savannah sparrow	NONE	SE	Salt marshes, dunes, and shores.	From Santa Barbara south through San Diego County.	NE
<i>Hirundinidae</i> <i>Riparia riparia</i>	Swallow Family Bank swallow	NONE	ST	Riparian and other lowland habitats west of the desert.	Uncommon spring and fall migrant on the coast.	NE
Comments: The species was not observed on-site and is not expected due to the absence of suitable habitat.						

OB = Observed; EX = Species expected to occur on-site; NE = Species not expected to occur on-site; P = Species has potential to occur on-site

Biological Resources Assessment  
PCR Services Corporation

Anacapa Townhomes

Table 2 (Continued)  
Sensitive Wildlife Species Known to Occur in the Vicinity of the Project Site

VERTEBRATES						
Scientific Name	Common Name	Federal	State	Preferred Habitat	Distribution	Occurrence On-site
Laridae	Gulls, Terns, Skuas, and Skimmers					
<i>Sterna antillarum browni</i>	California least tern	FE	SE	Sea beaches, bays, large rivers, bars.	San Francisco Bay to Baja CA.	NE
Comments: The species was not observed on-site and is not expected due to the absence of suitable habitat.						
Strigidae	True Owl Family					
<i>Athene cucularia</i>	Burrowing owl	NONE	CSC	Dry grasslands, desert habitats, open pinyon-juniper and ponderosa pine woodlands below 5,300 feet. Prefers berms, ditches, and grasslands adjacent to rivers, agricultural, and scrub areas.	Year-round resident of lowlands of southern CA.	NE
Comments: Phase I and II surveys for Burrowing owl were conducted in November 2009; no signs of the species being present were observed.						
Vireonidae	Vireos					
<i>Vireo bellii pusillus</i>	least Bell's vireo	FE	SE	Willows and other low, dense valley foothill riparian habitat and lower portions of canyons.	From Santa Barbara south through San Diego County.	NE
Comments: The species was not observed on-site and is not expected due to the absence of suitable habitat.						
<b>MAMMALS</b>						
Muridae	True mice and rats					
<i>Microtus californicus stephensi</i>	South coast marsh vole	NONE	CSC	Coastal Sand Dunes.	Throughout California.	NE
Comments: The species was not observed on-site and is not expected due to the highly degraded condition of the habitat and extremely low cover of native vegetation						
Soricidae	Shrew Family					
<i>Sorex ornatus salicornicus</i>	Southern California saltmarsh shrew	NONE	CSC	Salt Marsh.	San Francisco Bay to Baja CA.	NE
Comments: The species was not observed on-site and is not expected due to the absence of suitable habitat.						
<b>Key to Species Listing Status Codes</b>						

OB = Observed; EX = Species expected to occur on-site; NE = Species not expected to occur on-site; P = Species has potential to occur on-site  
Biologic Resources Assessment  
PCR Services Corporation

Table 2 (Continued)

Sensitive Wildlife Species Known to Occur in the Vicinity of the Project Site

VERTEBRATES						
Scientific Name	Common Name	Federal I	State	Preferred Habitat	Distribution	Occurrence On-site
FE	<i>Federally Listed as Endangered</i>	SE	State Listed as Endangered	GSC	<i>California Special Concern Species</i>	
FT	<i>Federally Listed as Threatened</i>	ST	State Listed as Threatened			
FPE	<i>Federally Proposed as Endangered</i>	SCE	State Candidate for Endangered			
FPT	<i>Federally Proposed as Threatened</i>	SCT	State Candidate for Threatened			
FPD	<i>Federally Proposed for Delisting</i>	SFP	State Fully Protected			
FC	<i>Federal Candidate Species</i>	SR	State Rare			

**TABLE 1**  
**Soil Analytical Results**  
 reported in milligrams per kilogram (mg/kg or parts per million, ppm), unless otherwise noted  
 Wooley Road and Harbor Boulevard, Oxnard, California

Location	Depth	Metals (mg/kg)						VOCs (ug/kg)			Total Petroleum Hydrocarbons (TPH) mg/kg			
		Arsenic	Barium*	Chromium	Cobalt	Copper	Lead	Nickel	Vanadium	Zinc	Toluene	GRO	DRO	ORO
SB-6	5	0.83	26.6	3.14	0.95	1.22	0.93	2.38	5.66	5.94	ND	ND	ND	ND
	10	1.36	11.8	4.01	0.80	1.32	0.69	2.03	4.20	5.08	ND	ND	ND	ND
SB-7	5	2.54	4,790 [ND <0.1 mg/l]	11.40	3.83	8.89	12.9	12.3	21.9	39.5	2.74	ND	139	161
	10	1.12	29.1	5.64	1.42	1.79	1.84	4.30	8.29	8.93	ND	ND	ND	ND
SB-8	5	1.14	34.7	7.67	1.42	2.21	1.84	4.30	11.6	11.0	ND	ND	ND	ND
	10	1.27	12.0	5.07	1.32	1.32	1.84	3.85	5.12	7.27	ND	ND	ND	ND
PRG (residential)		0.39**	15,000	200	23	3,100	150	1,600	550	23,000	5,000,000***	NA	NA	NA
Regulatory Action Level		---	10,000 [100 mg/l]*									100 - 1,000	1,000 - 5,000	1,000 - 10,000

TPH-GRO: Total Petroleum Hydrocarbons - volatile range (C4 to C10, carbon chain length range)  
 TPH-DRO: Total Petroleum Hydrocarbons - diesel range (C10 to C28)  
 TPH-ORO: Total Petroleum Hydrocarbons - oil range (C28+)  
 VOCs: Volatile Organic Compounds  
 ND - Not Detected (below minimum laboratory detection limit)  
 [ND] - Not Detectable for Toxicity Characteristic Leaching Procedure (TCLP) analysis

NOTE: All other metals not listed but analyzed for were reported below minimum detection limits.

PRG: Preliminary Remediation Goal established by EPA Region 9 (California), health based risk soil screening levels. (residential) most conservative level established under residential land use scenario (long term exposure scenario)

Regulatory Action Level: Ranges listed for TPH are standard threshold action levels established by various regulatory agencies in Southern California. Remedial thresholds for soil for TPH are general determined by site specific conditions.

\*Barium: Regulatory action levels listed for Barium are waste characterization thresholds for hazardous waste. Total Threshold Limit Concentration (TTLC, 10,000 mg/kg for soil for barium). Soluble Threshold Limit Concentration (STLC, 100 mg/l soluble fraction concentrations for barium). Barium concentration below TTLC but 10 times above STLC; therefore, TCLP analysis was conducted to determine if leachable barium concentrations exceeded STLC (result was non-detect).

\*\*PRG for arsenic in soil (residential scenario) is below natural background concentrations for arsenic in soil. DTSC has established a background concentration for arsenic in California soils at 12 mg/kg.

\*\*PRG for toluene in soil (residential scenario) 5,000 mg/kg, converted to ug/kg (parts per billion) to be consistent with units for lab result.

**TABLE 2**  
**Groundwater Analytical Results**  
 (reported in micrograms per liter [ug/L or parts per billion, ppb])  
**West 5th Street and South Harbor Boulevard**  
**Oxnard, California**

Location	VOCs		Total Petroleum Hydrocarbons (TPH)		
	Acetone	MEK	GRO	DRO	ORO
SB-1	11.4	ND	ND	ND	ND
SB-2	22.6	7.0	ND	ND	ND
SB-3	44.2	7.6	ND	NA	NA
SB-4	ND	ND	ND	ND	ND
SB-5	18.2	ND	ND	ND	ND
SB-6	ND	ND	ND	ND	ND

GRO: Total Petroleum Hydrocarbons - volatile range (C6 to C10)

DRO: Total Petroleum Hydrocarbons - diesel range (C10 to C28)

ORO: Total Petroleum Hydrocarbons - oil range (C28+)

ND - Not Detected (below minimum detection limit)

NA-Not Analyzed

MEK - Methyl ethyl ketone

NOTE:

All other VOCs analyzed for were reported as ND. A complete list of analytes can be found in the laboratory report in Attachment C.



# United States Department of the Interior

FISH AND WILDLIFE SERVICE  
Ventura Fish and Wildlife Office  
2493 Portola Road, Suite B  
Ventura, California 93003



IN REPLY REFER TO:  
2009-FA-0150

RECEIVED

October 6, 2009

OCT 07 2009

PLANNING DIVISION  
CITY OF OXNARD

Brian Foote, Associate Planner  
City of Oxnard Planning Division  
214 South C Street  
Oxnard, California 93030

Subject: Comments on the Mitigated Negative Declaration for the Anacapa Townhomes,  
Oxnard, Ventura County, California

Dear Mr. Foote:

We are writing in response to the City of Oxnard's (City) notice of intent to adopt a mitigated negative declaration (MND) for the construction of the Anacapa Townhomes project pursuant to the requirements of the California Environmental Quality Act (CEQA). The proposed project includes the development of 70 condominium units, parking garages, a pool, recreation building, and landscaping on a 4-acre vacant property at the northeast corner of Harbor Boulevard and Wooley Road in Oxnard, Ventura County, California. The City has made the determination that there is no substantial evidence that the proposed project may have a significant effect on the environment, and has prepared the subject MND. Despite the City's conclusions outlined in the MND, we are concerned about the potential effects of the project on the federally endangered *Astragalus pycnostachyus* var. *lanosissimus* (Ventura marsh milk-vetch).

The U.S. Fish and Wildlife Service's (Service) responsibilities include administering the Endangered Species Act of 1973, as amended (Act), including sections 7, 9, and 10. Section 9 of the Act and its implementing regulations prohibit the taking of any federally listed endangered or threatened species. Section 3(18) of the Act defines take to mean to harass, harm, pursue, hunt, shoot, wound, kill, trap, capture, or collect, or to attempt to engage in any such conduct. Service regulations (50 CFR 17.3) define harm to include significant habitat modification or degradation which actually kills or injures wildlife by significantly impairing essential behavioral patterns, including breeding, feeding or sheltering. Harassment is defined by the Service as an intentional or negligent action that creates the likelihood of injury to wildlife by annoying it to such an extent as to significantly disrupt normal behavioral patterns which include, but are not limited to, breeding, feeding, or sheltering. The Act provides for civil and criminal penalties for the unlawful taking of listed species. Exemptions to the prohibitions against take may be obtained through coordination with the Service in two ways. If a project is to be funded, authorized, or carried out by a Federal agency and may affect a listed species, the Federal agency must consult with the Service, pursuant to section 7(a)(2) of the Act. If a proposed project does not involve a Federal agency but may result in the take of a listed animal species, the project proponent should apply to the Service for an incidental take permit, pursuant to section 10(a)(1)(B) of the Act.

As it is not our primary responsibility to comment on documents prepared pursuant to CEQA, our comments on the MND do not constitute a full review of potential project impacts. We are providing our comments based upon a review of the sections addressing biological resources and our concerns for listed species within our jurisdiction related to our mandates under the Act.

The MND references a 2005 Biological Resources Assessment prepared by PRC Services Corporation to support the conclusion that the project would have a less than significant impact on any special status species. The 2005 Biological Resources Assessment indicates that a field survey was conducted on January 22, 2004, during which the habitat at the site was characterized and the potential to support sensitive species was evaluated. Table 1 (reproduced in Appendix 2 of the MND) indicates that *Astragalus pycnostachyus* var. *lanosissimus* is not expected to occur on-site due to the lack of suitable habitat. The study indicates that no sensitive plant species were observed on-site during the site visit, but it does not appear that targeted surveys were conducted to verify the presence or absence of sensitive plant species, but rather the absence of *Astragalus pycnostachyus* var. *lanosissimus* was presumed based on the observation that coastal salt marsh does not occur at the project site.

*Astragalus pycnostachyus* var. *lanosissimus* was presumed to be extinct until its rediscovery in 1997 on a parcel approximately 0.6-mile north of the project site. The rediscovery site was used for the disposal of oil wastes between 1955 and 1981, and was capped with locally imported fill that likely contained *Astragalus pycnostachyus* var. *lanosissimus* seeds (Soza et al. 2003). Although this site does not support the coastal salt marsh habitat that the species was thought to be associated with, the oil waste likely produced an artificially perched shallow aquifer that allowed *Astragalus pycnostachyus* var. *lanosissimus* to germinate and persist at the site. Due to the proximity of the project site to the only known native population of this species, and because the habitat at the proposed project site is similar to habitat at the rediscovery site, the presence of *Astragalus pycnostachyus* var. *lanosissimus* cannot be ruled out due to lack of coastal salt marsh habitat. Targeted surveys are recommended to identify the presence or absence of this species.

We appreciate the opportunity to provide comments on the Anacapa Townhomes project and look forward to working with the City in the future to address and minimize the potential impacts to federally listed species within your jurisdiction. If you have any questions regarding these comments, please contact Jenny Marek of our staff at (805) 644-1766, extension 325.

Sincerely,



Chris Dellith  
Senior Biologist

cc: Mary Meyer, California Department of Fish and Game

## REFERENCES CITED

PRC Services Corporation. 2005. Biological Resources Assessment, Oxnard Shores, Oxnard California. Irvine, California.

Soza, V., M. Wall, D. Hannon. 2003. Experimental Introduction of the Ventura Marsh Milk-vetch (*Astragalys pchnostachyus* var. *lanosissimus*) at Carpinteria Salt Marsh Reserve and McGrath State Beach. Rancho Santa Ana Botanic Garden, Claremont, California.



California Natural Resources Agency  
 DEPARTMENT OF FISH AND GAME  
 South Coast Region  
 4949 Viewridge Avenue  
 San Diego, CA 92123  
 (858) 467-4201  
 http://www.dfg.ca.gov

ARNOLD SCHWARZENEGGER, Governor

JOHN MCCAMMAN, Director

SCH# 2010011069



February 18, 2010

Brian Foote, Associate Planner  
 City of Oxnard, Planning Division  
 214 South C Street  
 Oxnard, CA 93030  
 Fax No.: (805) 385-7417

Clear  
 2-23-10  
 e

RECEIVED  
 FEB 22 2010  
 STATE CLEARING HOUSE

**Subject: Draft Mitigated Negative Declaration for the Anacapa Townhomes Project**  
 SCH # ~~2010011069~~

Dear Mr. Foote:

The Department of Fish and Game (Department), has reviewed the above Draft Mitigated Negative Declaration (DMND) for impacts to biological resources. The project applicant proposes to construct 70 condominium units and related site improvements on a four acre site located at the northeast corner of Harbor Boulevard and Wooley Road, in the City of Oxnard. Habitat types proposed for removal include four acres of disturbed coastal scrub/ruderal.

Wildlife with the potential to be impacted by the project include the Federal and State Endangered least Bell's vireo (*Vireo bellii pusillus*), and the State Special Concern Species silvery legless lizard (*Anniella pulchra*) and burrowing owl (*Athene cunicularia*). Measures proposed to mitigate impacts include avoidance of the bird nesting season during grubbing and grading and conducting bird nest surveys, if grubbing and grading during the bird nesting season cannot be avoided.

The following statements and comments have been prepared pursuant to the Department's authority as Trustee Agency with jurisdiction over natural resources affected by the project (CEQA Guidelines §15386(a)). As trustee for the State's fish and wildlife resources, the Department has jurisdiction over the conservation, protection, and management of fish, wildlife, native plants, and habitat necessary for biologically sustainable populations of those species.

**California Wildlife Action Plan**

The California Wildlife Action Plan, a Department guidance document, identified the following stressors affecting wildlife and habitats within the project area: 1) growth and development; 2) water management conflicts and degradation of aquatic ecosystems; 3) invasive species; 4) altered fire regimes; and 5) recreational pressures. The Department looks forward to working with the City of Oxnard to minimize impacts to fish and wildlife resources with a focus on these stressors.

**Impacts to Sensitive Biological Resources**

Rare Plants - The DMND states that no rare or listed plants or animals have been identified on the site, and concludes that impacts to special status wildlife will be less than significant. This conclusion was based in part on plant surveys conducted in winter 2004 and fall 2008 and 2009. The Department does not believe surveys conducted for rare plants were adequate.

*Conserving California's Wildlife Since 1870*

The Department recommends surveys be conducted by qualified biologists for rare plants according to the "Department Guidelines for Assessing the Effects of Proposed Projects on Rare, Threatened, and Endangered Plants and Natural Communities" (attached). The Guidelines give clear instructions on how surveys for rare plants should be conducted. One of the instructions is to conduct surveys at the proper time of year when rare species are both evident and identifiable. Usually, this is when the plants are flowering. Surveys conducted in winter and fall are problematic for several species of rare plants that can only be reliably identified when flowering in the spring.

The Department therefore recommends spring surveys be conducted for those spring-flowering rare plants with the potential for occurring on the project site. Discovery of a rare plant may lead to recirculation of the DMND (CEQA Guidelines §15073.5(b)(1)). If additional surveys are not conducted then the project applicant should assume presence of rare and listed plants and implement complete avoidance of all areas that are, or have the potential to be, suitable habitat for those species.

Least Bell's vireo (*Vireo bellii pusillus*) – The DMND proposes to conduct pre-construction nesting bird surveys if grubbing and grading must occur during the nesting season. As reported in the Biological Resources Assessment (BRA) prepared for the project in December, 2009, the least Bell's vireo (*Vireo bellii pusillus*) was recently detected in southern willow scrub habitat on a parcel of land at the southeast corner of Harbor Boulevard and Fifth Street (Merak 2009; Meyer 2009). This parcel is in close proximity to the proposed project site (two-tenths of a mile north). Furthermore, southern willow scrub habitat suitable for LBV nesting exists across Wooley Road from the proposed project site, within 100 feet.

The least Bell's vireo (LBV) is susceptible to noise and vibration disturbance leading to nest abandonment up to 500 feet from the source of the noise (Hein, 1997). The project as proposed would occur less than 500 feet from suitable LBV nesting habitat, and has potential to generate noise leading to nest abandonment (including noise associated with construction of the new buildings). The proposed project, therefore, has potential for take of a listed species if grubbing, grading, or construction occurs during the recognized LBV nesting season April 10 to July 31. Proposed pre-construction nesting bird surveys must include protocol-level surveys for LBV, according to current U.S. Fish and Wildlife Service Guidelines for 2009.

A California Endangered Species Act (CESA) incidental take permit is required, if the project has the potential to result in "take" of species of plants or animals listed under CESA, either during construction or over the life of the project, pursuant to Fish and Game Code Section 2050 et seq. CESA permits are issued to conserve, protect, enhance, and restore State-listed threatened or endangered species and their habitats. The project as proposed, if conducted during the LBV nesting season, has potential for take of a State listed endangered animal, and therefore a CESA permit would be required. The procedure for obtaining a CESA permit may be found at the Department's website at <http://www.dfg.ca.gov/hcpb/ceqacesa/cesa/cesa.shtml>.

Silvery legless lizard (*Anniella pulchra*) – The BRA concluded that preferred habitat for silvery legless lizard (SLL) occurs on less than five percent of the proposed project site, and together with the disturbed and degraded condition of the remainder means that "...any population of legless lizard on site is neither significant nor sustainable." Formal surveys for SLL were not conducted, and the existence of SLL on-site was not ruled out.

The definition by the Department of a Species of Special Concern includes species with "...naturally small populations exhibiting high susceptibility to risk from any factor(s) that if

realized, could lead to declines that would qualify it for State threatened or endangered status." This definition agrees with the definition in CEQA for a rare species; "Although not presently threatened with extinction, the species is existing in such small numbers throughout all or a significant portion of its range that it may become endangered if its environment worsens" (CEQA Guidelines §15380(b)(2)(A)).

The project as proposed, has potential for take of a California Species of Special Concern (and a rare species under CEQA), therefore pre-construction surveys for SLL should be conducted according to the attached protocol.

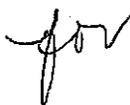
### Mitigation Measures

If nesting bird surveys are required and result in detection of nesting LBV within 500 feet of proposed construction activities, the Department and the U.S. Fish and Wildlife Service should be notified. Mitigation should include installation of a sound wall to prevent noise exceeding 60 DBa from reaching the active nest(s). The Department also recommends the project include cowbird trapping as an additional LBV mitigation measure to offset the temporary loss of habitat due to noise. The Department recommends a minimum of two cowbird traps, with the traps becoming operational beginning April 1 thru late fall, no later than November 30 of each year for five years.

Silt fencing should be installed along the project site eastern boundary to prevent egress of lizards onto the project site during construction. The fencing should be keyed into the ground three inches. If pre-construction surveys result in detection of SLL, the Department should be notified. SLL should be re-located to nearby off-site suitable habitat. Mitigation measures, such as off-site habitat enhancement at a 2:1 enhancement to impact ratio should be incorporated into the proposed project, if the species is found.

Thank you for this opportunity to provide comment. Questions regarding this letter and further coordination on these issues should be directed to Mr. Martin Potter, Environmental Scientist, at (805) 640-3677.

Sincerely,



Edmund Pert  
Regional Manager  
South Coast Region

### Attachments (2)

Hein, E. 1997. The Impact of Noise on Federally-Listed Species  
Subsequent Mandates for Mitigation. U.S. Fish and Wildlife Service.

cc: Ms. Helen Birss, Los Alamitos  
Ms. Betty Courtney, Newhall  
Mr. Dan Blankenship, Santa Clarita  
Mr. Martin Potter, Ojai  
Ms. Mary Meyer, Ojai  
Mr. Roger Root, U.S. Fish and Wildlife Service, Ventura  
Mr. Scott Morgan, State Clearinghouse, Sacramento

RECEIVED

OCT 07 2009

RESOURCE MANAGEMENT AGENCY

county of ventura

PLANNING DIVISION  
CITY OF OXNARD

Planning Division

Kimberly L. Rodriguez  
Dir.

October 7, 2009

City of Oxnard  
Planning Division  
214 South C Street  
Oxnard, CA 93030  
Attn.: Brian Foote

E-mail: [Brian.Foote@ci.oxnard.ca.us](mailto:Brian.Foote@ci.oxnard.ca.us)

Subject: Comments on MND No. 09-03; Anacapa Townhomes

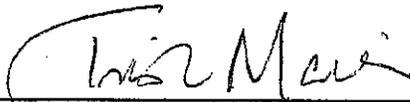
Dear Mr. Foote:

Thank you for the opportunity to review and comment on the subject document. Attached are the comments that we have received resulting from intra-county review of the subject document. Additional comments may have been sent directly to you by other County agencies.

Your proposed responses to these comments should be sent directly to the commenter, with a copy to Laura Hocking, Ventura County Planning Division, L#1740, 800 S. Victoria Avenue, Ventura, CA 93009.

If you have any questions regarding any of the comments, please contact the appropriate respondent. Overall questions may be directed to Laura Hocking at (805) 654-2443.

Sincerely,



Tricia Mater, Manager  
Program Administration Section

Attachment

County RMA Reference Number 09-046

800 South Victoria Avenue, L# 1740, Ventura, CA 93009 (805) 654-2481 Fax (805) 654-2509



Printed on Recycled Paper



ATTACHMENT 8  
PAGE 162 OF 185

RECEIVED

OCT 07 2009

PLANNING DIVISION  
CITY OF OXNARD



**PUBLIC WORKS AGENCY  
TRANSPORTATION DEPARTMENT  
Traffic, Advance Planning & Permits Division  
MEMORANDUM**

**DATE:** September 30, 2009

**TO:** RMA – Planning Division  
Attention: Laura Hocking

**FROM:** Behnam Emami, Engineering Manager II

**SUBJECT: REVIEW OF DOCUMENT 09-046**

Notice of Intent (NOI) to adopt a Mitigated Negative Declaration (MND), a MND, and an Initial Study (IS) for the Anacapa Townhomes Condominium Development. 4781 and 5001 W. Wooley Road, in the Oxnard Dunes neighborhood.

Lead Agency: **City of Oxnard**  
APN 196-0-033-29

Pursuant to your request, the Public Works Agency -- Transportation Department has reviewed the NOI to adopt a MND, a MND, and an IS for the Anacapa Townhomes Condominium Development. The project consists of Planning & Zoning Permit Nos. 08-400-04 (Coastal Development Permit), 09-300-01 (Tentative Tract Map), and 09-590-01 (Coastal Zone Variance). A request to construct the "Anacapa Townhomes" on a four-acre vacant property. A tentative subdivision map would create five parcels and subdivide for 70 air-rights condominium units; a Coastal Development Permit to construct five multifamily three-story structures with a total of 70 condominium dwelling units, parking garages, pool and recreation building, and landscaping; and a Coastal Zone Variance to modify the development standards for front, side, and rear yard setbacks to permit architectural projections. The project is located 4781 and 5001 W. Wooley Road, at the northeast corner of Harbor Boulevard and Wooley Road, in the Oxnard Dunes neighborhood.

We offer the following comments:

1. We generally concur with the comments in the NOI to Adopt a MND, MND, and IS for those areas under the purview of the Transportation Department. The IS indicates that this project would generate additional traffic. Page 67, 2<sup>nd</sup> paragraph (also in Table 7, Project Trip Generation) provides that a total of 407 Average Daily Trips, 31 trips in the morning peak hours, and 36 trips will occur in the evening peak hour.
2. Page 68, 2<sup>nd</sup> paragraph of the IS provides that the project would be consistent with the Ventura County General Plan by complying with the terms of the Reciprocal Traffic Mitigation Agreement adopted in February 2, 1993. By paying the mandatory County/City Traffic Impact Mitigation Fee (TIMF), the developer will mitigate the project's impacts on local roads and intersections. With the payment of the fee, any potential impacts will be less than significant. Furthermore, Page 71 of the IS, Mitigation Measure O-7 provides that the Developer shall pay the applicable County/City TIMF prior to issuance of a building permit.

The cumulative impacts of the development of this project, when considered with the cumulative impact of all other approved (or anticipated) development projects in the County, will be potentially significant. To address the cumulative adverse impacts of traffic on the County Regional Road Network, the appropriate TIMF should be paid to the County when development occurs. Based on the information provided in the IS, and the reciprocal agreement between the City of Oxnard and the County of Ventura, the fee due to the County is:

$$70 \text{ Condominium Units} \times \$256.96/\text{Other Housing} = \underline{\$17,987.20}$$

The above estimated fee may be subject to adjustment at the time of deposit, due to provisions in the TIMF Ordinance allowing the fee to be adjusted for inflation based on the Engineering News Record Construction Cost Index. The above is an estimate only based on information provided in the MND.

Our review is limited to the impacts this project may have on the County's Regional Road Network.

Please contact me at 654-2087 if you have questions.

F:\transport\LanDev\Non\_County\09-046.doc

RECEIVED

JAN 12 2010

PLANNING DIVISION  
CITY OF OXNARD



**VENTURA COUNTY**  
**WATERSHED PROTECTION DISTRICT**  
**PLANNING AND REGULATORY DIVISION**  
800 South Victoria Avenue, Ventura, California 93009

**DATE:** January 12, 2010

**TO:** Laura Hocking, Case Planner and  
Paul Wendt, city of Oxnard

**FROM:** Robin Jester, Acting Permit Manager  
Planning and Regulatory

**SUBJECT:** Revision to comments dated October 1, 2009  
RMA 09-046 – MND 09-03 – Coastal Development Permit  
08-0400-04 Tentative Tract 09-300-1  
Coastal Variance 09- 590-01  
APN 196-0-033-295

**Project Description:**

A request to construct the "Anacapa Townhomes" on a 4-acre vacant property located at the northeast corner of Harbor Boulevard and Wooley Road, in the Oxnard Dunes neighborhood. A tentative subdivision map would create five parcels and subdivide for 70 air rights condominium units; a Coastal Development Permit to construct five multifamily three story structures with a total of 70 condominium dwelling units, parking garages, pool and recreation building, and landscaping, and a Coastal Zone Variance to modify the development standards for front, side, and rear yard setbacks to permit architectural projects.

**Comments:**

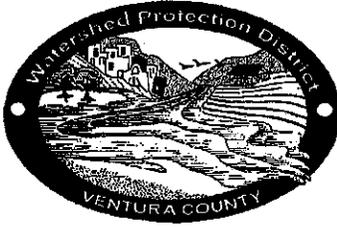
The comments provided by the Watershed Protection District in the October 1, 2009 memorandum have been reconsidered. The following statement provides our amended entire comments for the above project.

The Watershed Protection District has reviewed the project above. There are no red line channels in close proximity to the proposed project; therefore the District has no comments or concerns.

**End of Text**

RECEIVED

OCT 07 2009



PLANNING DIVISION  
CITY OF OXNARD

**Ventura County**  
**Watershed Protection District**  
**Water & Environmental Resources Division**  
**Water Quality Section**

**MEMORANDUM**

---

DATE: September 22, 2009

TO: Laura Hocking, RMA- Planning Division

FROM: Paul Tantet

**SUBJECT: RMA 09-046 – Mitigated Negative Declaration No 09-03, City of Oxnard**

I have reviewed the Mitigated Negative Declaration No. 09-03 and accompanying hydrology report and concur with document's finding of no significant effect on the environment. The hydrology report's Detention Concept and Stormwater Treatment section includes sufficient surface water quality protection data to warrant this finding. Furthermore, the Watershed Protection District will not require further review of the proposed surface water quality treatment systems.

RECEIVED

OCT 07 2009

PLANNING DIVISION  
CITY OF OXNARD



**VENTURA COUNTY**  
**WATERSHED PROTECTION DISTRICT**  
**PLANNING AND REGULATORY DIVISION**  
800 South Victoria Avenue, Ventura, California 93009

**DATE:** October 1, 2009

**TO:** Laura Hocking, Case Planner

**FROM:** Robin Jester, Acting Permit Manager  
Planning and Regulatory

**SUBJECT:** RMA 09-046 – MND 09-03 – Coastal Development Permit  
08-0400-04 Tentative Tract 09-300-1  
Coastal Variance 09- 590-01  
APN 196-0-033-295

**Project Description:**

A request to construct the "Anacapa Townhomes" on a 4-acre vacant property located at the northeast corner of Harbor Boulevard and Wooley Road, in the Oxnard Dunes neighborhood. A tentative subdivision map would create five parcels and subdivide for 70 air rights condominium units; a Coastal Development Permit to construct five multifamily three story structures with a total of 70 condominium dwelling units, parking garages, pool and recreation building, and landscaping, and a Coastal Zone Variance to modify the development standards for front, side, and rear yard setbacks to permit architectural projects.

**Comments:**

The Watershed Protection District has reviewed the project above. There are no red line channels in close proximity to the proposed project. However, the channels located within the City of Oxnard are considered impacted facilities and are deficient for capacities based on the current level of development. Therefore there can be no increase in peak runoff reaching District facilities over all frequencies.

**End of Text**

RECEIVED

OCT 07 2009

PLANNING DIVISION  
CITY OF OXNARD

**VENTURA COUNTY  
AIR POLLUTION CONTROL DISTRICT**

**Memorandum**

TO: Laura Hocking/Dawnyelle Addison, Planning      DATE: October 5, 2009

FROM: Alicia Stratton

SUBJECT: Request for Review of MND No. 09-03, Mike Marlow, Oxnard Shores  
Co. LLC, City of Oxnard (Reference No. 09-046)

Air Pollution Control District staff has reviewed the subject project, which is a request to construct the "Anacapa Townhomes" on a four-acre property, located at the northeast corner of Harbor Blvd. and Wooley Road in the Oxnard Dunes neighborhood in the City of Oxnard. A tentative subdivision map would create five parcels and subdivide for 70 air-rights condominium units, a coastal development permit to construct five multifamily three-story structures with a total of 70 condominium units, parking garages, pool and recreation building, and landscaping; and a coastal zone variance to modify the development standards for front, side, and rear and setbacks to permit architectural projections.

Section C of the MND addresses air quality, greenhouse gas, and project AQMP consistency issues. We concur with the findings of this discussion that significant air quality impacts will not result from the project. Table 5, *Area Source & Operational Emissions*, indicates that total mitigated emissions from the project would be 7.6 lbs/day ROC and 3.7 lbs/day NOx. These are below the 25 lbs/day thresholds of significance for Ventura County.

Short-term air quality impacts will be minimized through implementation of Mitigation Measures C1 through C-6. In addition to these, we recommend the following project condition be applied to site preparation and project construction activities:

All project construction and site preparation operations shall be conducted in compliance with all applicable VCAPCD Rules and Regulations with emphasis on Rule 50 (Opacity), Rule 51 (Nuisance), and Rule 55 (Fugitive Dust), as well as Rule 10, (Permits Required).

If you have any questions, please call me at (805) 645-1426.

RECEIVED

**From:** "Todd McNamee" <Todd.McNamee@ventura.org>  
**To:** Brian.Foote@ci.oxnard.ca.us  
**Date:** 2/2/2010 3:20:34 PM  
**Subject:** Comment on MND No. 09-03

FEB 02 2010  
PLANNING DIVISION  
CITY OF OXNARD

Dear Mr. Foote,

Thank you for the opportunity to review the Notice of Intent to Adopt a Mitigated Negative Declaration (MND No. 09-03) for the Anacapa Townhomes project.

Please accept the following as comments made on behalf of the County of Ventura Department of Airports.

Page 42, Section G - Hazards and Hazardous Materials. It is currently stated "No Impact" under item 5 as it relates to the project being within an airport land use plan or within two miles of a public use airport.

The site DOES fall within the Oxnard Airport Sphere of Influence and is therefore also covered by the Ventura County Comprehensive Land Use Plan (ACLUP). Although the site is not under a direct flight pattern for the airport, it is subject to overflight of aircraft arriving and departing the Oxnard airport. As such I feel it is appropriate to show the impact as "less than significant" rather than "no impact"

Page 45, section 5&6. As mentioned above, the project site does lie within the ACLUP and is subject to overflight by aircraft arriving and departing the Oxnard airport. Impact level should be changed to less than significant impact with no mitigation required. You can confirm this with an Air Hazard Safety Study if you choose.

Page 54, Section K - Noise. Same comments as above as the project site is located within the ACLUP and the site is subject to overflight. Aircraft noise may be present but it is correctly stated that the project falls outside of the 65 CNEL noise contour for the Oxnard Airport. Additional sound insulation should be considered when constructing the homes to minimize the impact on future residents from aircraft noise.

While not required, it is recommended that the developer grant the County of Ventura an Avigation Easement over the parcel that is consistent with the FAA model Avigation Easement.

Thank you again for the opportunity to comment.

Todd McNamee  
Director of Airports  
County of Ventura  
805-388-4200

**CC:** erin.powers@ventura.org; sdegeorge@goventura.org

ATTACHMENT 8  
PAGE 169 OF 185

**From:** "Lynne Vander Kamp" <lynnevka@gmail.com>  
**To:** Brian.Foote@ci.oxnard.ca.us  
**Date:** 1/28/2010 7:11:37 PM  
**Subject:** Anacapa townhomes

RECEIVED

PLANNING DIVISION  
CITY OF OXNARD

January 29, 2010

Brian Foote, Associate Planner

City of Oxnard Planning Division

214 South C Street

Oxnard, CA 93030

Dear Brian Foote, Associate Planner:

Subject: MND No.09-03

As an owner and resident of property at 1040 Canal Street, I am very concerned about the addition of "Anacapa Townhomes" literally on my doorstep. I have read the "Environmental Documents" online and find it unbelievable that 70 units in 5 multifamily, 3 story buildings will have NO NEGATIVE IMPACT on this neighborhood, to say nothing of the parking garages, exterior pool and recreation building. To say that the parking of approximately 2.5 cars per unit in the 180 to 200 spaces being made available in this project will not have an impact on traffic flow, parking and noise is ludicrous. There will not be any parking available on Wooley or on Harbor, forcing the overflow to park on Canal where there is NO street parking currently available. There are no laws or codes restricting the number of vehicles per address leaving most of us without the convenience of guest parking as it is. It defies reason to unequivocally state that the introduction of approximately 280 people and 175 cars in a 4 acre area will not adversely change the face and the makeup of this community for the worse.

The construction of exterior stairs to the upper units will also create a significant amount of noise along with all the parking and other utilities planned for the rear of this project which will now constitute the VIEW FOR THOSE OF US WHO NOW SEE THE DUNES AND HAVE A PARTIAL OCEAN VIEW. Rooftops, staircases, parking lots and dumpsters are not why we live here and pay these taxes. There are no longer full mature trees in this area to act as a noise or dirt buffer.

Additionally, the only concession being made for the influx of at least 175 more cars is a turn lane on Canal St. What exactly will control the rerouted traffic on Dunes and the one lane crossing the Edison Canal Bridge? I find it hard to believe that a row of blinking lights mitigates these problems. The widening of the existing alley will only encourage additional traffic use to access Harbor Blvd. If past performance is any indication, Mr. Marlow, who has never cared for the appearance or cleanliness of this dumping ground, has very little concern for community public opinion.

Reliant Energy has recently installed a barbed wire fence in our back yards already giving it a prison like appearance and severely limiting what should be an environmentally attractive green space. I did not see any provisions

being made to beautify any surrounding areas for the current residents as was provided at Westport or Seabridge. There was discussion involving the removal of power poles on Wooley but none concerning the 3 attending poles along the canal. So, to recap, this project will deliver only a NEGATIVE effect on our personal living space after what will most probably be an extensive and intrusive, noisy and dirty inconvenient construction period beginning at 7AM with heavy machinery for the better part of a year.

We respectively disagree with your findings and request that these plans be reviewed with an eye toward the overall effect on the whole community and not just the generated revenue for the city. We are all concerned with our future property values. Accommodating this project is not in the best interest of the existing homeowners.

Sincerely,

Lynne Vander Kamp (805-382-1515)

cc: Mandalay Shores Homeowners Association

RECEIVED

JAN 2 2007

PLANNING DIVISION  
CITY OF OXNARD

**From:** Suzanne Schechter <suzannes5@roadrunner.com>  
**To:** Brian.Foote@ci.oxnard.ca.us  
**Date:** 9/23/2009 3:31:39 PM  
**Subject:** planning & zoning permit #08-400-04,09-300-1,09590-01

RECEIVED

PLANNING DIVISION  
CITY OF OXNARD

Dear Mr. Foote

I am a resident of Oxnard Shores( for more than 30 years) I want to voice my very strong objections to development, as proposed, of the 4 acres at the northeast corner of Harbor Bl and Wooley Road of 70 condominiums.

70 UNITS! that is an unacceptable density for the 4 acres.

Consider the impact on traffic on Wooley Rd., Harbor Bl, and Victoria. How will the parking spaces be fitted in? What exactly does "air rights" mean in this instance? three stories? the tallest structure in the Shores area, therefore? and are these proposed architectural projections going to stick out, how?

Additionally, there are currently unsold units, in the recent Seabridge projects adjacent to this proposed development, and on the corner of Harbor and 5th St, construction on that project has stopped, after extensive ground preparation, due to the economic downturn, and real estate crash.

Jamming this poorly conceived project onto this small space, will destroy the ambience of the seaside neighborhood, and negatively impact the lives of hundreds of residents, bringing increased traffic, noise and pollution.

I was not able to access the Environmental Documents, at City planning website.

Thank you for reading this,  
Sincerely,  
Suzanne Schechter  
4824 Amalfi Way  
Oxnard 93035 805 9858146 email: suzannes5@roadrunner.com

JUN 18 2010

PLANNING DIVISION  
CITY OF OXNARD

PCR

June 2, 2010

Mr. Brian Foote, Associate Planner  
**CITY OF OXNARD**  
Planning Division  
214 South C Street  
Oxnard, California 93030

**Re: ANACAPA TOWNHOMES - RESPONSE TO COMMENTS RECEIVED FROM THE CALIFORNIA DEPARTMENT OF FISH AND GAME**

Dear Mr. Foote:

The purpose of this letter is to respond to comments received from the California Department of Fish and Game regarding the Biological Resources Assessment **PCR Services Corporation (PCR)** prepared for the Anacapa Townhomes project located in the Oxnard Shores area in the City of Oxnard (City). PCR prepared a final Biological Resources Assessment for the subject project, after incorporating comments from the City, U.S. Fish and Wildlife Service (USFWS) and California Department of Fish and Game (CDFG), dated December 2009. In a subsequent letter of comment from CDFG on the Draft Mitigated Negative Declaration for the project, dated February 18, 2010, CDFG provided additional comments to which the City requested the applicant and PCR respond. Following are PCR's responses.

#### **CALIFORNIA WILDLIFE ACTION PLAN**

The comments provided in CDFG's letter are for context and no response is needed.

#### **IMPACTS TO SENSITIVE BIOLOGICAL RESOURCES**

##### **Rare Plants**

In response to CDFG's comments regarding the adequacy of the plant surveys conducted in January 2004, November 2008, and October and November 2009, PCR conducted a spring survey on April 14, 2010. The latest survey in 2010 is considered to have addressed CDFG's concern that plant surveys should be completed at the proper time(s) of year when all rare species potentially occurring on a project site are both evident and identifiable. All plant surveys were conducted by PCR biologists (Kristen Szabo, Jason Berkley, Zeke Cooley and Steve Nelson) who have experience in conducting floristic field surveys, have knowledge of plant taxonomy and plant community ecology, and have familiarity with plants of the area. It should also be noted that a reference site for the Ventura marsh milk-vetch (*Astragalus pycnostachyus* var. *lanosissimus*) was visited (with a biologist from the U.S. Fish and Wildlife Service) to confirm the species was evident and identifiable at the time of the October 20, 2009 survey. Other species targeted for the survey are listed in Table 1, Sensitive Floral Species Known to Occur in the Vicinity of the Anacapa



Townhomes, beginning at page 27 of PCR's Biological Resources Assessment, dated December 2009. Finally, due to the relatively small size of the project site (4.1 acres), survey coverage of the entire site was assured.

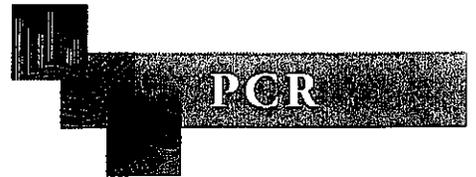
For purposes of updating Appendix B: Floral and Faunal Compendia, of PCR's December 2009 report, the following plant species that were not observed prior to the Spring 2010 survey, are noted: barley (*Hordeum vulgare*), wild oats (*Avena sp.*), both non-native species, and dodder (*Cuscuta sp.*). None of these species are considered to be sensitive. For purposes of clarification, red sand verbena (*Ambrosia maritime*), was also observed on site, as it has in previous site surveys. This species is included on the California Native Plant Society List 4, indicating it is of limited distribution in California, but whose existence does not appear to be susceptible to threat. For this reason, it is not considered to be a potential issue under CEQA.

**Least Bell's Vireo (*Vireo bellii pusillus*)**

PCR acknowledges that, under certain circumstances, construction-related noise and vibration can disturb nesting birds resulting in nest abandonment. PCR also acknowledges that if construction noise and vibration were to affect a threatened or endangered species, the appropriate "take" permits would be required. In the case of the Anacapa Townhomes project, however, circumstances are such that no take will occur.

CDFG's concern in this regard is for the least Bell's vireo, a State and federally-listed species. As indicated in the CDFG letter and reported in the December 2009 PCR report, the least Bell's vireo had been previously reported in southern willow scrub habitat near the southeast corner of Harbor Boulevard and Fifth Street. In connection with PCR's April 14, 2010 spring survey for plants, the area where the vireo had been reported was surveyed for the species. A vireo was heard and observed on that date within southern willow scrub habitat, as previously reported. The vegetation along the nearby canal was also characterized and evaluated for its suitability to support least Bell's vireo as part of a breeding territory.

No take of least Bell's vireo, either directly or indirectly, is anticipated to result from the project for several reasons. First, the location where the species was observed was at least 2,000 linear feet from the closest part of the project site (on the order of 0.5 mile) which is well beyond the 500-foot distance cited in the CDFG letter as being potentially affected by noise and vibration. Second, the nearest willow scrub habitat that would be considered important as a component of a vireo breeding territory is found approximately 600 linear from the nearest part of the project site. Vegetation that generally follows the canal nearer to the project site consists of non-natives such as *Myoporum sp.* which is not the type or preferred habitat for least Bell's vireo. Third and importantly, there is a street with houses on both sides between the project site and all southern willow scrub to the north where the vireo has been observed (see Figure 4 on page 17 of the December 2009 PCR report). These houses are two-story and will effectively serve as sound and visual buffer between project construction and the area where the vireo has been found. In fact, a



two story house, rising some 25 feet from the ground, will be taller and more effective than a sound wall or berm built for the same purpose. Therefore, the potential for indirect take is less than expected from implementation of the mitigation measures recommended in CDFG's letter.

**Silvery Legless Lizard (*Anniella pulchra pulchra*)**

The comments included in the CDFG letter regarding this species are acknowledged. Mitigation in the form of pre-construction surveys will be incorporated into the Mitigated Negative Declaration (MND) findings.

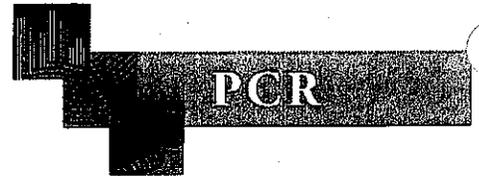
**MITIGATION MEASURES**

The CDFG letter recommended mitigation measures for potential impacts to the least Bell's vireo and silvery legless lizard. PCR will concede that most of the CDFG recommendations are reasonable and will incorporate them into its findings. However, some of the recommendations are unreasonable and/or are not warranted. Therefore, we recommend the following mitigation language be incorporated into the MND.

**Least Bell's Vireo**

*In concert with the mitigation for impacts to nesting birds during the breeding season, focused pre-construction surveys for the least Bell's vireo will be conducted in suitable habitat within 500 linear feet of construction activities. The surveys will follow agency protocol as to survey methodology (i.e., time of day and climatic conditions). However, a modified survey schedule is appropriate given the site specific circumstances. Therefore, surveys of the area within 500 feet of construction activities and the area where the vireo has been reported will be initiated if commencement of construction is to occur during the breeding season for the species (April 10 to July 31). If no least Bell's vireos are detected, two additional surveys will be performed no less than 10 days apart. If no least Bell's vireos are detected within 500 feet of the construction activities during any of the three survey bouts, no further action is required. If, however, least Bell's vireo are detected within 500 feet of construction activities, CDFG and the U.S. Fish and Wildlife Service (USFWS) will be contacted to determine what, if any, actions need to be taken to avoid adverse effects on the species. Such actions, if necessary, may include one or more of the following: phasing the construction activities to avoid certain construction activities within 500 feet of occupied vireo habitat until after the breeding season has ended; noise monitoring; and nest monitoring. If least Bell's vireo is detected in suitable habitat farther away than 500 feet, these areas will be monitored during each of the three survey bouts so as to gain assurances that occupied habitat remains more than 500 feet from construction activities. The findings of the surveys will be documented and submitted to CDFG and USFWS.*

PCR believes that given the relatively high site fidelity of least Bell's vireo from year to year, the approach outlined above is reasonable and appropriate. A modified approach to survey protocol is also provided for in the survey guidelines issued by USFWS. As stated in the guidelines



"However, we (USFWS) may concur, on a case by case basis, with a reduced effort if unusual circumstances dictate that this is a prudent course of action. For instance, intensive surveys of small, marginal or extralimital habitats by experienced personnel may well result in defensible conclusions that eight (or more) individual surveys are unnecessary)." In the case of the Anacapa Townhomes project and as described above, the circumstances at and surrounding the site would support such a modified approach. In addition, CDFG's recommendation for cowbird trapping is not warranted. The Anacapa Townhomes project is by its nature a fairly dense development with relatively little area that will be conducive to establishing turf grass area, which are believed to attract cowbirds. In comparison, there are several hundred acres of active agriculture immediately beyond the canal that parallels the project site's eastern edge. Agricultural activities also, perhaps to a much greater extent, attract cowbirds. In PCR's opinion, The Anacapa Townhomes project should not be burdened with a cowbird trapping program that it, in and of itself, is not expected to result in a cowbird problem.

#### **Silvery Legless Lizard**

*Prior to the initiation of construction activities, a temporary silt fence will be installed along the project site's eastern boundary to prevent ingress of silvery legless lizards onto the site for the duration of construction activities. The fencing will be keyed into the ground to a depth of at least three inches. Preconstruction surveys for silvery legless lizards will then be conducted within areas of the site that could be used by the species. The surveys will be conducted according to the following protocol provided by CDFG.*

*Surveys will be conducted on a warm day (70 to 80 degrees Fahrenheit) in the mid-morning and no longer than two weeks prior to the commencement of soil disturbance activities. Relative soil moisture should be approximately 65 to 80 percent with soil temperature being 60 to 70 degrees Fahrenheit. Two person teams will work together with one person using a hand rake to gently rake the loose litter and soil down to firmer substrate. The other person will search the areas being raked for legless lizards.*

*Surveys will employ a "three-pass" methodology to locate as many legless lizards as possible. If legless lizards are found during the first pass, an overnight period of no disturbance will occur before the second pass. Similarly, if legless lizards are found during the second pass, another overnight period of no disturbance will occur before the third pass. If during any pass no legless lizards are found, the survey will be concluded.*

*Any legless lizards found will be placed in a lidded, vented box containing clean sand. Areas of moist and dry sand will be present in the box. Boxes will be kept out of direct sunlight and protected from temperatures over 72 degrees Fahrenheit. The temperature of the sand in the box will be kept under 66 degrees Fahrenheit. Information on each lizard captured will be recorded, including date of capture, location, length, color, age, and tail condition. All lizards captured will*

Mr. Brian Foote, Associate Planner  
**CITY OF OXNARD**  
June 2, 2010 - Page 5



*be relocated to suitable off-site habitat the same day as capture. GPS location, of released lizards will be recorded. CDFG will be notified about any injured legless lizards.*

*All work will be performed by personnel who hold active permits to conduct the work from CDFG. Documentation of survey results and associated records will be submitted to CDFG at the conclusion of the surveys.*

In its comment letter, CDFG recommended that if silvery legless lizard is found on site, off-site habitat enhancement should be required at a 2:1 ratio. In PCR's opinion, this is not warranted. As described in PCR's December 2009 Biological Resources Assessment (see pages 29 and 34), less than five percent of the 4.1-acre project site appears to have surface vegetation and soils even marginally suitable for the species. Therefore, as concluded in the December 2009 assessment any losses of habitat will be incrementally small and do not warrant this level of mitigation because they cannot reasonably be considered significant to either local or regional population numbers.

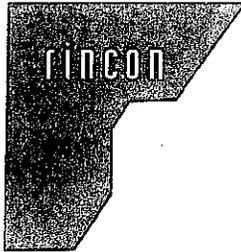
If you have any questions or comments about the above responses to CDFG's comments, please contact me at (949) 753-7001 or by e-mail at [s.nelson@pcrnet.com](mailto:s.nelson@pcrnet.com).

Sincerely,  
**PCR SERVICES CORPORATION**

A handwritten signature in black ink that reads "Steven G. Nelson". The signature is written in a cursive style.

Steven G. Nelson  
Director of Biological Services

RECEIVED



PLANNING DIVISION  
CITY OF OXNARD

Rincon Consultants, Inc.  
790 East Santa Clara Street  
Ventura, California 93001

805 641 1000  
FAX 641 1072

info@rinconconsultants.com  
www.rinconconsultants.com

May 25, 2010  
Job No. 10-95830

**Brian Foote, Associate Planner**  
Development Services Department  
City of Oxnard  
214 South C. Street  
Oxnard, CA 93030

**Subject: Peer Review of Anacapa Townhomes Biological Resources Assessment**

Dear Mr. Foote:

Rincon Consultants has conducted a peer review of the Biological Resources Assessment (BRA, December 2009) prepared by PCR Services for the property located at the northeast corner of Harbor Boulevard and Wooley Road, as requested. Authorization to conduct this peer review was received on May 7, 2010. In addition to the PCR document, we have also reviewed the City's IS/MND for the project (MND No. 09-03, January 15, 2010) that was prepared subsequently after the BRA and comments on the IS/MND provided by the California Department of Fish and Game (CDFG) and the United States Fish and Wildlife Service (USFWS).

#### METHODOLOGY

The BRA was reviewed with respect to the following parameters:

- Adequate description of the site biological setting, including recent field reconnaissance and identification of possible sensitive species issues;
- Adequacy of methodologies used from the perspective of prevailing biological studies practices;
- Description (map) of the site's biological characteristics and the location of sensitive species, if any;
- Discussion of the biological regulatory setting pertinent to the site;
- Description of the proposed land uses and appropriate determination of impacts associated with such uses; and
- Adequacy of mitigation measures that are proposed or that may be necessary.

In addition to review of the provided material, Rincon conducted a review of sensitive resource occurrences from the CDFG California Natural Diversity Data Base (CNDDB), Biogeographic Information and Observation System (BIOS - [www.bios.dfg.ca.gov](http://www.bios.dfg.ca.gov)), and USFWS Critical Habitat Portal (<http://criticalhabitat.fws.gov>). Also, a brief field reconnaissance was undertaken by Dr. Duane Vander Pluym and Mr. Stephen Hongola on



May 12, 2010, to verify the description of resources within the BRA and specifically to check for possible sensitive species issues.

## PROJECT PERSONNEL AND MANAGEMENT

This peer review has been performed by and under the direction of Duane Vander Pluym, D.Env., Rincon's Principal Biologist who has over 30 years of experience in preparing biological and environmental reports and has also provided expert witness testimony with respect to biological resources. Mr. Hongola also aided in the field reconnaissance. He is a specialist in avian field studies who holds a federal 10(a)(1)(A) permit to conduct protocol surveys for coastal California gnatcatcher and has managed or assisted in focused surveys for burrowing owl, least Bell's vireo, southwestern willow flycatcher, California least tern, western snowy plover, desert tortoise, arroyo toad, California red-legged frog, and a variety of sensitive plants.

## REVIEW OF BIOLOGICAL RESOURCES ASSESSMENT

### Description of Biological Setting

The description of the onsite resources is considered generally adequate in that it describes biological resources present and expected to be present, provides appropriate maps and other graphics illustrative of the onsite resources, and lists most of the flora and faunal elements that typify the site conditions. The description of the vegetation as ornamental/ruderal is generally correct, though from Rincon's brief observations, somewhat more than 5% of the aerial coverage of the site was comprised of native plants, including coyote brush, beach evening primrose, and sand verbena. Nonetheless, hottentot-fig (aka iceplant) was the predominant plant present, along with recently weed-whacked non-native grasses and ruderal weeds. The floral compendium appeared to be generally inclusive of plants observed, as was the faunal lists.

Section 3.7.2 simply states that no sensitive communities are present at the site. It should also reference the City's Coastal Land Use Plan (February 1982) which indicates that no sensitive coastal habitats are present (Map No. 7) as this provides a long term perspective on the lack of important coastal biological resources at this site. Similarly, Section D Items 5 & 6 of the IS/MND (page 33) should also reference this map.

It is noted that other invertebrates are present at the site besides the Monarch butterfly listed in the BRA, most obviously various ants and darkling beetles (*Eleodes* sp.). Given that no sensitive invertebrates are known to exist in the area and the habitat and its long term disturbance is not appropriate for dune-related sensitive species (globose dune beetle, sandy beach tiger beetle), no further information is required. However, these dune-related sensitive species could have been listed in Table 2 of the BRA since they are recorded within the California Natural Diversity Data Base within 4 miles of the site.

No explanation with respect to Belding's savannah sparrow was provided within Table 2. This is a resident subspecies of southern pickleweed saltmarshes where it breeds, but it also forages along tidal channels and near shore habitats adjacent to saltmarshes. Pickleweed is present in small linear patches adjacent to the site in the Edison Canal, but insufficient



amounts are present to sustain a Belding's savannah sparrow population and it would not be expected to occur as a foraging species at the site. This information should be included in Table 2.

A Cooper's hawk nest was observed in a willow on the south side of Wooley Road approximately 75 feet from the site during the Rincon site visit. This species is considered a CDFG "Special Animal" as it is on the "Watch List" (CDFG, July 2009). As a raptor, this species and its nest are protected under Fish and Game Code Section 3503.5. Given the proximity of the nest, the hawk probably forages occasionally at the site, though most foraging would be expected along the Edison Canal and the dune habitats south of the site. This species should be added to Table 2 of the report and discussed in the IS/MND under Section D Items 1 & 2.

### Methodologies

The CDFG Biogeographic Information and Observation System (BIOS - <http://bios.dfg.ca.gov/>) with Ventura County information checked should have been included in the literature review. Nonetheless, a review of this literature source conducted by Rincon did not yield any additional information pertinent to the project site.

The literature search should also have included information from Kuhnz, et al (2005) for more up-to-date information on the use of sand dunes by legless lizards and difficulties with finding them using standard techniques, especially in low density situations. The methodology used (site walkover examining soils and turning over objects) is not conclusive of absence. In addition, except for the January 2004 survey during which time it is unclear if legless lizard was surveyed for, the latest surveys by PCR were not conducted within the time frame for near surface activities by this animal (December - May; see California Coastal Commission, 2006). This issue is further discussed below.

While it is typical to conduct rare plant surveys for undisturbed lands within the timeframe at which rare plants are identifiable (typically the bloom period) per the CDFG comments (CDFG, February 2010), this is also relative to the sensitive species of concern and the degree and length of time that a site has been continually disturbed. When those plants are perennials that are identifiable in all seasons, then surveys outside the typical bloom period are reasonable and effective. For this site, the plant of concern, Ventura marsh milk-vetch, is a perennial that is recognizable at least to genera in all seasons. PCR conducted visits to the reference site in 2009 to confirm that this plant was recognizable at the time of the field visits and therefore, the rare plant survey is considered adequate. The adequacy of this methodology was confirmed by USFWS in the email message of December 10, 2009.

The methodology employed for the burrowing owl was reasonable and effective at determining presence during the time frame of the field surveys. No additional surveys are warranted.

### Impact Assessment

CDFG states that Least Bell's vireos (LBVI) could potentially be present in the willow stands south of the project site based on a recent discovery in willow scrub habitat at the southeast



corner of Harbor Boulevard and Fifth Street in 2009. Rincon specifically examined the willow riparian habitat located on the adjacent parcel during the timeframe when LBVI are known to currently be present in the Ventura/Oxnard region. No LBVI were observed. Also, the total suitable habitat for this species available to the south of the site is less than 1 acre in several smaller stands. Typical territory size for LBVI is about 2 acres (Kus, 2002), making these patches inadequate in size. In contrast, the area referenced by CDFG southeast of Harbor Boulevard and Fifth Street contains about 10 acres of potentially suitable habitat, with an additional 17 acres of possible habitat located northwest of Harbor Boulevard and Fifth Street.

Further, it is noted that a Cooper's hawk nest was observed in the willows southeast of Canal Street in May 2010. This bird is predatory on smaller birds, including vireos, and it is unlikely that a Least Bell's vireo would choose to nest near this hawk. Therefore, it is highly unlikely that LBVI would be located in the vicinity of the project site.

CDFG also raised concerns regarding the silvery legless lizard and burrowing owl. While marginal habitat is present at the site for burrowing owl (disturbed ruderal areas with annual grasses and ground squirrel burrows), no nesting owls have been known to occur in this vicinity for decades (Center for Biological Diversity, et al, April 2003). During Rincon's brief field visit in May 2010, ground squirrel burrows were checked for owl sign and none was seen, though it should be noted that the brief field visit did not constitute a protocol level search. Nonetheless, this species is typically highly visible and conspicuous when present and none were observed. This further confirms PCR's negative observations and no further surveys or mitigation for this species is warranted.

Silvery legless lizards have been reported to occur in the dune areas in the vicinity of the project site, and were anticipated to potentially be present less than 0.25 miles from the site (California Coastal Commission staff report, 2006). As indicated by Kuhn, et al (2005), standard survey methods for this species are not adequate when populations are at low densities and the methodology employed by PCR would not have been adequate to find this animal. However, further, more detailed surveys are likely to yield a similar result since given past disturbances to the site (particularly disking, which is highly detrimental to this lizard), since if silvery legless lizard is present, it would be at very low densities. Therefore, the question becomes, if it is assumed that this lizard is present at very low densities, would the impact of site development be considered "a substantial adverse effect" per Section D Item 1 of the IS/MND? We agree with the rationale presented on page 35 of the PCR (December 2009) report that the few lizards that may be lost as a consequence of development of the site would not have a substantial effect on this lizard's local population. In particular, the presence of much more suitable dune habitat (and likely much greater numbers) to the south that is protected under the City's Coastal Land Use Plan ameliorates any loss of individuals as a consequence of site development. Further, legless lizards have been found within residential areas in the Santa Barbara area (personal communication, Lawrence Hunt, 2009) and so have at least some potential for persisting within the proposed land use. Therefore, no further surveys or additional mitigation measures appear to be warranted.

The PCR report states that the sensitive coast horned lizard is absent from the site based on the dominance of hottentot-fig, absence of native ants, and past site disking. During the



Rincon field visit, native ants were observed within the site. In addition, coast horned lizard was reported from a disturbed (but not disked) dune habitat parcel on 1.6 acres north of Whitecap Street in 2006 less than 0.25 miles west of the site (California Coastal Commission staff report, 2006). Nonetheless, a substantial population of coast horned lizard would not be expected at this site given past disking and the adjacent residential land use. Rincon also conducted a brief search around the native ant nest searching for horned lizard or their sign (scat) and nothing was observed. Similar to the finding for the silvery legless lizard, if any coast horned lizards were present within these disturbed conditions, the loss of those animals during site development would be considered adverse, but less than significant.

Section D Item 4 of the IS/MND is not fully consistent with the discussion contained on page 35 of the PCR December 2009 report, and migratory and other native birds were observed at the site. As stated within the PCR report, a low potential exists for migratory birds to nest at the site, and this potential impact would be mitigated by Mitigation Measure D-1 of the IS/MND.

#### Adequacy of Mitigation Measures

The project site and adjacent open habitat to the east and south are exposed to substantial traffic noise levels from both Wooley Road and Harbor Boulevard, with noise levels from Wooley Road alone exceeding an Leq of 65 dBA at a distance of 50 feet from the centerline. As a consequence, birds that may be nesting near the site are already exposed to relatively high sound levels, and construction noise would be expected to have less of an effect on such habituated animals than a similar level of noise in a quieter, more natural setting. Therefore, suggested sound wall measures by CDFG regarding least Bell's vireo would not be considered necessary for this site. In any event, it is highly unlikely for least Bell's vireos to nest in the willows south of the site and no mitigation regarding this species is considered necessary.

Measure D-1 of the IS/MND is generally sufficiently protective of the Cooper's hawk nest observed south of the site. However, the following revision (underlined text) is recommended for clarity purposes:

The developer shall avoid grubbing or grading the site during the breeding season (i.e. February 15 through August 31) as the preferred mitigation. If avoidance is not feasible and grubbing/grading will occur during the breeding season, then the project site and adjacent open space areas shall be surveyed by a qualified biologist for the presence of nesting birds prior to removal of vegetation. The developer shall be required to submit a report documenting the findings of the site survey to the Planning Division for review and approval. The site survey shall be conducted within 30 days of the beginning of any grubbing/grading activity. If any active nests are detected, then a 300-foot buffer (or as otherwise deemed appropriate by the biological monitor in consultation with the City and CDFG) shall be placed around the nest site until the nestlings have successfully fledged.



## CONCLUSION

The PCR Biological Resources Assessment (December 2009) contains reasonable information for use in environmental documentation for the planned land use at the site. While several additions to the study could be made for clarification purposes, the basic findings from a California Environmental Quality Act perspective are adequate. Rincon recommends that Mitigation Measure D-1 be revised as indicated above to clarify the mitigation requirements.

---

Thank you for choosing Rincon Consultants for your environmental services needs. If you have any questions with respect to this peer review, please contact the undersigned.

Sincerely,  
RINCON CONSULTANTS, INC.

Duane Vander Pluym, D.Env.  
Principal

Attach: Bird Survey Form, 12 May 2010



## REFERENCES

- California Coastal Commission, July 27, 2006. *Staff Report - City of Oxnard Local Coastal Program Amendment 1-05 (Oxnard Shores) for Public Hearing and Commission Action at the August 9, 2006, Commission Meeting.*
- California Department of Fish and Game, 18 February 2010. Letter re Draft Mitigated Negative Declaration for the Anacapa Townhouses Project.
- California Department of Fish and Game (July 2009). *Special Animals*. Habitat Conservation Division, Wildlife and Habitat Data Analysis Branch.
- Center for Biological Diversity, Santa Clara Valley Audubon Society, Defenders of Wildlife, San Bernardino Valley Audubon Society, California State Park Rangers Association, and Tri-County Conservation League (Petitioners), April 2003. *Petition To The State Of California Fish And Game Commission And Supporting Information For Listing The California Population Of The Western Burrowing Owl (Athene cunicularia hypugaea) As An Endangered Or Threatened Species Under The California Endangered Species Act.*
- Germano, D.J. and D. J. Morafka, 1996. Diurnal aboveground activity by the fossorial silvery legless lizard, *Anniella pulchra*. *Great Basin Naturalist* 56(4): 379-380.
- Kuhnz, Linda A., Burton, Robert K., Slattery, Peter N., Oakden, James M., 2005: Microhabitats and Population Densities of California Legless Lizards, with Comments on Effectiveness of Various Techniques for Estimating Numbers of Fossorial Reptiles. *Journal of Herpetology*: Vol. 39, No. 3, pp. 395-402
- Kus, B., 2002. Least Bell's Vireo (*Vireo bellii pusillus*). In *The Riparian Bird Conservation Plan: a strategy for reversing the decline of riparian-associated birds in California*. California Partners in Flight. [http://www.prbo.org/calpif/html/docs/riparian\\_v-2.html](http://www.prbo.org/calpif/html/docs/riparian_v-2.html)
- PCR Services Corporation, December 2009. *Biological Resources Assessment - Anacapa Townhouses, Oxnard, Ca.*

# Bird Survey Form

Project: City of Oxnard, Anacapa Townhomes  
 Location: NE corner Wooley/Harbor Blvd, and offsite dune habitat to south  
 % Cloud Cover: 0  
 Wind: 2 to 4 mph  
 Temperature: Start: 62 F End: 64 F

Date: 5/12/2010  
 Time - Start: 945  
 End: 1045  
 Observers: S. Hongola  
D. Vander Pluym

Species	Sensitive?	Breeding?	Habitat/Location	Behavior	Comments
HOFI		X	Scrub, disturbed	Flying, calling	
BEWR		X	Willow, scrub	Singing	offsite
CATO		X	Willow, scrub	Calling	offsite
YEWA	Yes	X	Willow, channel	Singing	offsite
WAVI		X	Willow	Foraging	3 observed offsite in willows
CLSW			Aerial	Flying/foraging	
HOOR		X	Eucalyptus	Flying/calling	offsite
AMCR		X	Disturbed, scrub, residential	Flying/foraging	Seen throughout area
EUST		X	Disturbed	Flying/calling	
NOMO		X	Disturbed, scrub	Singing	
SPTO		X	Willow, scrub	Calling	offsite
SOSP		X	Willow, scrub	Singing	offsite
ATFL		X	Willow, scrub	Perched, calling	offsite
ANHU		X	Willow, scrub	Perched, singing	offsite
VEGU			Flyover		
RWBB		X	Edison channel	Flying, calling	offsite
LEGO		X	Willow, scrub	Flying, calling	
BUSH		X	Willow, scrub	Calling, foraging	offsite
WIWA		X	Willow, scrub	Foraging	offsite
COHA	Yes	ON	Willow stand	Sitting on nest	Adult observed on nest ~10 feet off ground within relatively open willow ~35 feet from south edge Wooley Rd.
BASW		O	Aerial	Flying/foraging	
RTHA		X	Eucalyptus	Flying	offsite
BHCO		X	Disturbed, willow	Calling	offsite
SAPH		X	Scrub	Singing	onsite
Swallow species		N	Residential	Nesting	Mud nest observed within eave of house north of site
KILL		O	Along canal	Fly over	
LBCU		O	Fly over		
HOSP		O	Disturbed, iceplant	Foraging	

Wildlife observed: CA ground squirrel, side-blotched lizard, many pocket gopher burrows, native and Arg ants, dog

Sign: O=Observed, T=Tracks, S=Scat, V=Vocalization, B=Burrow, C=Carcass, Fe=Feathers, Fu=Fur

Breeding Bird:	O	Present, does not breed within habitat at site.	NB	Nest building
	X	Seen or heard in appropriate breeding habitat.	DD	Distraction display
	S	Singing male seen in appropriate habitat	UN	Used nest
	P	Pair observed in suitable habitat	FL	Fledglings
	T	Territorial behavior	FS	Adult with fecal sac
	A	Agitated behavior	FY	Adult with food
	C	Courtship or copulation	ON	Occupied nest
	N	Visiting probable nest site or cavity	NE	Nest with eggs
			NY	Nest with young

