



Planning Division

PLANNING COMMISSION STAFF REPORT

TO: Planning Commission

FROM: Kathleen Mallory, AICP, Contract Planner

DATE: March 20, 2008

SUBJECT: Planning and Zoning Permit Nos. 07-600-06 (GPA), 07-570-04 (ZC), 07-300-13 (TPM), and 07-500-14 (SUP) – Southwest corner of Channel Islands Boulevard and Rose Avenue.

1) **Recommendation:** That the Planning Commission:

- a. Adopt a resolution approving PZ 07-500-14 (Special Use Permit) for the proposed grocery store with alcohol sales and additional retail buildings, subject to certain findings and conditions.
- b. Adopt a resolution recommending that City Council approve PZ 07-300-13 (Tentative Parcel Map).
- c. Adopt a resolution recommending that the City Council approve PZ 07-600-06 (General Plan Amendment).
- d. Adopt a resolution recommending that the City Council approve PZ 07-570-04 (Zone Change).

- 2) **Project Description and Applicant:** A request for a special use permit (PZ 07500-14) for development of a Fresh and Easy grocery store (Tesco – 13,929 sq. ft.) with alcohol sales (for off-site consumption), and approximately 5,630 square feet for additional retail uses. The request includes a general plan amendment (PZ 07-600-06) and zone change (PZ 07-570-04) to amend the land use designation, from Residential Low Medium to General Commercial, and change the zoning district, from R-2 (Multiple Family) to C-2-PD (General Commercial, Planned Development) (see Attachment A). A tentative parcel map (PZ 07-300-13) is requested to subdivide the vacant 4.05 acre property (APN 221-0-232-515) located on the southwest corner of Channel Islands Boulevard and Rose Avenue into three parcels (see Attachment B). At this time, development would be proposed on two parcels, and a future church use is anticipated at a later date under separate permit. Filed by Tom Davies, Churchyard Development, LLC., 2225 Glastonbury Road, Westlake Village, CA 91361.

- 3) **Existing & Surrounding Land Uses:** The project site is currently vacant (see Attachment A, Vicinity, General Plan and Zoning Maps):

LOCATION	ZONING	GENERAL PLAN	EXISTING LAND USE
Project Site	Existing - R-2-PD Proposed – C-2-PD	Existing - Low Medium Density Residential Proposed – General Commercial	Vacant
<i>North</i>	C-2-PD	General Commercial	Retail Shopping Center
<i>South</i>	C-R	School	Channel Islands High School
<i>East</i>	R-2- PD	Park	Future College Park
<i>West</i>	R-2-PD	Multi-family Residential Planned Development	Single-family Residential

- 4) **General Plan Policies and Land Use Designation Conformance:** The existing General Plan land use designation on the project site is low medium density residential. According to the General Plan, this designation applies for properties intended for development as lower density apartments or condominiums and higher density detached single family residences. The proposed general plan amendment to General Commercial would allow for free-standing commercial uses along thoroughfares, and may also include office uses. According to the Circulation Element of the General Plan (Figure VIA-5), Rose Avenue is a primary arterial (6-lane roadway) and Channel Islands Boulevard is a secondary arterial (4-lane roadway). In September 2007, the City Council conducted a pre-application review of the proposed general plan amendment. Comments received as part of this discussion generally were favorable towards the proposed general plan amendment and Tesco project. Support of the land use change was based upon the traffic volumes present on the adjacent roadways and the limitations associated with the size of the parcel. With approval of the general plan amendment and zone change, the project would be in conformance with all code requirements.
- 5) **Environmental Determination:** The proposed development is subject to review in accordance with the California Environmental Quality Act (CEQA). An initial study was prepared to analyze potentially significant adverse environmental effects of the proposed project. For environmental analysis purposes, CEQA requires consideration of potential full build-out on all three parcels. Therefore, within the environmental document staff's analysis included 6,000 sq. ft. of future church development on Parcel 3. Staff identified areas of potentially impact as follows: air quality, cultural resources, hazards and hazardous materials, noise, and traffic and

transportation. Mitigation measures were included to reduce the potentially significant impacts to less than significant levels.

The applicant agreed in writing to accept the mitigation measures recommended by staff, and the Mitigated Negative Declaration (MND) No. 07-11 was made available for public review and comment, from February 15, 2008 through March 5, 2008 (see Attachment C).

Staff received ten comment letters on the environmental document. Comment letters from the following agencies were received: Ventura County Air Pollution Control District; and County of Ventura, Public Works Agency, WaterShed Protection District, and Transportation Department. Comment letters were also received from the Southern California Association of Governments (SCAG) and Calleguas Municipal Water District. The Air Pollution Control District, and County Public Works Agency and Transportation Department concurred with the conclusions contained within the environmental document. SCAG and Calleguas concurred with the analysis contained within the MND.

The County WaterShed Protection Agency had concerns regarding the water quality and hydrology analysis contained in the MND. This letter requests that the environmental document identify specific measures to address runoff and water quality impacts due to development on the site. Further, the letter requests City review of the hydrology study prior to Commission action on the development project. The City's standard conditions of approval require project compliance with best management practices (BMP's) and submittal of a City approved Storm Water Pollution Prevention Plan to address water quality and run off impacts, respectively. The creation of special conditions to address water quality and water runoff is unnecessary due to the size of the proposed project and given that the City's standard conditions of approval adequately address hydrology and water quality impacts. Further, the submittal of and City approval of a hydrology study prior to consideration of a project is not appropriate. Prior to issuance of a grading permit, the applicant will be required to submit a Storm Water Pollution Prevention Plan and hydrology study for City review and approval.

Staff also received four letters supporting the project. Two letters were received from Channel Islands High School (Dr. Rocky Valles, Principal and Mark Contreras, Assistant Principal, Facilities and Campus Supervision) supporting alcohol sales associated with the Tesco project provided they comply with the conditions of approval imposed by the Police Department. Two other letters of support were also received from local residents (Lisa Chavez and Nancy Brown) (see Attachment C).

6) **Analysis:**

- a. **General Discussion:** The proposed Tesco (Fresh and Easy) grocery store would operate from 7:00 a.m. to 11:00 p.m. Monday through Sunday. A condition of approval is recommended prohibiting the sale of alcohol from 10:00 p.m. and 6:00 a.m.. The grocery store expects to employ approximately 25 people during varying shifts. Sale of alcohol for off-site consumption (Type 21) is proposed as part of the

grocery operation. The Police Department has reviewed the request and determined that the potential negative impacts of alcohol sales on the project site can be effectively mitigated through the adoption of all recommended conditions of approval (see discussion in section i. of this staff report).

Adjoining the grocery store use is 5,630 square feet of speculative retail space. The applicant proposes to divide the space into five retail shops. Neighborhood serving commercial uses such as a dry cleaner, coffee shop, bagel shop, or beauty supply are anticipated to occupy these shops in the future, in compliance with the City Code.

A Tentative Parcel Map (PM) is proposed to subdivide the subject 4.05 gross acre parcel into three parcels as follows:

Parcel Number	Size	Proposed Use
1	1.51 net acres	13,929 sq. ft. Fresh and Easy Grocery Store (Tesco)
2	.66 net acres	5,630 sq. ft. retail buildings
3	1.79 net acres	Approximately 6,000 sq. ft. New Progressive Christian Missionary Baptist Church

Phase One of the development would include construction of the Tesco grocery store and retail development on Parcels 1 and 2. Phase Two would include development of the church on Parcel 3; an SUP is required for a church use and this will be presented to the Commission in the future. Other than approval of the parcel map, no discretionary permits are being contemplated on the future church parcel at this time. Therefore, there is no restriction on when the initiation of construction of the church must take place.

- b. **Relevant Project and Property History, Related Permits:** No prior development permits have been initiated on the project site.
- c. **Zoning Compliance:** With approval of the general plan amendment and zone change, the proposed project will be located within the General Commercial Planned Development (C-2-PD) zone district. Applicable development standards of the C-2-PD zone have been compared with the proposed project, as follows:

DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	COMPLIES?
Max. building height	Two stories @ 35 feet	22 feet, clarify measurement from nearest adjacent public street	YES
Front yard setback ¹	10 feet along Rose Ave.	For Parcel 1:	YES

¹ The project is located on the corner and therefore is considered to have two front yards (Rose Ave. and Channel Islands Blvd.)

	and Channel Islands Blvd.	<ul style="list-style-type: none"> • 70 feet on Rose Avenue • 142 feet on Channel Islands Blvd. For Parcel 2: <ul style="list-style-type: none"> • 272 feet on Rose Avenue • 142 feet on Channel Islands Blvd. 	
Side yard setback	A five foot setback is required in the C2 zone No setback is required where a lot zoned general commercial abuts another lot so zoned.	For Parcel 1: <ul style="list-style-type: none"> • 40 feet For Parcel 2: <ul style="list-style-type: none"> • 100 feet 	YES
Rear yard setback (Cota Circle)	10 foot setback alley required since abuts an alley	For Parcel 1: <ul style="list-style-type: none"> • 80 feet For Parcel 2: <ul style="list-style-type: none"> • 10 feet 	YES
Parking areas landscape requirements (S. 16-641)	<ul style="list-style-type: none"> ➤ Min. 10 landscape strip parallel to street ➤ Parking areas... 36 inch landscape berm or a combo wall/landscaping ➤ Backflow systems screened by landscaping ➤ 15 percent of the lot area landscaped ➤ One tree for every 40 feet of property line 	<ul style="list-style-type: none"> ➤ 20 feet on Channel Islands; and 16 feet 6 inches on Rose Ave. ➤ 18% landscaping 	YES
Parking	1 space per 250 square feet = 79 parking spaces 1 motorcycle space	90 spaces	YES
Lighting	Comply with City Code	Shoe box type; metal halide	YES
Bicycle Rack	None required; future uses may require bicycle spaces	1 provided within concreted sidewalk area out front of the entrance of grocery store; 1 near smaller retail space	YES, to be conditioned
Parking Lot Striping	City Standard	9' x 19'	YES
Loading Zone	Two loading and unloading spaces for total building sq. ftg. Spaces to be 12' X 40' Cross Hatched 'No Parking'	Two loading and unloading space for total sq. ftg. 12' x 40' for Tesco	YES

d. **Site Design Analysis:** As proposed, the site layout accommodates potential concerns of the adjacent residential uses to the west. A six foot tall capped screen wall to match the existing wall along Cota Circle will provide a visual and vehicular access

barrier. Pedestrian paths on-site provide connections to the surrounding community. The design provides a combination of colored and scored concrete pathways to denote the pedestrian path of travel. Colored and scored concrete will be provided at ingress and egress points on Channel Islands Boulevard and Rose Avenue. Care has been taken to provide an access point south to Channel Islands High School. In addition to a pedestrian path of travel on site, sidewalks will be provided along Channel Islands Boulevard and Rose Avenue. In addition to the landscape improvements along Channel Islands Boulevard as part of Phase 1 of the project, full sidewalk improvements will occur along the entire frontage along Rose Avenue; full sidewalk and parkway improvements will also occur along Channel Islands Boulevard. The Tesco grocery store will function as the anchor tenant for this center.

The buildings on Parcel 1 and 2 have been designed so that they appear as one seamless building frontage. Loading and unloading and refuse collection would occur in the rear of the buildings. Truck deliveries would occur via Rose Avenue and Raider's Way. The shopping center will be called the shops at College Park and pylon signs will be placed at ingress and egress points on Channel Islands Boulevard and Rose Avenue. All lighting has been designed to comply with City standard condition of approval.

- e. **Circulation and Parking Analysis:** Access into the project will occur from Channel Islands Boulevard and Rose Avenue. Access into the project will not occur from Cota Circle. Enhanced paving at these driveway locations will provide added aesthetic appeal and richness to the shopping center. A 6-foot screen wall will provide a visual and vehicular access barrier between the proposed retail use and the existing residential use to the west.

The parking requirements for the project have been calculated based upon the neighborhood parking standard which is one parking space per 250 sq. ft. of gross floor area. A total of 79 parking spaces are required for the project and 90 parking spaces are provided. A reciprocal parking and access agreement will be required on all three parcels. The number of parking spaces required for the project is in excess of the code requirement as follows:

Parcel Number	Sq. Ftg./Use	Required No. of Spaces (1 per 250 sq. ft.)	Provided No. of Spaces
1	13,929 sq. ft. – Tesco	55 parking spaces	59 parking spaces
2	5,630 sq. ft. – Neighborhood Retail	23 parking spaces	31 parking spaces
Total	19,559 sq. ft.	78 parking spaces	90 parking spaces

In order to complete the environmental document on the project, a traffic study was completed (see Attachment C, Traffic Study within the IS/MND). The traffic study

evaluated the traffic impacts associated with development on Parcels 1 through 3. The traffic study included the future construction of a 6,000 square foot church on Parcel 3. The Traffic Study prepared by AllianceJB, Incorporated on November 7, 2007, concluded that the additional traffic generated by the project would not create significant impacts. The City Traffic Engineer reviewed the applicant's traffic study and supports the study's findings of no significant traffic impact associated with the development on Parcels 1 through 3.

The traffic study projected that the project will generate 966 Average Daily Trips (ADTs) based upon ITE land codes for a Supermarket, Specialty Retail, and Church. The study determined that the project would generate 27 AM peak hour trips and 87 PM peak hour trips. The traffic study evaluated seven intersections for traffic analysis. These intersections were: Rose Avenue / Wooley Road; Rose Avenue / Oxnard Boulevard; Rose Avenue / Channel Islands Boulevard; Rose Avenue / Raiders Way; Rose Avenue / Bard Road; Statham Boulevard / Channel Islands Boulevard; and Rice Avenue / Channel Islands Boulevard. The traffic study concluded that all of the analyzed intersections currently operated at an acceptable Level of Service (LOS), which is considered the range of A through C, during the AM and PM peak hours under existing conditions. All of the analyzed intersections would also operate at an acceptable LOS (A-C) during the AM peak hour under existing plus approved/pending development conditions. The projects would not cause any project-specific impacts (as identified by a change in the volume capacity ratio of 0.02 or more at LOS C-F). Therefore, the proposed projects are consistent with City policy and are not considered to negatively impact the City's transportation network. The results from the traffic study indicate the projects would not cause any year 2030 impacts (as identified by a change in the V/C ratio of 0.02 or more at LOS C-F). Therefore, there are no negative cumulative impacts associated with the projects and the projects are consistent with City policy thereby not negatively impacting the City's transportation network.

- f. **Building Design Analysis:** The proposed building is a contemporary stucco building with modern-style detailing, including metal roof elements and metal lattice work compatible with the other commercial buildings to the north of this shopping center. The building has a warm color palette of creams, tan and green. An angled metal roof element with suspension canopy above the entrance to the Tesco grocery store will provide a focal point on the building and denote a sense of arrival. Cart storage will be provided behind a low smooth face clock wall in front of the store facing Channel Islands Blvd. The metal suspension canopies are repeated along the frontage of the smaller retail spaces. Stone veneer along the base of the entire northerly building frontage will enhance the building elements. Glass storefronts will be accentuated by the placement of columns and canopy roof elements. All building elevations have architectural treatment and detailing.

The westerly elevation facing Cota Circle has been designed without windows but has metal lattice detailing, and suspension canopy roof detailing. The loading dock faces Cota Circle but will be behind the six-foot tall screen wall. A condition of approval is recommended requiring that the loading and unloading door for Tesco be closed while not in use and recommending restrictions on hours of deliveries for all of the uses on the site.

A mural along the easterly building frontage of Tesco is proposed to be constructed by Channel Islands High School and will provide an interesting street scene along Rose Avenue.

The southerly building elevation will face the future church use on Parcel 3. Building elements of stucco, stone veneer, and columns will break up this building elevation. The future construction of the church will screen the retail use from the high school to the south. The architectural detailing of the southerly elevation has been designed to be acceptable should the church not be constructed in the future.

The design of the trash enclosure is in compliance with the City code and is consistent with the modern architectural design of the building. Decorative exterior lighting fixtures on all elevations are consistent with the architectural style. The City's architectural consultant and the applicant worked together to improve the overall appearance of the building, and staff recommends approval of the resulting proposed design.

- g. **Signs:** A three foot tall capped garden wall will be placed parallel to Channel Islands Boulevard and Rose Avenue. This wall will provide center identification and improve the aesthetic appearance from the adjacent roadway by improving the street scene with landscaping and accent garden walls. A 12 foot 6 inch tall pylon sign is proposed at the ingress and egress points along Rose Avenue and Channel Islands Blvd. The angles and colors associated with the sign are consistent with the building. Channel lettering is proposed on the face of the signs. Additionally, a low ground center monument sign three feet in height is proposed on the corner of Channel Island Boulevard and Rose Avenue. Building signage for Tesco is proposed along the northerly and easterly building elevations. Signage associated with the in line retail space is also proposed above the suspension canopies. A condition of approval has been imposed requiring Planning Department approval of the Master Sign Program for the neighborhood shopping center. The total amount of sign area proposed is less than the total amount of sign area allowed by the Municipal Code.
- h. **Landscaping & Open Space Compliance:** Landscaping parallel to Channel Islands Boulevard and Rose Avenue will be provided along with landscaping within the parking lot. As indicated in the Zoning Compliance section, the project meets all applicable landscape standards. Over 18% of the site will be landscaped. Lush landscaped parkways with accent walls will be provided along Rose Avenue, Channel Islands Boulevard and Cota Circle. Significantly sized trees, 36 and 24

inches, respectfully will be planted within the parkway frontage along the Channel Islands Boulevard and Rose Avenue. One specimen tree to be 48 inch in size will be planted at the corner of Channel Islands Boulevard and Rose Avenue. Significantly sized trees and landscaping will be provided along Cota Circle. The design of the landscaping planters, and overall landscape design is consistent with the City code.

- i. **Alcohol Sales:** In accordance with City requirements, an over concentration report was prepared by the Police Department (see Attachment C). This report provided a summary of alcohol outlets in proximity of the site as follows:

Alcohol outlets located within 350 feet of the proposed site include:

BUSINESS NAME	LOCATION	LICENSE TYPE	LICENSE TITLE	BUSINESS TYPE	ALCOHOL ALLOWED
1. ARCO AM/PM	2850 S. Rose Ave.	Type 20	Off-Sale Beer and Wine	Gas Station / Market	Beer and Wine

Alcohol outlets located within 1000 feet of the proposed site include:

BUSINESS NAME	LOCATION	LICENSE TYPE	LICENSE TITLE	BUSINESS TYPE	ALCOHOL ALLOWED
1. Sharks Grill	2701 S. Rose Ave.	Type 41	On-Sale Beer and Wine (food)	Restaurant	Beer and Wine
2. Tacos Mi Pueblo	2701 S. Rose Ave.	Type 41	On-Sale Beer and Wine (food)	Restaurant	Beer and Wine

The applicant is requesting approval of a Type 21 off-site alcohol consumption permit. The report concluded that the crime statistics within 1,000 feet of the Tesco site were 8.5% higher than the average crime rate citywide, but that this was not considered significant. The Police Department’s primary concern was the proximity of the Tesco site to Channel Islands High School. The school is approximately 350 feet south of the proposed site and the Police Department is of the opinion that the shopping center is likely to be a popular destination for students before and after school. The proposed use was discussed with the Principal at Channel Islands High School (Dr. Rocky Valles) who, in turn, shared the information with several groups including student organizations, boosters, Parent Faculty Organization (PFO) and school staff. Principal Valles concluded that, while there were some risks associated with the use, they were not opposed as long as there were strict conditions to minimize problems. The statistical analysis showed that the area had a crime rate that

was slightly higher than the city-wide average and that disturbance-related calls are also above average; however, the area is generally not considered to be a policing problem and the addition of an Off-Sale alcohol outlet should not significantly aggravate existing issues.

There is one similar use within 350 feet of the proposed site so there is an issue of undue concentration by local standards. The presumption of undue concentration has been rebutted by a preponderance of evidence, which shows that the establishment is in a retail center where restaurant establishments selling alcoholic beverages for consumption on the premises are appropriate. The guidelines for the Department of Alcoholic Beverage Control (ABC) use census tract populations to determine the "allowable" number of alcohol outlets in a particular census tract. In this case, the census tract surrounding this proposal allows for five Off-Sale Outlets. Currently, there is only one Off-Sale license in the tract so there is no undue concentration by state standards.

According to the Police Department, the proximity of the Tesco project site to Channel Islands High School and to another similar use are the only significant issues to consider. The Police Department believes that the potential negative impacts of this use can be effectively mitigated through the adoption of all of the standard and special conditions of approval identified in the Planning Commission SUP resolution. It is the experience of the Police Department that similar uses, when responsibly operated and adequately conditioned, do not typically increase policing problems.

- 7) **Development Advisory Committee (DAC) Consideration:** The Development Advisory Committee (DAC) reviewed this project on April 4, October 3, and December 5, 2007. The applicant has addressed the comments received by DAC and therefore, DAC members have prepared conditions of approval for the project.
- 8) **Community Input:** A Community Workshop took place on the project on January 28, 2008. Members of the College Park and Estates, and Channel Pointe Community were notified of the community meeting. The majority of the comments received at this meeting were in support of the project. A few members of the public asked questions regarding the type of alcohol sold on the project site, hours of operation, and how alcohol would be secured to minimize underage consumption. Proximity to Channel Islands High School was raised as a potential concern. Members from the school district did not attend nor have they sent letters in objection to the project.

PZ 07-500-14 (Tesco)

Planning Commission Date: March 20, 2008

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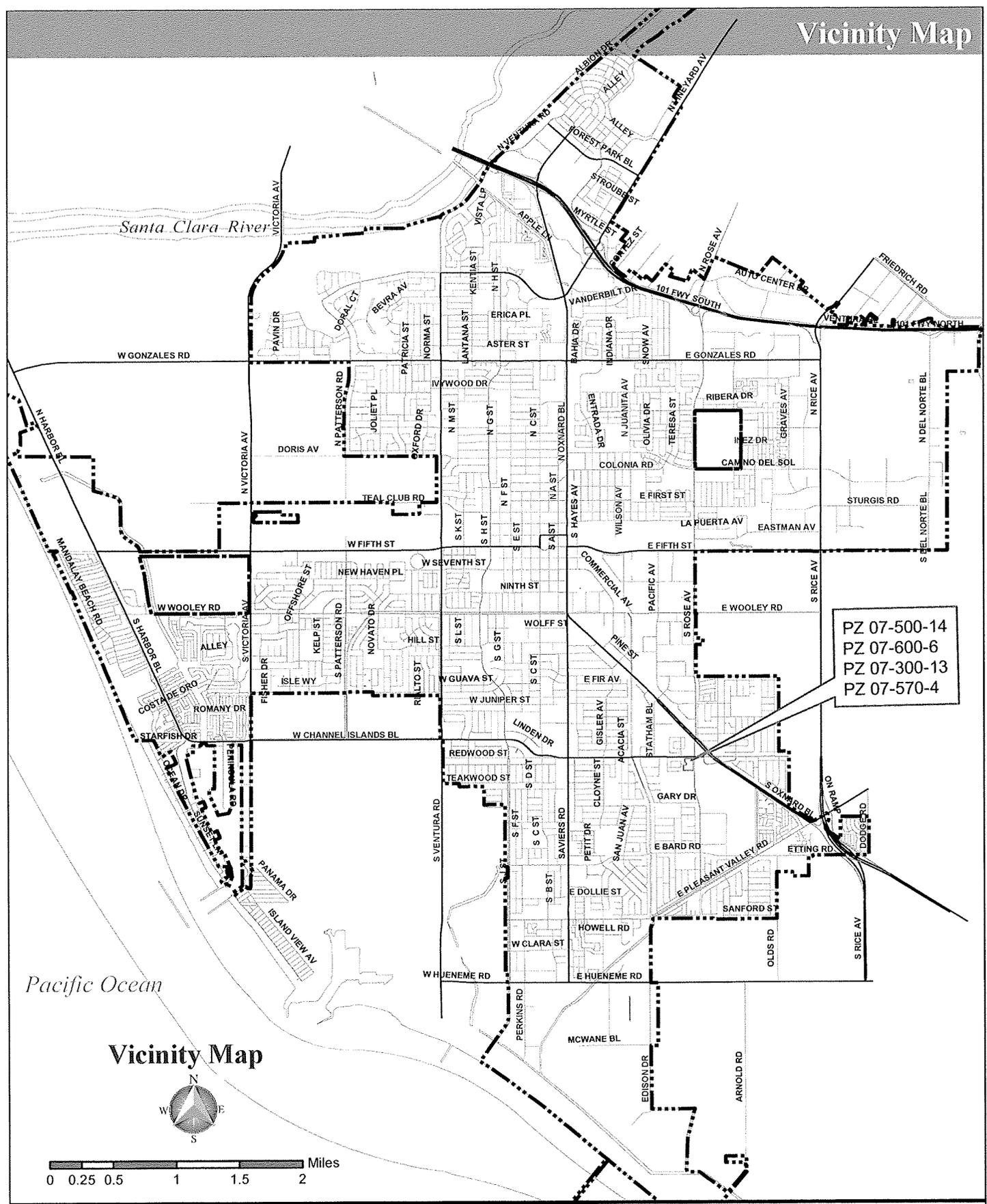
9) Attachments:

- A. Maps (Vicinity, General Plan, Zoning)
- B. Reduced project Plans & Tentative Map
- C. MND No. 07-11, with comment letters
- D. Police Department memorandum
- E. Resolutions

Prepared by: <u> KM </u> KM
Approved by: <u> SM </u> SM

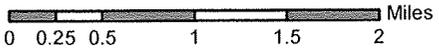
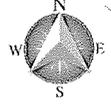
ATTACHMENT A

Vicinity Map



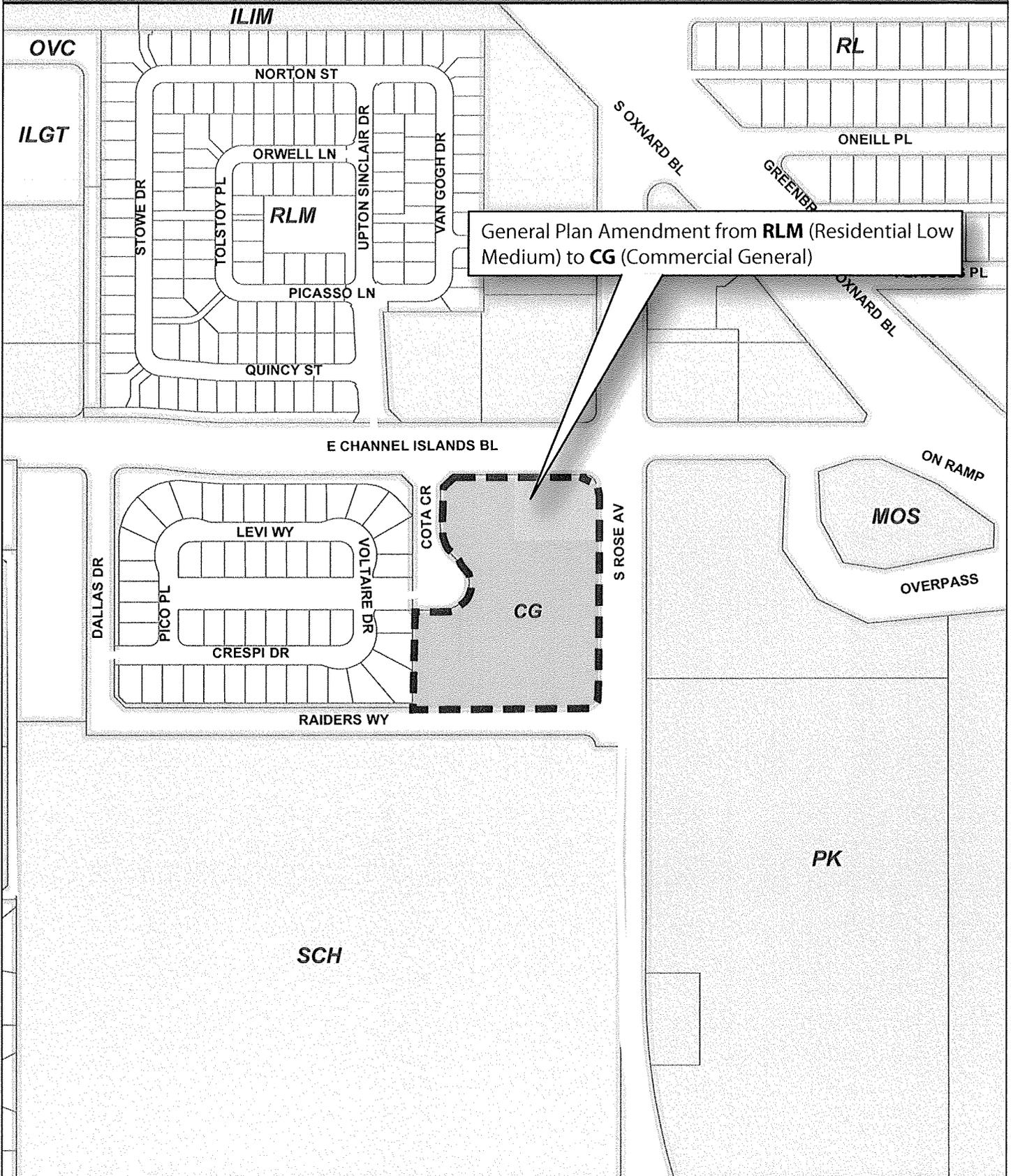
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 PZ 07-600-6
 PZ 07-300-13
 PZ 07-570-4

Vicinity Map



PZ 07-500-14 PZ 07-600-6
 PZ 07-300-13 PZ 07-570-4
 Location: SWC Rose Av & Channel Islands Bl
 APN: 221023251
 Churchyard Dev., LLC

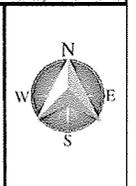
General Plan Amendment



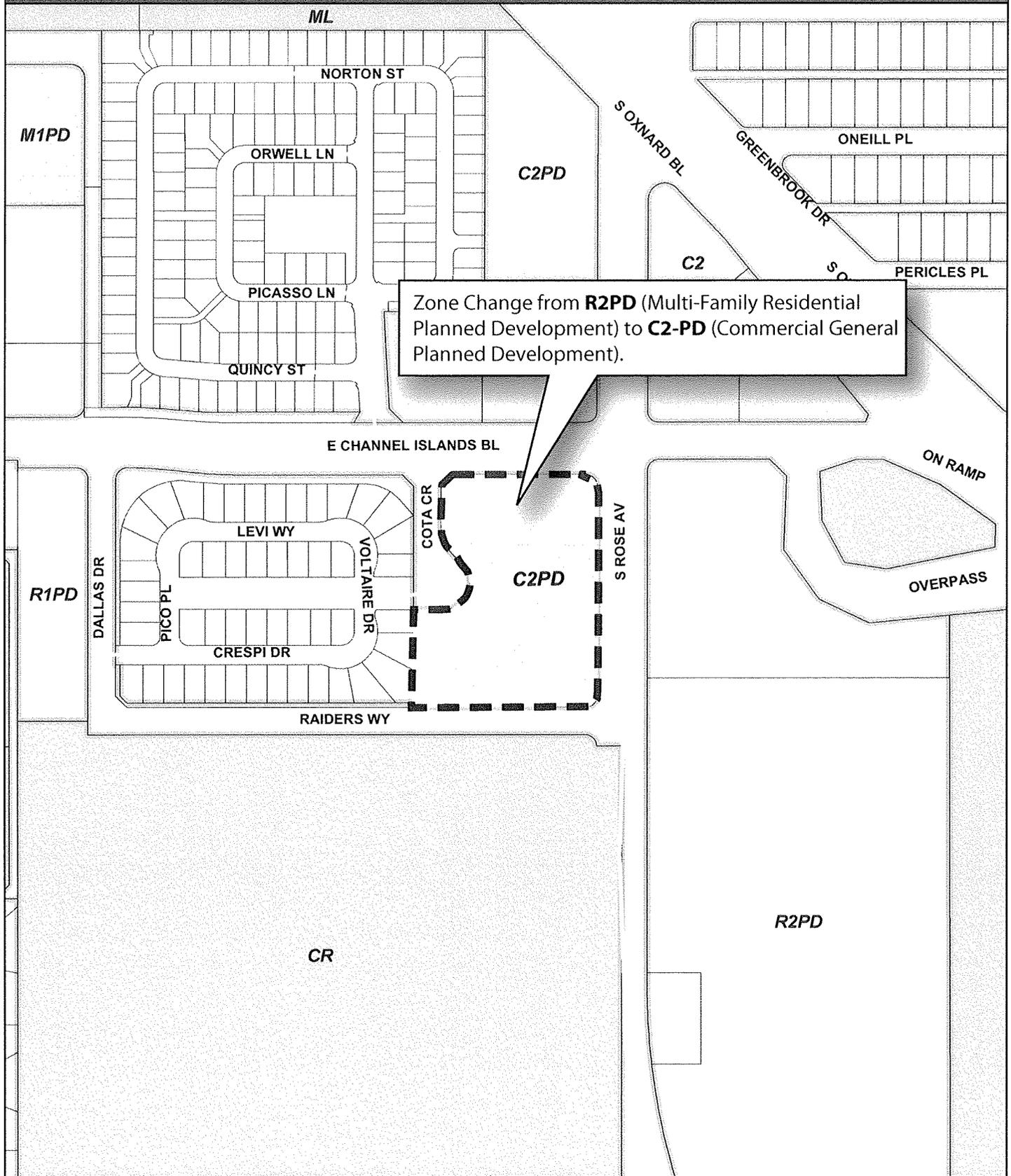
PZ 07-500-14 PZ 07-600-6
PZ 07-300-13 PZ 07-570-4
Location: SWC Rose Av & Channel Islands Bl
APN: 221023251
Churchyard Dev., LLC

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General Plan Amendment

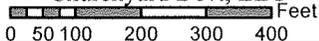


Zone Change

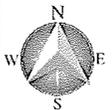


Oxnard Planning
August 23, 2007

PZ 07-500-14 PZ 07-600-6
PZ 07-300-13 PZ 07-570-4
Location: SWC Rose Av & Channel Islands Bl
APN: 221023251
Churchyard Dev., LLC



Zone Change



ATTACHMENT B



Heathcote & Associates Architecture
3336 Willow Lane
Village Center
Suite 200
Folsom, CA 95630
Phone 916-437-4700

DATE: 10/11/07
T3
LOADING EXPLANATION SHEET

ROSE OXNARD CENTER
ROSE AVE & GAMMEL (LANE) BLVD
OXNARD, CA

DYNAMIC AND COMPLEMENTARY
OCTOBER 2007
JOB NO. 07-001
DATE 10/11/07

REVISIONS

STEEL SQUARE POLES

GENERAL NOTES:

1. ALL SQUARE POLES SHALL BE FABRICATED FROM A36 STEEL.
2. ALL SQUARE POLES SHALL BE GALVANNEAL COATED TO MEET THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
3. ALL SQUARE POLES SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
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SECTION 05110 - STRUCTURAL STEEL

STEEL SQUARE POLES

1. SUMMARY

- A. Section Includes:
 - 1. Steel square poles

2. REFERENCES

- A. AISC 360 - Specification for Structural Steel Buildings

3. RELATED SECTIONS

- A. Foundations

4. NOTES

1. ALL SQUARE POLES SHALL BE FABRICATED FROM A36 STEEL.
2. ALL SQUARE POLES SHALL BE GALVANNEAL COATED TO MEET THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
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GREENBRIAR - FLAT LENS

GENERAL NOTES:

1. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED FROM A36 STEEL.
2. ALL GREENBRIAR FLAT LENS SHALL BE GALVANNEAL COATED TO MEET THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
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SECTION 05110 - STRUCTURAL STEEL

GREENBRIAR - FLAT LENS

1. SUMMARY

- A. Section Includes:
 - 1. Greenbriar flat lens

2. REFERENCES

- A. AISC 360 - Specification for Structural Steel Buildings

3. RELATED SECTIONS

- A. Foundations

4. NOTES

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STEEL SQUARE POLES

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SECTION 05110 - STRUCTURAL STEEL

STEEL SQUARE POLES

1. SUMMARY

- A. Section Includes:
 - 1. Steel square poles

2. REFERENCES

- A. AISC 360 - Specification for Structural Steel Buildings

3. RELATED SECTIONS

- A. Foundations

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GREENBRIAR - FLAT LENS

GENERAL NOTES:

1. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED FROM A36 STEEL.
2. ALL GREENBRIAR FLAT LENS SHALL BE GALVANNEAL COATED TO MEET THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
3. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
4. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
5. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.

SECTION 05110 - STRUCTURAL STEEL

GREENBRIAR - FLAT LENS

1. SUMMARY

- A. Section Includes:
 - 1. Greenbriar flat lens

2. REFERENCES

- A. AISC 360 - Specification for Structural Steel Buildings

3. RELATED SECTIONS

- A. Foundations

4. NOTES

1. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED FROM A36 STEEL.
2. ALL GREENBRIAR FLAT LENS SHALL BE GALVANNEAL COATED TO MEET THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
3. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
4. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.
5. ALL GREENBRIAR FLAT LENS SHALL BE FABRICATED TO THE REQUIREMENTS OF THE AISC 360 SPECIFICATION.

ROSE OXNARD CENTER
ROSE AVE & GAMMEL (LANE) BLVD
OXNARD, CA

HEATHCOTE & ASSOCIATES ARCHITECTURE
3336 WILLOW LANE
VILLAGE CENTER
SUITE 200
FOLSOM, CA 95630
PHONE 916-437-4700

DATE: 10/11/07
T3
LOADING EXPLANATION SHEET

REVISIONS

DRAWING INFO
CONTRACT TITLE
DATE
DRAWN BY
CHECKED BY
SCALE
DATE
PROJECT NO.

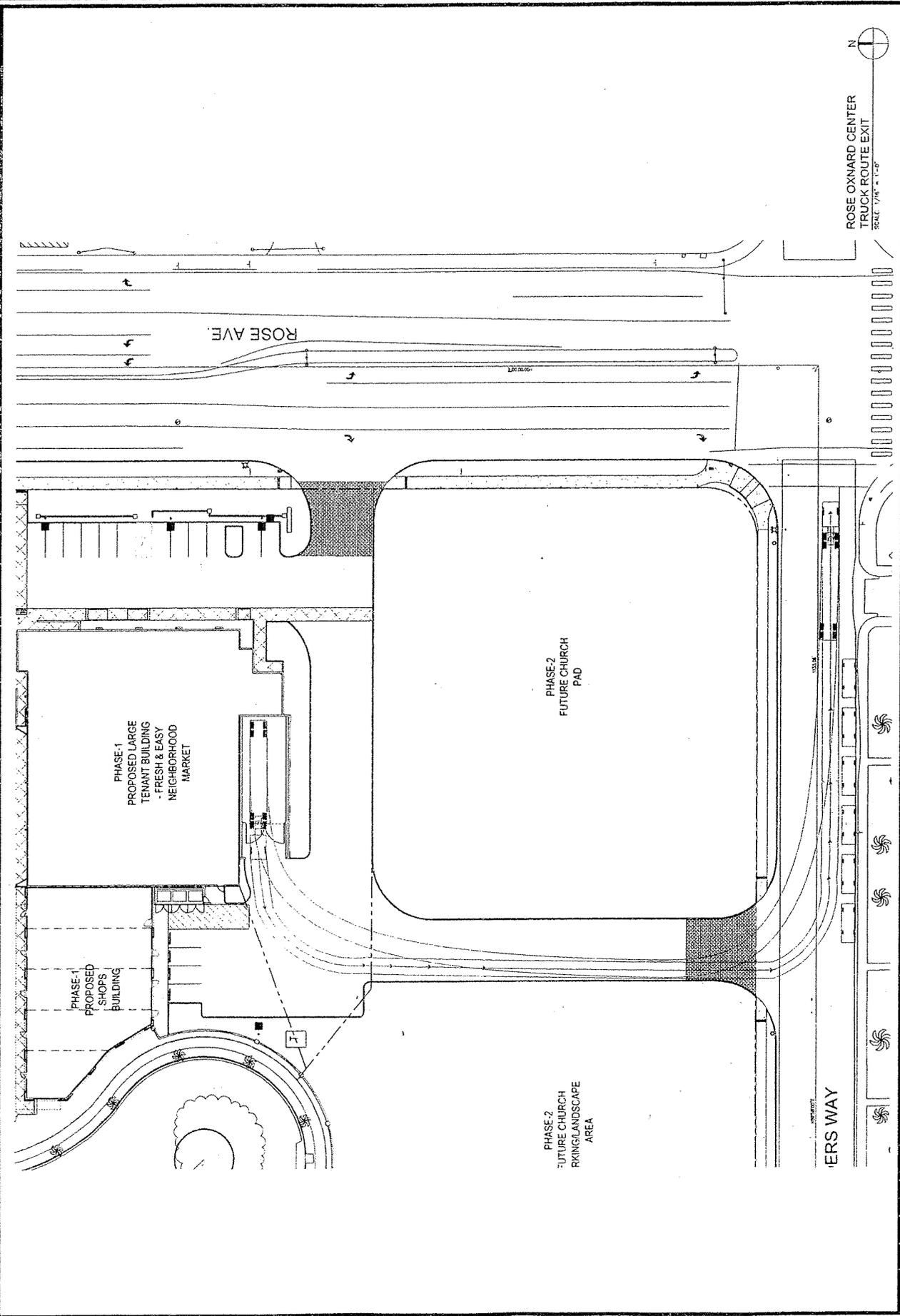
ROSE AVE & CHANNEL ISLANDS BLVD
OXNARD, CA

ROSE OXNARD CENTER



Healthcote & Associates
Architecture
3356 Willow Lane
Oxnard, CA 93030
Cellphone: 805-497-4700
Phone: 805-497-4700

SHEET
T5
TRUCK ROUTE



ROSE OXNARD CENTER
TRUCK ROUTE EXIT
SCALE: 1/8" = 1'-0"

TRUCK EXIT ROUTE

REVISION

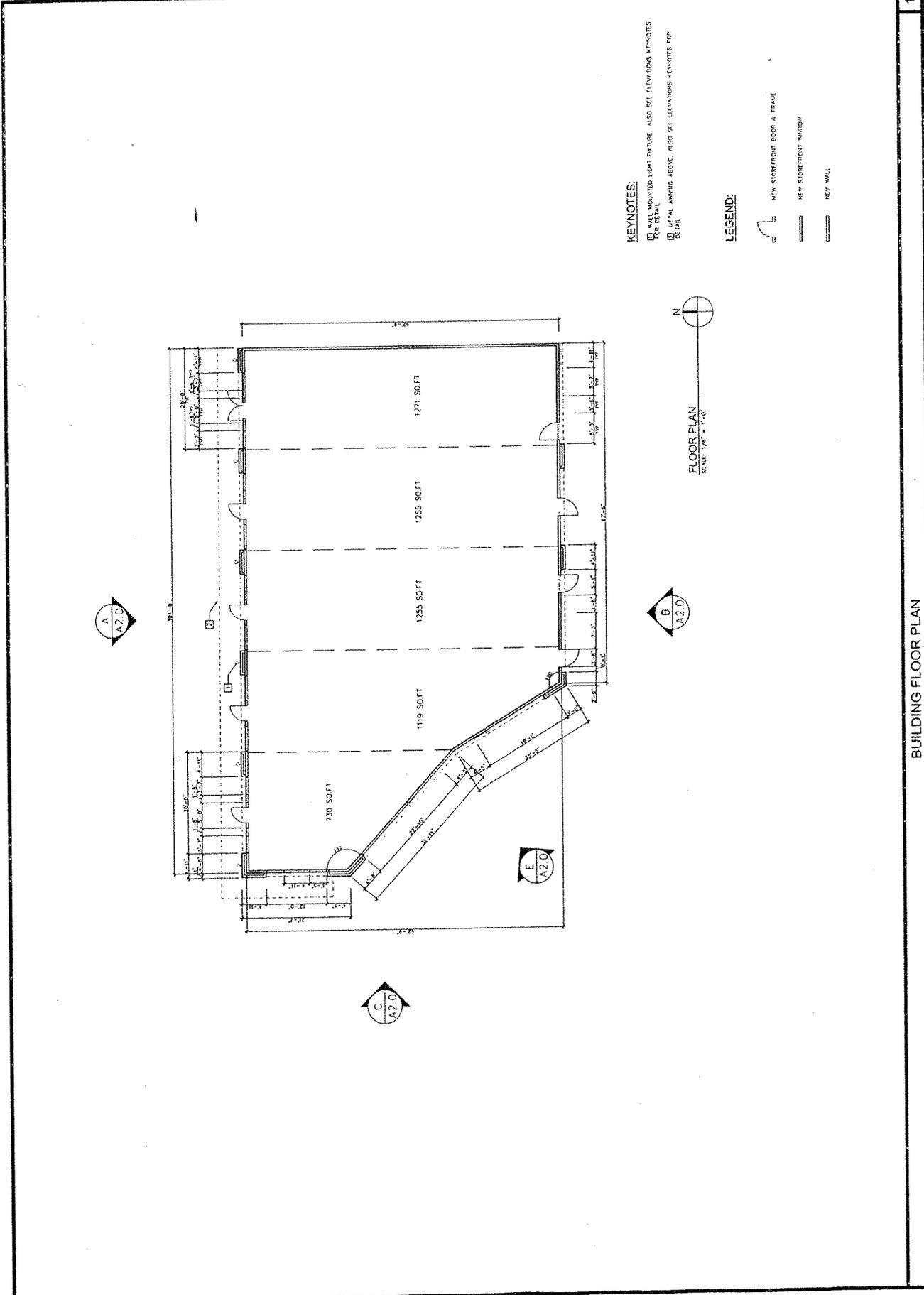
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COMPONENT	TEXT
BY	
DATE	11/13

ROSE OXNARD CENTER
 OXNARD, CA
 ROSE AVE & CHANNEL ISLANDS BLVD



Heathcote
 Architecture & Associates
 3396 Willow Lane
 Westlake, California, Suite 200
 Phone 805-497-4700

PRELIMINARY NOT FOR CONSTRUCTION
 SHEET
 A1.1
 FLOOR PLAN



KEYNOTES:
 1. WALL MOUNTED LIGHT FIXTURE. ALSO SEE ELEVATIONS KEYNOTES FOR DETAIL.
 2. CEILING LIGHT FIXTURE. ALSO SEE ELEVATIONS KEYNOTES FOR DETAIL.
 3. WALL MOUNTED LIGHT FIXTURE. ALSO SEE ELEVATIONS KEYNOTES FOR DETAIL.

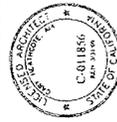
LEGEND:
 [Symbol] NEW STOREFRONT DOOR & FRAME
 [Symbol] NEW STOREFRONT WINDOW
 [Symbol] NEW WALL

FLOOR PLAN
 SCALE: 1/8" = 1'-0"

BUILDING FLOOR PLAN

Healthcote
Architecture
3396 Willow Lane
Westlake, Suite 200
California
Phone 805-491-4700

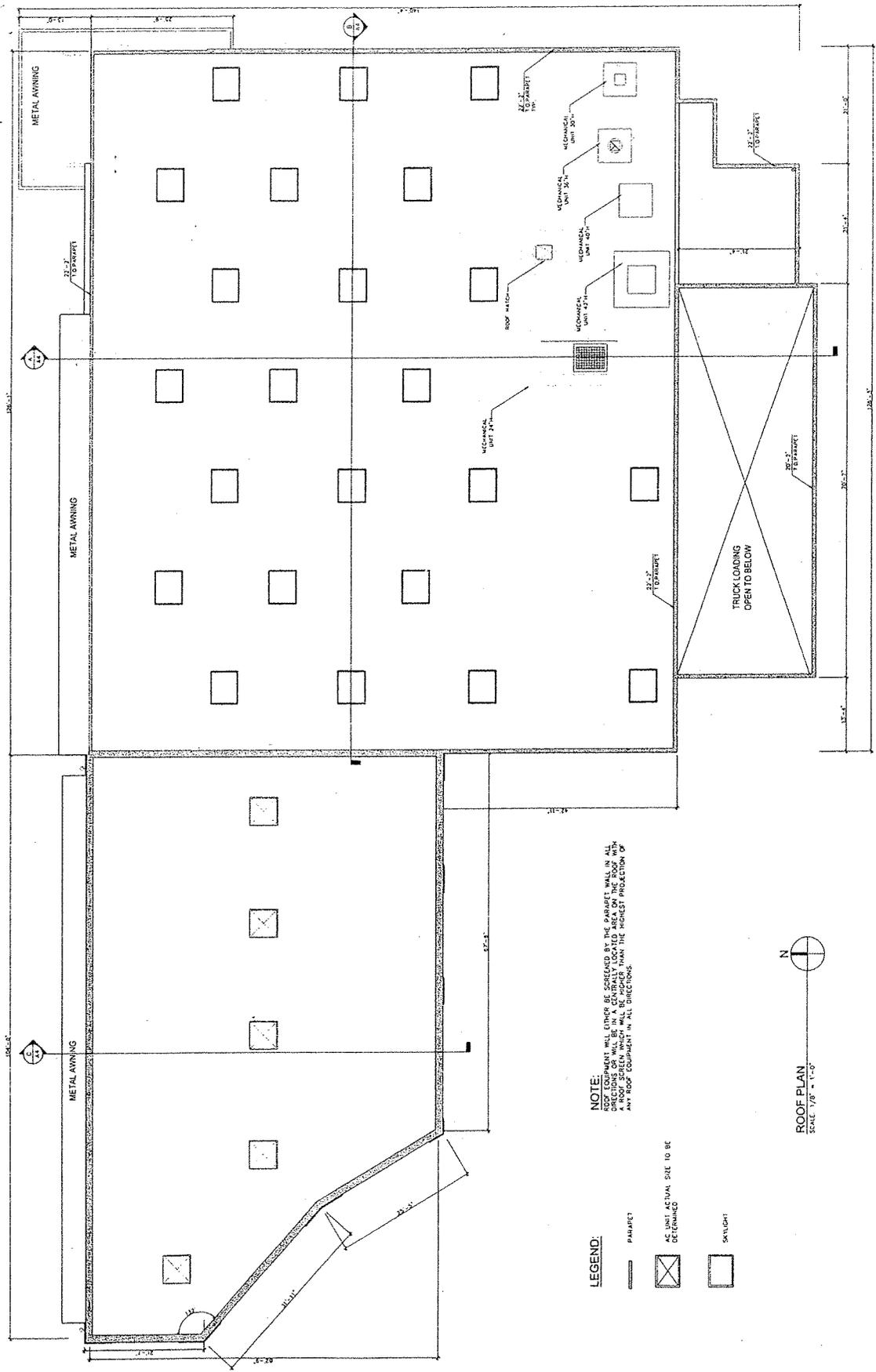
2461
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ROOF PLAN



ROSE OXNARD CENTER
ROSE AVE & CHANNEL ISLANDS BLVD
OXNARD, CA

PROJECT NO.
DATE
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FLOORS



NOTE:
ROOF ELEMENTS WILL BE SCREENED BY THE PARAPET WALL IN ALL DIRECTIONS OR WILL BE IN A CENTRALLY LOCATED AREA ON THE ROOF WITH A HEIGHT THAT EXCEEDS THAT OF ANY ROOF EQUIPMENT IN ALL DIRECTIONS.

- LEGEND:**
- PARAPET
 - AC UNIT ACTUAL SIZE TO BE DETERMINED
 - SKYLIGHT





Heathcote & Associates
Architecture
3396 Willow Lane
Westlake Village
California Suite 200
Phone 805-497-4700

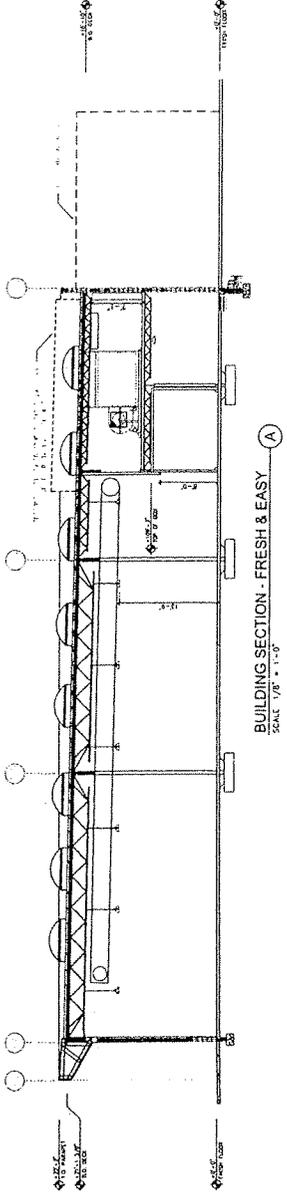


ROSE OXNARD CENTER
ROSE AVE & CHANDEL ISLANDS BLVD
OXNARD CA

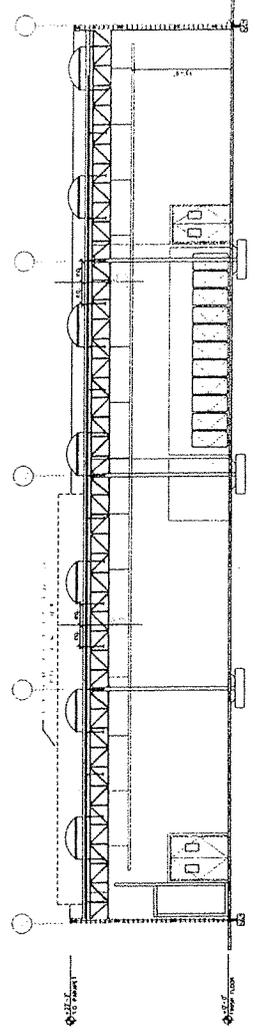
DATE: OCTOBER, 2007
SCALE: 1/8" = 1'-0"

Table with 2 columns: Description, Quantity. Includes items like 'ROOF PANELS' and 'ROOF TRUSS'.

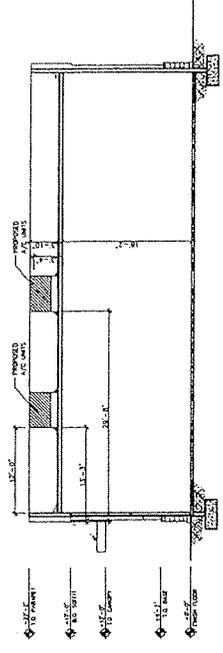
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FOR COMPANIES WILL EITHER BE SCREENED BY THE CONTRACTOR'S WILL IN ALL
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A ROOF SYSTEM WHICH WILL BE HIGHER THAN THE HIGHEST PROJECTION OF
ANY ROOF EQUIPMENT IN ALL DIRECTIONS.



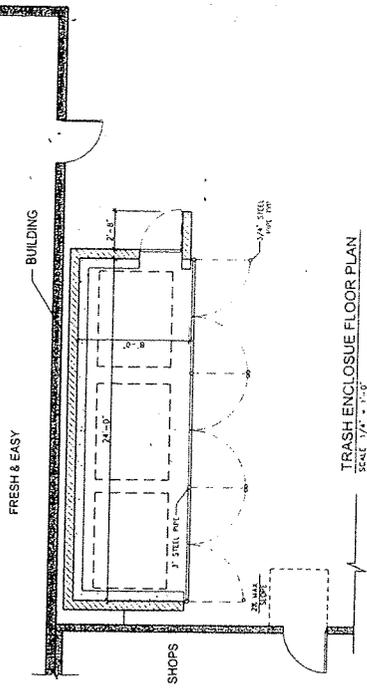
BUILDING SECTION - FRESH & EASY
SCALE: 1/8" = 1'-0"



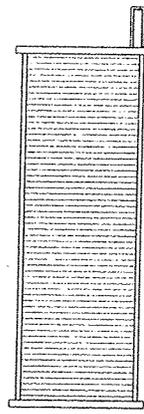
BUILDING SECTION - FRESH & EASY
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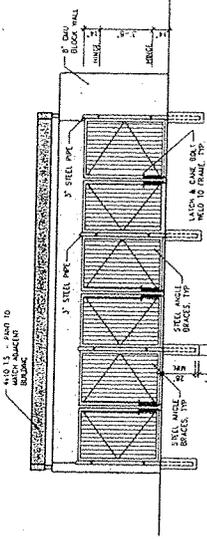
BUILDING SECTION - SHOPS
SCALE: 1/8" = 1'-0"



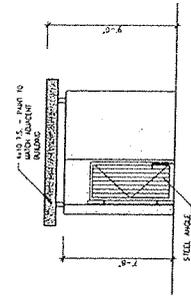
TRASH ENCLOSURE FLOOR PLAN
 SCALE 1/4" = 1'-0"



TRASH ENCLOSURE ROOF PLAN
 SCALE 1/4" = 1'-0"



TRASH ENCLOSURE FRONT ELEVATION
 SCALE 1/4" = 1'-0"



TRASH ENCLOSURE SIDE ELEVATION
 SCALE 1/4" = 1'-0"

NOTES:

1. SUBMITTING BLOCK, REINFORCED AND CONCRETE FOUNDATION TO BUILDING SAFETY DEPARTMENT.
2. ALL STEEL SHALL BE STEEL FRAMED WITH STEEL ON THE INSIDE AND THE EXTERIOR SHALL BE WOOD SHINGLES TO MATCH EXISTING BUILDING.
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Heathcote & Associates
 Architecture

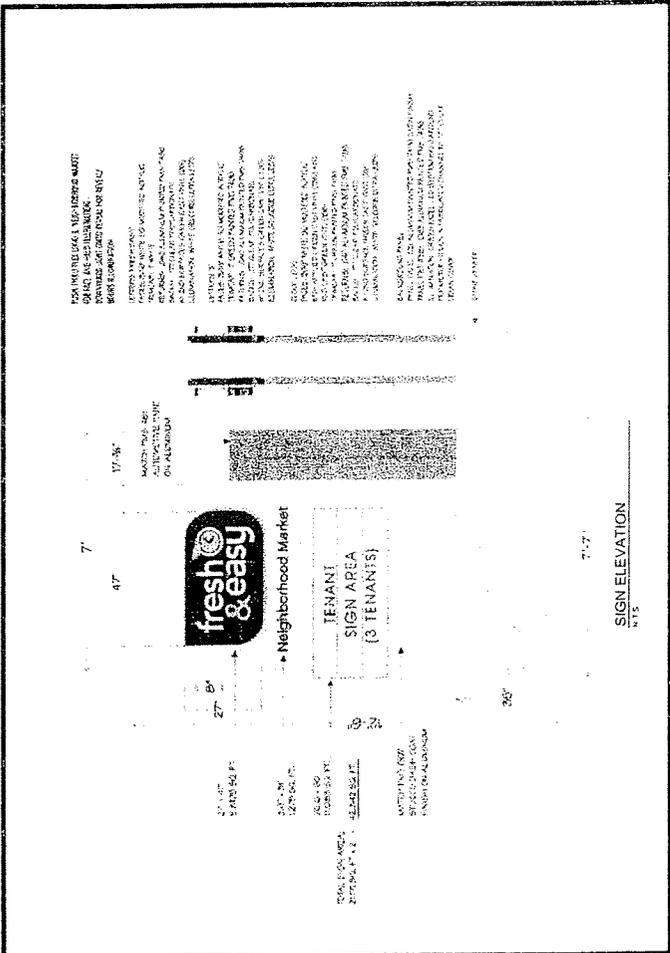
A6
 SIGNAGE & WALL ELEVATIONS
 SHEET



ROSE OXNARD CENTER
 ROSE AVE. & CHANNEL ISLANDS BLVD
 OXNARD, CA

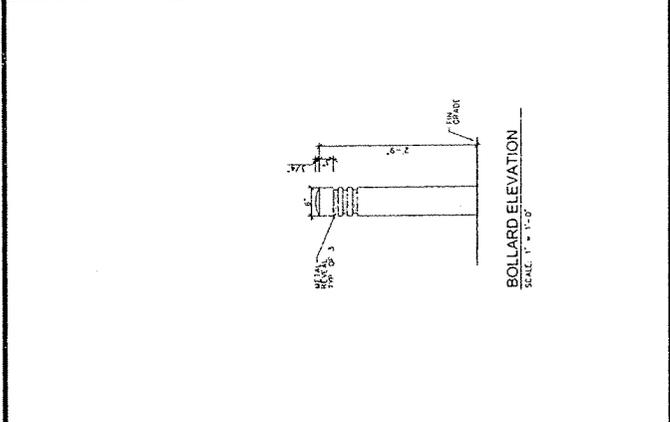
REVISIONS

DATE	10/11/07
BY	
DATE	OCTOBER 2007
BY	
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BY	

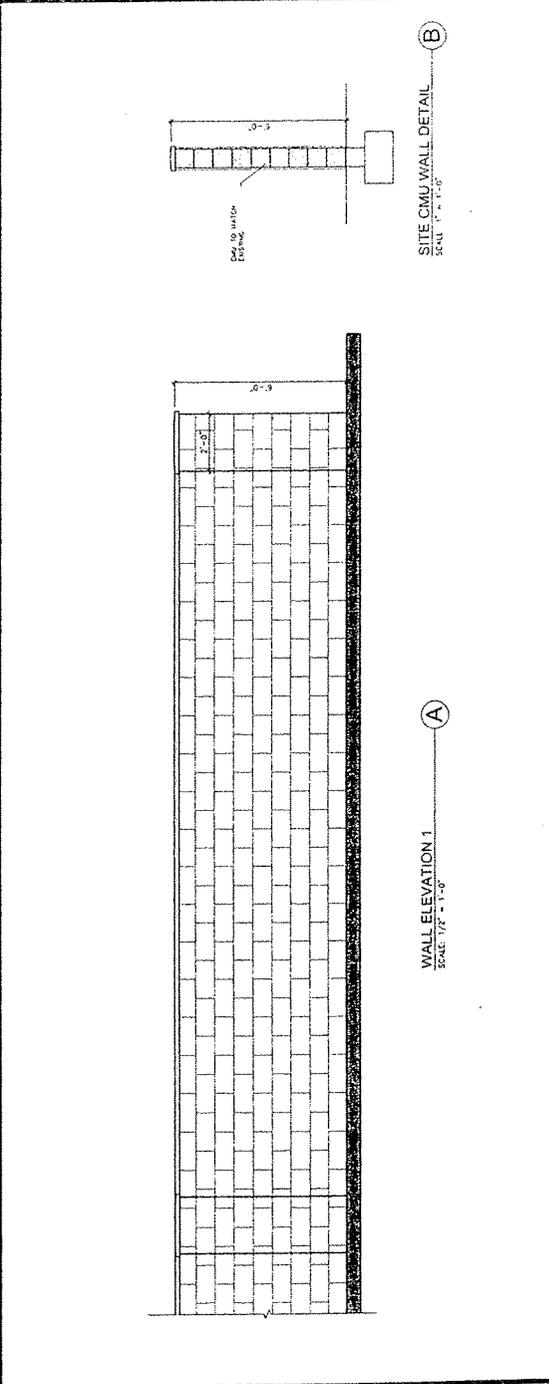


FOR INFORMATION & REFERENCE ONLY:
 CONTRACT DOCUMENTS:
 1. GENERAL CONDITIONS FOR BIDDING
 2. SPECIFICATIONS
 3. CONTRACT AGREEMENT
 4. PERMITS AND REGULATIONS
 5. LOCAL ORDINANCES AND REGULATIONS
 6. LOCAL AND STATE STATUTES
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2 SIGNAGE DESIGN



3 BOLLARD



1 WALL ELEVATION & SECTION

PRELIMINARY PLAN

ROSE OXNARD CENTER
OXNARD, CALIFORNIA

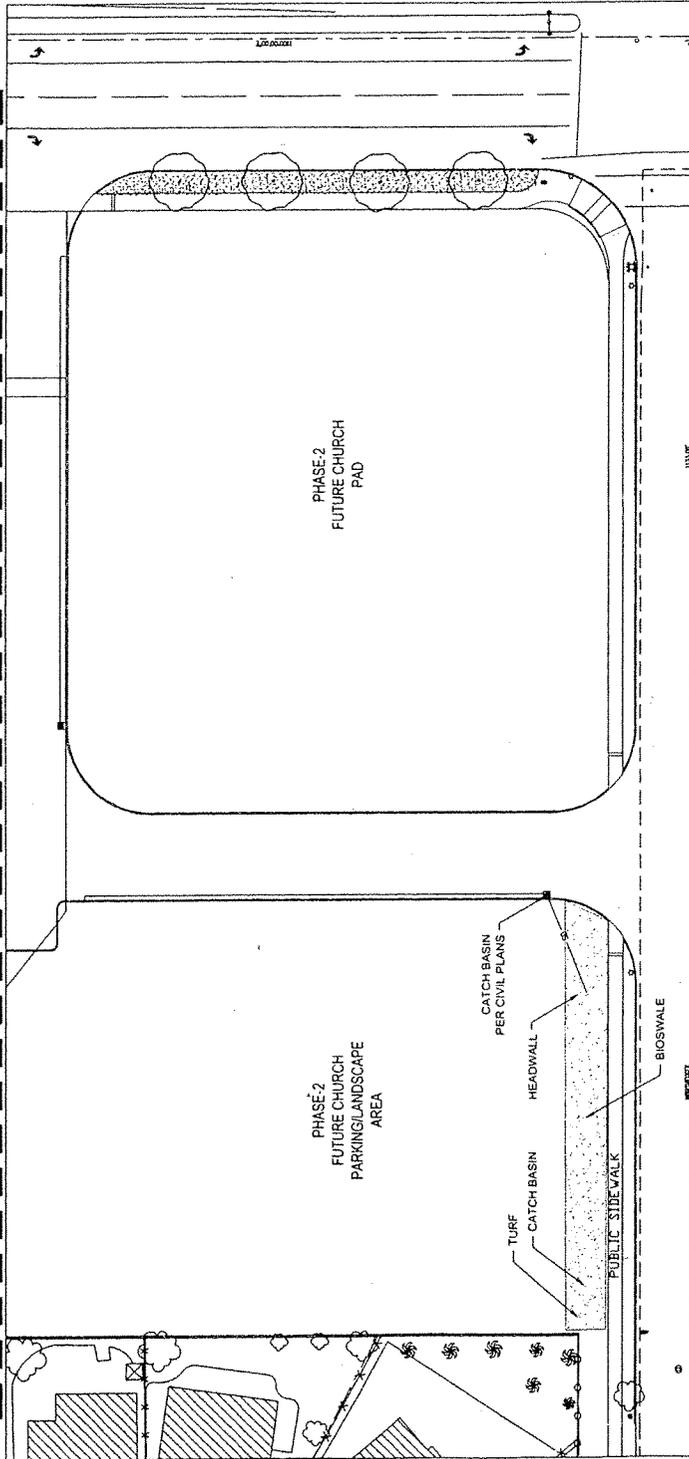
ROSE AVE. & CHANNEL ISLAND BLVD.

JAMES DEAN ARCHITECTURE
LANDSCAPE ARCHITECT / PLANNER
223 E. THOUSAND OAKS BLVD., STE. 304
THOUSAND OAKS, CALIFORNIA 91320
805-494-0823

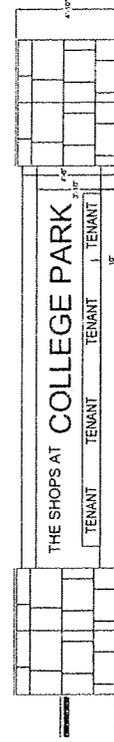
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MATCH LINE SEE SHEET 1



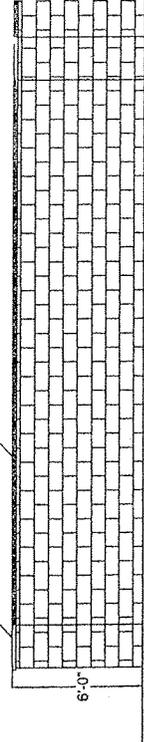
PLAN VIEW
SCALE 1" = 30'



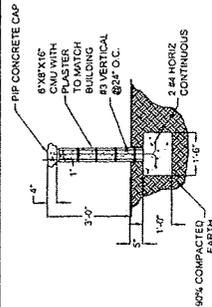
PROJECT IDENTIFICATION SIGN ELEVATION
SCALE 3/8" = 1'-0"

SCALE 3/8" = 1'-0"

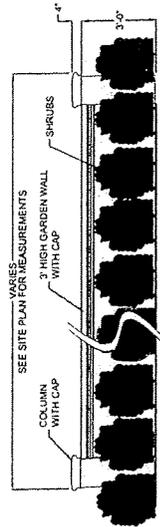
PILASTER WITH CAP
WALL WITH CAP



6' WALL ELEVATION
SCALE 3/8" = 1'-0"



GARDEN WALL DETAIL
SCALE 3/8" = 1'-0"



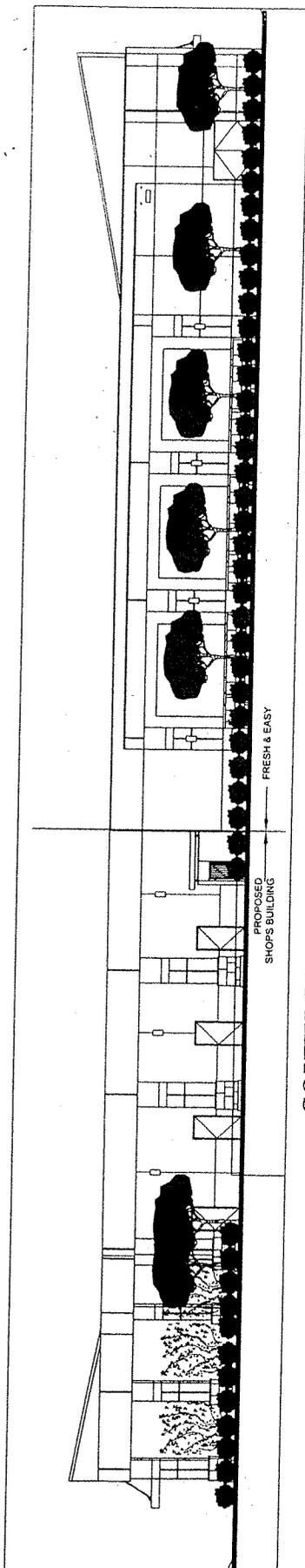
GARDEN WALL ELEVATION
SCALE 1/2" = 1'-0"

PRELIMINARY PLAN

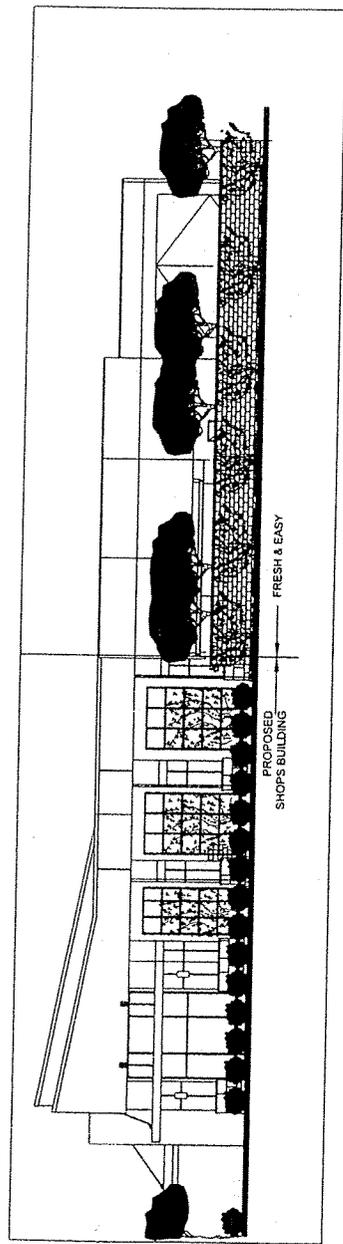
ROSE OXNARD CENTER
 OXNARD, CALIFORNIA
 ROSE AVE. & CHANNEL ISLAND BLVD.

JAMES MERIC CIVIL
 LANDSCAPE ARCHITECT & PLANNER
 232E HIGHLAND AVENUE, SUITE 304
 THOUSAND OAKS, CALIFORNIA 91320
 805.494.0623

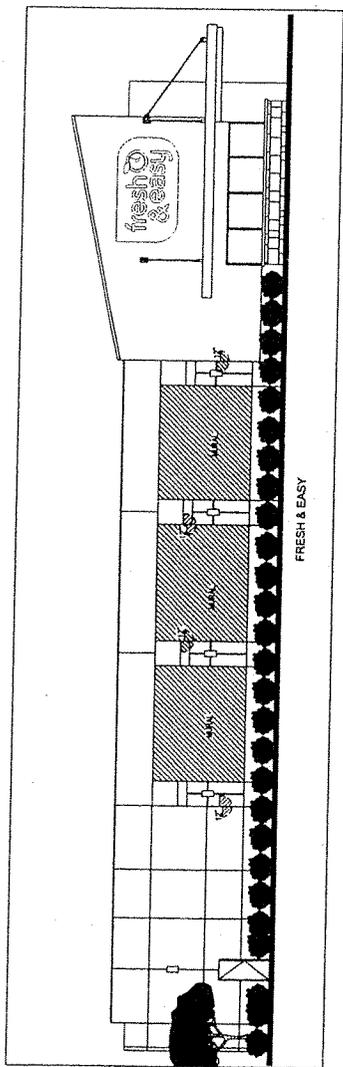
DATE: 12/10/07 3 of 3



SOUTH ELEVATIONS
 SCALE 1/8" = 1'-0"



WEST ELEVATIONS
 SCALE 1/8" = 1'-0"



EAST ELEVATIONS

ATTACHMENT C



PLANNING DIVISION
214 SOUTH C STREET
OXNARD, CALIFORNIA 93030

MITIGATED NEGATIVE DECLARATION NO. 07-11

On the basis of an initial study, and in accordance with Section 15070 of the California Code of Regulations, the Planning Division has determined that there is no substantial evidence that the proposed project may have a significant effect on the environment:

Planning and Zoning Permit Nos. 07-600-06 (GPA), 07-570-04 (ZC), 07-300-13 (TPM), 07-500-14 (SUP), and 07-500-19 (SUP). The proposed project involves a general plan amendment (GPA) to change the land use designation on the southwest corner of Channel Island Blvd. and Rose Avenue from Residential Low Medium Density to General Commercial. The zone change (ZC) is proposed to change the zoning from R-2-PD (Multiple-family) to C-2-PD (General Commercial). A tentative parcel map (TPM) is also proposed to subdivide the project area resulting in three parcels. A special use permit (SUP No. 07-500-14) is proposed for construction of a 13,929 sq. ft. Fresh and Easy grocery store (Tesco) with alcohol sales, and approximately 5,630 sq. ft. of additional retail building square footage. SUP No. 07-500-19 is for the construction of an approximately 6,000 sq. ft. church facility located on the northwest corner of Rose Avenue and Raiders Way (New Progressive Christian Missionary Baptist Church). As part of the SUP for the church, administrative relief is being requested in order to construct 35 parking spaces instead of 50 parking spaces required by the City's code. The GPA, ZC, and TPM will facilitate development of the commercial and church projects. The entire project is 4.05 acres. The parcel to be subdivided is Assessor's Parcel Number 221-0-232-515. Filed by Tom Davies, 2225 Glastonbury Road, Westlake Village, CA 91361

Attached is a copy of the initial study documenting the reasons to support the finding of no significant effect on the environment. Mitigation measures are included in the initial study to reduce the identified potential effects to a less than significant level:

- Air Quality
- Cultural Resources
- Hazards and Hazardous Materials
- Noise
- Transportation

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
Air Quality (Short-term)	Less than Significant with Mitigation Temporary Minor Impact	<p>C-1 Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.</p> <p>C-2 At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.</p> <p>C-3 During construction and on non-construction days (including Sundays) during periods of high wind (see C-6), Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.</p> <p>C-4 During construction, Developer shall control dust by the following activities:</p> <ul style="list-style-type: none"> • All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads. • All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible. <p>C-5 During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.</p> <p>C-6 During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.</p> <p>C-7 Throughout construction, Developer shall sweep adjacent streets and roads at least once per day.</p>	Less Than Significant Impact	At plancheck: Planning Division Onsite: Development Services

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measure	Significance After Mitigation	Responsible Party
		<p>preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.</p> <p>C-8 Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements.</p> <p>C-9 Open material stockpiles shall be covered, seeded, periodically watered, or treated with environmentally safe dust suppressants.</p>		
Cultural Resources	Less than Significant with Mitigation Temporary Short Term	<p>E-1 The Developer shall contract with an Archaeological Monitor and/or Native American Monitor to be present during all subsurface grading, trenching or construction activities deeper than 16 inches. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final certificate of occupancy.</p> <p>E-2. In the event archaeological resources are discovered, the Archaeological Monitor and/or Native American Monitor shall be empowered to suspend work in the immediate area of the discovery until such time as a data recovery plan can be developed and implemented. Work outside of the area of the find shall proceed with the continuation of archaeological monitoring. If a data recovery plan is implemented, all cultural materials from testing, monitoring, and data recovery phases of the project, except burial-related artifacts and as otherwise required by law, shall be cleaned, catalogued and permanently curated at an institution meeting the standards defined in the State of California Guidelines of the Curation of Archaeological Collections (May 1993). Repatriation of materials shall be handled in accordance with the requirements of the California Native American Graves Protection and Repatriation Act. All artifacts and all faunal materials shall be analyzed.</p>	Less Than Significant Impact	<p>Prior to grading permit issuance: Planning Division.</p> <p>Onsite: Development Services</p>
Hazards and Hazardous Materials	Less than Significant with Mitigation Temporary Short term	<p>G-1. Prior to issuance of a grading permit, the applicant shall conduct a subsurface assessment for the presence of agricultural chemicals as recommended in the Phase I ESA.</p> <p>G-2. If during future grading activities, suspect conditions including but not limited to stained or odorous soils are encountered at the project site, the applicant shall conduct further soil sampling. A qualified environmental scientist shall be retained to analyze the degree of contamination and all required state and county remediation standards and practices shall be followed for remediation. A</p>	Less Than Significant Impact	<p>Prior to grading permit issuance: Planning Division and Development Services</p>

Environmental Impact	Significance Before Mitigation	Recommended Mitigation Measure	Significance After Mitigation	Responsible Party
Noise	Less than Significant with Mitigation Temporary Short Term	<p>report and recommendations for remediation shall be submitted to the project planner for review and approval prior to re-initiating further grading work.</p> <p>K-1. Construction times shall be limited to 7 a.m. to 7 p.m. Monday through Saturday or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.</p> <p>K-2. All deliveries of construction material and equipment will occur on-site within the construction barricades and only during the hours of 7 am and 7 pm Monday through Saturday. The queuing of construction vehicles outside the site before 7 am or after 7 pm will be strictly prohibited. The vehicles shall all utilize mufflers and other devices to minimize noise levels. All materials and equipment will be stored on-site and within the confines of the construction barricades.</p> <p>K-3. Truck traffic related to the construction will be limited to the routes specified by the City of Oxnard. Truck traffic through residential neighborhoods shall be minimized.</p> <p>K-4. All construction related workers will be required to park on-site (i.e. behind the construction barricades or in designated off-site parking area) outside of the residential area to the west.</p> <p>K-5. During construction activities, except as otherwise required by law, all vehicle horns shall remain silent except in the case of emergency.</p> <p>K-6. Catering trucks providing service to construction workers at the site will be required to park within the site at all times. Catering trucks shall not be permitted to park on the street nor to sound their horns near or within the site.</p> <p>K-7. Construction workers shall not be permitted to loiter on the jobsite or on any street, whether before, during or after work hours, on weekdays or on weekends.</p> <p>K-8. Developers shall setup staging areas on-site to minimize off-site transportation of heavy construction equipment.</p>	Less Than Significant Impact	Planning Division

Environmental Impacts	Significance Before Mitigation	Recommended Mitigation Measures	Significance After Mitigation	Responsible Party
Transportation	Less than Significant with Mitigation	<p>K-9. Construction equipment shall be fitted with modern sound-reduction equipment. (PL, 4.9.3)</p> <p>K-10. During all grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.</p> <p>O-1. Prior to issuance of a certificate of occupancy for the retail parcel (parcels 1 and 2) or the church (parcel 3), proof of a reciprocal parking and access agreement shall be submitted to the project planner. Said agreement shall be recorded on all three parcels. The agreement shall be reviewed and approved by the project planner prior to recordation.</p>	Less than Significant	Planning Division

- Attachments:
- Vicinity, General Plan and Zoning Maps
 - Reduced Project Plans
 - Applicant's Agreement to Mitigation Measures
 - Initial Study/MND 07-11



2225 Glastonbury Road
Westlake Village, CA 91361
805 496 6449

February 4, 2008

Mr. Tom Davies
2225 Glastonbury Road
Westlake Village, CA 91361

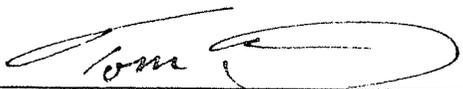
RE: Initial Study and Mitigated Negative Declaration (MND 07-11) - Victoria and Hemlock Project

To: Planning Division Manager

Pursuant to Section 15070 (Negative or Mitigated Negative Declaration Process) of the State Guidelines implementing the California Environmental Quality Act, I/we, acting as agents for the property owner/developer, hereby agree to all of the following:

- 1) The draft initial study identifies potentially significant effects from the project, but the study also identifies mitigation measures that would avoid or mitigate the effects to a level where clearly no significant effects would occur;
- 2) The mitigation measures are hereby incorporated into the project prior to releasing the draft initial study and mitigated negative declaration for public comment;
- 3) I/we agree to the mitigation measures as necessary to avoid or mitigate significant effects that would otherwise arise from the project. I/we accept the mitigation measures included in the draft initial study and have resolved all questions and concerns regarding the mitigation measures;
- 4) If during the public comment period and/or decision-making process, substitute or additional mitigation measures are proposed, the appropriate process must take place for determining whether or not to substitute or apply additional measures;
- 5) This agreement is binding upon the applicant for this project and any successors in interest or assignees.

This acknowledgment is binding upon the applicant and any successors in interest or assignees:

	FEBRUARY 4, 2008
Signature	Date
TOM DAVIES	MANAGING MEMBER
Print Name	Title

This acknowledgment is to be attached to the draft initial study and mitigated negative declaration for the project and then released for the applicable public comment period.



Planning Division
214 South C Street
Oxnard, California 93030
805/385-7858
FAX 805/385-7417

**INITIAL STUDY
MITIGATED NEGATIVE DECLARATION #2007-11**

**The Shops at College Park and
New Progressive Christian Missionary Baptist Church Development Projects**

PZ 07- 600-06 (General Plan Amendment)
PZ 07-570-04 (Zone Change)
PZ 07-300-13 (Tentative Parcel Map)
PZ 07-500- 14 (Special Use Permit for commercial retail component)
PZ 07-500-19 (Special Use Permit for church component)

February 15, 2008

INTRODUCTION

This *Initial Study* has been prepared in accordance with relevant provisions of the *California Environmental Quality Act (CEQA) of 1970*, as amended, and the 2007 State CEQA Guidelines. Section 15063(c) of CEQA Guidelines indicates that the purposes of an Initial Study are to:

1. Provide the Lead Agency (The City of Oxnard) with information to use as the basis for deciding whether to prepare an Environmental Impact Report (EIR) or Negative Declaration;
2. Enable an applicant or Lead Agency to modify a project, mitigating adverse impacts before an EIR is prepared, thereby enabling the project to qualify for a Negative Declaration;
3. Assist the preparation of an EIR, if one is required, by:
 - (A) Focusing the EIR on the effects determined to be significant;
 - (B) Identifying the effects determined not to be significant;
 - (C) Explaining the reasons why potentially significant effects would not be significant; and
 - (D) Identifying whether a program EIR, tiering, or another appropriate process can be used for analysis of the project's environmental effects.
4. Facilitate environmental assessment early in the design of a project;
5. Provide documentation of the factual basis for the finding in a Negative Declaration that a project will not have a significant effect on the environment;
6. Eliminate unnecessary EIRs; and
7. Determine whether a previously prepared EIR could be used with the project.

The City of Oxnard *Threshold Guidelines - Initial Study Assessment* (February 1995) was used along with other pertinent information for preparing the *Initial Study* for this project.

The purpose of the *Threshold Guidelines* is to inform the public, project applicants, consultants, and City staff of the threshold criteria and standard methodology used in determining whether or not a project (individually or cumulatively) could have a significant effect on the environment. Furthermore, the *Threshold Guidelines* provide instructions for completing the *Initial Study* and determining the type of environmental document required for individual projects.

Determining the significance of environmental impacts is a critical and often controversial aspect of the environmental review process. It is critical because a determination of significance may require that the project be substantially altered, or that mitigation measures be readily employed to avoid the impact or reduce it below the level of significance. If the impact cannot be reduced or avoided, an Environmental Impact Report (EIR) must be prepared. An EIR is a detailed statement that describes and analyzes the significant environmental impacts of a proposed project, discusses ways to reduce or avoid them, and suggests alternatives to the project, as proposed. The preparation of an EIR can be a costly and time-consuming process.

Determining the significance of impacts is often controversial because the decision requires staff to use their judgment regarding a subject that is not clearly defined by the law. The State CEQA *Guidelines* define the term "significant impact on the environment" as a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project. However, there is no iron-clad definition of what constitutes a substantial change because the significance of an activity may vary according to location.

To help clarify and standardize decision-making in the environmental review process, the City of Oxnard (City) has developed thresholds of environmental significance. Thresholds are measures of environmental change that are quantitative for subjects like noise, air quality, and traffic; and qualitative for subjects like aesthetics, land use compatibility, and biology. These thresholds are used in the absence of other empirical data to define the significance of impacts. For some projects, however, special studies and/or the professional judgment of City staff may enter into the decision-making process. Therefore, the City's thresholds are intended to serve as guidelines, and to augment existing CEQA provisions governing the definition of significance.

The City's environmental thresholds will be periodically updated as new information becomes available, or as standards regarding acceptable levels of environmental change are reevaluated. For example, air quality thresholds adopted by the City of Oxnard were established through State and Federal legislation. These standards and the methodology used to compute them may change over time. When this occurs, the City will evaluate the data and, if necessary, modify the thresholds to reflect improved awareness.

When other agencies have jurisdiction over a given site, the project proponent will have to meet the design, mitigation, and monitoring requirements imposed by those agencies, as well as, any additional requirements established by the City of Oxnard.

market parcel (Parcel 1 - 1.51 gross acres), retail shops (Parcel 2 - 0.66 gross acres), and an approximately 6,000 sq. ft. Church (Parcel 3 - 1.79 acres).

Phase One of the development would include construction of the Tesco market/retail development on Parcels 1 and 2. Phase Two would include development of the church on Parcel 3.

Driveways to access all three parcels are proposed as follows:

- Full-access driveway at Raiders Way (to provide access to the southern portion of the site)
- Right-in/right-out access driveway at Channel Islands Boulevard (primary east-west street)
- Right-in/right-out access driveway at Rose Avenue (primary north-south street)

The proposed commercial/retail project includes parking that complies with the City zoning codes: 124 parking spaces with an additional six (6) parking spaces designated as handicap-accessible parking for a total of 130 parking spaces. Additional project-related improvements include pedestrian sidewalks and landscape enhancements.

On the church parcel, a total of 35 parking spaces are provided. The applicant has submitted a parking study (February 4, 2008, attached) requesting relief from the City code parking requirements. Approval of this parking space reduction is requested as part of the SUP for the alcohol sales within the market. Additionally, a reciprocal parking and access agreement will be required between Parcels 1, 2 and 3. All roadway and pedestrian improvements along Rose Avenue will be constructed as part of Phase One, construction of the Tesco/retail project.

Water and sewer services will be provided by the City of Oxnard, electricity will be provided by Southern California Edison, and natural gas will be provided by the Gas Company. Storm water treatment will occur through a series of on site grass swale bio-filters.

9. Surrounding Land Uses and Setting: The proposed project is located within the City of Oxnard, College Estates Neighborhood.

The site is relatively flat and disturbed and is adjacent to a retail shopping center to the north and an ARCO-branded gasoline station to the northeast. College Park is to the east of the project site. To the west of the site is single family residential land use and to the south of the site is the Channel Islands High School. **Table 1** depicts the General Plan Land Uses, Zone designations, and existing land uses in the proposed project area:

Table 1 – Project Area General Plan Designation, Zoning, & Existing Land Use

LOCATION	ZONING	GENERAL PLAN	EXISTING LAND USE
Project Site	R-2-PD Medium Density Residential	Low to Medium Residential	Vacant
<i>North</i>	C-2-PD Commercial Planned Development	General Commercial	Retail Shopping Center
<i>South</i>	C-R General Commercial	School	Channel Islands High School
<i>East</i>	R-2- PD Residential Planned Development	Park	Future Park
<i>West</i>	R-2- Residential Planned Development PD	Multi-family Residential Planned Development	Single-family Residential

10. Other agencies whose approval is required (e.g., permits, financing approval, or participating agreement): Bureau of Alcohol and Beverage Control, Ventura County Air Pollution Control District, Calleguas Municipal Water District, Ventura County Environmental Health Division, and the Regional Water Quality Control Board.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below will be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” or as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agricultural Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology/Soils |
| <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Land Use/Planning |
| <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise | <input type="checkbox"/> Population/Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

DETERMINATION: (To be completed by the Lead Agency) On the basis of this initial evaluation:

- I find the proposed project COULD NOT have a significant effect on the environment and a NEGATIVE DECLARATION will be prepared.
- I find that although the project could have a significant effect on the environment there will not be a significant effect in this case because revisions in the project have been made or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Kathleen M. Mallory *Feb 15, 2008*
Kathleen Mallory, AICP, Contract Planner February 15, 2008

EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is

substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.

4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analyses,” cited in support of conclusions reached in other sections may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration, as per CEQA Guidelines Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a. Earlier Analysis Used—Identify and state where they are available for review.
 - b. Impacts Adequately Addressed—Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c. Mitigation Measures—For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. The explanation of each issue should identify: a) The significance criteria or threshold, if any, used to evaluate each question; and b) The mitigation measure identified, if any, to reduce the impact to less than significance.

A. AESTHETICS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect on a scenic vista? (2020 General Plan, VIII - Open Space/ Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? (2020 General Plan, VIII - Open Space/ Conservation Element; XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Substantially degrade the existing visual character or quality of the site and its surroundings? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Create a source of substantial light or glare, which would adversely affect day or nighttime views in the area? (2020 General Plan, VIII - Open Space/Conservation Element, XII - Community Design Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-3) The proposed project is located within the College Estates Neighborhood. The existing visual character of the project site is one of a vacant and disturbed site with limited weedy vegetation along Channel Islands Boulevard and Cota Circle. The surrounding area consists primarily of commercial, residential, and park uses to the north, south, west, and east.

The proposed project would alter the existing views toward the site from adjacent and nearby commercial and residential development from that of a vacant and disturbed site to a single-level neighborhood supermarket, retail shops and church, surface parking lots, landscaping, and associated infrastructure improvements. The building design and landscaping plan will be designed to be consistent with the City's development standards and will be in character with

the surrounding neighborhood. The overall project design will be consistent with other existing commercial development in the vicinity of the project site. Implementation of the proposed project would have a beneficial aesthetic impact over existing conditions.

Varied architectural treatments and designs for the proposed project would enhance the visual quality and view corridors of the area to motorists and pedestrians alike. Phase One of the proposed project would include development and construction on Parcel 1 and Parcel 2, which are both aligned on the northern portion of the overall project site. The new structures will be set back from Channel Islands Boulevard allowing the parking lot, monument signage, entry and landscaping to be visible from the entrances along Channel Islands Boulevard and Rose Avenue.

The supermarket and retail shop building designs would have north and south facing parapet fronts to enhance the aesthetics of the entrance and the overall building appearance and screen rooftop equipment. The proposed project includes stucco, metal reveal, sandstone veneer, glass, and aluminum exterior materials.

Phase Two of the proposed project (Parcel 3) would involve development and construction on the southern portion of the project site, excluding the right of way improvements along Rose Avenue and the service driveway which will be completed in Phase One. Ingress and egress for Phase Two to the church facility will be from Raiders Way. The church building orientation will be in a roughly north-south orientation. The exterior will be stucco, glass, and aluminum with a mission style clay tile roof.

Two freestanding signs for the retail commercial uses will be placed near the project driveways along Channel Islands Blvd. and along Rose Avenue. Another monument sign for the church will be located on Parcel 3 near Rose Avenue. In accordance with City standards, project signage will be placed to ensure that no visual obstruction is created for vehicular or pedestrian traffic.

Additional improvements consist of enhanced paving (stamped or scored colored concrete), motorcycle parking, bicycle racks, shopping cart areas, fully-covered trash enclosures, and light posts throughout the project site.

The City of Oxnard's Art in Public Places policy requires significant artworks in major new developments or the payment of a fee. Although not required on this project, a mural will be installed on the eastern elevation of the grocery store with participation from art students at Channel Islands High School and will be visible from Rose Avenue.

The entire site will be landscaped with a City-approved varied and attractive plant palette.

The proposed project site will not affect other natural scenic resources within the City (beaches, the coastline, agricultural areas and parks). The proximity of College Park, which

lies southeast and across Rose Avenue from the project site, would require the project to comply with the City's landscape standards and provisions for street trees, landscape for parking lots, street parkways, and medians. **Therefore, no adverse or significant impacts related to scenic resources are expected to result from this project and no mitigation measures are required.**

- 4) The proposed project would not create a significant source of light or glare which would adversely affect day or night time views in the project area. The project would install new lighting on the site for security purposes. All exterior lighting will be required to comply with standard conditions of approval requiring shielding of lighting fixtures, sharp lighting fixture cut offs, and lighting to minimize glare. **Therefore, no significant impacts related to lighting and glare is expected to result from this project and no mitigation measures are required.**

B. AGRICULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Conflict with existing zoning for agricultural use, or a Williamson Act contract? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland, to nonagricultural use? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.7 - Agricultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

- 1-3) Findings from the Phase I Environmental Site Assessment report prepared by Hargrave Environmental Consulting Inc. (June 12, 2007) concluded that portions of the project site were historically used for agricultural purposes from at least 1947 until 1974. Currently, the

site is vacant and supports little vegetation (refer to Biological Resources, and Hazards and Hazardous Materials for further discussion). Implementation of the proposed project would not affect the availability or use of existing agricultural lands in the City of Oxnard. Further, the site is surrounded by urban development and roadways. **Therefore, the proposed project would not have a significant impact to agricultural resources, and no mitigation measures are required.**

C. AIR QUALITY

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Conflict with or obstruct implementation of the applicable air quality plan? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; URBEMIS 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Violate any air quality standard or contribute substantially to an existing or projected air quality violation? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; URBEMIS 2002 Computer Program)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is nonattainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; URBEMIS 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Expose sensitive receptors to substantial pollutant concentrations? (FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; URBEMIS 2002 Computer Program)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

C. AIR QUALITY

Would the project:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

5. Create objectionable odors affecting a substantial number of people? (*FEIR 88-3, 4.5 - Air Quality; Ventura County Air Quality Assessment Guidelines; URBEMIS 2002 Computer Program*)

Discussion:

It is the applicant’s responsibility to contact the Ventura County Air Pollution Control District (APCD) to verify compliance with any APCD permitting requirements.

1-5) *Short-term impacts:* Short-term impacts to air quality will result from grading and other construction activities associated with the project, such as earth-moving and heavy equipment vehicle operations. According to the Ventura County APCD, any combustion equipment onsite, which is rated at 50 horsepower or greater, must have either an APCD Permit to Operate (PTO), or be registered with the California Air Resources Board’s (CARB) Portable Equipment Registration Program (PERP). Examples of such equipment include portable electrical generators and portable air compressors. The applicant is responsible for contacting the APCD to verify compliance with any APCD permitting needs. The proposed project could potentially expose construction workers to pollutants and excessive amounts of air-borne matter. Standard conditions of project approval, as recommended by the APCD, will be included to minimize such emissions and maximize dust suppression onsite. **With the inclusion of the mitigation measures, impacts to short-term air quality will be reduced to less than significant.**

Greenhouse Gases (GHG): Due to the uncertainties in quantification methods, the inability to predict the effectiveness of regulatory efforts still under development, and the lack of established thresholds of significance it is not possible to determine if the proposed project would result in a significant increase in GHG emissions. The State has accounted for projected increases in population and accompanying demand for commercial activities when it established its over-all GHG reduction targets for 2020 in AB32.

Long-term impacts: Anticipated long-term impacts of the proposed project will be attributed to the increased traffic associated with commercial activity within the area.

The City’s adopted threshold for Reactive Organic Compound (ROC) and Nitrogen Oxide (NOx) emissions is 25 pounds per day (ppd). Project-specific emissions have been calculated using the URBEMIS 2002 Version 7.5 computer modeling program for Target Year 2008; the actual model runs are attached. Based upon this model run, 11.67 pounds per day of ROC and 17.55 pounds per day of NOx will be generated by the Tesco, retail and church development projects. These

totals do not exceed the City's air quality thresholds. Therefore, there is no need to mitigate long term air quality emissions and **cumulative long-term air quality impacts will be considered less than significant.**

Mitigation Measures for commercial retail (Parcels 1 and 2) and church property (Parcel 3):
The following mitigation measure shall apply to short-term impacts:

- C-1 Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.
- C-2 At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.
- C-3 During construction and on non-construction days (including Sundays) during periods of high wind (see C-6), Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
- C-4 During construction, Developer shall control dust by the following activities:
- All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads.
 - All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible.
- C-5 During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.
- C-6 During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.

- C-7 Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.
- C-8 Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements.
- C-9 Open material stockpiles shall be covered, seeded, periodically watered, or treated with environmentally safe dust suppressants.

Monitoring: The Planning Division will verify that all dust control measures (C-1 through C-9) are included on the grading plans. The Planning Division, or designees, will monitor all applicable measures in the field until construction is completed.

Result After Mitigation: **As proposed, the project would not result in any significant adverse impacts to air quality in the project area or in the project vicinity. No adverse residual impacts would remain after mitigation.**

D. BIOLOGICAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

D. BIOLOGICAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
3. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? (2020 General Plan, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.10 - Biological Resources; and Local Coastal Plan)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1-6) According to the City Of Oxnard’s 2020 General Plan and General Plan Updated Background Report (2006), no candidate, sensitive, or special status species are known to inhabit the subject property; neither are there any environmentally sensitive habitat, wetlands, riparian corridors, or migratory corridors exist on, or within the vicinity of, the proposed project site. No native plant communities or areas of unique or sensitive habitat identified in a local,

regional or state habitat conservation plans are located within the vicinity of the proposed project site.

A Tree Inventory Report was prepared by James Dean, A.S.L.A. ISA (November 2007) for the project site by request of the City's Development Advisory Committee (DAC). The Tree Inventory Report states "the only vegetation is the perimeter street trees on Channel Islands Boulevard and Cota Circle". The Tree Inventory Report cataloged ten (10) Queen Palms (*Syragrus rommanzoffianum*) equally spaced within the parkway along Cota Circle. All 10 Queen Palms appeared in good health and were of varied height from 16' and 22' brown trunk height. All ten of the trees will remain and standard City conditions of approval will be imposed to protect these trees during construction. Three (3) *Myoporum* shrubs (*Myoporum laetum*) were located as street trees along Channel Islands Boulevard. These specimen shrubs are overgrown and displayed as trees. According to the tree report, these plants are of poor health and are conflicting with overhead utility lines. Further, the *Myoporum* shrubs present an unacceptable aesthetic quality due to the distorted shape which has occurred due to negligent pruning which has occurred over the years; therefore, removal of the *Myoporum* shrubs is recommended. **No significant or adverse impacts to biological resources are anticipated and no mitigation measures are required.**

E. CULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 - Cultural Resources)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Directly or indirectly destroy a unique paleontological resource or site or unique geological feature? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.12 - Aesthetic Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E. CULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
4. Disturb any human remains, including those interred outside of formal cemeteries? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.11 – Cultural Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1-4) The proposed project involves the development of a supermarket, retail shops, and a church on a vacant site located in the southerly portion of the City. The project site is located in an area consisting of commercial and residential land uses. The project requests a General Plan Amendment, Zoning Change, Tentative Parcel Map, and two Special Use Permits for the proposed commercial and church development projects. The project parcel was previously used for agricultural purposes from approximately 1947 until 1974 as stated in the Phase One Environmental Site Assessment (ESA) prepared by Hargrave Environmental Consulting, Inc. on June 12, 2007. In addition, the Phase I ESA determined “neither the (project) Site nor properties within one-half mile of the Site are listed on the (Tribal Lands) Federal Land Use database”.

However, because the proposed project involves a General Plan Amendment, compliance with Senate Bill 18 (SB 18) is triggered. SB 18 requires local governments to consult with the State of California Native American Heritage Commission prior to the adoption or amendment of a General Plan. The intent of SB 18 coordination is to obtain information regarding the presence of traditional lands, cultural places or sacred lands within the project site. The City initiated consultation with the Native American Heritage Commission in December 2007. Through this consultation, on site monitoring was recommended and has been made a mitigation measure.

Due to the historic cultural resources within the City, subsurface resources might exist on site that could be disturbed by grading and other subsurface activities the proposed development. **With the inclusion of the mitigation measure, impacts to cultural resources would be reduced to less than significant.**

Mitigation Measures for commercial retail (Parcels 1 and 2) and church property (Parcel 3):

E-1. The Developer shall contract with an Archaeological Monitor and/or Native American Monitor to be present during all subsurface grading, trenching or construction activities deeper than 16 inches. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior

to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final certificate of occupancy.

- E-2. In the event archaeological resources are discovered, the Archaeological Monitor and/or Native American Monitor shall be empowered to suspend work in the immediate area of the discovery until such time as a data recovery plan can be developed and implemented. Work outside of the area of the find shall proceed with the continuation of archaeological monitoring. If a data recovery plan is implemented, all cultural materials from testing, monitoring, and data recovery phases of the project, except burial-related artifacts and as otherwise required by law, shall be cleaned, catalogued and permanently curated at an institution meeting the standards defined in the State of California Guidelines of the Curation of Archaeological Collections (May 1993). Repatriation of materials shall be handled in accordance with the requirements of the California Native American Graves Protection and Repatriation Act. All artifacts and all faunal materials shall be analyzed.

Monitoring: The Planning Division will review the Archaeological / Native American monitoring contract(s) and reports prior to issuance of any grading permits and final certificate of occupancy, respectively. Development Services staff will monitor onsite construction activities, as necessary.

Result after Mitigation: Upon implementation of the above mitigation measures, the project will not result in any residual significant adverse effects on the environment related to cultural resources. **No further monitoring needed.**

F. GEOLOGY AND SOILS

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
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Would the project:

1. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - a. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of known fault? Refer to Division of Mines and Geology Special Pub. 42. (2020 General Plan, IX-Safety Element; FEIR 88-3, 4.8 - Earth Resources)
 - b. Strong seismic ground shaking? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

F. GEOLOGY AND SOILS

	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
Would the project:				
c. Seismic-related ground failure, including liquefaction? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Landslides? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Result in substantial soil erosion, or the loss of topsoil? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? (2020 General Plan, IX - Safety Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-4) The City of Oxnard, as with most other jurisdictions in California, lies in a seismically active region. However, there are no known active faults within the City. There are a number of potentially active faults in the region including the Oak Ridge, Pitas Point-Ventura, Anacapa, and Malibu Coast faults. These potentially active faults are located within five (5) to 10 miles of the City. Through the plan check process, the City's Development Services Division requires the submittal and approval of soils, geologic and structural evaluation report prepared by a registered soils engineer and/or structural engineer for all new development.

According to the City's 2020 General Plan, the City of Oxnard is located in an area with a slight seismic-related ground shaking potential. The subject site is located in an area that has been identified as having a moderate to high potential for liquefaction (Figure IX-2, Safety Element). Liquefaction can occur from seismic activity when an unstable ground condition in which water-saturated soils change from a solid to semi-liquid state because of a sudden shock or strain. The potential for liquefaction exists throughout most of the City because there

is a thick section of alluvial deposits and a high groundwater level. The primary determinant for liquefaction in the Oxnard Plain is the depth of the water table.

The potential for landslides to occur onsite is considered minimal due to the relatively flat topography of the site. Construction of the proposed projects will involve grading and other site preparation activities that may result in short-term wind driven soil erosion. With regular wetting of the soil during construction, implementation of the projects are not anticipated to result in the significant loss of topsoil and no long term wind-erosion impacts are anticipated.

Grading plans and erosion control plans in accordance with the City's Grading Ordinance must be submitted for plan check and approval by the City's Land Development Engineer prior to any final approval of the projects. **Therefore, with implementation of an approved grading plan and erosion control plan, impacts to geology and soils will be less than significant and no additional mitigation measures are required.**

G. HAZARDS AND HAZARDOUS MATERIALS

Would the projects:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? (2020 General Plan, IX - Safety Element)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

G. HAZARDS AND HAZARDOUS MATERIALS

Would the projects:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| <p>5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? (2020 General Plan, IX - Safety Element)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>6. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? (2020 General Plan, IX - Safety Element)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>7. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? (2020 General Plan, IX - Safety Element; City of Oxnard Emergency Preparedness Plan and Response Manual)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>8. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? (2020 General Plan, IX - Safety Element)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion:

- 1) The proposed projects would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazards materials. Hazardous materials would not be permitted to be stored on site during the construction phases of the projects. Once the supermarket, retail shops and church are constructed, routine items used for commercial purposes, such as landscape herbicides, and similar items will be present on site. The proposed projects would not create a significant hazard to the public or the environment through reasonably foreseeable up-set and accident conditions involving the release of hazardous materials into the environment.

- 2-3) According to the Phase I Environmental Site Assessment (ESA) prepared for the project site by Hargrave Environmental Consulting, Inc on June 12, 2007, there was no evidence of recognized environmental hazard conditions in connection with the site with the exception of the following:

- Potential residual levels of pesticides from historical agricultural land-use activities. “Historical aerial photographs and owner questionnaires indicated that portions of the project site were historically used for agricultural purposes from at least 1947 until 1974. Although it is unlikely that pesticides were applied as part of the farming practices, it is unknown if elevated levels of such compounds are present in the underlying site”; and therefore represent a potential recognized environmental condition, and
- The presence of an exploratory well in the southwest corner of the property. “Historically, the drilling of oil wells included the construction and use of reserve pits. Such pits were typically less than five (5) feet deep and used to contain mud and other materials generated during the well drilling”. Further hazardous wastes generated from these activities were generally disposed of in reserve pit areas and such substances could represent a potential recognized environmental condition.

Therefore, to address these concerns the report contained recommendations to mitigate the potential impact to hazards and hazardous materials.

Additionally, according to the Phase 1 report there was no evidence seen of surface staining or spills pertaining to hazardous substances and/or petroleum products at the project site. There was no evidence seen of any waste being generated, stored, or disposed at the project site. No evidence of either underground or above ground storage tanks were observed on site. No transformers, hydraulic cylinders, or other equipment potentially that may have used polychlorinated biphenyls (PCBs) or dielectric fluids were observed on site. An underground utility vault owned and operated by Southern California Edison was observed along the eastern portion of the northern property line. **With the inclusion of the mitigation measure, impacts to hazards and hazardous materials would be less than significant.**

Mitigation Measures for commercial retail (Parcels 1 and 2) and church property (Parcel 3):

- G-1. Prior to issuance of a grading permit, the applicant shall conduct a subsurface assessment for the presence of agricultural chemicals as recommended in the Phase I ESA.
- G-2. If during future grading activities, suspect conditions including but not limited to stained or odorous soils are encountered at the project site, the applicant shall conduct further soil sampling. A qualified environmental scientist shall be retained to analyze the degree of contamination and all required state and county remediation standards and practices shall be followed for remediation. A report and recommendations for remediation shall be submitted to the project planner for review and approval prior to re-initiating further grading work.

Monitoring: The Planning Division shall review plans to ensure compliance with the above mitigation measures.

Result After Mitigation: Upon implementation of the above mitigation measures, the projects will not result in any residual significant adverse effects on the environment related to hazards and hazardous materials. **Therefore, with the inclusion of the aforementioned mitigation measures, no significant impacts to hazards and hazardous materials are anticipated.**

- 4) The proposed projects are not located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5. **Therefore, implementation of the projects would not create a significant hazard to the public or the environment.**
- 5-6) The project area is not located within an airport approach or clear zone adjacent to the Oxnard Airport, as depicted on Figure IX-4 of the Safety Element of the City of Oxnard 2020 General Plan, nor is it located near a private airstrip. **Therefore, significant airport hazards are not anticipated.**
- 7-8) The proposed commercial and church projects would not interfere with an adopted emergency response plan or emergency evacuation plan. The proposed projects would not expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands. **Therefore, implementation of the projects would not create a significant hazard with an adopted emergency response or evacuation plan.**

H. HYDROLOGY AND WATER QUALITY

Would the projects:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

1. Violate any water quality standards or waste discharge requirements? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)

2. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/ Conservation Element; FEIR 88-3, 4.9 - Water Resources)

H. HYDROLOGY AND WATER QUALITY

Would the projects:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| <p>3. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site? (2020 General Plan, VIB - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>4. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner, which would result in substantial erosion or siltation on- or off-site? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>5. Create or contribute runoff water, which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>6. Otherwise substantially degrade water quality? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

H. HYDROLOGY AND WATER QUALITY

Would the projects:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
7 Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
8 Place within a 100-year flood hazard area structures which would impede or redirect flood flows? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
9. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
10. Inundation by seiche, tsunami, or mudflow? (2020 General Plan, VII - Public Facilities Element, VIII - Open Space/Conservation Element, IX - Safety Element; FEIR 88-3, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

- 1, 4-6) The project includes development of a supermarket, retail shops, and a church with surfaces having impermeable asphalt pavement parking lots, concrete sidewalks, and landscaped areas. The existing project area is a flat and vacant parcel. A Hydrology Report and Stormwater Quality Control Measures reports were prepared for the project site by DTR Engineering on October 15, 2007. The proposed projects would drain southerly after being filtered through six (6) vegetated bioswales, located along the perimeter of the site, and into an existing 24 inch stormdrain pipe located five (5) feet northerly of the center line of Raiders Way. The proposed improvements to the site would not have adverse effects to existing drainage conditions and would not cause flooding to on-site and off-site facilities. The treatment of stormwater on all three parcels will occur through vegetated bio-swales. The applicant will be

required to comply with standard conditions of approval pertaining to treatment and retention on site.

Water quality impacts associated with the proposed commercial and church uses would primarily be those associated with motor vehicles and landscape maintenance. The primary source of contaminants will be oil, grease, and particulates emitted by motor vehicles.

The proposed projects would not alter the course of a stream or river in a manner which would result in substantial erosion or siltation on- or off-site. There are no wetlands in the vicinity of the project area.

The proposed projects comply with the National Pollutant Discharge Elimination System (NPDES) program. As the proposed projects would result in the grading, excavation and potential soil remediation, and paving, a Storm Water Pollution Prevention Plan (SWPPP) will be required and will be subject to the review and approval of the City of Oxnard in order to verify compliance with NPDES requirements. A SWPPP, which outlines plans to control storm water pollution during and after construction, is completed by implementing project-appropriate best management practices (BMPs). **With the inclusion of standard conditions of approval, impacts to hydrology and water quality will be less than significant.**

- 2) The project area will be served by municipal water; the City obtains most of its water from the Calleguas Water District, which in turn purchases most its water from the Metropolitan Water District of Southern California. Other sources of water include local well water from United Water Conservation District and City wells. In order to address water supply needs at a regional level, representatives of the City of Oxnard, the Port Hueneme Water Agency (PHWA), the United Water Conservation District (UWCD), and the Calleguas Municipal Water District (CMWD) meet regularly. A collective effort to ensure continued delivery of high quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program; a new, regional groundwater desalination facility is associated with this program and is intended to serve Oxnard and Port Hueneme.

Given the recent implementation of the GREAT program, the proposed increase in water demand associated with the projects is not anticipated to result in a substantial increase in future anticipated City-wide water demand to City groundwater sources. **Therefore, no impacts to groundwater supply or quality would occur with the projects.**

- 3) There are no surface water bodies or wetlands in the project vicinity which could be affected by the proposed development. However, development of the project site would result in additional impervious areas, which would place increased demands on the City's water resources. **Therefore, no significant impacts are anticipated.**
- 7-10) According to the City's 2020 General Plan, the project site is not located in an area that has been identified as being within a 100-year flood plain zone (Figure IX-3, Safety

Element). The proposed project would not place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map. The proposed projects would not expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam. In addition, the proposed project is not located near the coastline, creeks, or drainage courses and, as such, is not located within an area that is prone to inundation by seiche, tsunami, or mudflow. **Therefore, no significant impacts to hydrology and water quality due to these risks are anticipated.**

Standard conditions of approval commercial retail (Parcels 1 and 2) and church property (Parcel 3):

- H-1 Prior to issuance of a grading permit, the Applicant shall obtain all necessary permits in order to install an underground drainage pipe equivalent to the flow capacity of the existing drainage. The design of said improvement shall be subject to review and approval by the City Engineer of the Public Works Department.
- H-2 The Applicant is required to design a Storm Water Pollution Prevention Plan (SWPPP); this SWPPP will be subject to the review and approval of the City of Oxnard in order to verify compliance with NPDES requirements.

Monitoring: The Development Services Division and the Public Works Department shall review and approve the grading permit (compliance with mitigation measure H-2) and shall approve the plan prior to issuance of a site improvement permit (compliance with mitigation measure H-1).

Result After Mitigation: Upon implementation of the above mitigation measure, the project will not result in any residual significant adverse effects on the environment related to hydrology and water quality.

I. LAND USE AND PLANNING

Would the projects:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Physically divide an established community? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? (2020 General Plan; City adopted Specific Plans; Local Coastal Program; and Zoning Ordinance; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Conflict with any applicable habitat conservation plan or natural community conservation plan? (2020 General Plan, VIII - Open Space/Conservation Element; FEIR 88-3, 4.1 - Land Use)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1-3) The proposed projects includes applications for the following: a General Plan Amendment, Zone Change, Tentative Parcel Map, and two Special Use Permits for a shopping center and a church. Approval of the aforementioned permits would enable the construction and development of a Fresh and Easy Neighborhood Market (13,929 square feet (sq. ft.)), retail shops (5,630 sq. ft.), and the future construction of an approximately 6,000 sq. ft. church.

In addition to a Special Use Permit (SUP) for the construction of the church, the New Progressive Christian Missionary Baptist Church has also requested that the SUP be granted in order to allow administrative relief from the Municipal Code regarding the number of required parking spaces for the church. A total of 35 parking spaces will be provided although 50 parking spaces are required for the church inclusive of all ancillary church uses. The applicant has submitted a parking study (February 4, 2008 and attached to this document) demonstrating that relief from the city parking code requirements will not negatively impact parking on Raiders Way or Parcels 1 and 2. Additionally, a reciprocal parking and access agreement will be required between Parcels 1, 2 and 3.

The proposed projects would provide an increased sense of community by alleviating the need for current and potential residents within the project area to drive to another shopping center or

frequent another place of worship. The proposed projects are generally consistent with surrounding land uses and is not anticipated to adversely affect existing residential uses to the west. Further, the projects would not conflict with any applicable habitat conservation plan or natural community conservation plan given that none exists in the vicinity of this project site. **Therefore, no impacts to land use and planning are anticipated and no mitigation measures are required.**

J. MINERAL RESOURCES

Would the projects:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Result in the loss of availability of a known mineral resource that will be of value to the region and the residents of the state? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? (2020 General Plan, V - Land Use Element; FEIR 88-3, 4.8 - Earth Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

1-2) The City of Oxnard’s 2020 General Plan outlines areas of mineral/sand/gravel deposits and designates the project site is within an area containing MRZ-3a deposits (Figure VIII-7). The State Division of Mines and Geology defines this zone as an area judged on the basis of the limited available geologic data and fieldwork to have a higher potential as sources of aggregate materials suitable for Portland cement concrete than other deposits classified as MRZ-3 (which contain mineral deposits of unknown significance).

The project site does not fall under the County of Ventura’s Mineral Resource Management Plan, thus is not subject to extraction or buffering requirements. Project implementation would not result in the loss of availability of a known or locally important mineral resource. **Therefore, no impacts to mineral resources are anticipated and no mitigation measures are required.**

K. NOISE

Would the projects result in:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

- | | | | | |
|---|--------------------------|-------------------------------------|-------------------------------------|-------------------------------------|
| <p>1. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)</p> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| <p>2. Exposure of persons to or generation of excessive ground borne vibration or ground borne noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>3. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>4. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels without the project? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>5. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

K. NOISE

Would the projects result in:

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
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6. For a project located within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? (2020 General Plan, X - Noise Element; FEIR 88-3, 4.4 - Noise; Oxnard Sound Regulations - Sections 19-60.1 through 19-60.15)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion:

1-3) Potential noise impacts associated with the proposed projects are related to proposed short-term construction-related activities, project-related traffic, and on-site activities associated with the proposed use. Construction noise is governed by the City’s Noise Ordinance and is listed in the City’s 2020 Noise Element of the General Plan. According to the Noise Element, the project site is located in an area designed with 65 A-weighted decibels (dBA) Community Noise Element Level (CNEL) in the year 2020.

Existing Conditions/Sensitive Receptors

The project site is bounded by Channel Islands Boulevard to the north, Rose Avenue to the east, Raiders Way to the south, and Cota Circle and existing residential uses to the west. The primary noise sources currently existing in the vicinity of the project site are as follows:

- Roadway noises originating from traffic traveling on Highway 1 and Rose Avenue - east of the project site),
- Channel Islands Boulevard which is a major arterial roadway in the City (northern boundary of the project site), and
- The Union Pacific Railroad line (west of the project site). The Union Pacific Railroad line runs on a 24-hour basis and operates eight (8) trains in Oxnard.

The City of Oxnard Noise Element of the General Plan states that the significance criteria for commercial development is 65 dBA CNEL as measured outdoors. Noise levels that exceed this threshold are considered to result in a significant impacts.

Construction Noise

Construction noise is governed by the City’s Noise Ordinance, which limits construction activities to 7:00 a.m. to 7:00 p.m., Monday through Saturday. Noise associated with traffic and/or other off-site noise generators is regulated under the City’s Noise Element, which identifies exterior noise levels that are acceptable for various land uses. **To ensure**

construction-related noise impacts of the projects are minimized, avoided, or reduced, the mitigation measures listed below will be required.

Operational Outdoor Noise

The proposed projects will be expected to utilize fixed source HVAC units for heating and cooling purposes. Depending on the size and number of units selected for the projects, the final radiated sound is undetermined at this time. However, it should also be noted that functionally, not every HVAC unit will be operating simultaneously.

Operation of all on-site noise sources for compliance with applicable property line noise standards will be based on the current engineering design; and requires review and approval by the Planning Division. No other potential operational noise sources are associated with the proposed projects. The HVAC units will be required to meet the daytime and nighttime property line noise standards as established in the City's Municipal Code, which will be verified through the building permit process. **Therefore, noise associated with operation of the proposed projects will be less than significant.**

Traffic Noise

According to the Site Specific Traffic Impact Study prepared by AllianceJB (November 7, 2007) the proposed projects would generate an estimated 966 average daily trips, which would contribute to noise produced by traffic activities on the surrounding roadways. However, the additional traffic generated by the projects would not be expected to create a significant noise impact. The proposed projects would not be expected to exceed the City's noise thresholds as a result of construction activities, operation of the projects, or traffic generated by the projects.

- 5-6) The projects are not located with an airport land use area or within the vicinity of a private airstrip. **Therefore, no airport or aircraft-related noise impacts are anticipated to affect the proposed projects.**

Mitigation Measures for commercial retail (Parcels 1 and 2) and church property (Parcel 3):

- K-1. Construction times shall be limited to 7 a.m. to 7 p.m. Monday through Saturday or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.
- K-2. All deliveries of construction material and equipment will occur on-site within the construction barricades and only during the hours of 7 am and 7 pm Monday through Saturday. The queuing of construction vehicles outside the site before 7 am or after 7 pm will be strictly prohibited. All construction delivery vehicles shall utilize mufflers and other devices to minimize exterior noise impacts associated with construction activities. All materials and equipment will be stored on-site and within the confines of the construction area.

- K-3. Truck traffic related to the construction will be limited to the routes specified by the City of Oxnard. Truck traffic through residential neighborhoods shall be minimized.
- K-4. All construction related workers will be required to park on-site (i.e. behind the construction barricades or in designated off-site parking area) outside of the residential area to the west.
- K-5. During construction activities, except as otherwise required by law, all vehicle horns shall remain silent except in the case of emergency.
- K-6. Catering trucks providing service to construction workers at the site will be required to park within the site at all times. Catering trucks shall not be permitted to park on the street nor to sound their horns near or within the site.
- K-7. Construction workers shall not be permitted to loiter on the jobsite or on any street, whether before, during or after work hours, on weekdays or on weekends.
- K-8. Developers shall setup staging areas on-site to minimize off-site transportation of heavy construction equipment.
- K-9. Construction equipment shall be fitted with modern sound-reduction equipment. (PL, 4.9-3)
- K-10. During all grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.

Monitoring: The aforementioned mitigation measures shall be identified on the grading plan as required mitigation measures. They will be enforced by the Development Services Department as part of the regular grading and building permit inspection process, and upon complaint.

Result After Mitigation: Short-term noise impacts will be generated by grading and construction activities; however, with mitigation measures short-term noise impacts will be less than significant. **With implementation of mitigation measures, no significant adverse effects relating to noise are anticipated.**

I. POPULATION AND HOUSING

Would the projects:

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
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- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| <p>1. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through an extension of roads or other infra-structure)? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>2. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>3. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? (2020 General Plan, IV - Growth Management Element, V - Land Use Element, Revised 2000-2005 Housing Element, FEIR 88-3, 4.2 - Population, Housing and Employment, 5.0 - Growth-Inducing Impacts)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion:

1-3) The project area is located in the College Estates Neighborhood which includes a mixture of commercial and residential land uses and is currently served by an existing circulation system of highways, arterials, and collectors. The proposed development would not induce growth, remove a barrier to growth, remove existing homes, or displace residents, since the vacant project site would provide a supermarket, retail shops, and church and is in a developed and populated area within the City. All public urban infrastructure services are presently provided in the vicinity of the projects and all public services are available to serve the proposed projects (refer to the Public Services section, below). **Therefore, no adverse impacts to population and housing are anticipated and no mitigation measures are required.**

M. PUBLIC SERVICES

Would the projects result in substantial adverse physical impacts to the following:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
1. Fire protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Police protection? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
3. Schools? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Parks? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Other public facilities? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.13 - Public Services)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

- 1-5) The proposed projects have been designed to include adequate fire hydrants, vehicular and pedestrian access, signage, smoke detectors, and all requirements of the Uniform Fire Code in order to minimize any potential impacts on fire services. In addition, standard Fire Department conditions will be incorporated into the proposed projects. Furthermore, the Fire Department can access additional manpower and equipment through an automatic aid agreement with Ventura County and a mutual aid agreement with the City of Ventura and Point Mugu Naval Air Station. The projects will be served by the Oxnard Fire Department. **With the inclusion of standard Fire Department conditions, potential impacts on fire services will be less than significant.**

As stated within the City’s General Plan Background Report (2006), in 1990, the City had a staffing ratio of 1.1 officers per thousand residents. Currently, the ratio is 1.3 officers per thousand residents, which is well below the national average of 1.9 officers per thousand. In 2005, there were 242 sworn officers and over 152 civilians providing law enforcement services for the City of Oxnard.

The Oxnard Police Department’s Five Year Staffing Plan, projects the need for an additional 49 to 102 sworn officers and 36 to 46 civilian staff members in order to meet the projected additional calls for service based on the City’s increasing population and desired services levels. The increase in tax base generated by the projects and cumulative projects would help

fund the projects share of necessary police service expansion within the City. **Therefore, potential impacts on police services will be less than significant.**

The proposed project includes the construction and operation of a supermarket, retail shops, and a church facility. The projects do not include any residential uses and is not anticipated to result in the addition of new residents to the City of Oxnard. **Therefore, implementation of the proposed projects would not be expected to affect school services or facilities, parks or other facilities.**

N. RECREATION

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
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1. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)

2. Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment? (2020 General Plan, XIII - Parks and Recreation Element; FEIR 88-3, 4.12 - Aesthetic Resources, 4.13 - Parks and Recreation Services)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

- 1-2) The projects would not remove any park land or other recreational facilities within the City. No significant increases on demands of the existing recreational system are expected to be generated by the projects. Additionally, commercial and church projects are not required to pay the City required Quimby park fee. **Therefore, no significant adverse effects on the environment related to recreation are expected to result from the projects and no mitigation measures are required.**

O. TRANSPORTATION/TRAFFIC

Would the projects:

Potentially Significant Impact Less Than Significant With Mitigation Less than Significant Impact No Impact

- | | | | | |
|---|--------------------------|-------------------------------------|-------------------------------------|-------------------------------------|
| <p>1. Cause an increase in traffic, which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>2. Exceed, either individually or cumulatively, a level of service standard established by the County congestion management agency for designated roads or highways? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>3. Result in a change in traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>4. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/ Circulation)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>5. Result in inadequate emergency access? (2020 General Plan, VI - Circulation Element; FEIR 88-3, 4.3 - Transportation/Circulation)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <p>6. Result in inadequate parking capacity? (Zone Ordinance - Parking Regulations and Parking Lot Design Standards)</p> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| <p>7. Conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Bicycle Facilities Master Plan)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion:

- 1-3) The proposed project site is bounded by Channel Islands Boulevard to the north, Rose Avenue to the east, Raiders Way to the south, and by Cota Circle and an existing residential development to the west. The proposed projects require approval of a General Plan Amendment and Zone Change to allow the development of a Fresh and Easy Neighborhood Market, retail shops, and a church. Additionally, SUP's are required for the alcohol sale associated with the Tesco/market. An SUP is also required to construct the church with reduced parking in the C-2-PD zoning district. New Progressive Christian Missionary Baptist Church has requested that the SUP be granted in order to allow administrative relief from the Municipal Code regarding the number of required parking spaces for the church. A total of 35 parking spaces will be provided although 50 parking spaces are required for the church inclusive of all ancillary church uses. The applicant has submitted a parking study (February 4, 2008 and attached to this document) demonstrating that relief from the city parking code requirements will not negatively impact parking on Raiders Way or Parcels 1 and 2. Additionally, a reciprocal parking and access agreement will be required between Parcels 1, 2 and 3.

According to the Traffic Study prepared by AllianceJB, Incorporated on November 7, 2007, the proposed projects would not significantly impact the adjacent residential neighborhoods. Further, project access is proposed via driveways accessing the site from Channel Islands Boulevard, Rose Avenue, and Raiders Way. No access to the site is proposed from Cota Circle.

As required by the City Traffic Engineer, the Traffic Study (November 2007) presents the Intersection Capacity Utilization (ICU) Methodology. This ICU methodology involves a calculation of a volume-to-capacity ratio which is related to a level of service (LOS) A to F. The City of Oxnard's criteria for evaluating project impacts is based on the change in ICU/LOS attributable to the project (per City resolution No. 10,453).

The Traffic Study includes traffic data addressing project site access and internal circulation, existing conditions, existing + approved/pending development conditions (pre-project conditions), existing + approved/pending + project conditions (post-project conditions), and year 2030 conditions with the proposed projects. The potential effects of projects traffic within the adjacent residential neighborhoods was also evaluated.

Project trip generation was estimated by reviewing various publications, including the Institute of Transportation Engineers (ITE) publication entitled Trip Generation (Seventh Addition). Trip generation estimates were made for the projects morning and evening inbound and outbound traffic using the ITE land codes for a Supermarket, Specialty Retail, and Church.

For the entire project site, the new total trips generated for the proposed project site are estimated to be 27 in the morning peak hour and 87 in the afternoon peak hour. The new daily trips generated for the proposed project site are an estimated 966 average vehicle trips per day.

The list of study intersections was developed in coordination with City staff and in consideration for the City's intersection evaluation guidelines. Seven existing intersections were selected for analysis, they are as follows:

- Rose Avenue / Wooley Road
- Rose Avenue / Oxnard Boulevard
- Rose Avenue / Channel Islands Boulevard
- Rose Avenue / Raiders Way
- Rose Avenue / Bard Road
- Statham Boulevard / Channel Islands Boulevard
- Rice Avenue / Channel Islands Boulevard

Projects Access and Internal Circulation

The projects propose three access driveways which would include a full-access driveway at Raiders Way, a right-in/right-out driveway at Channel Islands Boulevard, and a right-in / right-out access driveway at Rose Avenue. Project access and internal circulation has been adjusted on the site plan to allow truck turning movements from the Rose Avenue curb lane, a minimum 25 feet radius driveway and a two (2) foot offset at parking stalls that are located at the corner of the perpendicular aiseways. Project access from these three proposed access driveways was also evaluated.

The primary north-south street providing access to the project site will be Rose Avenue. The primary east-west street providing access to the project site will be Channel Islands Boulevard. Raiders Way would also provide access to the southern portion of the project site. Oxnard Boulevard would serve as the primary regional access facility in immediate proximity to the site.

Traffic Study Findings

All of the analyzed intersections currently operate at an acceptable LOS (A-C) during the AM and PM peak hours under existing conditions. All of the analyzed intersections would also operate at an acceptable LOS (A-C) during the AM peak hour under existing plus approved/pending development conditions. There will be two (2) intersections that would operate at an unacceptable LOS (D-F) during the peak PM hour under existing plus approved/pending development conditions. These intersections are: 1) Rose Avenue / Wooley Road and 2) Rose Avenue / Oxnard Boulevard. However, the projects would not cause any project-specific impacts (as identified by a change in the volume capacity ratio of 0.02 or

more at LOS C, D, E, or F). Therefore, the proposed projects are consistent with City policy and is not considered to negatively impact the City's transportation network.

The results from the traffic study indicate that one of the intersections studied will operate at an unacceptable level of service (D-F) during the year 2030 AM peak hour. There will be four intersections that would operate at an unacceptable LOS (D-F) during the peak PM hour under the year 2030 conditions. These intersections are: 1) Rose Avenue / Wooley Road, 2) Rose Avenue / Oxnard Boulevard, 3) Rose Avenue/Channel Islands Boulevard, and 4) Rice Avenue/Channel Islands Boulevard. However, the projects would not cause any year 2030 impacts (as identified by a change in the V/C ratio of 0.02 or more at LOS C, D, E, or F). Therefore, the proposed projects are consistent with City policy and is not considered to negatively impact the City's transportation network.

The projects would not significantly impact any adjacent residential neighborhoods. Therefore, **impacts on Transportation and Traffic are considered to be less than significant.**

- 4, 5, & 7 The proposed commercial development projects would have a minimal change on existing traffic patterns in the project area but would create two new project driveways along Secondary Arterials. Access to the church will be via Raiders Way. The projects have been designed to minimize disruption of traffic flow and existing traffic patterns. The projects have been designed according to City Fire Department regulations. The proposed projects would not result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks.

The proposed projects would not substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses. The proposed projects would not result in inadequate emergency access. The proposed projects would not conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks). **Therefore, the proposed commercial development would not affect emergency access or parking capacity, and would not conflict with adopted policies supporting alternative transportation modes. As proposed, the proposed projects would not create any potentially significant impacts to transportation or traffic in the project area or vicinity and no mitigation measures are required.**

6. On the commercial retail project (Parcels 1 and 2) a total of 78 parking spaces are required and 90 parking spaces are provided. On the church parcel (Parcel 3) a total of 50 parking spaces are required and 35 parking spaces are provided. A total of 125 parking spaces are provided on all three parcels. A Parking Study prepared by AllianceJB dated February 4, 2008 supports the applicant's request for a modification to the number of parking spaces required for the church parcel. The report asserts that no other uses will occur during sanctuary use. The applicant has agreed to a condition of approval that restricts use of the lobby during sanctuary operation and restricts concurrent use of building square footage during operation of the sanctuary. Further, a reciprocal parking and access agreement will be

required between Parcels 1, 2 and 3. **With the inclusion of the following mitigation measure, impacts are considered less than significant.**

Mitigation Measures for commercial retail (Parcels 1 and 2) and church property (Parcel 3):

O-1. Prior to issuance of a certificate of occupancy for the retail parcel (Parcels 1 and 2) or the church (Parcel 3), proof of a reciprocal parking and access agreement shall be submitted to the project planner. Said agreement shall be recorded on all three parcels. The agreement shall be reviewed and approved by the project planner prior to recordation.

Monitoring: Proof of compliance with the aforementioned mitigation measure shall occur through recordation of a reciprocal access and parking agreement to be submitted to the project planner.

Result After Mitigation: Parking impacts will be less than significant. **With implementation of mitigation measures, no significant adverse effects relating to parking are anticipated.**

P. UTILITIES AND SERVICE SYSTEMS

Would the projects:

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
--------------------------------------	--	------------------------------------	-----------

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| <p>1. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>2. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>3. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

P. UTILITIES AND SERVICE SYSTEMS

Would the projects:	Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
4. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
7. Comply with federal, state, and local statutes and regulations related to solid waste? (2020 General Plan, VII - Public Facilities Element; FEIR 88-3, 4.6 - Public Utilities, 4.9 - Water Resources)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

1-7) The City of Oxnard would serve the proposed projects for waste wastewater treatment. The City of Oxnard currently provides wastewater collection and treatment services through the Public Works Wastewater Division. The Oxnard Wastewater Treatment Plant (OWWTP) services the City of Oxnard, City of Port Hueneme, the U.S. Navy Construction Battalion Station, the Point Mugu Naval Air Station, and some limited adjacent areas. According to the City's 2020 General Plan, the OWWTP has a current design capacity of 22.6 million gallons per day, with a rated capacity of 25.0 million gallons per day. Current usage ranges from 18 to 19 million gallons per day. The City is currently in the process of expanding the OWWTP.

The City has established the Wastewater Conveyance Fund to pay for operations, maintenance and capital costs of the wastewater collection system, and has established the Wastewater

Treatment Plant Fund to pay for operations, maintenance and capital costs of wastewater treatment. The City has also used State and Federal grants to pay for a portion of the Wastewater Treatment Plant Expansion. The City also collects sewer connection fees, and/or requires developers to build improvements, to expand the wastewater collection system to service new customers. Sewer connection fees are assessed for every new development, and consist of two components: (1) treatment plant expansion; and (2) conveyance system improvements. The developer is also required to provide on-site sewer lines, and to extend or improve off-site sewer lines where necessary to serve the proposed development (proposed improvements are described above in Public Services section). Off-site improvements costs borne by the developer are credited toward their connection fees.

As designed, the proposed projects would not be expected to exceed wastewater treatment requirements of the RWQCB, nor would it require or result in the construction of new water or wastewater treatment facilities. Adequate sewage connections will be available to serve the projects and no impacts will be anticipated.

The City of Oxnard currently uses City storm drain facilities, which are maintained by the Public Works Department Operations Division, and County of Ventura flood control channels to handle storm water runoff. Funding for storm drain maintenance is provided by the City's general fund. The existing City storm drain network does not have the capacity to accommodate increased runoff produced by full build-out of the 2020 General Plan. Therefore, while developers are required to convey drainage to the storm drain system and pay appropriate fees, storm drain capacity in the main lines may not be adequate.

The City has three existing flood planning policies. These policies are (1) a 1974 storm drain fund fee of 3 cents per square foot aimed at new development, (2) a requirement that all new development convey water generated by their project and all upstream water to the nearest adequate storm drain facility, and (3) drainage standards defining the appropriate hydrology method and roughness factors for use in all storm drainage conveyance system designs.

All detention basins shall be designed in accordance with City standards and policies. The proposed projects will be required to comply with measures to treat stormwater runoff by implementing Best Management Practices (BMP's) in accordance with National Pollutant Discharge Elimination System (NPDES) requirements. Based on recent and similar projects, the City expects that these projects can be served without significantly impacting existing services. During the plan check review process, the developer will be required to submit utility service plans which will include details, specifications, and reporting information demonstrating that the infrastructure utility and service systems will be designed to support the proposed development. **Associated impacts would, therefore, be less than significant.**

The City of Oxnard would serve the proposed projects with water supplied in part by Calleguas Municipal Water District (Calleguas) and Metropolitan Water District of Southern California (Metropolitan). The project site is currently located within the Calleguas Municipal Water District and annexation is not required. City Water Division records

indicate that water demand is growing at a compound rate of 2 percent per year. To meet this increased demand, the City is expanding the supply system (Springville Reservoir Project) and the distribution system (Blending Station #4 and Water Pressure Separation Vaults) to ensure that enough water can be delivered at adequate fire flow levels as new customers come on line. The ultimate water transmission and distribution system will be capable of delivering approximately 106 cubic feet per second (the equivalent of approximately 68 million gallons per day); this meets fire flow levels required by the State of California. This expanded system would adequately supply water to the projected population in the City's 2020 General Plan. Future water supplies will continue to come from local wells (about 5 percent), United (about 25 percent) and from Calleguas (about 70 percent), which purchases its water from MWD. A collective effort to ensure continued delivery of high quality water to the area has been initiated through the Groundwater Recovery Enhancement and Treatment (GREAT) Program; a new, regional groundwater desalination facility is associated with this program and is intended to serve Oxnard. This MND incorporates by reference the GREAT EIR. The GREAT EIR is available on the City's website which is www.ci.oxnard.ca.us. The City of Oxnard has established a Water Enterprise Fund to pay for operations, maintenance, and capital costs associated with supply and distribution of water. Operation and maintenance costs are financed by customer user fees. The City also collects water connection fees, and/or requires developers to build improvements, to expand the water transmission or distribution system to service new customers. **Therefore, adequate water supplies will be available to serve the projects and no impacts would be anticipated.**

The proposed projects would generate additional solid waste entering into refuse facilities in the region. The City of Oxnard currently collects and disposes in excess of 140,000 tons of refuse annually through the City-owned Del Norte Regional Recycling and Transfer Station (DNRRTS). The DNRRTS accepts refuse from Oxnard and several other cities and areas in western Ventura County. It is capable of recycling 50 percent to 80 percent of the refuse brought to it. The remaining refuse is presently hauled to other landfill sites in Ventura County. The City anticipates that it will continue to provide public refuse collection services in the future.

Waste generation in Oxnard is expected to double by the year 2020, to about 330,386 tons/year. If these trends continue, and if the Weldon Canyon project is not approved, landfill capacity within Ventura County could be exceeded prior to build-out of the 2020 General Plan. Standard conditions of approval require compliance with the City's recycling programs, which require solid waste recycling and disposal plans and reporting during construction and operation of the facility. Standard conditions of approval require compliance with the City's recycling programs, which require solid waste recycling and disposal plans and reporting during construction and operation of the facility. **Therefore, no significant impacts to landfills or other solid waste facilities or resources are anticipated.**

The City of Oxnard has the capacity to serve the proposed projects for sewage disposal. Funding for sewer system improvements is derived from a combination of general fund monies and sewer connection fees. Sewer connection fees are assessed for every new

development, and consist of two components: (1) treatment plant expansion; and (2) conveyance system improvements. The developer is also required to provide on-site sewer lines, and to extend or improve off-site sewer lines where necessary to serve that particular proposed development. Off-site improvements costs borne by the developer are credited toward their connection fees. **Therefore, adequate sewage connections will be available to serve the projects and no impacts would be anticipated.**

During the plan check and permitting process, the City of Oxnard Development Services Division will assess and determine the projects impact fees that are required for the proposed developments. Development impact fees typically involve, but are not limited: Planned Traffic Circulation System Facilities Fees (Traffic Impact); Planned Water Facilities Fee; Planned Wastewater Facilities Fee; Planned Drainage Facilities Fee; Landscape Maintenance Assessment District fees, and a Growth Requirement Capital Fee.

Therefore, with the payment of these development impact fees, impacts to public utilities and services will be less than significant and no mitigation measures are required.

Q. MANDATORY FINDINGS OF SIGNIFICANCE

Potentially Significant Impact	Less Than Significant With Mitigation	Less than Significant Impact	No Impact
--------------------------------	---------------------------------------	------------------------------	-----------

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| <p>1. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>2. Does the project have impacts that are individually limited, but cumulatively considerable (<i>"Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects</i>)?</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <p>3. Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?</p> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion:

1. The proposed projects do not have the potential to degrade the quality of the environment, reduce the habitat of any sensitive plant or animal species, or eliminate important examples of California history or prehistory. No known sensitive biological or cultural resources exist on the project site. However, precautionary mitigation measures to protect unknown cultural resources onsite have been included.
2. Implementation of the proposed projects would not individually result in any significant impacts. Therefore, the projects contribution to cumulative impacts will be negligible.
3. As discussed in the sections pertaining to Air Quality, Cultural Resources, Hazardous Materials and Safety, and Noise, mitigation measures have been incorporated into the proposed projects to reduce all short term and long term impacts below significance thresholds. Therefore, the proposed projects would not cause substantial adverse effects, either directly or indirectly, to human beings.

Based on this environmental analysis, the proposed projects will not degrade the quality of the environment or have substantial adverse effects on human beings, either directly or indirectly.

Mitigation measures have been incorporated into the projects and made a part of the Mitigated Negative Declaration Mitigation Monitoring and Reporting Program.

In view of the above analysis, it is determined that the projects will not have a significant impact on the environment and an Environmental Impact Report is not required.

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3. California, State of, Office of Planning and Research, *California Environmental Quality Act Statutes*, Sacramento, California: January 1, 2002.
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5. California, State of, Office of Planning and Research, *Planning, Zoning and Development Laws*, November 2000.
6. City of Oxnard, *The Municipal Code of the City of Oxnard - Zoning Ordinance*, current edition.
7. City of Oxnard, Development Services Department, Planning Division, *Coastal Zoning Regulations and Zone Maps*, current edition.
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13. Institute of Transportation Engineers, *Trip Generation Manual*, Seventh Edition, Washington, DC, 2003.
14. United States Federal Emergency Management Agency, National Flood Insurance Program, *FIRM Flood Insurance Rate Maps for the City of Oxnard*, October 1985.
15. City of Oxnard, Public Works Department, *Master Sewer Plan*, current edition.
16. City of Oxnard, Public Works Department, *Master Drainage Plan*, current edition.
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18. California State University - Fullerton South Central Coastal Information Center, *California Historical Resources Information System*, Department of Anthropology, Fullerton, California.
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20. Ventura County Cultural Heritage Board, *Ventura County Historical Landmarks & Points of Interest—August 1991*, Ventura County Recreation Services.
21. Ventura County, Property Administration Agency, *Final Report: Cultural Heritage Survey, Phase I*, Oxnard and Santa Paula, 1981.

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22. City of Oxnard, *FEIR 94-1 for the Oxnard Factory Outlet, Phase III.*
23. City of Oxnard, *FEIR 95-2 for the Shopping Center at Lockwood and Rose Avenue.*
24. City of Oxnard, *FEIR 95-3 for the Redevelopment Project.*
25. City of Oxnard, *FEIR 96-1 for the Northshore Project at Harbor Boulevard and Fifth Street.*
26. City of Oxnard, *FEIR 96-2 for the Northwest Specific Plan.*
27. City of Oxnard, *FEIR 97-1 for the Ormond Beach Specific Plan.*
28. City of Oxnard, *FEIR 98-1 (Supplemental) for the Northeast Specific Plan.*
29. City of Oxnard, *FEIR 98-2 (Supplemental) for the Westport at Mandalay Bay Project (Tract 5196).*

Specific Plans

30. City of Oxnard, *Northfield and Seagate Specific Plan*, July 3, 1984.
31. City of Oxnard, *Mandalay Bay Specific Plan*, January 9, 1985.
32. City of Oxnard, *Oxnard Town Center Specific Plan*, October 1, 1986.
33. City of Oxnard, *Rose-Santa Clara Corridor Specific Plan*, July 15, 1986.
34. City of Oxnard, *McInnes Ranch Business Park Specific Plan*, December 20, 1988.
35. City of Oxnard, *Northeast Community Specific Plan*, February 8, 1994.
36. City of Oxnard, *Northwest Community Specific Plan*, July 20, 1998.

URBEMIS 2002 For Windows 8.7.0

File Name: C:\Program Files\URBEMIS 2002 Version 8.7\Projects2k2\Church Yard Development.urb
Project Name: Church Yard Development, Tesco and Baptist Church
Project Location: Ventura County
1-Road Motor Vehicle Emissions Based on EMFAC2002 version 2.2

SUMMARY REPORT
(Pounds/Day - Summer)

AREA SOURCE EMISSION ESTIMATES

	ROG	NOx	CO	SO2	PM10
TOTALS (lbs/day,unmitigated)	0.75	0.26	2.55	0.00	0.01

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	ROG	NOx	CO	SO2	PM10
TOTALS (lbs/day,unmitigated)	10.92	17.29	133.69	0.13	12.53

SUM OF AREA AND OPERATIONAL EMISSION ESTIMATES

	ROG	NOx	CO	SO2	PM10
TOTALS (lbs/day,unmitigated)	11.67	17.55	136.24	0.13	12.54

URBEMIS 2002 For Windows 8.7.0

File Name: C:\Program Files\URBEMIS 2002 Version 8.7\Projects2k2\Church Yard Development.urb
Project Name: Church Yard Development, Tesco and Baptist Church
Project Location: Ventura County
On-Road Motor Vehicle Emissions Based on EMFAC2002 version 2.2

DETAIL REPORT
(Pounds/Day - Summer)

AREA SOURCE EMISSION ESTIMATES (Summer Pounds per Day, Unmitigated)					
Source	ROG	NOx	CO	SO2	PM10
Natural Gas	0.02	0.25	0.21	0	0.00
Hearth - No summer emissions					
Landscaping	0.37	0.01	2.34	0.00	0.01
Consumer Prdcts	0.00	-	-	-	-
Architectural Coatings	0.36	-	-	-	-
TOTALS(lbs/day,unmitigated)	0.75	0.26	2.55	0.00	0.01

UNMITIGATED OPERATIONAL EMISSIONS

	ROG	NOX	CO	SO2	PM10
Place of worship	0.40	0.56	4.30	0.00	0.40
Strip mall	1.55	2.42	18.73	0.02	1.76
Supermarket	8.98	14.31	110.66	0.11	10.37
TOTAL EMISSIONS (lbs/day)	10.92	17.29	133.69	0.13	12.53

Does not include correction for passby trips.
 Does not include double counting adjustment for internal trips.

OPERATIONAL (Vehicle) EMISSION ESTIMATES

Analysis Year: 2005 Temperature (F): 75 Season: Summer

EMFAC Version: EMFAC2002 (9/2002)

Summary of Land Uses:

Unit Type	Acreage	Trip Rate	No. Units	Total Trips
Place of worship		9.11 trips/1000 sq. ft.	6.00	54.66
Strip mall		42.94 trips/1000 sq. ft.	5.60	240.46
Supermarket		102.24 trips/1000 sq. ft.	13.90	1,421.14
Sum of Total Trips				1,716.26
Total Vehicle Miles Traveled				8,251.24

Vehicle Assumptions:

Vehicle Fleet Mix:

Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	56.10	2.30	97.10	0.60
Light Truck < 3,750 lbs	15.10	4.00	93.40	2.60
Light Truck 3,751- 5,750	15.50	1.90	96.80	1.30
Med Truck 5,751- 8,500	6.80	1.50	95.60	2.90
Lite-Heavy 8,501-10,000	1.00	0.00	80.00	20.00
Light-Heavy 10,001-14,000	0.30	0.00	66.70	33.30
Medium-Heavy 14,001-33,000	1.00	10.00	20.00	70.00
Heavy-Heavy 33,001-60,000	0.80	0.00	12.50	87.50
Line Haul > 60,000 lbs	0.00	0.00	0.00	100.00
Urban Bus	0.10	0.00	0.00	100.00
Motorcycle	1.60	87.50	12.50	0.00
School Bus	0.30	0.00	0.00	100.00
Taxi Home	1.40	14.30	78.60	7.10

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
Urban Trip Length (miles)	12.0	7.8	10.0	10.0	4.7	4.7
Rural Trip Length (miles)	15.0	10.0	10.0	15.0	15.0	15.0
Trip Speeds (mph)	40.0	40.0	40.0	40.0	40.0	40.0
% of Trips - Residential	27.4	17.7	54.9			

% of Trips - Commercial (by land use)

Place of worship	3.0	1.5	95.5
Strip mall	2.0	1.0	97.0
Supermarket	2.0	1.0	97.0

Changes made to the default values for Land Use Trip Percentages

Changes made to the default values for Area

Changes made to the default values for Operations

Site Specific Traffic Impact Study

Southwest Corner of Rose Avenue & Channel Islands Boulevard City of Oxnard

November 7, 2007

Prepared by:

AllianceJB

**Traffic and Transportation
Planning and Engineering**

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President, AllianceJB, Inc.



EXECUTIVE SUMMARY

This report presents the results of a Traffic Impact Analysis for the proposed development in the Southwest Corner of Rose Avenue & Channel Islands Boulevard in the City of Oxnard. The project site is bounded by Cota Circle and an existing residential development to the west, Raiders Way to the south, Rose Avenue to the east, and Channel Islands Boulevard to the north. The project proposes to construct a 13,929 square foot supermarket, 5,630 square feet of retail shops and a 6,000 square foot church. The 4.05 acre property is zoned Medium Density Residential (R-2) with a general plan land use designation of Low to Medium Residential. The applicant is requesting a General Plan Amendment to Commercial General (CG), and a zone change to General Commercial Planned Development (C-2-PD) to allow the development of the supermarket, retail shops and church. The total net new trips generated for the proposed project site are estimated to be 27 in the morning peak hour and 87 in the afternoon peak hour. The new daily trips generated for the proposed project site are estimated to be 966 trips per day.

All of the analyzed intersections currently operate at an acceptable level of service (A-C) during the AM and PM peak hours under existing conditions. All of the analyzed intersections will also operate at an acceptable level of service (A-C) during the AM peak hour under existing plus approved/pending development conditions. There will be two intersections that will operate at an unacceptable level of service (D-F) during the PM peak hour under existing plus approved/pending development conditions.

The project will not cause any project-specific impacts (as identified by a change in the V/C ratio of 0.02 or more at LOS C, D, E or F).

There will be one intersection that will operate at an unacceptable level of service (D-F) during the AM peak hour under Year 2030 conditions. There will be four intersections that will operate at an unacceptable level of service (D-F) during the PM peak hour under Year 2030 conditions.

The project will not cause any Year 2030 impacts (as identified by a change in the V/C ratio of 0.02 or more at LOS C, D, E or F).

The project will not significantly impact the adjacent residential neighborhoods. Project access is proposed via driveways accessing directly onto Channel Islands Boulevard, Rose Avenue, or Raiders Way.

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INTRODUCTION

This report presents the methodology and findings of a Traffic Impact Analysis for a proposed project consisting of a supermarket, retail shops and a church at the Southwest Corner of Rose Avenue & Channel Islands Boulevard in the City of Oxnard, California, and is in compliance with the California Environmental Quality Act and City of Oxnard Requirements.

Purpose and Scope

This analysis addresses project access and internal circulation, existing conditions, existing + approved/pending development conditions (pre-project conditions), and existing + approved/pending + project conditions (post-project conditions), and year 2030 conditions with the proposed project. The potential effects of project traffic within the adjacent residential neighborhoods are also evaluated.

Intersections Considered in the Analysis

The list of study intersections was developed in coordination with City of Oxnard staff and in consideration of the City's intersection evaluation guidelines. Seven existing intersections, as shown in Exhibit 1, were selected for analysis (and new traffic count data):

- Rose Avenue / Wooley Road
- Rose Avenue / Oxnard Blvd
- Rose Avenue / Channel Islands Blvd
- Rose Avenue / Raiders Way
- Rose Avenue / Bard Road
- Statham Blvd / Channel Islands Blvd
- Rice Avenue / Channel Islands Blvd

Project access from the three proposed project access driveways was also evaluated.

PROJECT DESCRIPTION

The project site is bounded by an existing residential development to the west, Raiders Way to the south, Rose Avenue to the east, and Channel Islands Boulevard to the north. The proposed project site plan is shown in Exhibit 2. The project site is bounded by Cota Circle and an existing residential development to the west, Raiders Way to the south, Rose Avenue to the east, and Channel Islands Boulevard to the north. The project proposes to construct a 13,929 square foot supermarket, 5,630 square feet of retail shops and a 6,000 square foot church. The 4.05 acre property is zoned Medium Density Residential (R-2) with a general plan land use designation of Low to Medium Residential. The applicant is requesting a General Plan Amendment to Commercial General (CG), and a zone change to General Commercial Planned Development (C-2-PD) to allow the development of the supermarket, retail shops and church.

The three proposed primary project access driveways will include a full-access driveway at Raiders Way, a right-in, right-out access driveway at Channel Islands Boulevard, and a right-in, right-out access driveway at Rose Avenue.

Project Trip Generation

Project Trip Generation was estimated by reviewing various publications, including the Institute of Transportation Engineers (ITE) publication entitled Trip Generation (Seventh Edition). Trip generation estimates were made for project morning and evening inbound and outbound traffic, using the ITE land use codes for Supermarket, Specialty Retail and Church. An interaction factor of 20 percent and a pass-by factor of 35 percent, as supported by ITE Trip Generation surveys,

were applied to the Supermarket and Specialty Retail land uses. The interaction factor (also known as internal capture) accounts for patrons visiting more than one project land use during a trip, and the pass-by factor accounts for patrons visiting the project site land uses as they are driving by the project site to another destination. The Project Trip Generation results are shown in Exhibit 3, which shows a total of 966 trips on a daily basis, a total of 27 trips occurring during the morning peak hour and a total of 87 trips occurring during the afternoon peak hour.

Project Trip Distribution and Assignment

Traffic distribution and assignment patterns for the traffic from the proposed project were developed based on a review of the area-wide traffic circulation system, existing traffic count information, and aerial photographs. Project Trip Distribution is shown in Exhibit 4. The resulting AM and PM peak hour intersection turning movement volumes from the assignment of the project traffic to specific routes are shown in Exhibit 5.

PROJECT ACCESS AND INTERNAL CIRCULATION

The project's primary full-access driveway at Raiders Way, in addition to the project's two primary right-in and right-out access driveways at Rose Avenue and Channel Islands Boulevard, will adequately serve the project site. Project access and internal circulation has been adjusted on the site plan to allow truck turning movements from the Rose Avenue curb lane, a minimum 25' radius driveway curb return and a 2 foot offset at parking stalls that are located at the corner of perpendicular aisleways.

PROJECT IMPACT DETERMINATIONS

Project impacts were evaluated by comparing pre-project and post-project traffic conditions for various scenarios. Since the traffic volumes on the City's street system are focused at intersections, it is appropriate to analyze the effects of project traffic on intersection level of service.

Intersection LOS Methodology

The methodology used to analyze the operational efficiency of signalized intersections in this study was the Intersection Capacity Utilization (ICU) Methodology. The ICU methodology involves the calculation of a volume-to-capacity ratio which is related to a level of service (A to F). The ICU value that is calculated with this method represents the proportion of an hour required to accommodate traffic if all approaches operate at capacity. Although intersection operational conditions may be more congested during short periods within the peak hour, an ICU analysis of the entire peak hour is the generally accepted method of quantifying intersection operation. A description of operating conditions and the definition of the ICU range for each level of service is provided in the Appendix.

Evaluation Scenarios

The scenarios evaluated in this study were developed in coordination with City of Oxnard staff and in consideration of the City's traffic study guidelines. Six scenarios were selected for quantitative evaluation of AM and PM peak hour conditions:

- a. Existing
- b. Existing + Pending/Approved
- c. Existing + Pending/Approved + Project
- d. Existing + Pending/Approved + Project + Mitigation
- e. Year 2030
- f. Year 2030 + Project

Year 2030 traffic conditions were also qualitatively evaluated by comparing proposed project land use trip generation to the project site trip generation that would be anticipated under General Plan land use buildout conditions.

Impact Significance Criteria

The City of Oxnard's criteria for evaluating project impacts at intersections is based on the change in ICU/LOS attributable to the project (per City Resolution No. 10,453). The City's minimum acceptable intersection operational standard is LOS C. If an intersection operates in the LOS "C", "D", "E", or "F" ranges with existing-plus-pending developments traffic and a change in ICU of 0.02 or greater is generated by the project under study, the impact is considered significant. The level of service must then be mitigated to the ICU level identified without the project traffic.

EXISTING CONDITIONS

Field reviews were conducted to establish the existing transportation system conditions in the vicinity of the project site and within the study area.

Existing Street Network

The primary north-south street providing access to the project will be Rose Avenue. The primary east-west street providing access to the project will be Channel Islands Boulevard. Raiders Way will also provide access to the southern portion of the project site. Oxnard Boulevard (Highway 1) will serve as the primary regional access facility.

Existing Volumes and Levels of Service

Seven new AM and PM peak hour counts were conducted at the local intersections in the immediate vicinity of the project site. Existing peak hour traffic volumes are shown in Exhibit 6a and Exhibit 6b. Traffic Count Data is provided in the Appendix. The existing AM and PM peak hour traffic volumes, lane configurations, signal phasing and LOS for each of the study intersections are presented within the Appendix. Detailed intersection LOS calculation sheets are also provided in the Appendix. The existing AM and PM peak hour intersection LOS conditions are summarized in Exhibits 7 and 8.

The results indicate that each of the intersections studied operate at an acceptable level of service (A-C) during the AM peak hour for existing conditions. Each of the study intersections also operate at an acceptable level of service (A-C) during the PM peak hour for existing conditions.

EXISTING + APPROVED/PENDING DEVELOPMENT CONDITIONS

The existing + approved/pending developments traffic volumes were obtained from the City of Oxnard traffic model, and from intersection specific traffic projections for the local intersections in the immediate vicinity of the project site. The existing + approved/pending developments traffic volumes are shown in Exhibit 9a and Exhibit 9b, and model output volume tables are provided in the Appendix. The US 101 Freeway / Oxnard Boulevard interchange was assumed to be fully operational in this scenario.

The lane configurations and the signal phasing at the study intersections under this scenario were assumed to be the same as those observed under existing conditions. The existing + approved/pending developments AM and PM peak hour traffic volumes, lane configurations, signal phasing and LOS for each of the study intersections are presented within the Appendix. Detailed intersection LOS calculation sheets are also provided in the Appendix, and the resultant AM and PM peak hour intersection LOS conditions are summarized in Exhibits 7 and 8.

The results indicate that each of the intersections studied operates at an acceptable level of service (A-C) during the AM peak hour for existing + approved/pending development conditions. Five of the existing study intersections will also operate at an acceptable level of service (A-C) during the PM peak hour. The two study intersections that are anticipated to operate at an unacceptable level of service for existing + approved/pending development conditions include Rose Avenue / Wooley Road (LOS D) and Rose Avenue/ Oxnard Boulevard (LOS D).

EXISTING + APPROVED/PENDING + PROJECT CONDITIONS

The existing + approved/pending + project traffic volumes were developed by adding the project traffic volumes from Exhibit 5 to the existing + approved/pending developments traffic volumes. The existing + approved/pending + project traffic volumes are shown in Exhibit 10a and Exhibit 10b. The US 101 Freeway / Oxnard Boulevard interchange was again assumed to be fully operational.

The lane configurations and signal phasing at the study intersections under this scenario were assumed to be the same as those observed under existing conditions and the pre-project scenario. The existing + approved/pending + project AM and PM peak hour traffic volumes, lane configurations, signal phasing and LOS for each of the study intersections are presented within the Appendix. Detailed intersection LOS calculation sheets are also provided in the Appendix, and the AM and PM peak hour intersection LOS conditions are summarized in Exhibits 7 and 8.

These results indicate that the project will cause a less than significant increase in the V/C ratio at each of the intersections studied. The largest project-related increases in the V/C ratio during the AM peak hour will occur at the Rose Avenue / Raiders Way intersection (V/C increase = 0.005). The largest project-related increases in the V/C ratio during the PM peak hour will occur at the Rose Avenue / Raiders Way intersection (V/C increase = 0.019), the Rose Avenue / Bard Road intersection (V/C increase = 0.009), the Rose Avenue / Channel Islands intersection (V/C increase = 0.006), and the Rose Avenue / Oxnard Boulevard intersection (V/C increase = 0.006).

PROJECT-SPECIFIC IMPACTS ANALYSIS AND MITIGATION

A review of Exhibits 7 and 8 identifies that no project-specific impacts (as identified by an LOS C, D, E or F change in the V/C ratio of 0.02 or more) will occur during either the AM or PM peak hours, and no mitigation will be necessary.

YEAR 2030 CONDITIONS

The projected year 2030 transportation system conditions without the proposed project were evaluated for intersections in the vicinity of the project site and within the study area. No anticipated roadway construction projects were included in the traffic evaluations. This year 2030 scenario establishes the year 2030 base conditions that would occur without the proposed project.

Year 2030 Street Network

The primary north-south street providing access to the project in year 2030 will continue to be Rose Avenue; and the primary east-west street providing access to the project in year 2030 will continue to be Channel Islands Boulevard. Raiders Way will provide access to the southern portion of the project site and Oxnard Boulevard (Highway 1) will serve as the primary regional access facility. No major roadway widening projects were assumed to be operational at the study intersections for the year 2030 traffic evaluations. The existing travel lanes were incorporated into the year 2030 intersection LOS analyses.

Year 2030 Volumes and Levels of Service

The year 2030 traffic volumes (without project traffic) were obtained from the City of Oxnard traffic model, and from intersection specific traffic projections for the local intersections in the immediate vicinity of the project site. The year 2030 traffic volumes (without project traffic) are shown in Exhibit 11a and Exhibit 11b, and model output volume tables are provided in the Appendix. The year 2030 AM and PM peak hour traffic volumes, lane configurations, signal phasing and LOS for each of the study intersections are presented within the Appendix. Detailed intersection LOS calculation sheets are also provided in the Appendix, and the year 2030 AM and PM peak hour intersection LOS conditions are summarized in Exhibits 12 and 13.

The results indicate that one of the intersections studied will operate at an unacceptable level of service (D-F) during the year 2030 AM peak hour, and four of the intersections studied will operate at an unacceptable level of service (D-F) during the year 2030 PM peak hour. The study intersection that is anticipated to operate at an unacceptable level of service during the year 2030 AM peak hour is Rose Avenue/ Wooley Road (LOS D). The four study intersections that are anticipated to operate at an unacceptable level of service during the year 2030 PM peak hour include Rose Avenue / Wooley Road (LOS F), Rose Avenue/ Oxnard Road (LOS E), Rose Avenue / Channel Islands (LOS E), and Rice Avenue/ Channel Islands Boulevard (LOS E).

YEAR 2030 WITH PROJECT CONDITIONS

The year 2030 with project conditions were developed by considering the year 2030 traffic conditions from the City of Oxnard traffic model, subtracting the traffic volumes that would result from buildout of the project site under the existing general plan residential land uses (medium residential at 8-12 units per acre), and then adding the traffic volumes that would result from buildout of the project site under the proposed project land uses (supermarket, retail shops and church). The traffic volumes that would result from buildout of the project site under the existing general plan residential land uses and the traffic volumes that would result from buildout of the project site under the proposed project land uses are shown in Exhibit 14. The net difference in the traffic volumes produced by these land uses is also shown in Exhibit 14. The year 2030 with project traffic volumes are shown in Exhibit 15a and Exhibit 15b. The lane configurations and signal phasing at the study intersections under this scenario were assumed to be the same as those assumed under the year 2030 (without project) scenario.

The year 2030 + project AM and PM peak hour traffic volumes, lane configurations, signal phasing and LOS for each of the study intersections are presented within the Appendix. Detailed intersection LOS calculation sheets are provided in the Appendix, and the AM and PM peak hour intersection LOS conditions are summarized in Exhibits 12 and 13.

The results for the year 2030 + project conditions scenario are similar to the results for the existing + approved/pending developments + project conditions scenario. The year 2030 + project results indicate that the project will not cause any significant increase in the V/C ratio at the studied intersections. There will be no significant project-related increases in the V/C ratio at any of the intersections studied during the AM peak hour (with the V/C ratio remaining essentially unchanged at each intersection). The largest project-related increases in the V/C ratio during the PM peak hour will occur at the Rose Avenue / Raiders Way intersection (V/C increase = 0.008), the Rose Avenue / Bard Road intersection (V/C increase = 0.003), the Rose Avenue / Channel Islands intersection (V/C increase = 0.003), and the Rose Avenue / Oxnard Boulevard intersection (V/C increase = 0.003).

YEAR 2030 PROJECT IMPACTS ANALYSIS AND MITIGATION

The project will not cause any significant Year 2030 impacts (as identified by an LOS C, D, E or F change in the V/C ratio of 0.02 or more) at the studied intersections and will not require any mitigation measures.

ADJACENT RESIDENTIAL NEIGHBORHOODS TRAFFIC EVALUATION

The proposed project will be constructed adjacent to a residential neighborhood located to the west of the proposed project site. This residential neighborhood consists of existing single family residences within a gated community that can be accessed via Cota Circle. Project access is proposed via driveways accessing directly onto Channel Islands Boulevard, Rose Avenue, or Raiders Way. The project will not significantly impact this adjacent residential neighborhood.

The single family homes residential neighborhood to the west of the project site takes its access from Cota Circle, and will be relatively isolated from the effects of project traffic. There will be no reason for the project's traffic to utilize the street system within the single family homes residential neighborhood, except for a minimal amount of destination-specific trips between the residential neighborhood and the proposed commercial development. Consequently, the proposed project will not impact the residential neighborhood to the west of the project.

CONCLUSIONS

The total trips generated for the proposed project site are estimated to be 27 in the morning peak hour and 87 in the afternoon peak hour. The daily trips generated for the proposed project site are estimated to be 966 trips per day.

All of the analyzed intersections currently operate at an acceptable level of service (A-C) during both the AM and PM peak hours under existing conditions.

There will be two of the analyzed intersections that will operate at an unacceptable level of service (D-F) during the PM peak hour under existing plus approved/pending development conditions.

The project will not cause any project-specific impacts under existing plus approved/pending plus project conditions (as identified by an LOS C, D, E or F change in the V/C ratio of 0.02 or more).

There will be four of the analyzed intersections that will operate at an unacceptable level of service (D-F) during the PM peak hour under Year 2030 conditions.

The project will not cause any significant cumulative impacts under year 2030 plus project conditions (as identified by an LOS C, D, E or F change in the V/C ratio of 0.02 or more).

The project will not significantly impact any adjacent residential neighborhoods.

Southwest Corner of Rose Avenue and Channel Islands

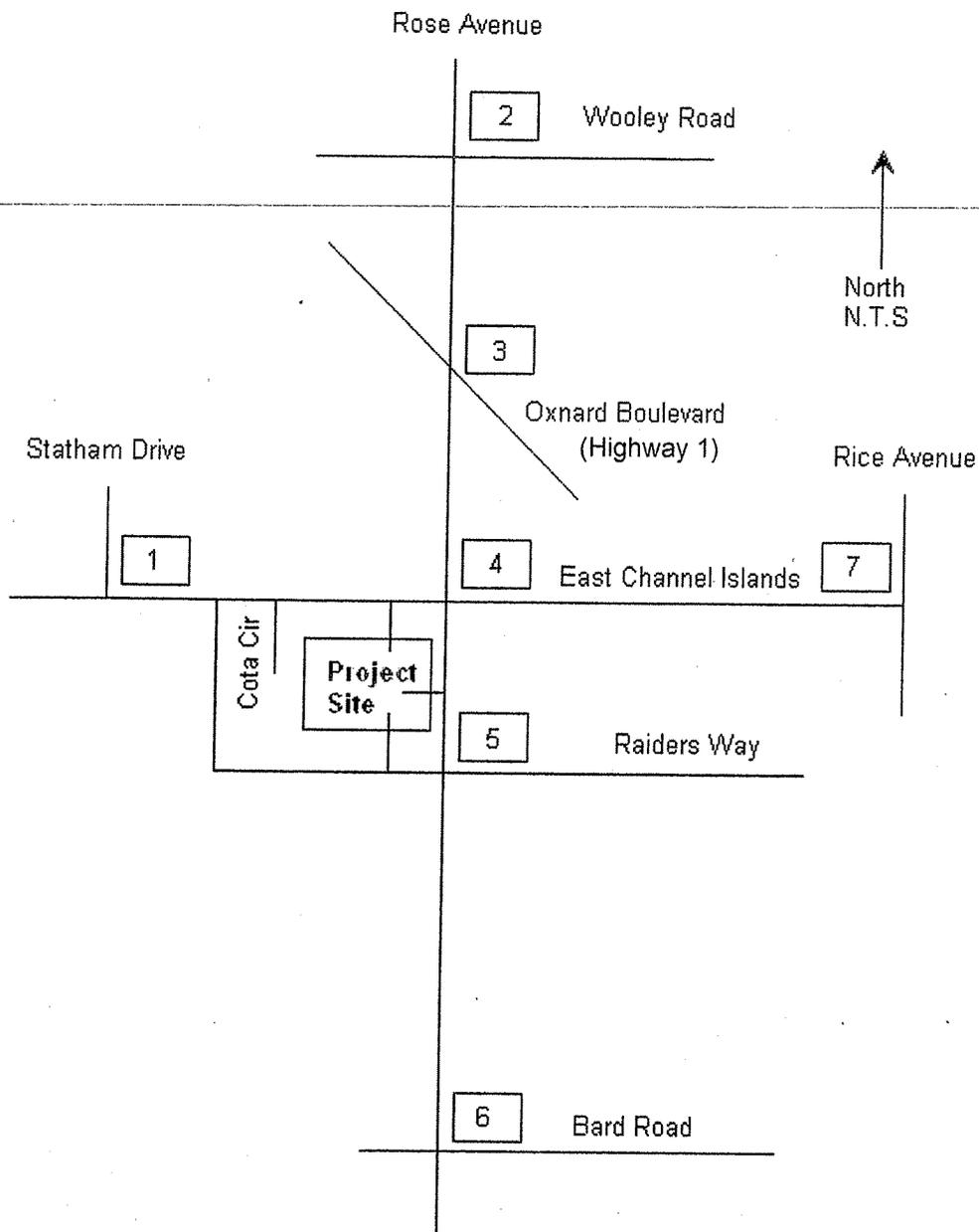


EXHIBIT 1 – Project Study Area and Intersections

EXHIBIT 2 - Project Site Plan

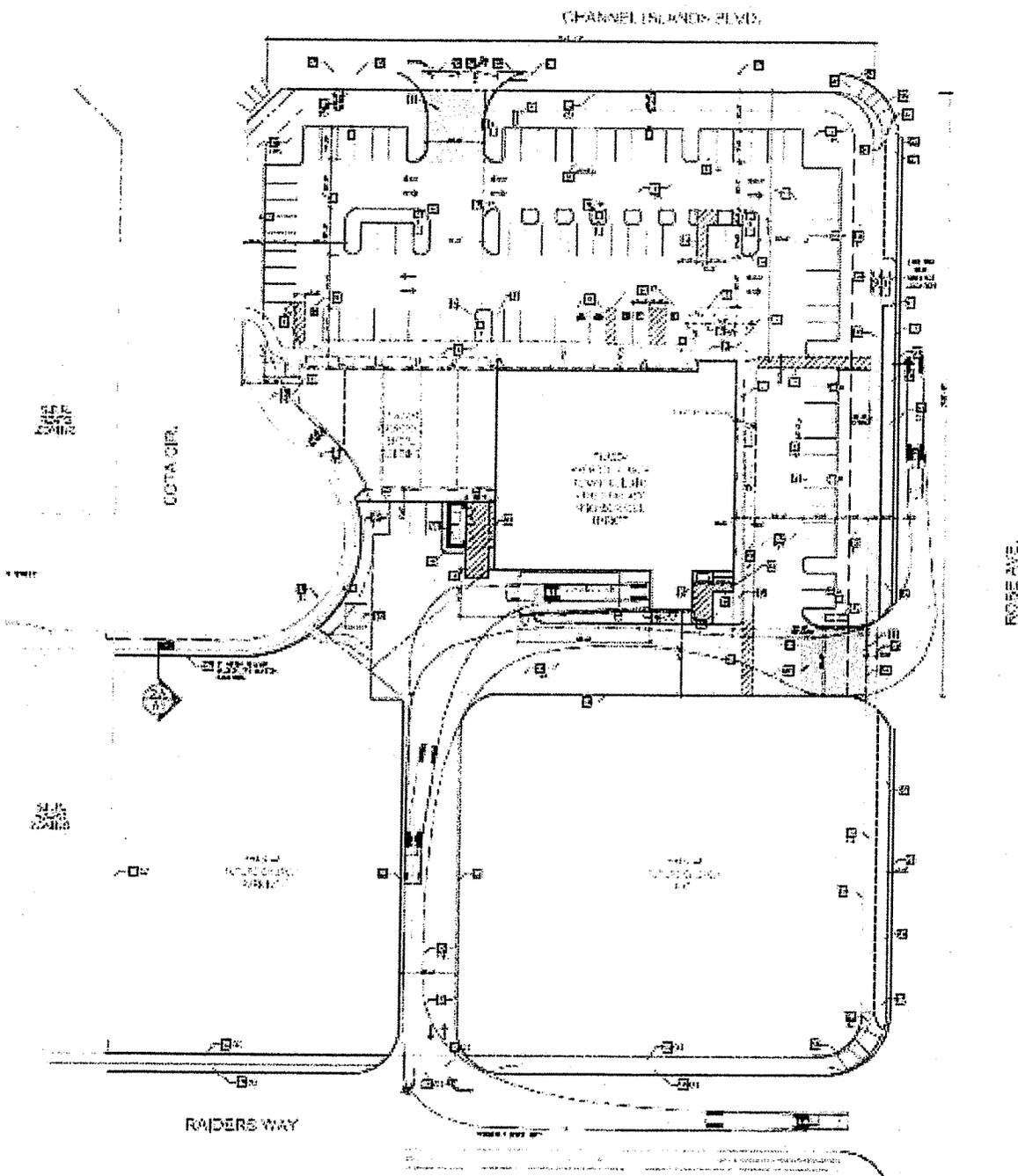


EXHIBIT 3 - Project Trip Generation Table

Southwest Corner of Rose Ave / Channel Islands Blvd Trip Generation

ITE 7th Edition Trip Generation Rates

Site Plan Land Use Descriptions	Land Use Category	Unit	AM Peak Hour Rate			PM Peak Hour Rate			ADT Rate
			Inbound %	Outbound %	Rate	Inbound %	Outbound %	Rate	
Tesco	Supermarket	1 Thousand Square-Feet	61%	39%	3.2500	51%	49%	10.4500	102.2400
Shops	Retail	1 Thousand Square-Feet	61%	39%	1.0300	48%	52%	3.7500	42.9400
Church	Church	1 Thousand Square-Feet	54%	46%	0.7200	52%	48%	0.6600	9.1100

Proposed Project Site Land Use Trip Generation

Proposed Project Site Descriptions	Proposed Project Site Land Uses	Amount	AM Peak Hour			PM Peak Hour			ADT
			Inbound	Outbound	Total	Inbound	Outbound	Total	
Tesco	Supermarket	13,929 Thousand Square-Feet	28	18	45	74	71	146	1,424
Shops	Retail	5.63 Thousand Square-Feet	4	2	6	10	11	21	242
Church	Church	6 Thousand Square-Feet	2	2	4	2	2	4	55
SUBTOTAL (Tesco + Shops)			31	20	51	84	82	167	1,666
TOTAL (Proposed Project Site Land Use Trips)			33	22	55	86	84	171	1,721

Project Trip Reduction based on 20% Reduction for Captured Trips

Site Plan Land Use Descriptions	Adjustment	Reduction Percentage	AM Peak Hour			PM Peak Hour			ADT
			Inbound	Outbound	Total	Inbound	Outbound	Total	
Tesco + Shops	Interaction Reduction	20% Reduction Amount	-6	-4	-10	-17	-16	-33	-333
Tesco + Shops TOTAL (based on 20% Interaction)			25	16	41	67	66	133	1,333

Project Trip Reduction based on 35% Reduction for Passby Trips

Site Plan Land Use Descriptions	Adjustment	Reduction Percentage	AM Peak Hour			PM Peak Hour			ADT
			Inbound	Outbound	Total	Inbound	Outbound	Total	
Tesco + Shops	Passby Reduction	35% Reduction Amount	-9	-6	-14	-24	-23	-47	-466
Tesco + Shops TOTAL (based on 20% Interaction and 35% Passby)			16	10	27	44	43	87	866

Total Project Trip Generation based on 20% Reduction for Captured Trips and 35% Reduction for Passby Trips

Site Plan Land Use Descriptions	Adjustment	Reduction Percentage	AM Peak Hour			PM Peak Hour			ADT
			Inbound	Outbound	Total	Inbound	Outbound	Total	
Tesco and Shops	Interaction + Passby Reduction		16	10	27	44	43	87	921
Church	None		2	2	4	2	2	3	46
TOTAL (based on 20% Interaction and 35% Passby)			18	12	27	46	44	87	966

Southwest Corner of Rose Avenue and Channel Islands

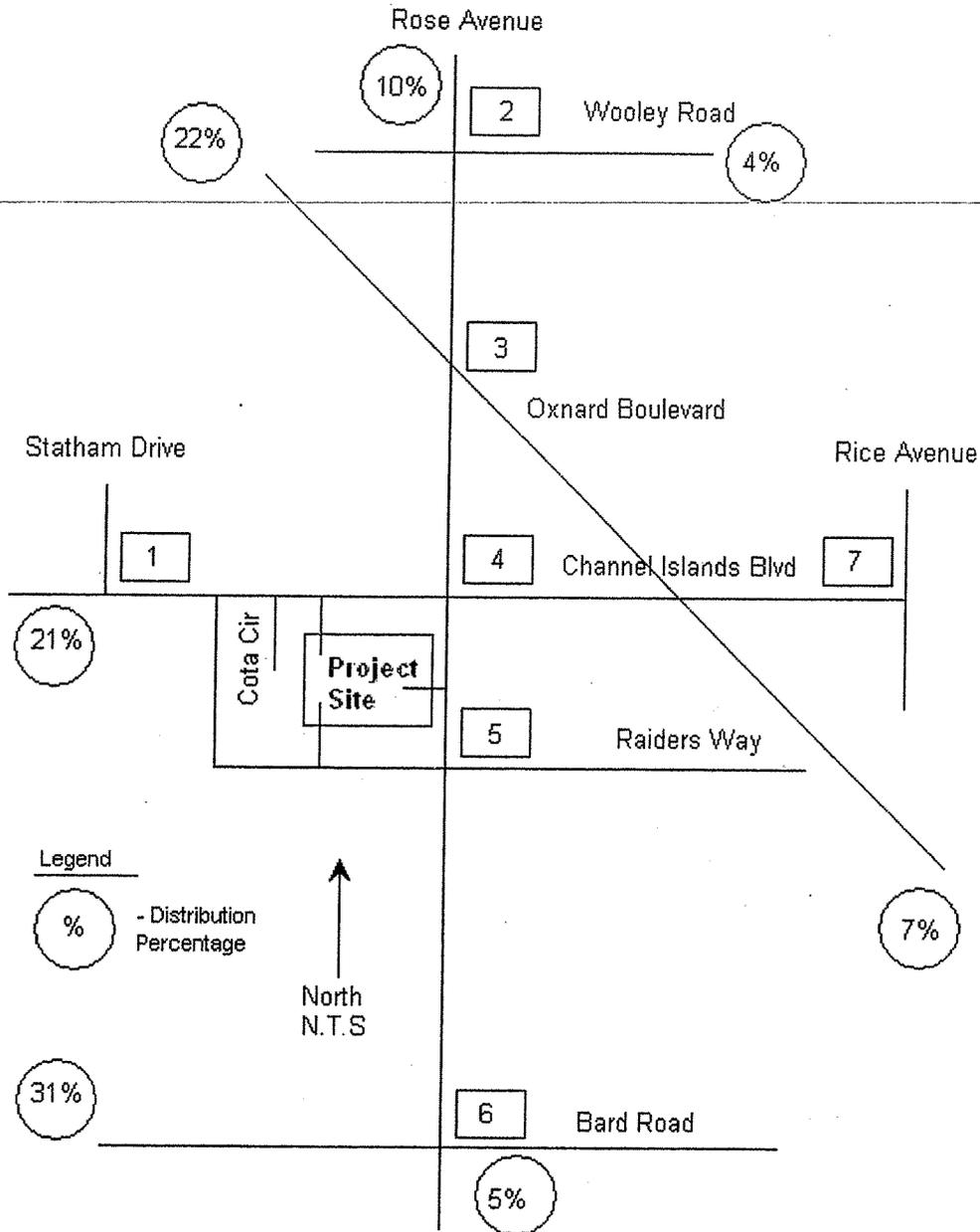


EXHIBIT 4 - Project Trip Distribution

EXHIBIT 5 - Project Traffic Assignment Table

Turning Movement Report
Project AM Trips

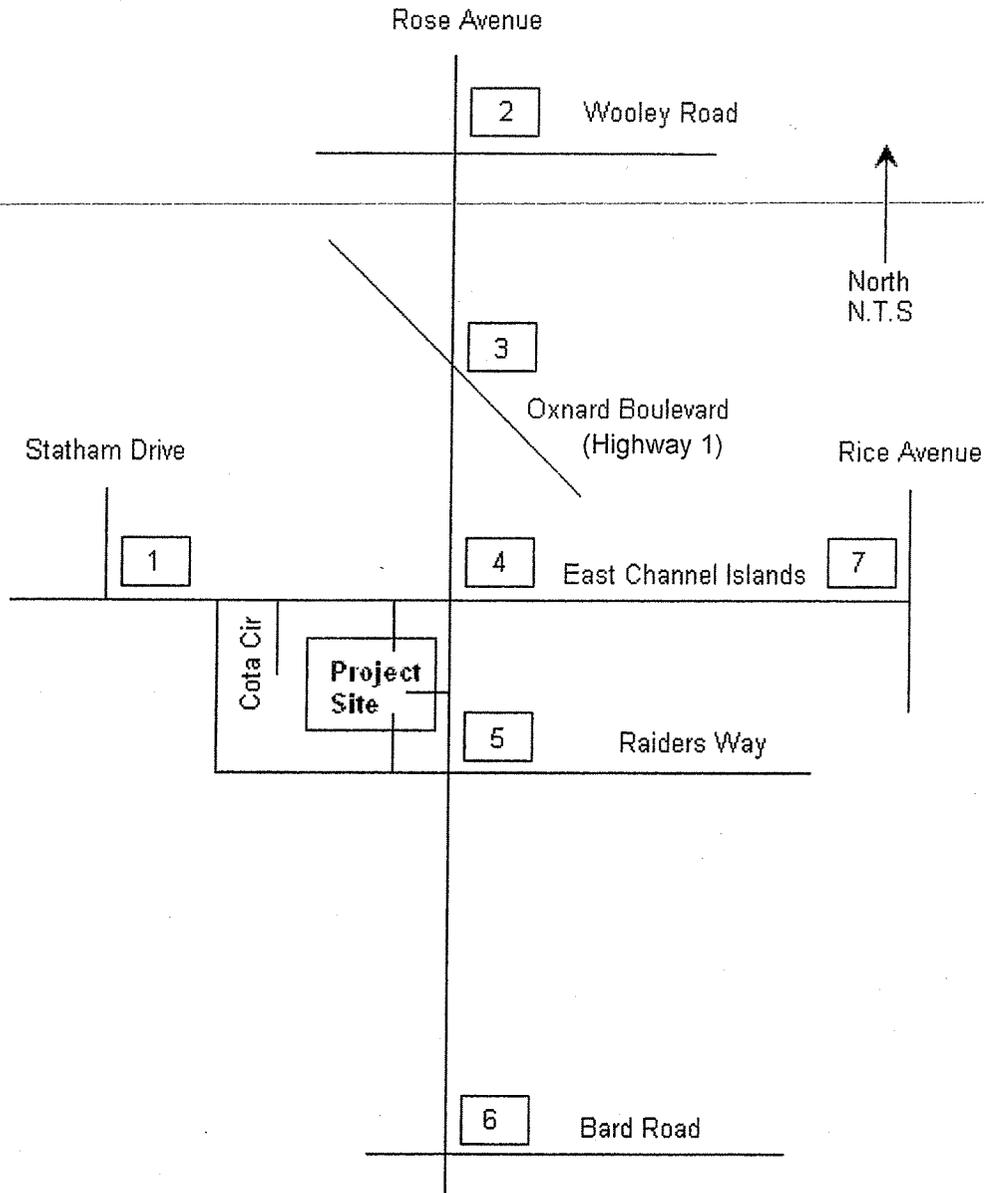
Int. No.	Northbound Left Thru Right	Southbound Left Thru Right	Eastbound Left Thru Right	Westbound Left Thru Right	Total Volume
#1 Statham Blvd / Channel Islands Blvd Added	0 0 0	0 0 0	0 4 0	0 0 3	1 8
#2 Rose Avenue / Wooley Road Added	0 1 0	2 0 0	0 0 0	0 0 0	3
#3 Rose Ave / Oxnard Blvd Added	2 1 0	0 0 0	0 0 4	0 0 0	9
#4 Rose Ave / Channel Islands Added	0 1 0	6 0 0	3 1 0	2 0 0	13
#5 Rose Ave / Raiders Way Added	6 0 0	2 0 0	1 0 2	0 0 0	11
#6 Rose Ave / Bard Rd Added	0 3 0	1 2 2	3 0 0	0 0 0	12
#7 Rice Ave / Channel Islands Blvd Added	1 0 0	0 0 0	1 0 0	0 0 0	2

EXHIBIT 5 - Project Traffic Assignment Table

Turning Movement Report
Project PM Trips

Int. No.	Northbound Left Thru Right	Southbound Left Thru Right	Eastbound Left Thru Right	Westbound Left Thru Right	Total Volume
#1 Statham Blvd / Channel Islands Blvd Added	0 0 0	0 0 0	0 10 0	0 0 0	3 22
#2 Rose Avenue / Wooley Road Added	0 5 0	0 0 5	0 0 0	0 0 0	10
#3 Rose Ave / Oxnard Blvd Added	7 5 0	0 5 0	0 0 0	10 0 0	27
#4 Rose Ave / Channel Islands Added	0 1 1	0 15 0	11 4 0	5 0 0	37
#5 Rose Ave / Raiders Way Added	17 0 0	0 6 0	1 0 10	0 0 0	34
#6 Rose Ave / Bard Rd Added	0 7 0	2 7 7	7 0 0	0 0 2	32
#7 Rice Ave / Channel Islands Blvd Added	1 0 0	0 0 0	2 2 0	0 0 0	5

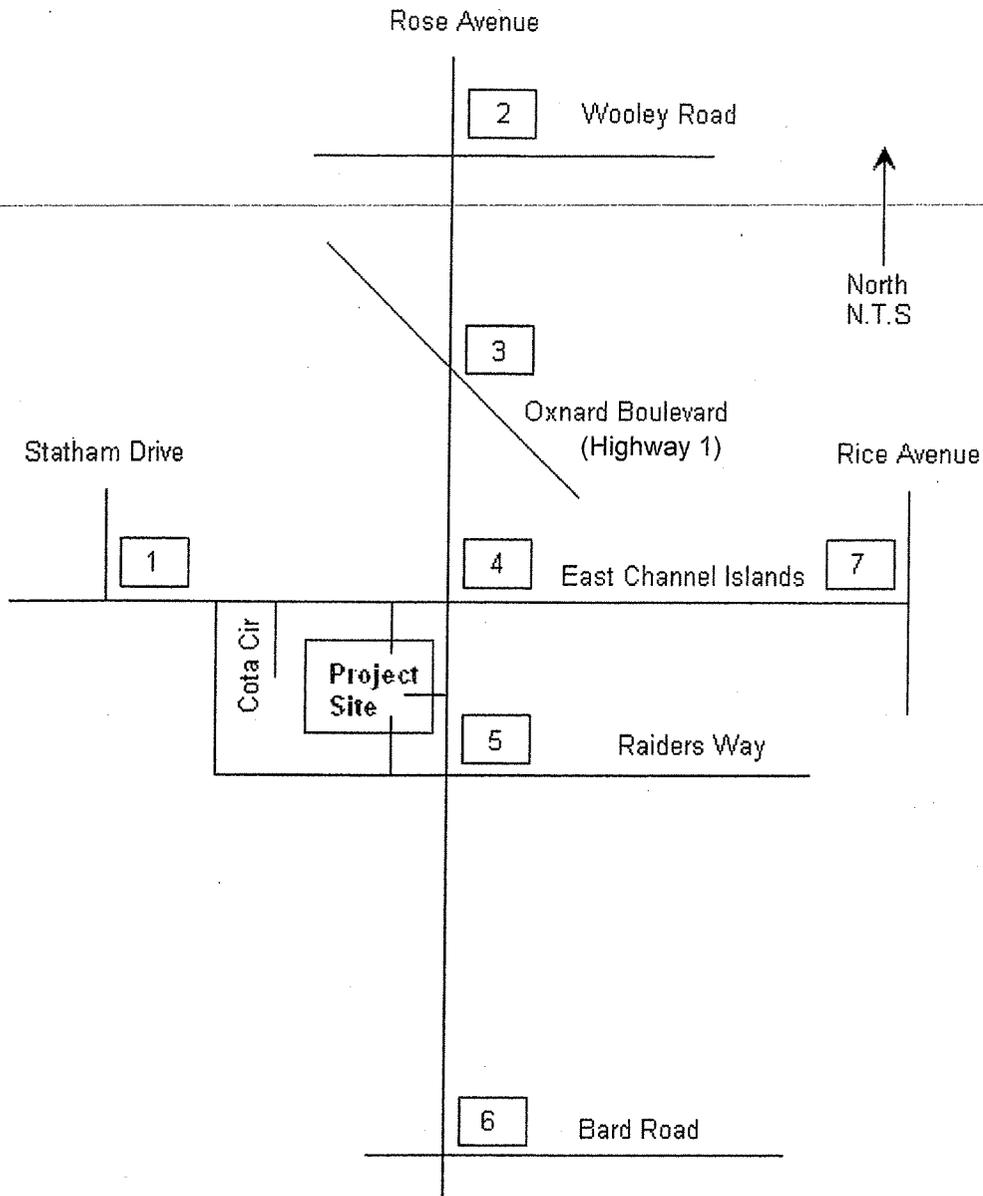
Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 22 \\ 85 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 272 \\ 1289 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 74 \\ 673 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 268 \\ 1048 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 18 \\ 18 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 3 \\ 143 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 31 \\ 1089 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 81 \\ 81 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 680 \\ 34 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 258 \\ 258 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 40 \\ 246 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 178 \\ 838 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 13 \\ 13 \end{matrix}$		
$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 116 \\ 561 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 460 \\ 782 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 97 \\ 97 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 3 \\ 367 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 300 \\ 611 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 91 \\ 91 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 125 \\ 746 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 139 \\ 52 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 236 \\ 164 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 221 \\ 880 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 97 \\ 470 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 236 \\ 164 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 33 \\ 33 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 105 \\ 105 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 121 \\ 104 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 15 \\ 15 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 146 \\ 663 \end{matrix}$ $\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 462 \\ 44 \end{matrix}$	$\begin{matrix} \uparrow \\ \downarrow \end{matrix} \begin{matrix} 89 \\ 639 \end{matrix}$

EXHIBIT 6a - Existing AM Peak Hour Traffic Volumes

Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \square \\ \downarrow \\ 210 \\ \uparrow \\ 86 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 45 \\ \uparrow \\ 1220 \end{matrix}$	$\begin{matrix} \square \\ \downarrow \\ 1393 \\ \uparrow \\ 27 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 1 \\ \uparrow \\ 408 \\ \uparrow \\ 188 \end{matrix}$	$\begin{matrix} \square \\ \downarrow \\ 1063 \\ \uparrow \\ 60 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 55 \\ \uparrow \\ 589 \end{matrix}$
$\begin{matrix} 166 \\ \downarrow \\ 842 \\ \uparrow \end{matrix}$		$\begin{matrix} 367 \\ \downarrow \\ 260 \\ \uparrow \\ 38 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 39 \\ \uparrow \\ 1019 \\ \uparrow \\ 68 \end{matrix}$	$\begin{matrix} 242 \\ \rightarrow \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 140 \\ \uparrow \\ 788 \\ \uparrow \\ 10 \end{matrix}$
$\begin{matrix} \square \\ \downarrow \\ 309 \\ \uparrow \\ 821 \\ \uparrow \\ 213 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 4 \\ \uparrow \\ 771 \\ \uparrow \\ 227 \end{matrix}$	$\begin{matrix} \square \\ \downarrow \\ 1141 \\ \uparrow \\ 5 \end{matrix}$		$\begin{matrix} \square \\ \downarrow \\ 227 \\ \uparrow \\ 550 \\ \uparrow \\ 81 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 98 \\ \uparrow \\ 347 \\ \uparrow \\ 15 \end{matrix}$
$\begin{matrix} 494 \\ \downarrow \\ 500 \\ \uparrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 230 \\ \uparrow \\ 611 \\ \uparrow \\ 36 \end{matrix}$	$\begin{matrix} 20 \\ \downarrow \\ 11 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 16 \\ \uparrow \\ 937 \end{matrix}$	$\begin{matrix} 159 \\ \downarrow \\ 144 \\ \uparrow \\ 33 \end{matrix}$	$\begin{matrix} \uparrow \\ \uparrow \\ 45 \\ \uparrow \\ 401 \\ \uparrow \\ 14 \end{matrix}$
					$\begin{matrix} \square \\ \downarrow \\ 610 \\ \uparrow \\ 798 \end{matrix}$
					$\begin{matrix} 233 \\ \downarrow \\ 38 \end{matrix}$
					$\begin{matrix} \uparrow \\ \uparrow \\ 307 \\ \uparrow \\ 924 \end{matrix}$

EXHIBIT 6b – Existing PM Peak Hour Traffic Volumes

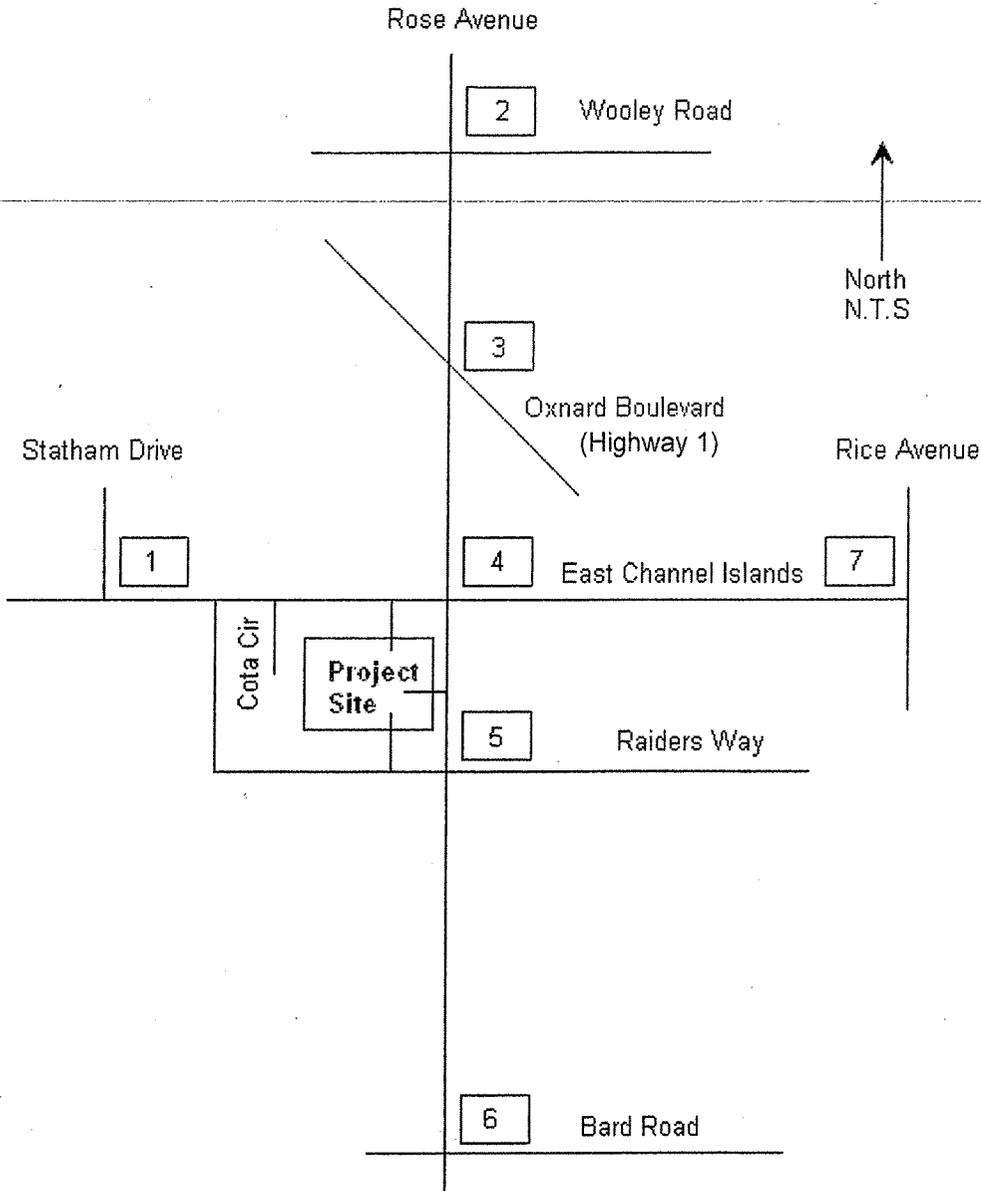
Exhibit 7 - AM Peak Hour Project-Specific LOS Evaluations Summary Table

Intersection	Existing AM		Existing plus PendAppr AM		Existing plus PendAppr plus Project AM		Project Specific Impacts		
	AM LOS	Crit V/C	AM LOS	Crit V/C	AM LOS	Crit V/C	Crit V/C Change	Total Project Trips	Project Impact?
#1 Statham Blvd / Channel Islands Blvd	A	0.43	A	0.566	A	0.567	0.001	8	No
#2 Rose Avenue / Wooley Road	A	0.502	A	0.569	A	0.569	0	3	No
#3 Rose Ave / Oxnard Blvd	A	0.401	A	0.503	A	0.505	0.002	9	No
#4 Rose Ave / Channel Islands	A	0.584	A	0.569	A	0.57	0.001	13	No
#5 Rose Ave / Raiders Way	A	0.458	A	0.484	A	0.489	0.005	11	No
#6 Rose Ave / Bard Rd	A	0.502	A	0.556	A	0.56	0.004	12	No
#7 Rice Ave / Channel Islands Blvd	A	0.453	A	0.4	A	0.401	0.001	2	No

Exhibit 8 - PM Peak Hour Project-Specific LOS Evaluations Summary Table

Intersection	Existing PM		Existing plus PendAppr PM		Existing plus PendAppr plus Project PM		Project Specific Impacts		
	PM LOS	Crit V/C	PM LOS	Crit V/C	PM LOS	Crit V/C	Crit V/C Change	Total Project Trips	Project Impact?
#1 Statham Blvd / Channel Islands Blvd	A	0.565	C	0.738	C	0.741	0.003	22	No
#2 Rose Avenue / Wooley Road	C	0.702	D	0.841	D	0.842	0.001	10	No
#3 Rose Ave / Oxnard Blvd	B	0.604	D	0.872	D	0.878	0.006	27	No
#4 Rose Ave / Channel Islands	C	0.719	C	0.742	C	0.748	0.006	37	No
#5 Rose Ave / Raiders Way	A	0.379	A	0.425	A	0.444	0.019	34	No
#6 Rose Ave / Bard Rd	A	0.438	A	0.506	A	0.515	0.009	32	No
#7 Rice Ave / Channel Islands Blvd	C	0.705	C	0.716	C	0.718	0.002	5	No

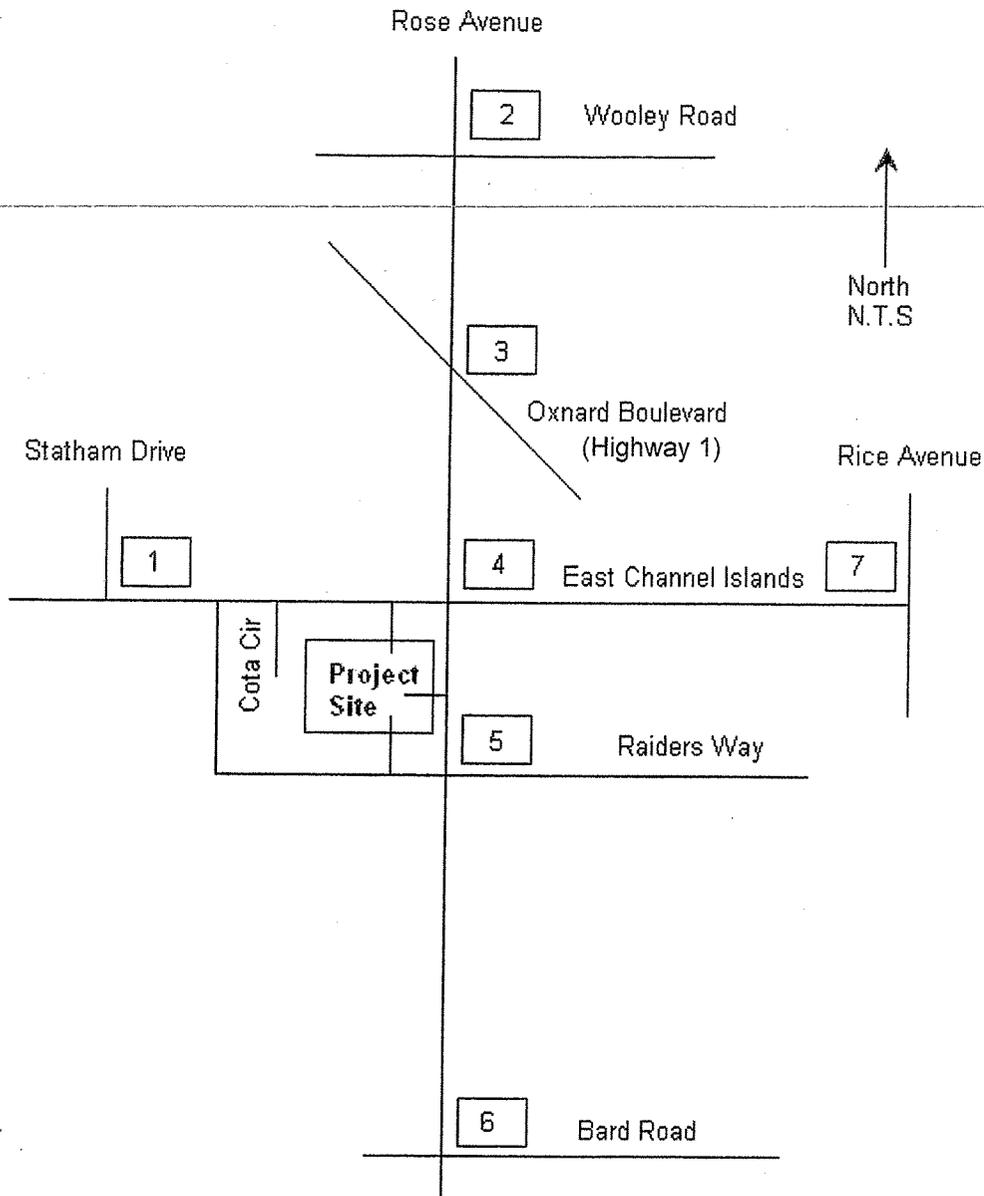
Southwest Corner of Rose Avenue and Channel Islands



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$\begin{matrix} 460 \\ \downarrow 1280 \\ \uparrow \end{matrix}$		$\begin{matrix} 370 \\ \downarrow 460 \\ \uparrow 30 \end{matrix}$	$\begin{matrix} \uparrow 20 \\ \downarrow 790 \\ \uparrow 120 \end{matrix}$	$\begin{matrix} 300 \\ \downarrow \end{matrix}$	$\begin{matrix} \uparrow 240 \\ \downarrow 1140 \\ \uparrow 30 \end{matrix}$		
$\begin{matrix} \boxed{4} \\ \downarrow 180 \\ \uparrow 800 \\ \downarrow 90 \end{matrix}$	$\begin{matrix} \uparrow 30 \\ \downarrow 310 \\ \uparrow 180 \end{matrix}$	$\begin{matrix} \boxed{5} \\ \downarrow 243 \\ \uparrow 875 \\ \downarrow 2 \end{matrix}$		$\begin{matrix} \boxed{6} \\ \downarrow 160 \\ \uparrow 320 \\ \downarrow 80 \end{matrix}$	$\begin{matrix} \uparrow 170 \\ \downarrow 110 \\ \uparrow 30 \end{matrix}$	$\begin{matrix} \boxed{7} \\ \downarrow 130 \\ \uparrow 610 \end{matrix}$	
$\begin{matrix} 570 \\ \downarrow 620 \\ \uparrow 140 \end{matrix}$	$\begin{matrix} \uparrow 90 \\ \downarrow 780 \\ \uparrow 120 \end{matrix}$	$\begin{matrix} 142 \\ \downarrow 105 \end{matrix}$	$\begin{matrix} \uparrow 195 \\ \downarrow 775 \end{matrix}$	$\begin{matrix} 370 \\ \downarrow 220 \\ \uparrow 60 \end{matrix}$	$\begin{matrix} \uparrow 20 \\ \downarrow 540 \\ \uparrow 80 \end{matrix}$	$\begin{matrix} 400 \\ \downarrow 110 \end{matrix}$	$\begin{matrix} \uparrow 70 \\ \downarrow 580 \end{matrix}$

EXHIBIT 9a - Existing + PendAppr AM Peak Hour Traffic Volumes

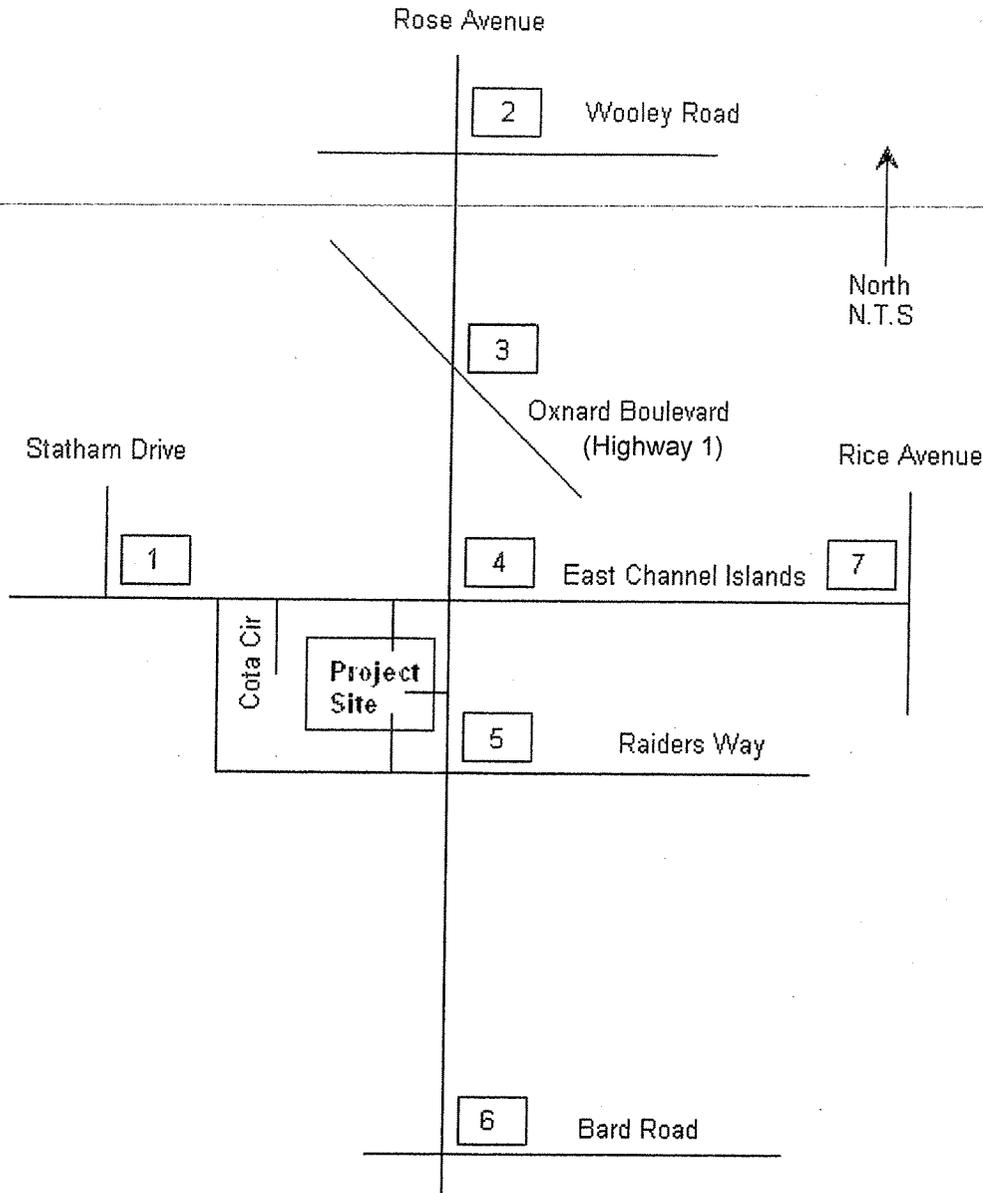
Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \uparrow \\ 460 \\ \downarrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ 70 \\ \downarrow \\ 1360 \end{matrix}$	$\begin{matrix} \uparrow \\ 460 \\ \downarrow \\ 1780 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 350 \\ \downarrow \\ 140 \end{matrix}$	$\begin{matrix} \uparrow \\ 1350 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 100 \\ \downarrow \\ 800 \end{matrix}$		
$\begin{matrix} \uparrow \\ 240 \\ \downarrow \\ 920 \end{matrix}$		$\begin{matrix} \uparrow \\ 320 \\ \downarrow \\ 470 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 910 \\ \downarrow \\ 120 \end{matrix}$	$\begin{matrix} \uparrow \\ 210 \\ \downarrow \end{matrix}$	$\begin{matrix} \uparrow \\ 280 \\ \downarrow \\ 1020 \\ \downarrow \\ 20 \end{matrix}$		
$\begin{matrix} \uparrow \\ 390 \\ \downarrow \\ 980 \\ \downarrow \\ 130 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 850 \\ \downarrow \\ 240 \end{matrix}$	$\begin{matrix} \uparrow \\ 32 \\ \downarrow \\ 1260 \\ \downarrow \\ 6 \end{matrix}$		$\begin{matrix} \uparrow \\ 220 \\ \downarrow \\ 590 \\ \downarrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ 220 \\ \downarrow \\ 230 \\ \downarrow \\ 30 \end{matrix}$	$\begin{matrix} \uparrow \\ 700 \\ \downarrow \\ 560 \end{matrix}$	
$\begin{matrix} \uparrow \\ 390 \\ \downarrow \\ 530 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 220 \\ \downarrow \\ 830 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 23 \\ \downarrow \\ 31 \end{matrix}$	$\begin{matrix} \uparrow \\ 19 \\ \downarrow \\ 1111 \end{matrix}$	$\begin{matrix} \uparrow \\ 240 \\ \downarrow \\ 120 \\ \downarrow \\ 50 \end{matrix}$	$\begin{matrix} \uparrow \\ 70 \\ \downarrow \\ 490 \\ \downarrow \\ 20 \end{matrix}$	$\begin{matrix} \uparrow \\ 190 \\ \downarrow \\ 60 \end{matrix}$	$\begin{matrix} \uparrow \\ 350 \\ \downarrow \\ 790 \end{matrix}$

EXHIBIT 9b - Existing + PendAppr PM Peak Hour Traffic Volumes

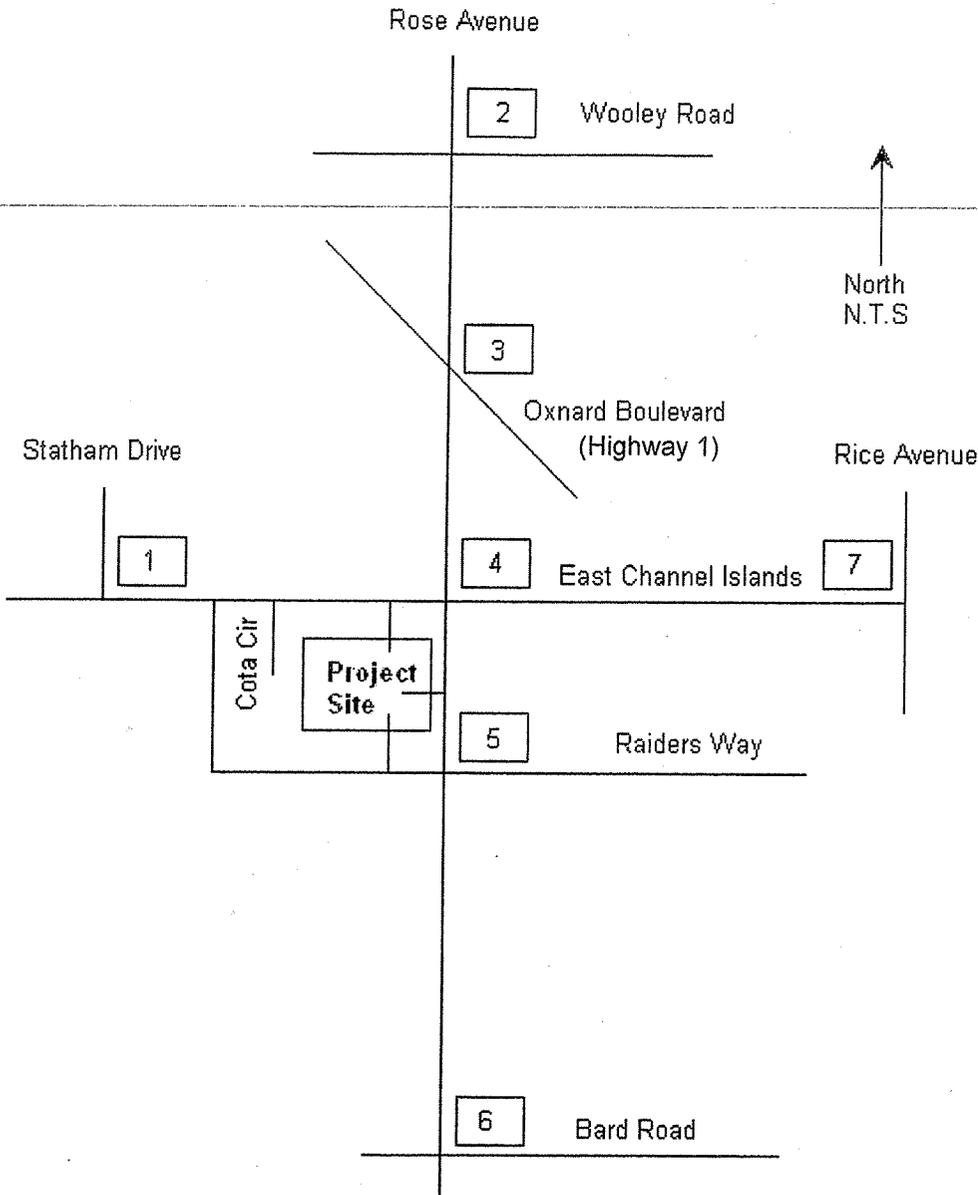
Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 130 \\ 80 \\ 91 \\ 663 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 350 \\ 1132 \\ 20 \\ 10 \\ 200 \\ 80 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 842 \\ 40 \\ 80 \\ 290 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 460 \\ 1284 \\ 30 \\ 20 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 370 \\ 460 \\ 30 \\ 20 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 300 \\ 791 \\ 120 \\ 242 \\ 1141 \\ 30 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 180 \\ 806 \\ 90 \\ 30 \\ 310 \\ 182 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 243 \\ 877 \\ 2 \\ 162 \\ 322 \\ 81 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 171 \\ 110 \\ 30 \\ 131 \\ 610 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 573 \\ 621 \\ 140 \\ 90 \\ 761 \\ 120 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 143 \\ 107 \\ 201 \\ 775 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 373 \\ 220 \\ 60 \\ 20 \\ 543 \\ 60 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 400 \\ 110 \\ 71 \\ 560 \end{matrix}$		

Exhibit 10a - Existing + PendAppr + Project AM Peak Hour Traffic Volumes

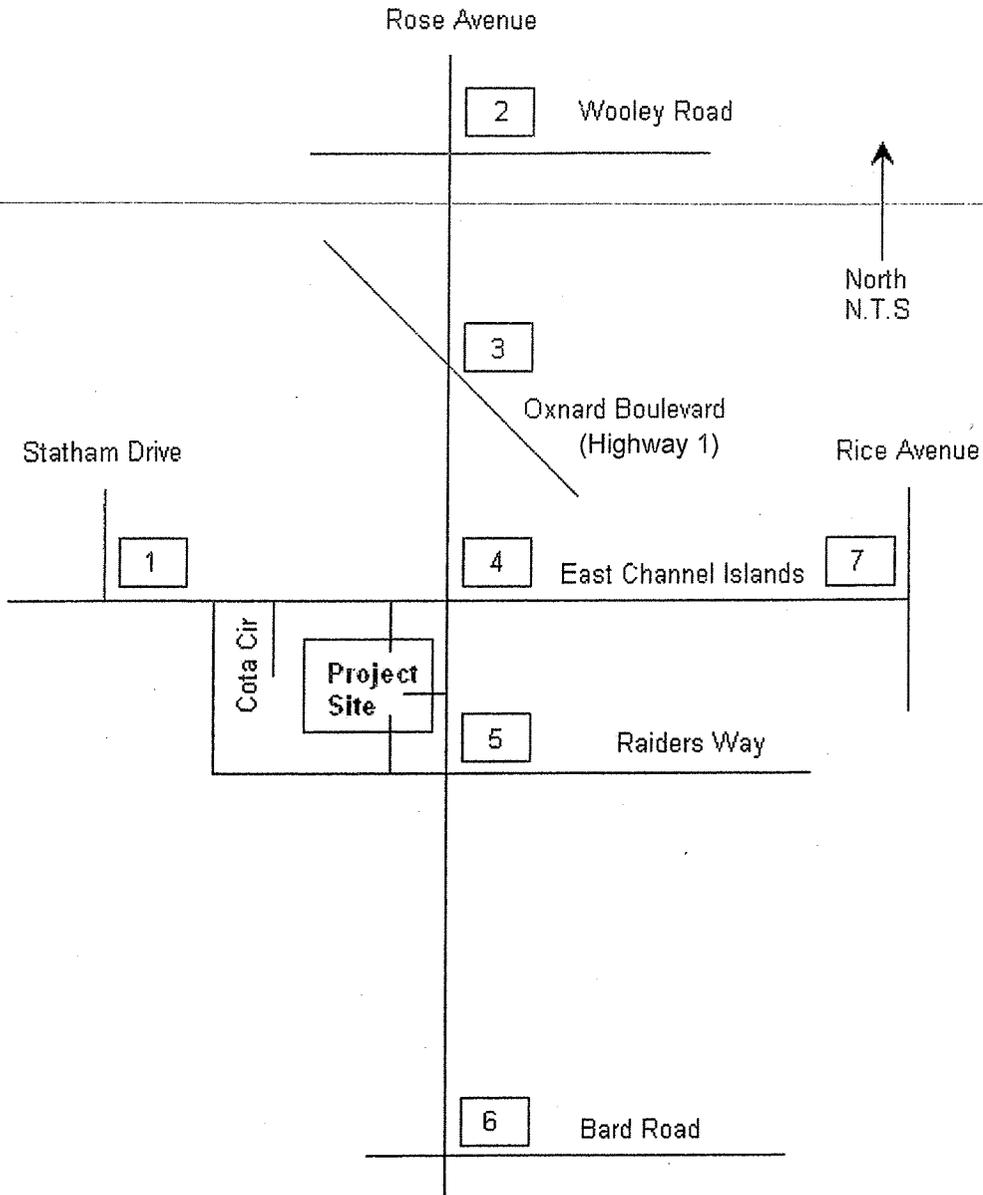
Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \text{L} \\ \downarrow \\ 460 \\ \uparrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ 73 \\ \downarrow \\ 1359 \end{matrix}$	$\begin{matrix} \text{L} \\ \downarrow \\ 460 \\ \uparrow \\ 1785 \\ \downarrow \\ 40 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 350 \\ \downarrow \\ 140 \end{matrix}$	$\begin{matrix} \text{L} \\ \downarrow \\ 1355 \\ \uparrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 100 \\ \downarrow \\ 880 \end{matrix}$		
$\begin{matrix} 240 \\ \downarrow \\ 930 \\ \uparrow \end{matrix}$		$\begin{matrix} 320 \\ \downarrow \\ 470 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} 40 \\ \downarrow \\ 915 \\ \downarrow \\ 120 \end{matrix}$	$\begin{matrix} 210 \\ \downarrow \end{matrix}$	$\begin{matrix} \uparrow \\ 287 \\ \downarrow \\ 1025 \\ \downarrow \\ 20 \end{matrix}$		
$\begin{matrix} \text{L} \\ \downarrow \\ 390 \\ \uparrow \\ 995 \\ \downarrow \\ 130 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 850 \\ \downarrow \\ 245 \end{matrix}$	$\begin{matrix} \text{L} \\ \downarrow \\ 34 \\ \uparrow \\ 1266 \\ \downarrow \\ 6 \end{matrix}$		$\begin{matrix} \text{L} \\ \downarrow \\ 227 \\ \uparrow \\ 597 \\ \downarrow \\ 102 \end{matrix}$	$\begin{matrix} \uparrow \\ 222 \\ \downarrow \\ 230 \\ \downarrow \\ 30 \end{matrix}$	$\begin{matrix} \text{L} \\ \downarrow \\ 702 \\ \uparrow \\ 560 \end{matrix}$	
$\begin{matrix} 401 \\ \downarrow \\ 534 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 220 \\ \downarrow \\ 831 \\ \downarrow \\ 81 \end{matrix}$	$\begin{matrix} 24 \\ \downarrow \\ 41 \end{matrix}$	$\begin{matrix} \uparrow \\ 36 \\ \downarrow \\ 1111 \end{matrix}$	$\begin{matrix} 247 \\ \downarrow \\ 120 \\ \downarrow \\ 50 \end{matrix}$	$\begin{matrix} \uparrow \\ 50 \\ \downarrow \\ 497 \\ \downarrow \\ 20 \end{matrix}$	$\begin{matrix} 192 \\ \downarrow \\ 60 \end{matrix}$	$\begin{matrix} \uparrow \\ 351 \\ \downarrow \\ 790 \end{matrix}$

EXHIBIT 10b - Existing + PendAppr + Project PM Peak Hour Traffic Volumes

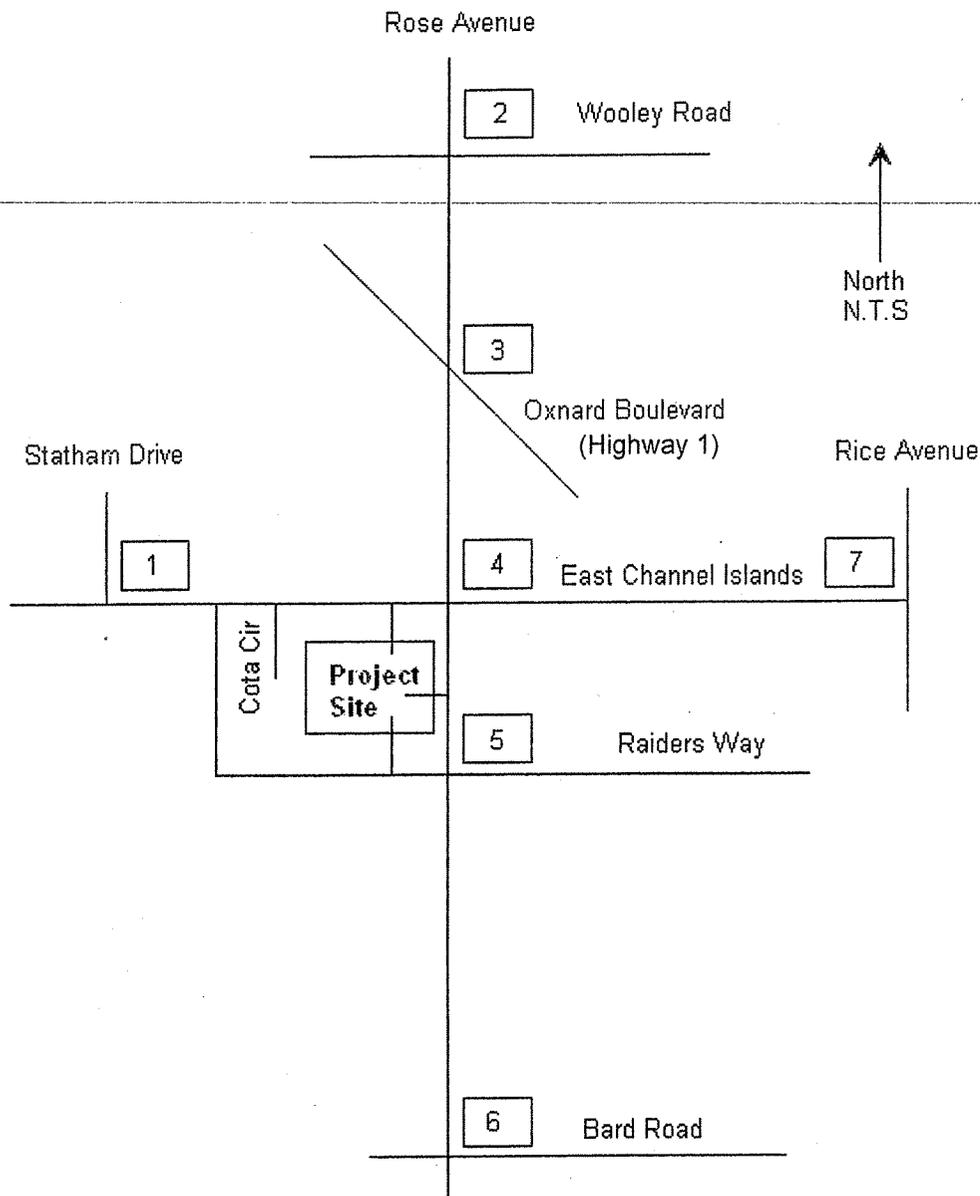
Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 120 \\ 80 \\ 40 \\ 880 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 250 \\ 1420 \\ 30 \\ 30 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 30 \\ 410 \\ 160 \\ 1050 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 70 \\ 160 \\ 20 \\ 20 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 400 \\ 1310 \\ 20 \\ 20 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 380 \\ 980 \\ 20 \\ 20 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 20 \\ 940 \\ 360 \\ 210 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 190 \\ 1580 \\ 50 \\ 190 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 190 \\ 1140 \\ 80 \\ 210 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 30 \\ 510 \\ 210 \\ 323 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 1164 \\ 3 \\ 180 \\ 720 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 140 \\ 130 \\ 40 \\ 280 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 660 \\ 610 \\ 140 \\ 120 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 30 \\ 510 \\ 210 \\ 110 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 151 \\ 112 \\ 142 \\ 566 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 480 \\ 180 \\ 80 \\ 30 \end{matrix}$
$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 890 \\ 100 \\ 140 \\ 1080 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 700 \\ 700 \\ 70 \\ 880 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 280 \\ 700 \\ 100 \\ 880 \end{matrix}$	$\begin{matrix} \text{L} \\ \text{L} \\ \text{L} \\ \text{L} \end{matrix} \begin{matrix} 700 \\ 700 \\ 70 \\ 880 \end{matrix}$

EXHIBIT 11a – 2030 AM Peak Hour Traffic Volumes

Southwest Corner of Rose Avenue and Channel Islands



$\begin{matrix} \uparrow \\ 380 \\ \downarrow \\ 90 \end{matrix}$	$\begin{matrix} \uparrow \\ 50 \\ \downarrow \\ 1560 \end{matrix}$	$\begin{matrix} \uparrow \\ 400 \\ \downarrow \\ 2110 \\ \downarrow \\ 40 \end{matrix}$	$\begin{matrix} \uparrow \\ 50 \\ \uparrow \\ 740 \\ \downarrow \\ 390 \end{matrix}$	$\begin{matrix} \uparrow \\ 1710 \\ \downarrow \\ 50 \end{matrix}$	$\begin{matrix} \uparrow \\ 80 \\ \downarrow \\ 570 \end{matrix}$		
$\begin{matrix} 230 \\ \downarrow \\ 1040 \\ \uparrow \end{matrix}$		$\begin{matrix} 320 \\ \downarrow \\ 780 \\ \downarrow \\ 80 \end{matrix}$	$\begin{matrix} \uparrow \\ 1040 \\ \downarrow \\ 210 \end{matrix}$	$\begin{matrix} 170 \\ \rightarrow \end{matrix}$	$\begin{matrix} \uparrow \\ 310 \\ \downarrow \\ 1330 \\ \downarrow \\ 20 \end{matrix}$		
$\begin{matrix} \uparrow \\ 390 \\ \downarrow \\ 1390 \\ \downarrow \\ 70 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \uparrow \\ 1070 \\ \downarrow \\ 380 \end{matrix}$	$\begin{matrix} \uparrow \\ 40 \\ \downarrow \\ 1813 \\ \downarrow \\ 8 \end{matrix}$		$\begin{matrix} \uparrow \\ 320 \\ \downarrow \\ 1010 \\ \downarrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ 180 \\ \downarrow \\ 190 \\ \downarrow \\ 30 \end{matrix}$	$\begin{matrix} \uparrow \\ 1140 \\ \downarrow \\ 1180 \end{matrix}$	
$\begin{matrix} 460 \\ \downarrow \\ 590 \\ \downarrow \\ 100 \end{matrix}$	$\begin{matrix} \uparrow \\ 240 \\ \uparrow \\ 1130 \\ \downarrow \\ 120 \end{matrix}$	$\begin{matrix} 30 \\ \downarrow \\ 41 \end{matrix}$	$\begin{matrix} \uparrow \\ 22 \\ \downarrow \\ 1268 \end{matrix}$	$\begin{matrix} 220 \\ \downarrow \\ 140 \\ \downarrow \\ 60 \end{matrix}$	$\begin{matrix} \uparrow \\ 60 \\ \downarrow \\ 880 \\ \downarrow \\ 30 \end{matrix}$	$\begin{matrix} 340 \\ \downarrow \\ 60 \end{matrix}$	$\begin{matrix} \uparrow \\ 220 \\ \downarrow \\ 1060 \end{matrix}$

EXHIBIT 11b – 2030 PM Peak Hour Traffic Volumes

Exhibit 12 - AM Peak Hour Cumulative LOS Evaluations Summary Table

Intersection	Existing AM		2030 AM GP Land Uses		2030 with Project AM Project Land Uses		Project Cumulative Impacts		
	AM LOS	Crit V/C	AM LOS	Crit V/C	AM LOS	Crit V/C	Crit V/C Change	Total Project Trips	Project Impact?
#1 Statham Blvd / Channel Islands Blvd	A	0.43	A	0.579	A	0.578	-0.001	8	No
#2 Rose Avenue / Wooley Road	A	0.502	D	0.869	D	0.869	0	3	No
#3 Rose Ave / Oxnard Blvd	A	0.401	A	0.581	A	0.581	0	9	No
#4 Rose Ave / Channel Islands	A	0.584	C	0.747	C	0.746	-0.001	13	No
#5 Rose Ave / Raiders Way	A	0.458	A	0.547	A	0.548	0.001	11	No
#6 Rose Ave / Bard Rd	A	0.502	C	0.713	C	0.713	0	12	No
#7 Rice Ave / Channel Islands Blvd	A	0.453	B	0.675	B	0.675	0	2	No

Exhibit 13 - PM Peak Hour Cumulative LOS Evaluations Summary Table

Intersection	Existing PM		2030 PM GP Land Uses		2030 with Project PM Project Land Uses		Project Specific Impacts		
	PM LOS	Crit V/C	PM LOS	Crit V/C	PM LOS	Crit V/C	Crit V/C Change	Total Project Trips	Project Impact?
#1 Statham Blvd / Channel Islands Blvd	A	0.565	C	0.766	C	0.768	0.002	22	No
#2 Rose Avenue / Wooley Road	C	0.702	F	1.203	F	1.204	0.001	10	No
#3 Rose Ave / Oxnard Blvd	B	0.604	E	0.906	E	0.909	0.003	27	No
#4 Rose Ave / Channel Islands	C	0.719	E	0.921	E	0.924	0.003	37	No
#5 Rose Ave / Raiders Way	A	0.379	B	0.606	B	0.614	0.008	34	No
#6 Rose Ave / Bard Rd	A	0.438	B	0.609	B	0.612	0.003	32	No
#7 Rice Ave / Channel Islands Blvd	C	0.705	E	0.969	E	0.969	0	5	No

EXHIBIT 14 - Trip Generation Comparison of Project Site Land Uses versus General Plan Land Uses Southwest Corner of Rose Ave / Channel Islands Blvd Trip Generation

ITE 7th Edition Trip Generation Rates

Site Plan Land Use Descriptions	Land Use Category	Unit	AM Peak Hour Rate		PM Peak Hour Rate		ADT Rate
			Inbound %	Outbound %	Inbound %	Outbound %	
Tesco	Supermarket	1 Thousand Square-Feet	61%	39%	51%	49%	10,4500
Shops	Retail	1 Thousand Square-Feet	61%	39%	48%	52%	3,7500
Church	Church	1 Thousand Square-Feet	54%	46%	52%	48%	0,6600
Housing	Single Family	1 Dwelling Unit	25%	75%	63%	37%	1,01

Allowable General Plan Land Use Trip Generation

General Plan Land Use Description	Amount	AM Peak Hour		PM Peak Hour		ADT
		Inbound	Outbound	Inbound	Outbound	
Low-Medium 8-12 DU/Acre	48 Dwelling Units	9	27	31	18	48
TOTAL (General Plan Land Use Trips)		9	27	31	18	48

Proposed Project Site Land Use Trip Generation

Proposed Project Site Descriptions	Amount	AM Peak Hour		PM Peak Hour		ADT
		Inbound	Outbound	Inbound	Outbound	
Tesco	13,929 Thousand Square-Feet	28	18	45	71	1,424
Shops	Retail	4	2	6	11	21
Church	6 Thousand Square-Feet	2	2	4	2	4
SUBTOTAL (Tesco + Shops)		31	20	51	82	1,666
TOTAL (Proposed Project Site Land Use Trips)		33	22	55	84	1,721

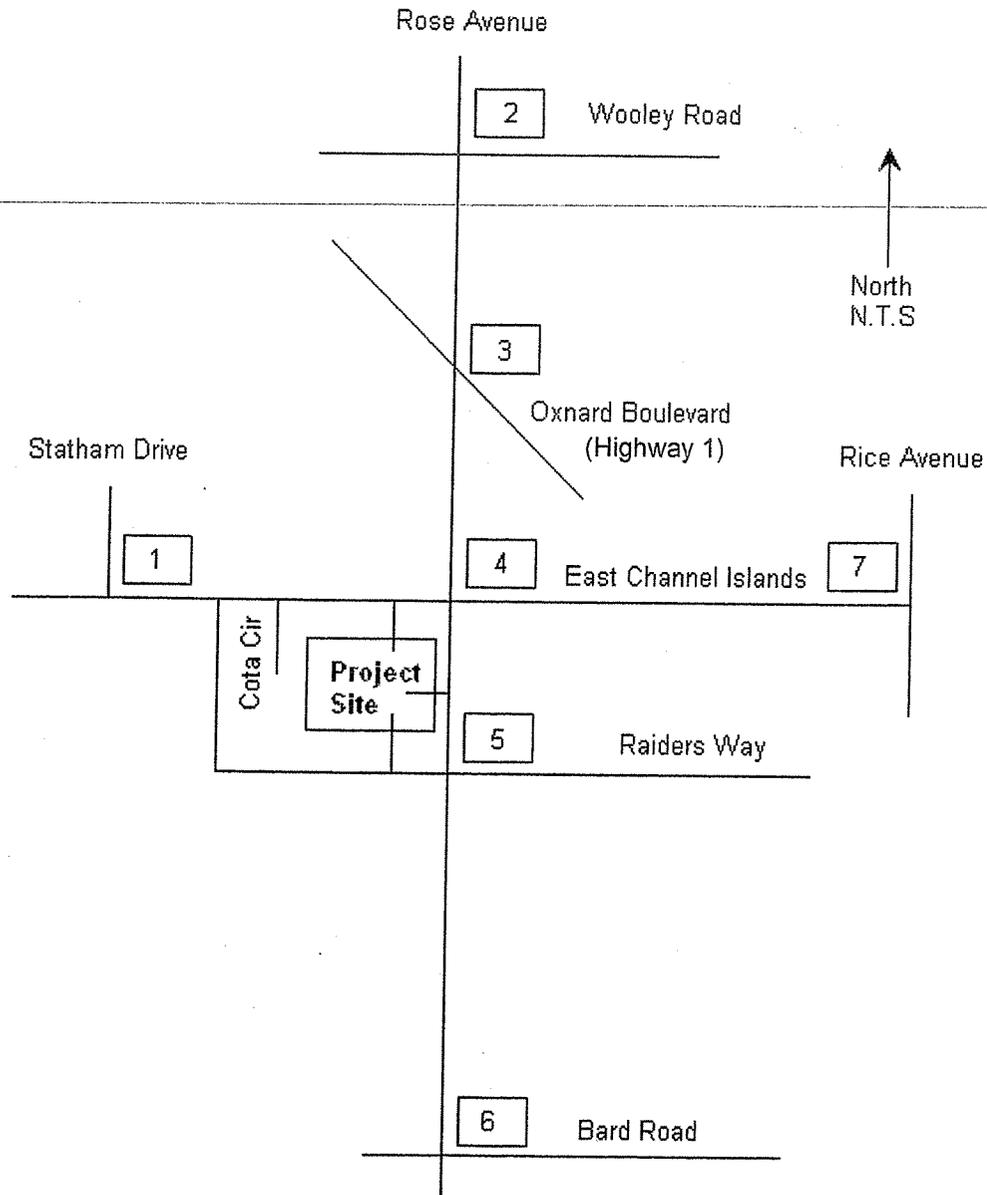
Total Project Trip Generation based on 20% Reduction for Captured Trips and 35% Reduction for Passby Trips

Site Plan Land Use Descriptions	Adjustment	AM Peak Hour		PM Peak Hour		ADT
		Inbound	Outbound	Inbound	Outbound	
Tesco and Shops	Interaction + Passby Reduction	16	10	27	43	87
Church	None	2	2	4	2	3
TOTAL (Project Land Use based on 20% Interaction and 35% Passby)		18	12	27	44	87

Net Difference Between Project Site Land Use Trip Generation and General Plan Land Use Trip Generation

Net Difference Between Project Site Land Use Trip Generation and General Plan Land Use Trip Generation	AM Peak Hour		PM Peak Hour		ADT
	Inbound	Outbound	Inbound	Outbound	
Net Difference Between Project Land Use TG & GP Land Use TG (TOTAL Proposed - TOTAL GP)	9	-15	-9	15	38
					507

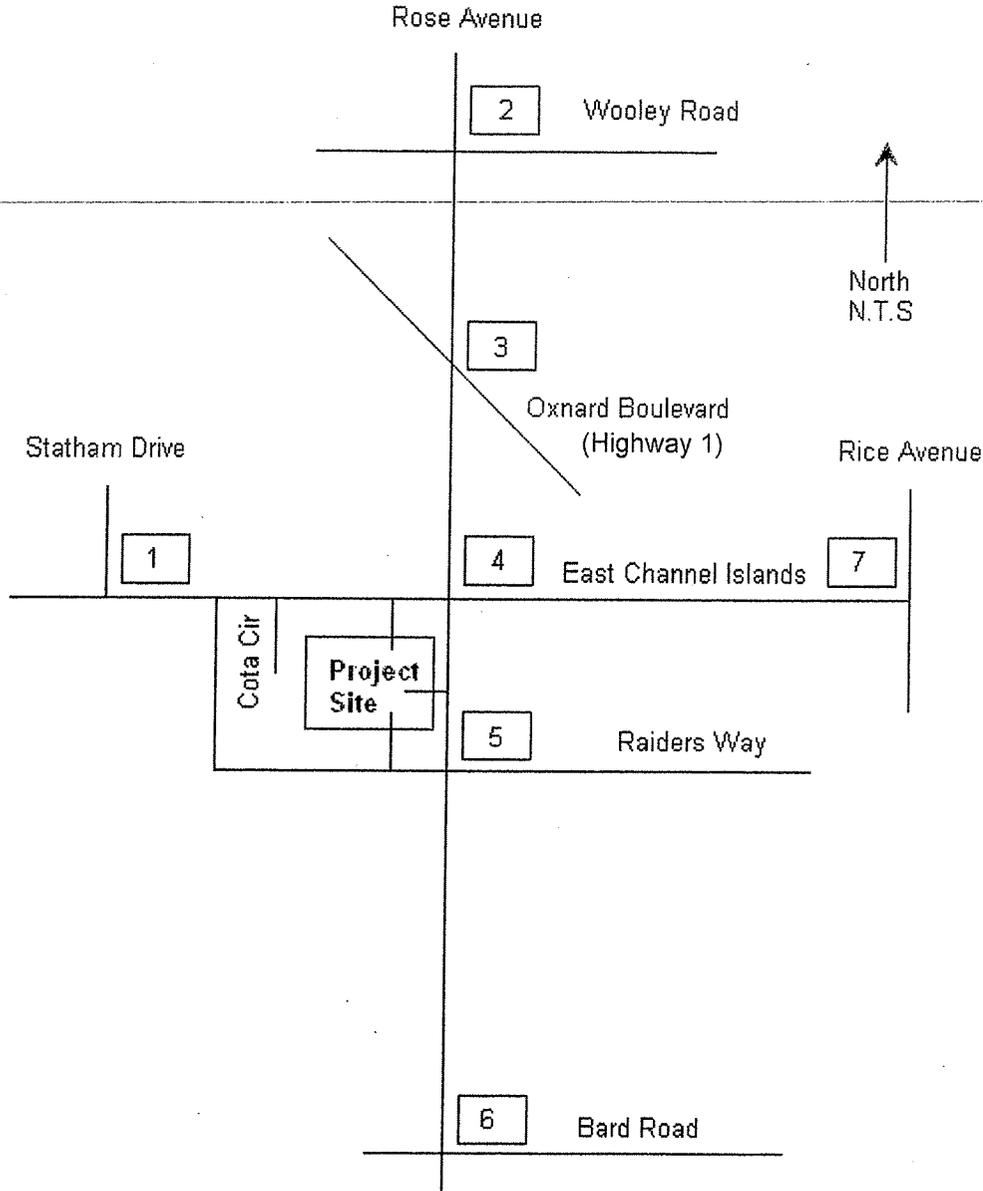
Southwest Corner of Rose Avenue and Channel Islands



1 120 80 39 877	2 250 1421 30	3 30 410 160	4 1051 20	5 70 160
400 1312	380 980 20	20 938 360	210	188 1578 50
180 1143 60	323 1162 3	178 718 79	140 130 40	280 700
656 608 140	120 1100 190	151 109	145 566	481 180 80
			30 881 70	889 100
				140 1080

EXHIBIT 14a - 2030 with Project AM Peak Hour Traffic Volumes

Southwest Corner of Rose Avenue and Channel Islands



¹ 380 ↓ 90 ↑	52 ↑ 565 ↓	² 400 ↓ 2112 ↓ 40 ↓	50 ↑ 740 ↓ 390 ↓	³ 1712 ↓ 50 ↓	80 ↑ 570 ↓
230 ↓ 1043 ↑		320 ↓ 780 ↓ 80 ↓	50 ↑ 1043 ↓ 210 ↓	170 ↓	314 ↓ 1333 ↓ 20 ↓
⁴ 390 ↓ 1395 ↓ 70 ↓	40 ↑ 1070 ↓ 382 ↓	⁵ 49 ↓ 1816 ↓ 8 ↓		⁶ 324 ↓ 1014 ↓ 101 ↓	⁷ 191 ↓ 190 ↓ 30 ↓
457 ↓ 593 ↓ 100 ↓	240 ↑ 1130 ↓ 120 ↓	30 ↓ 47 ↓	27 ↑ 1268 ↓	222 ↓ 140 ↓ 60 ↓	341 ↓ 80 ↓
					1141 ↓ 1180 ↓
					220 ↑ 1060 ↓

EXHIBIT 15b - 2030 with Project PM Peak Hour Traffic Volumes

Appendix A - Intersection LOS Thresholds

Characteristics of Traffic Operations by Level of Service

Level of Service	Traffic Conditions	Volume/Capacity Ratio
A	Unobstructed flow; no approach is fully utilized by traffic and no vehicle waits longer than one red indication.	0.00-0.60
B	Stable operation; an occasional approach phase is fully utilized and a substantial number are approaching full use.	0.61-0.70
C	Stable operation with intermittent loading. Occasionally, drivers may have to wait through more than one signal indication and backups may develop behind turning vehicles.	0.71-0.80
D	Delays to approaching vehicles may be substantial for short periods during the peak period, with periodic clearance of developing queues.	0.81-0.90
E	Unstable flow conditions with long queues over extended periods. Capacity occurs at the limits of this level.	0.91-1.00
F	Forced flow conditions, with demand exceeding capacity; highly variable delay and long backups.	Variable

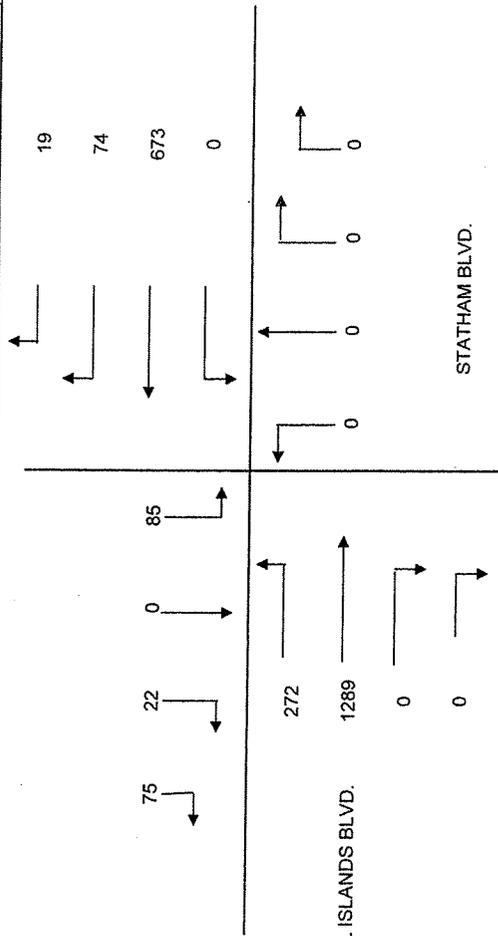
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECTION: STATHAM BLVD.
 E/W CHANNEL ISLANDS BLVD.

VEHICLE COUNTS

PERIOD	1R SBRT	1 SBRT	2 SBTH	3 SBLT	4R WBRT	4 WBRT	5 WBTH	6 WBLT	7R NBRT	7 NBRT	8 NBTH	9 NBLT	10R EBRT	10 EBRT	11 EBTH	12 EBLT	TOTAL
700-715	18	2	0	5	8	99	0	0	0	0	0	0	0	0	182	55	371
715-730	23	3	0	7	11	119	0	0	0	0	0	0	0	0	260	60	486
730-745	13	6	0	29	5	148	0	0	0	0	0	0	0	0	313	55	592
745-800	23	7	0	32	10	211	0	0	0	0	0	0	0	0	397	83	785
800-815	16	6	0	17	1	195	0	0	0	0	0	0	0	0	319	74	646
815-830	27	5	0	8	5	138	0	0	0	0	0	0	0	0	215	54	461
830-845	25	10	0	6	8	116	0	0	0	0	0	0	0	0	149	53	368
845-900	22	6	0	4	3	103	0	0	0	0	0	0	0	0	168	32	348
HOUR TOTALS																	
0700-0800	77	18	0	73	20	577	0	0	0	0	0	0	0	0	1152	253	2234
0715-0815	75	22	0	85	19	673	0	0	0	0	0	0	0	0	1289	272	2509
0730-0830	79	24	0	86	21	692	0	0	0	0	0	0	0	0	1244	266	2484
0745-0845	91	28	0	63	17	660	0	0	0	0	0	0	0	0	1080	264	2260
0800-0900	90	27	0	35	10	552	0	0	0	0	0	0	0	0	851	213	1823

AM PEAK HOUR
0715-0815



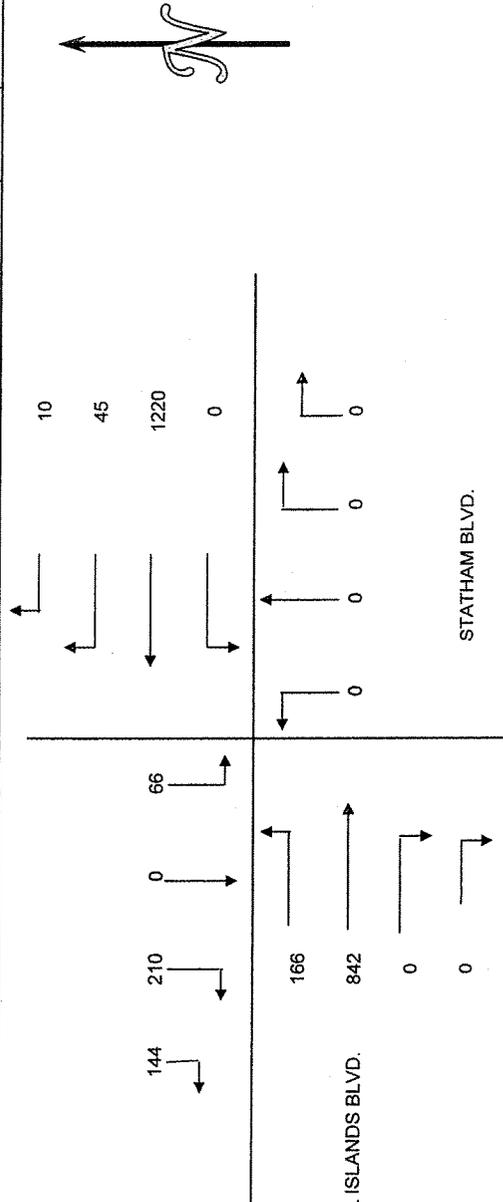
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTK N/S: STATHAM BLVD.
 EW: CHANNEL ISLANDS BLVD.

VEHICLE COUNTS

PERIOD	1R SBRT	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL	
15 MIN COUN ON RED																		
400-415	34	42	0	22	3	12	288	0	0	0	0	0	0	0	211	40	652	
415-430	38	42	0	20	0	9	278	0	0	0	0	0	0	0	217	54	658	
430-445	45	55	0	18	2	12	279	0	0	0	0	0	0	0	200	34	645	
445-500	26	58	0	17	6	14	309	0	0	0	0	0	0	0	207	37	674	
500-515	43	62	0	19	3	10	289	0	0	0	0	0	0	0	210	49	685	
515-530	42	35	0	20	0	12	331	0	0	0	0	0	0	0	200	39	679	
530-545	33	55	0	10	1	9	291	0	0	0	0	0	0	0	225	41	665	
545-600	28	31	0	12	8	20	288	0	0	0	0	0	0	0	228	48	663	
HOURL TOTALS																		
400-500	143	197	0	77	11	47	1154	0	0	0	0	0	0	0	835	165	2629	
415-515	152	217	0	74	11	45	1155	0	0	0	0	0	0	0	834	174	2662	
430-530	156	210	0	74	11	48	1208	0	0	0	0	0	0	0	817	159	2683	
445-545	144	210	0	66	10	45	1220	0	0	0	0	0	0	0	842	166	2703	
500-600	146	183	0	61	12	51	1199	0	0	0	0	0	0	0	863	177	2692	

PM PEAK HOUR
445-545

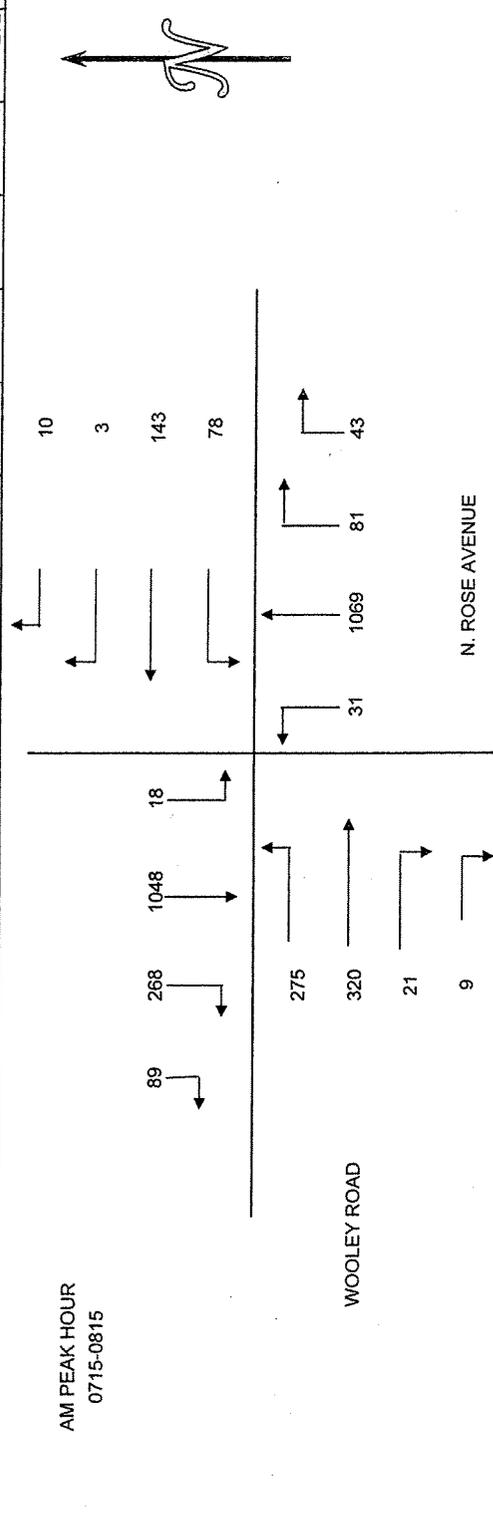


INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECT(K)N/S: N. ROSE AVENUE
 E/W: WOOLEY ROAD

VEHICLE COUNTS

PERIOD	1R: SBRT	1 SBRT	2 SBTH	3 SBLT	4R: WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R: NBRT ON RED	7 NBRT	8 NBTH	9 NBLT	10R: EBRT ON RED	10 EBRT	11 EBTH	12 EBTL	TOTAL
700-715	22	49	196	3	2	3	40	14	7	23	231	9	2	4	90	47	742
715-730	12	62	212	5	2	1	43	22	12	20	282	5	5	6	98	57	844
730-745	21	75	297	4	4	2	35	22	9	23	256	9	2	6	91	78	934
745-800	15	97	285	7	0	0	26	11	13	22	240	9	1	1	50	54	831
800-815	41	34	254	2	4	0	39	23	9	16	291	8	1	8	81	86	897
815-830	40	6	165	4	1	1	40	11	7	22	213	3	2	3	52	52	622
830-845	65	10	162	10	5	0	24	11	9	12	228	9	3	5	82	55	690
845-900	56	10	166	3	6	0	32	9	10	15	183	9	3	3	57	60	622
HOUR TOTALS																	
0700-0800	70	283	990	19	8	6	144	69	41	88	1009	32	10	17	329	236	3351
0715-0815	89	268	1048	18	10	3	143	78	43	81	1069	31	9	21	320	275	3506
0730-0830	117	212	1001	17	9	3	140	67	38	83	1000	29	6	18	274	270	3284
0745-0845	161	147	866	23	10	1	129	56	38	72	972	29	7	17	265	247	3040
0800-0900	202	60	747	19	16	1	135	54	35	65	915	29	9	19	272	253	2831



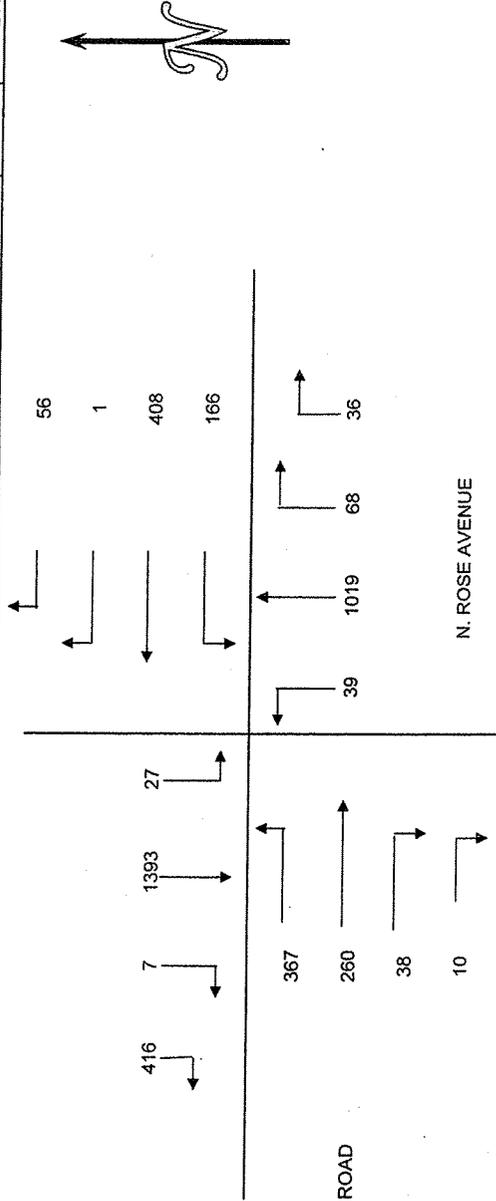
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTK N/S: N. ROSE AVENUE
 EW: WOOLEY ROAD

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
400-415	103	1	309	11	16	105	46	14	29	255	7	4	10	70	83	1064
415-430	113	5	307	1	12	81	34	11	19	239	4	3	9	41	57	936
430-445	117	0	354	3	18	85	38	11	17	261	11	6	11	71	85	1088
445-500	92	2	331	7	12	103	30	9	24	280	13	2	9	62	91	1067
500-515	124	2	376	9	14	123	58	7	13	252	7	1	11	78	124	1199
515-530	83	3	332	8	12	97	40	9	14	226	8	1	7	49	67	957
530-545	81	3	290	9	10	98	34	5	17	271	10	1	9	66	76	980
545-600	105	0	322	10	10	80	28	3	10	245	6	2	4	37	58	921
HOUR TOTALS																
400-500	425	8	1301	22	58	374	148	45	89	1035	35	15	39	244	316	4155
415-515	446	9	1368	20	56	392	160	38	73	1032	35	12	40	252	357	4290
430-530	416	7	1393	27	56	408	166	36	68	1019	39	10	38	260	367	4311
445-545	380	10	1329	33	48	421	162	30	68	1029	38	5	36	255	358	4203
500-600	393	8	1320	36	46	398	160	24	54	994	31	5	31	230	325	4057

PM PEAK HOUR
4:30-5:30

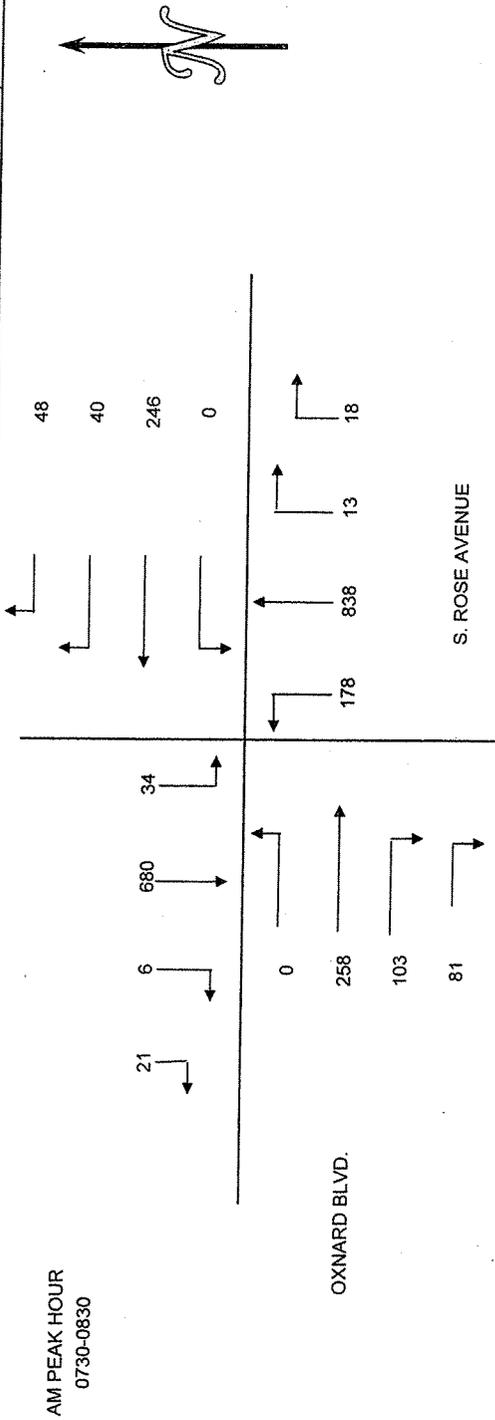


INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECT: S. ROSE AVENUE
 E/W OXNARD BLVD.

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT ON RED	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
700-715	2	0	112	6	4	9	48	0	4	4	148	22	6	12	54	0	431
715-730	2	0	125	6	6	9	50	0	4	4	183	25	4	13	56	0	487
730-745	5	2	171	13	15	5	66	0	4	6	198	36	18	31	63	0	633
745-800	5	1	228	11	12	12	70	0	3	3	246	48	21	33	67	0	760
800-815	8	1	141	4	12	12	56	0	5	2	221	53	24	27	67	0	633
815-830	3	2	140	6	9	11	54	0	6	2	173	41	18	12	61	0	538
830-845	4	3	111	9	1	8	44	0	2	5	152	24	8	11	45	0	427
845-900	2	1	105	7	6	5	56	0	0	2	157	28	19	9	41	0	438
HOUR TOTALS																	
0700-0800	14	3	636	36	37	35	234	0	15	17	775	131	49	89	240	0	2311
0715-0815	20	4	665	34	45	38	242	0	16	15	848	162	67	104	253	0	2513
0730-0830	21	6	680	34	48	40	246	0	18	13	838	178	81	103	258	0	2564
0745-0845	20	7	620	30	34	43	224	0	16	12	792	166	71	83	240	0	2358
0800-0900	17	7	497	26	28	36	210	0	13	11	703	146	69	59	214	0	2036

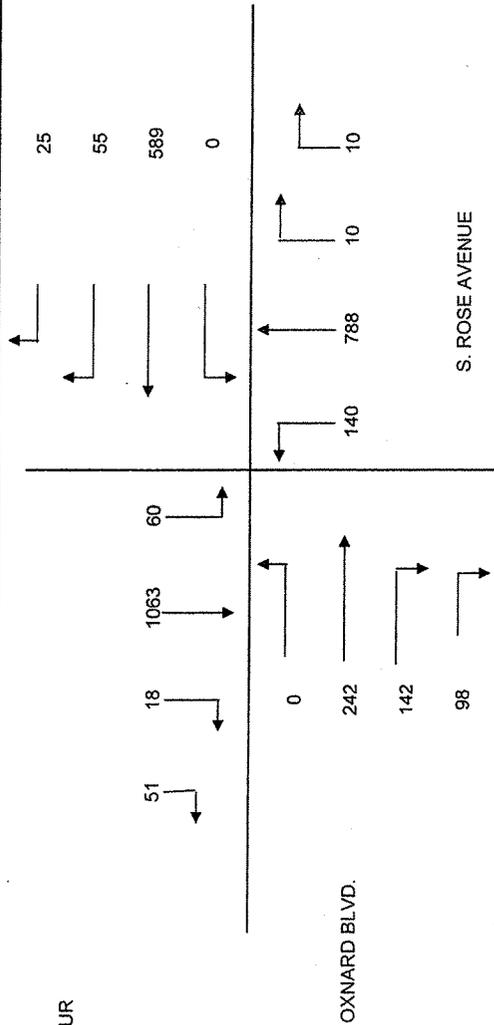


INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
PROJECT: SWC CHANNEL ISLANDS & ROSE
DATE: THURSDAY, MAY 11, 2007
PERIOD: 4:00 PM TO 6:00 PM
INTERSECT(N)S: S. ROSE AVENUE
 EW OXNARD BLVD.

PERIOD	1R SBRT ON RED	2 SBTH	3 SBLT ON RED	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
400-415	11	4	17	8	16	136	0	3	1	173	32	24	36	65	1	743
415-430	9	5	15	7	14	148	0	4	1	217	26	19	33	47	0	760
430-445	12	6	11	7	8	129	0	2	3	210	43	29	42	62	0	837
445-500	13	4	15	9	15	127	0	3	2	186	38	20	34	57	0	804
500-515	17	1	14	3	22	155	0	5	1	214	30	19	36	63	0	830
515-530	9	7	20	6	10	178	0	0	4	178	29	30	30	60	0	820
530-545	9	4	5	9	11	132	0	1	3	187	36	30	32	47	0	714
545-600	7	3	13	10	15	142	0	2	2	194	36	36	34	48	0	805
HOUR TOTALS																
400-500	45	19	58	31	53	540	0	12	7	786	139	92	145	231	1	3144
415-515	51	16	55	26	59	559	0	14	7	827	137	87	145	229	0	3231
430-530	51	18	60	25	55	589	0	10	10	788	140	98	142	242	0	3291
445-545	48	16	54	27	58	592	0	9	10	765	133	99	132	227	0	3168
500-600	42	15	52	28	58	607	0	8	10	773	131	115	132	218	0	3169

PM PEAK HOUR
430-530

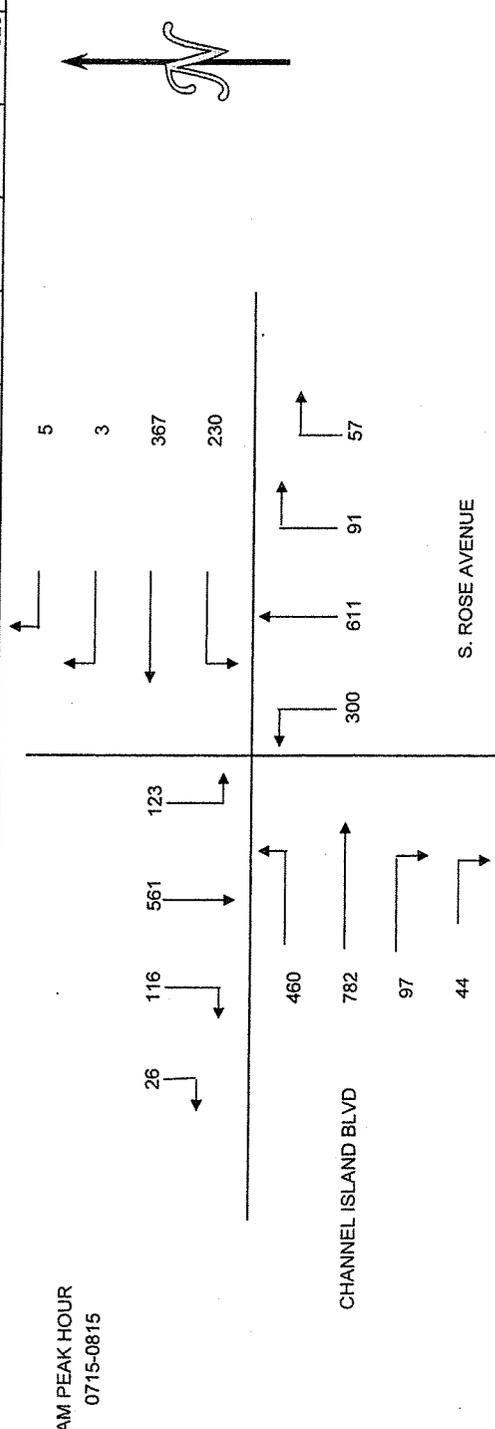


INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECTION/S: S. ROSE AVENUE
 E/W: CHANNEL ISLAND BLVD

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
15 MIN COUN ON RED																	
700-715	0	37	73	25	1	50	29	13	10	104	23	5	7	182	77	637	
715-730	0	37	89	19	1	63	40	11	18	122	43	11	7	165	94	722	
730-745	9	32	191	33	3	95	60	13	22	154	54	17	25	179	111	999	
745-800	8	35	194	46	1	129	100	24	31	192	104	8	45	225	120	1262	
800-815	9	12	87	25	0	80	30	9	20	143	99	8	20	213	135	890	
815-830	8	27	103	30	1	60	26	7	7	96	47	17	12	147	120	708	
830-845	7	20	105	23	1	59	21	6	10	90	41	13	13	90	92	591	
845-900	10	22	72	16	1	59	13	6	9	70	30	10	11	78	68	475	
HOUR TOTALS																	
0700-0800	17	141	547	123	6	337	229	61	81	572	224	41	84	751	402	3620	
0715-0815	26	116	561	123	5	367	230	57	91	611	300	44	97	782	460	3873	
0730-0830	34	106	575	134	5	364	216	53	80	585	304	50	102	764	486	3859	
0745-0845	32	94	489	124	3	328	177	46	68	521	291	46	90	675	467	3451	
0800-0900	34	81	367	94	3	258	90	28	46	399	217	48	56	528	415	2664	



AM PEAK HOUR
0715-0815

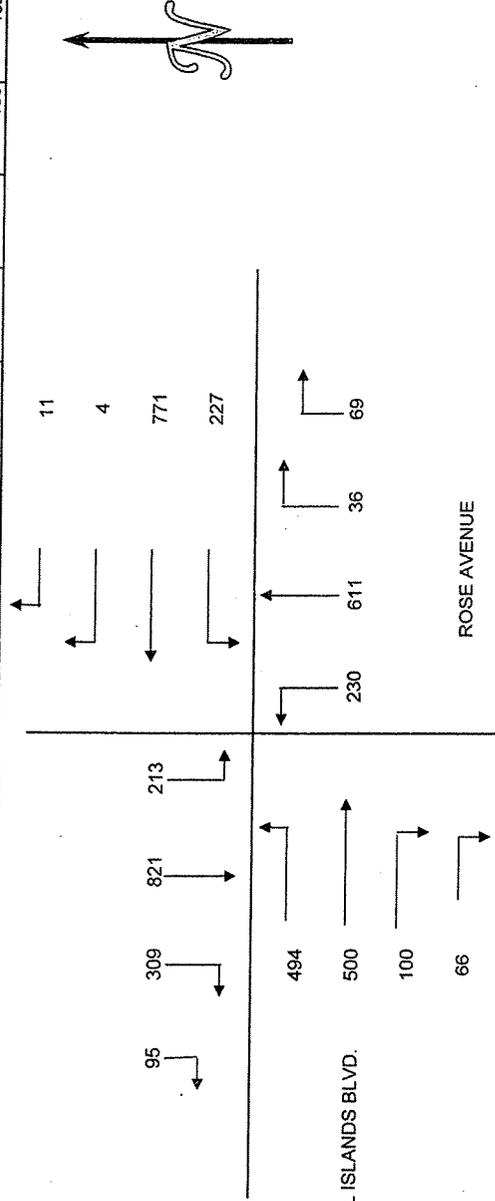
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTK N/S: ROSE AVENUE
 E/W: CHANNEL ISLANDS BLVD.

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	9 NBTH	9 NBLT	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
400-415	16	78	178	51	1	1	199	65	12	11	141	60	20	24	94	118	1069
415-430	10	60	170	32	2	3	197	48	16	4	123	52	12	16	100	129	974
430-445	34	82	196	58	6	1	193	64	24	8	181	57	19	22	141	132	1218
445-500	27	74	207	43	1	0	164	56	16	8	158	59	16	26	142	129	1126
500-515	11	65	239	65	2	2	215	50	13	15	152	69	13	23	100	98	1132
515-530	23	88	179	47	2	1	199	57	16	5	120	45	18	29	117	135	1081
530-545	16	71	183	60	0	0	187	73	7	7	143	64	27	31	126	142	1137
545-600	16	55	164	63	0	2	155	41	13	16	136	66	16	22	119	96	980
HOUR TOTALS																	
400-500	87	294	751	184	10	5	753	233	68	31	603	228	67	88	477	508	4387
415-515	82	281	812	198	11	6	769	218	69	35	614	237	60	87	483	488	4450
430-530	95	309	821	213	11	4	771	227	69	36	611	230	66	100	500	494	4557
445-545	77	298	808	215	5	3	765	236	52	35	573	237	74	109	485	504	4476
500-600	66	279	765	235	4	5	756	221	49	43	551	244	74	105	462	471	4330

PM PEAK HOUR
430-530



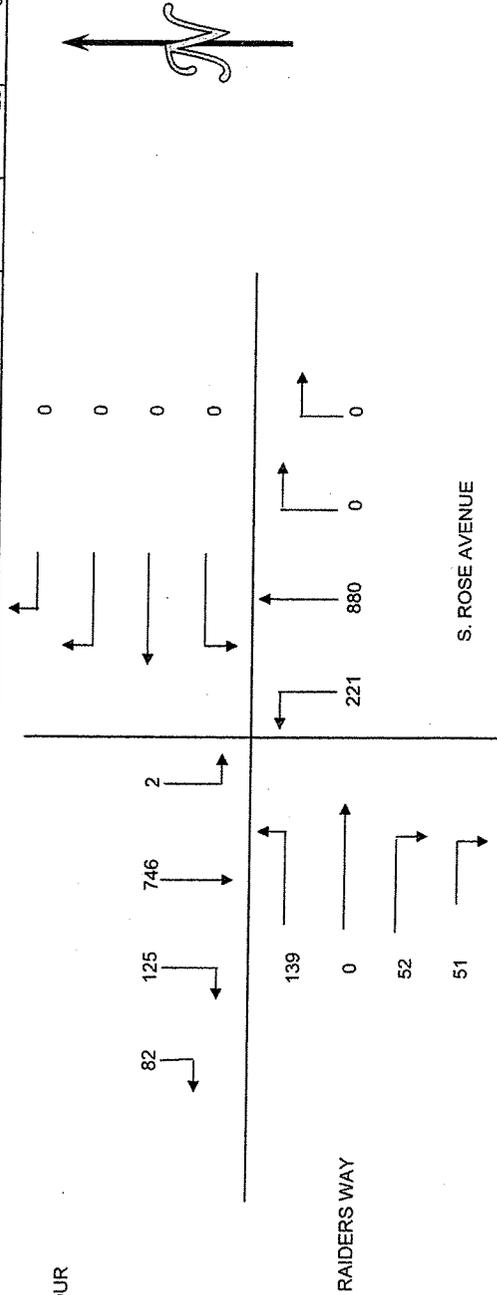
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECTION/S: S. ROSE AVENUE
 E/W: RAIDERS WAY

VEHICLE COUNTS

PERIOD	1R SBRT ON/RED	1 SBRT SBTH	2 SBRT SBTH	3 SBLT ON/RED	4R WBRT ON/RED	4 WBRT WBTH	5 WBRT WBTH	6 WBRT WBTH	7R NBRT ON/RED	7 NBRT NBTH	8 NBTH ON/RED	9 NBLT ON/RED	10R EBRT ON/RED	10 EBRT EBTH	11 EBTH ON/RED	12 EBLT ON/RED	TOTAL
700-715	3	11	108	1	0	0	0	0	0	0	142	9	1	2	0	4	281
715-730	6	12	101	2	0	0	0	0	0	0	192	12	3	5	0	8	341
730-745	25	29	209	0	0	0	0	0	0	0	221	69	8	13	0	47	621
745-800	44	76	266	1	0	0	0	0	0	0	278	96	11	15	0	48	835
800-815	10	12	136	0	0	0	0	0	0	0	193	47	28	18	0	34	478
815-830	3	8	135	1	0	0	0	0	0	0	188	9	4	6	0	10	364
830-845	1	6	118	0	0	0	0	0	0	0	150	2	5	0	0	4	286
845-900	4	9	112	0	0	0	0	0	0	0	134	8	2	1	0	3	273
HOUR TOTALS																	
0700-0800	78	128	684	4	0	0	0	0	0	0	833	186	23	35	0	107	2078
0715-0815	85	129	712	3	0	0	0	0	0	0	884	224	50	51	0	137	2275
0730-0830	82	125	746	2	0	0	0	0	0	0	880	221	51	52	0	139	2298
0745-0845	58	102	655	2	0	0	0	0	0	0	809	154	48	39	0	96	1963
0800-0900	18	35	501	1	0	0	0	0	0	0	665	66	39	25	0	51	1401

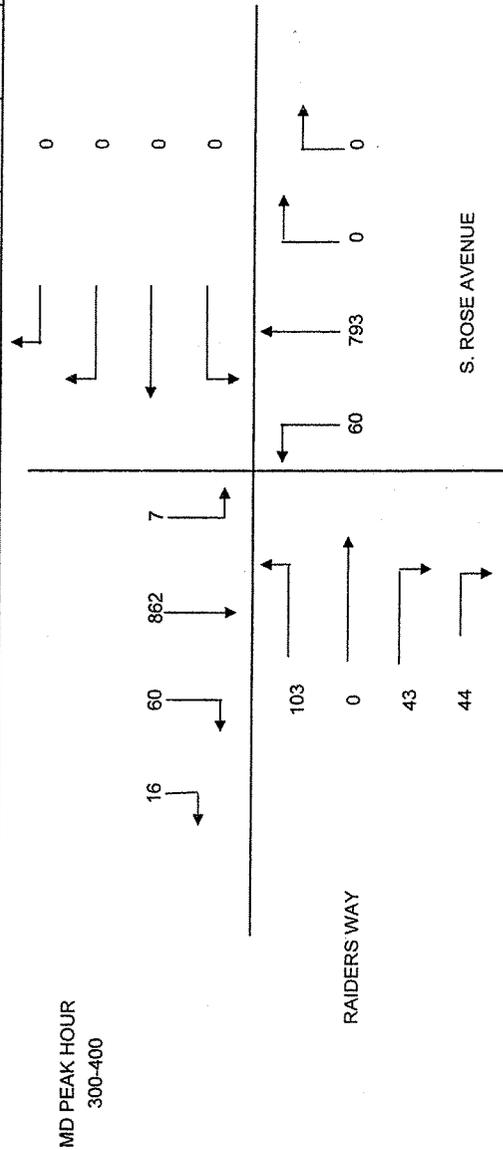
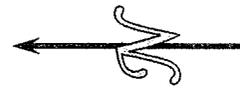
AM PEAK HOUR
0730-830



INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 2:00 PM TO 4:00 PM
 INTERSECTK N/S: S. ROSE AVENUE
 EW: RAIDERS WAY

PERIOD	1R SBRT	1 SBRT	2 SBTH	3 SBLT	4R WBRT	4 WBRT	5 WBTH	6 WBLT	7R NBRT	7 NBRT	8 NBTH	9 NBLT	10R EBRT	10 EBRT	11 EBTH	12 EBLT	TOTAL
15 MIN COUN ON RED																	
200-215	2	3	195	0	0	0	0	0	0	0	170	5	1	3	0	8	387
215-230	1	2	175	0	0	0	0	0	0	0	169	6	13	5	0	4	375
230-245	1	8	178	0	0	0	0	0	0	0	169	3	4	2	0	4	369
245-300	0	8	177	1	0	0	0	0	0	0	199	8	5	2	0	2	402
300-315	2	21	195	2	0	0	0	0	0	0	158	12	2	6	0	1	399
315-330	6	19	226	3	0	0	0	0	0	0	219	26	22	22	0	76	619
330-345	5	7	209	1	0	0	0	0	0	0	234	14	13	12	0	19	514
345-400	3	13	232	1	0	0	0	0	0	0	182	8	7	3	0	7	456
HOUR TOTALS																	
200-300	4	21	725	1	0	0	0	0	0	0	707	22	23	12	0	18	1533
215-315	4	39	725	3	0	0	0	0	0	0	695	29	24	15	0	11	1545
230-330	9	56	776	6	0	0	0	0	0	0	745	49	33	32	0	83	1789
245-345	13	55	807	7	0	0	0	0	0	0	810	60	42	42	0	98	1934
300-400	16	60	862	7	0	0	0	0	0	0	793	60	44	43	0	103	1988



MD PEAK HOUR
300-400

RAIDERS WAY

S. ROSE AVENUE

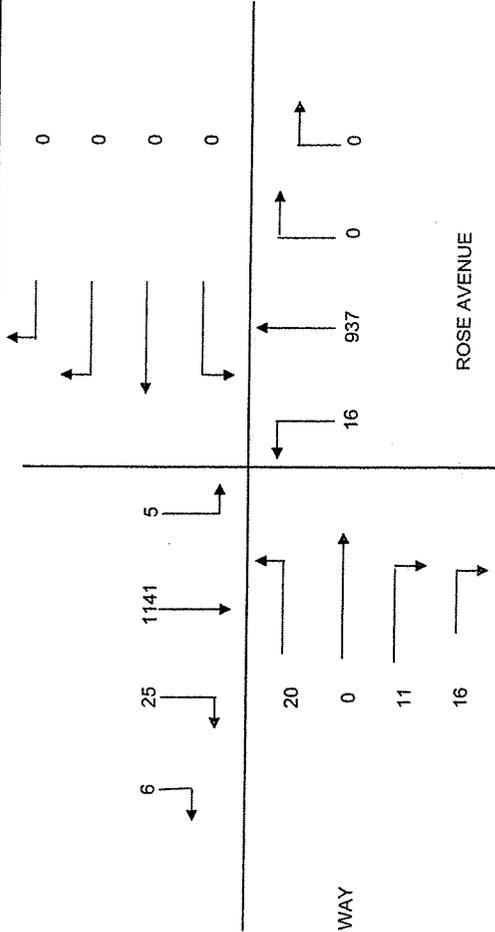
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTK N/S: ROSE AVENUE
 EW: RAIDERS WAY

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON/RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
400-415	1	8	275	1	0	0	0	0	0	0	225	5	9	5	0	11	540
415-430	3	7	255	4	0	0	0	0	0	0	205	4	8	6	0	4	496
430-445	0	5	246	0	0	0	0	0	0	0	223	4	4	1	0	5	488
445-500	1	6	321	0	0	0	0	0	0	0	269	2	1	0	0	6	606
500-515	2	7	319	1	0	0	0	0	0	0	240	6	3	4	0	5	587
515-530	1	1	293	1	0	0	0	0	0	0	185	3	2	0	0	3	489
530-545	0	4	267	0	0	0	0	0	0	0	207	5	1	0	0	4	488
545-600	0	8	255	4	0	0	0	0	0	0	217	9	4	2	0	3	502
HOUR TOTALS																	
400-500	5	26	1097	5	0	0	0	0	0	0	922	15	22	12	0	26	2130
415-515	6	25	1141	5	0	0	0	0	0	0	937	16	16	11	0	20	2177
430-530	4	19	1179	2	0	0	0	0	0	0	917	15	10	5	0	19	2170
445-545	4	18	1200	2	0	0	0	0	0	0	901	16	7	4	0	18	2170
500-600	3	20	1134	6	0	0	0	0	0	0	849	23	10	6	0	15	2066

PM PEAK HOUR
415-515



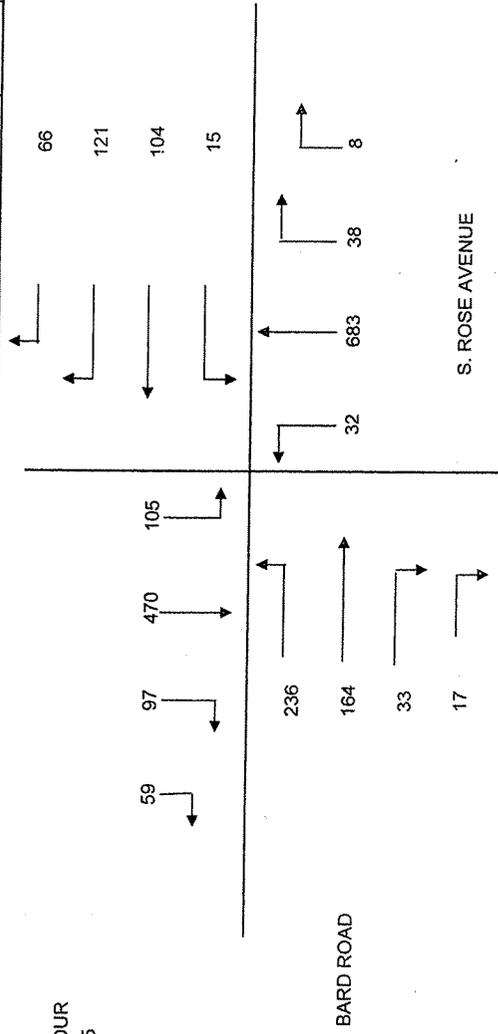
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECTION/S: S. ROSE AVENUE
 EW BARD ROAD

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
700-715	5	11	89	19	7	13	20	1	2	3	121	4	3	1	32	44	375
715-730	7	17	101	21	9	20	23	2	2	5	158	5	1	2	44	50	467
730-745	18	24	113	24	14	27	27	2	3	8	191	6	2	3	55	66	583
745-800	21	28	140	24	24	56	37	4	3	17	224	9	5	13	34	73	712
800-815	13	28	116	36	19	18	17	7	0	8	110	12	9	15	31	47	486
815-830	18	21	64	32	11	14	22	3	1	3	84	7	3	5	34	61	383
830-845	9	11	36	20	5	11	18	3	2	3	51	4	4	2	33	53	265
845-900	8	18	43	36	13	4	21	2	1	3	74	6	2	0	39	36	306
HOUR TOTALS																	
0700-0800	51	80	443	88	54	116	107	9	10	33	694	24	11	19	165	233	2137
0715-0815	59	97	470	105	66	121	104	15	8	38	683	32	17	33	164	236	2248
0730-0830	70	101	433	116	68	115	103	16	7	36	609	34	19	36	154	247	2164
0745-0845	61	88	356	112	59	99	94	17	6	31	469	32	21	35	132	234	1846
0800-0900	48	78	259	124	48	47	78	15	4	17	319	29	18	22	137	197	1440

AM PEAK HOUR
0715-0815



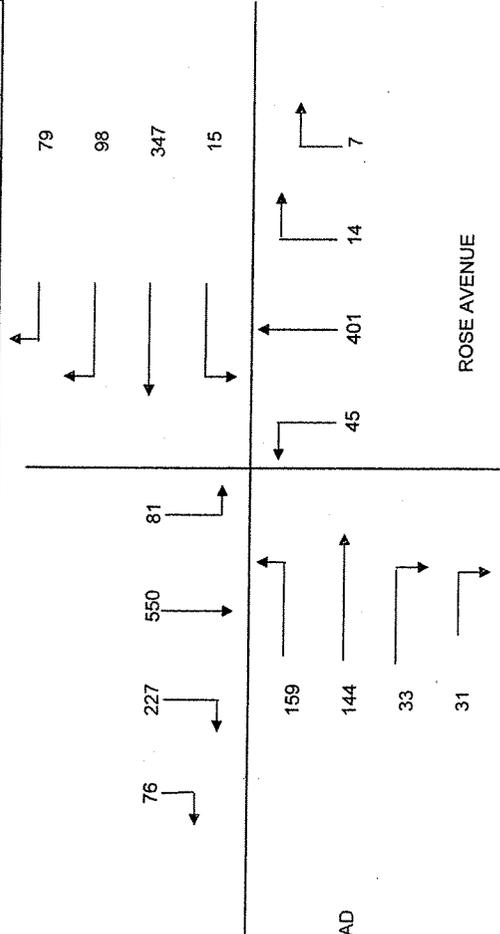
INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTI(N)S: ROSE AVENUE
 E/W: BARD ROAD

VEHICLE COUNTS

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT ON RED	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT ON RED	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT ON RED	TOTAL
400-415	27	56	135	21	16	33	87	5	1	5	115	15	8	16	44	38	622
415-430	22	52	125	22	15	20	78	1	2	3	100	16	11	8	36	42	553
430-445	21	35	96	18	15	18	53	2	1	1	94	8	2	2	35	37	438
445-500	27	59	134	20	22	32	86	2	3	3	111	10	11	11	34	47	612
500-515	20	73	160	16	21	15	80	0	0	5	102	9	11	7	37	49	605
515-530	17	52	121	24	18	24	95	5	1	4	92	12	4	7	29	38	543
530-545	12	43	135	21	18	27	86	8	3	2	96	14	5	8	44	25	547
545-600	24	53	119	20	19	32	71	1	4	2	97	12	11	6	29	54	554
HOUR TOTALS																	
400-500	97	202	490	81	68	103	304	10	7	12	420	49	32	37	149	164	2225
415-515	90	219	515	76	73	85	297	5	6	12	407	43	35	28	142	175	2208
430-530	85	219	511	78	76	89	314	9	5	13	399	39	28	27	135	171	2198
445-545	76	227	550	81	79	98	347	15	7	14	401	45	31	33	144	159	2307
500-600	73	221	535	81	76	98	332	14	8	13	387	47	31	28	139	166	2249

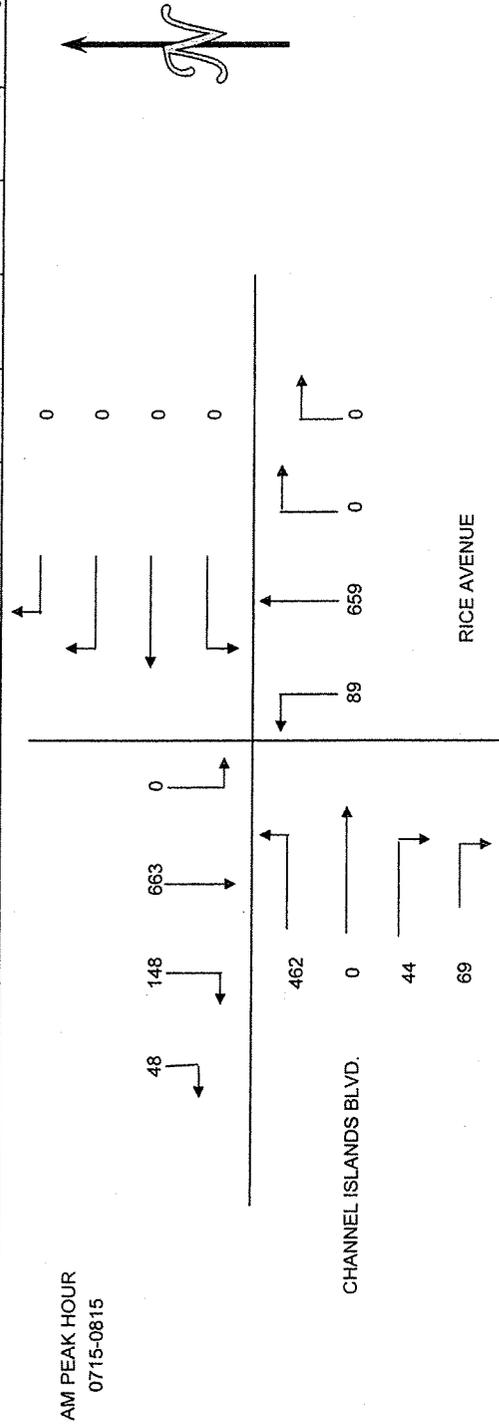
PM PEAK HOUR
445-545



INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 7:00 AM TO 9:00 AM
 INTERSECTION: RICE AVENUE
 E/W CHANNEL ISLANDS BLVD.

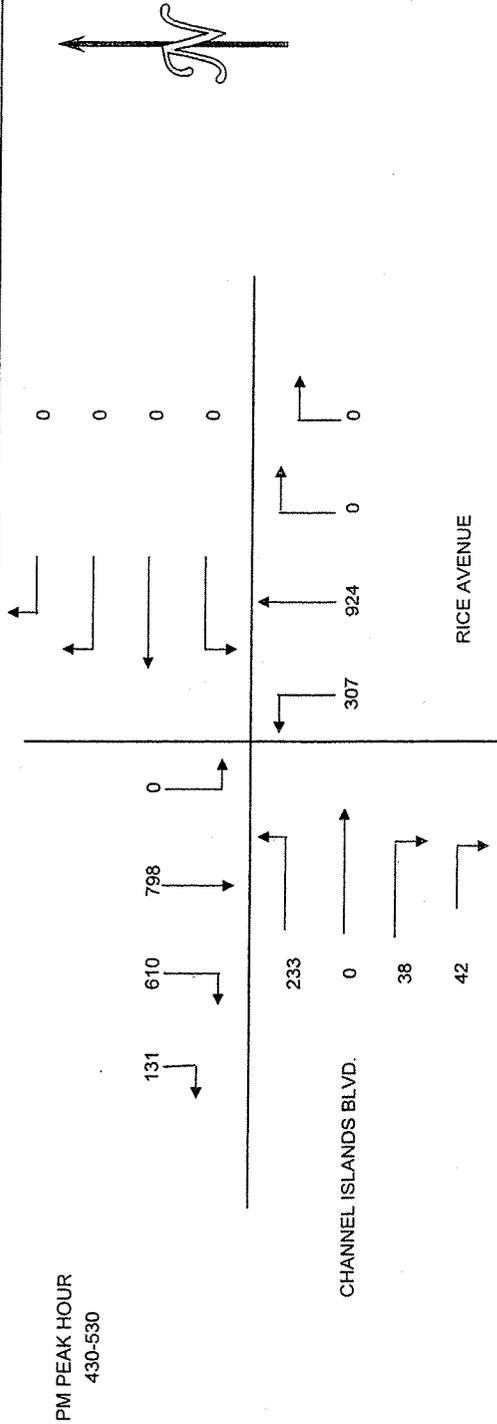
PERIOD	1R SBRT ON/RED	2 SBTH	3 SBLT ON RED	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT ON RED	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
700-715	6	25	139	0	0	0	0	0	0	96	16	7	10	0	74	373
715-730	9	38	151	0	0	0	0	0	0	126	17	20	8	0	110	479
730-745	19	50	205	0	0	0	0	0	0	191	30	22	16	0	128	661
745-800	8	38	140	0	0	0	0	0	0	222	19	16	13	0	138	594
800-815	12	22	167	0	0	0	0	0	0	120	23	11	7	0	86	448
815-830	4	26	101	0	0	0	0	0	0	113	18	11	9	0	63	345
830-845	6	23	103	0	0	0	0	0	0	107	17	9	11	0	55	331
845-900	5	24	103	0	0	0	0	0	0	140	30	16	13	0	59	390
HOUR TOTALS																
0700-0800	42	151	635	0	0	0	0	0	0	635	82	65	47	0	450	2107
0715-0815	48	148	663	0	0	0	0	0	0	659	89	69	44	0	462	2182
0730-0830	43	136	613	0	0	0	0	0	0	646	90	60	45	0	415	2048
0745-0845	30	109	511	0	0	0	0	0	0	562	77	47	40	0	342	1718
0800-0900	27	95	474	0	0	0	0	0	0	480	88	47	40	0	263	1514



INTERSECTION TURNING MOVEMENT COUNT SUMMARY

CLIENT: ALLIANCE JB
 PROJECT: SWC CHANNEL ISLANDS & ROSE
 DATE: THURSDAY, MAY 11, 2007
 PERIOD: 4:00 PM TO 6:00 PM
 INTERSECTI N/S: RICE AVENUE
 EW: CHANNEL ISLANDS BLVD.

PERIOD	1R SBRT ON RED	1 SBRT	2 SBTH	3 SBLT	4R WBRT ON RED	4 WBRT	5 WBTH	6 WBLT	7R NBRT ON RED	7 NBRT	8 NBTH	9 NBLT ON RED	10R EBRT ON RED	10 EBRT	11 EBTH	12 EBLT	TOTAL
400-415	20	130	183	0	0	0	0	0	0	0	259	75	9	2	0	47	725
415-430	21	122	173	0	0	0	0	0	0	0	242	70	8	5	0	59	700
430-445	24	133	173	0	0	0	0	0	0	0	251	68	15	11	0	71	746
445-500	34	137	191	0	0	0	0	0	0	0	242	86	6	5	0	53	754
500-515	23	162	200	0	0	0	0	0	0	0	235	74	13	10	0	47	764
515-530	34	162	213	0	0	0	0	0	0	0	196	79	8	12	0	62	766
530-545	40	149	194	0	0	0	0	0	0	0	205	64	11	6	0	54	723
545-600	21	107	173	0	0	0	0	0	0	0	185	70	12	10	0	66	644
HOURL TOTALS																	
400-500	99	522	720	0	0	0	0	0	0	0	994	299	38	23	0	230	2925
415-515	102	554	737	0	0	0	0	0	0	0	970	298	42	31	0	230	2964
430-530	115	594	777	0	0	0	0	0	0	0	924	307	42	38	0	233	3030
445-545	131	610	798	0	0	0	0	0	0	0	878	303	38	33	0	216	3007
500-600	118	580	780	0	0	0	0	0	0	0	821	287	44	38	0	229	2897



PM PEAK HOUR
430-530

Existing Traffic Counts and Oxnard Traffic Model Existing-Plus-Pending and 2020 Traffic Forecasts

Intersection	Southbound			Westbound			Northbound			Eastbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Statham & Channel Islands												
Existing Counts												
AM Peak Hour	85	0	97	0	673	93	0	0	0	272	1,289	0
PM Peak Hour	66	0	354	0	1,220	55	0	0	0	166	842	0
Existing-Plus-Pending (2008)												
AM Peak Hour	80	0	130	0	660	90	0	0	0	460	1,280	0
PM Peak Hour	100	0	460	0	1,350	70	0	0	0	240	920	0
Rose & Wooley												
Existing Counts												
AM Peak Hour	18	1,048	357	78	143	13	31	1,069	124	275	320	30
PM Peak Hour	27	1,393	423	166	408	57	39	1,019	104	367	260	48
Existing-Plus-Pending (2008)												
AM Peak Hour	20	1,130	350	80	200	10	20	790	120	370	460	30
PM Peak Hour	40	1,780	450	140	350	40	40	910	120	320	470	80
Rose & Oxnard												
Existing Counts												
AM Peak Hour	34	680	27	0	246	88	178	838	31	0	258	184
PM Peak Hour	60	1,063	69	0	589	80	140	788	20	0	242	240
Existing-Plus-Pending (2008)												
AM Peak Hour	40	840	20	0	290	80	240	1,140	30	0	300	220
PM Peak Hour	60	1,350	50	0	880	100	280	1,020	20	0	210	270
Rose & Channel Islands												
Existing Counts												
AM Peak Hour	123	561	142	230	367	8	300	611	148	460	782	141
PM Peak Hour	213	821	404	227	771	15	230	611	105	494	500	166
Existing-Plus-Pending (2008)												
AM Peak Hour	90	800	180	180	310	30	90	760	120	570	620	140
PM Peak Hour	130	980	390	240	850	40	220	830	80	390	530	80
Rose & Raiders Way												
Existing Counts												
AM Peak Hour	2	746	207	0	0	0	221	880	0	139	0	103
PM Peak Hour	5	1,141	31	0	0	0	16	937	0	20	0	27
Existing-Plus-Pending (2008)												
AM Peak Hour	2	875	243	0	0	0	195	775	0	142	0	105
PM Peak Hour	6	1,260	34	0	0	0	19	1,111	0	23	0	31

Existing Traffic Counts and Oxnard Traffic Model Existing-Plus-Pending and 2020 Traffic Forecasts

Intersection	Southbound			Westbound			Northbound			Eastbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Rose & Bard												
Existing Counts	105	470	156	15	104	187	32	683	46	236	164	50
PM Peak Hour	81	550	303	15	347	177	45	401	21	159	144	64
Existing-Plus-Pending (2008)	80	320	160	30	110	170	20	540	60	370	220	60
PM Peak Hour	100	590	220	30	230	220	50	490	20	240	120	50
Rice & Channel Islands												
Existing Counts	0	663	196	0	0	0	89	659	0	462	0	113
PM Peak Hour	0	798	741	0	0	0	307	924	0	233	0	80
Existing-Plus-Pending (2008)	0	610	130	0	0	0	70	560	0	400	0	110
PM Peak Hour	0	560	700	0	0	0	350	790	0	190	0	60

Existing Traffic Counts and Oxnard Traffic Model Existing-Plus-Pending and 2020 Traffic Forecasts

Intersection	Southbound			Westbound			Northbound			Eastbound		
	Right	Through	Left	Right	Through	Left	Right	Through	Left	Right	Through	Left
Statham & Channel Islands	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	97	0	85	93	673	0	0	0	0	0	1,289	272
PM Peak Hour	354	0	66	55	1,220	0	0	0	0	0	842	166
Existing-Plus-Pending (2008)	130	0	80	90	660	0	0	0	0	0	1,280	460
PM Peak Hour	460	0	100	70	1,350	0	0	0	0	0	920	240
Rose & Wooley	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	357	1,048	18	13	143	78	124	1,069	31	30	320	275
PM Peak Hour	423	1,393	27	57	408	166	104	1,019	39	48	260	367
Existing-Plus-Pending (2008)	350	1,130	20	10	200	80	120	790	20	30	460	370
PM Peak Hour	450	1,780	40	40	350	140	120	910	40	80	470	320
Rose & Oxnard	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	27	680	34	88	246	0	31	838	178	184	258	0
PM Peak Hour	69	1,063	60	80	589	0	20	788	140	240	242	0
Existing-Plus-Pending (2008)	20	840	40	80	290	0	30	1,140	240	220	300	0
PM Peak Hour	50	1,350	60	100	880	0	20	1,020	280	270	210	0
Rose & Channel Islands	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	142	561	123	8	367	230	148	611	300	141	782	460
PM Peak Hour	404	821	213	15	771	227	105	611	230	166	500	494
Existing-Plus-Pending (2008)	180	800	90	30	310	180	120	760	90	140	620	570
PM Peak Hour	390	980	130	40	850	240	80	830	220	80	530	390
Rose & Raiders Way	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	207	746	2	0	0	0	0	880	221	103	0	139
PM Peak Hour	31	1,141	5	0	0	0	0	937	16	27	0	20
Existing-Plus-Pending (2008)	243	875	2	0	0	0	0	775	195	105	0	142
PM Peak Hour	34	1,260	6	0	0	0	0	1,111	19	31	0	23
Rose & Bard	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	156	470	105	187	104	15	46	683	32	50	164	236
PM Peak Hour	303	550	81	177	347	15	21	401	45	64	144	159
Existing-Plus-Pending (2008)	160	320	80	170	110	30	60	540	20	60	220	370
PM Peak Hour	220	590	100	220	230	30	20	490	50	50	120	240
Rice & Channel Islands	0	0	0	0	0	0	0	0	0	0	0	0
Existing Counts	196	663	0	0	0	0	0	659	89	113	0	462
PM Peak Hour	741	798	0	0	0	0	0	924	307	80	0	233
Existing-Plus-Pending (2008)	130	610	0	0	0	0	0	560	70	110	0	400
PM Peak Hour	700	560	0	0	0	0	0	790	350	60	0	190

Appendix C -- Oxnard Traffic Model Data

Existing Traffic Counts and Oxnard Traffic Model Existing-Plus-Pending and 2020 Traffic Forecasts

March 17, 2006

Intersection	Southbound		Westbound		Northbound		Eastbound		
	Right	Left	Right	Left	Right	Left	Right	Left	
Statham & Channel Islands									
Existing Counts	97	0	93	673	0	0	0	1,289	272
AM Peak Hour	354	0	55	1,220	0	0	0	842	166
PM Peak Hour	33	0	-3	-13	0	0	0	-9	188
Existing-Plus-Pending (2008)	106	0	15	130	0	0	0	78	74
PM Peak Hour									
Rose & Wooley									
Existing Counts	357	1,048	13	143	78	124	1,069	31	30
AM Peak Hour	423	1,393	57	408	166	104	1,019	39	48
PM Peak Hour	-7	82	-3	57	2	-4	-279	-11	0
Existing-Plus-Pending (2008)	27	387	-17	-58	-26	16	-109	1	32
PM Peak Hour									
Rose & Oxnard									
Existing Counts	27	680	88	246	0	31	838	178	184
AM Peak Hour	69	1,063	80	589	0	20	788	140	240
PM Peak Hour	-7	160	-8	44	0	-1	302	62	36
Existing-Plus-Pending (2008)	-19	287	20	291	0	0	232	140	30
PM Peak Hour									
Rose & Channel Islands									
Existing Counts	142	561	8	367	230	148	611	300	141
AM Peak Hour	404	821	15	771	227	105	611	230	166
PM Peak Hour	38	239	22	-57	-50	-28	149	-210	1
Existing-Plus-Pending (2008)	-14	159	25	79	13	-25	219	-10	-86
PM Peak Hour									

Appendix C -- Oxnard Traffic Model Data

Existing Traffic Counts and Oxnard Traffic Model Existing-Plus-Pending and 2020 Traffic Forecasts

March 17, 2006

Intersection	Southbound		Westbound		Northbound		Eastbound	
	Right	Left	Right	Left	Right	Left	Right	Left
Rose & Raiders Way								
Existing Counts	207	746	0	0	0	880	103	0
AM Peak Hour	31	1,141	0	0	0	937	27	0
PM Peak Hour	36	129	0	0	0	-105	2	0
Existing-Plus-Pending (2008)	3	119	0	0	0	174	4	0
PM Peak Hour								3
Rose & Bard								
Existing Counts	156	470	187	104	46	683	50	164
AM Peak Hour	303	550	177	347	21	401	64	144
PM Peak Hour	4	-150	-17	6	14	-143	10	56
Existing-Plus-Pending (2008)	-83	40	43	-117	-1	89	-14	-24
PM Peak Hour								81
Rice & Channel Islands								
Existing Counts	196	663	0	0	0	659	113	0
AM Peak Hour	741	798	0	0	0	924	80	0
PM Peak Hour	-66	-53	0	0	0	-99	13	0
Existing-Plus-Pending (2008)	-41	-238	0	0	0	-134	-20	0
PM Peak Hour								-43

Existing-Plus-Pending (2008) is lower than Existing Counts

Appendix D - Localized Intersection Traffic Projection Data

Intersection Volume Estimates (Intersections not in Model)

PERIOD	N Link Total	SBRT	SBTH	SBLT	E Link Total	WBRT	WBTH	WBLT	S Link Total	NBRT	NBTH	NBLT	W Link Total	EBRT	EBTH	EBLT
Rose & Raiders Way																
715-815 AM 2007	955	207	746	2	0	0	0	0	1101	221	880	0	242	103	0	139
% of Link Volume		22%	78%	0%						20%	80%	0%				
2008 AM	1120	243	875	2	0	0	0	0	970	195	775	0	242	103	0	139
415-515 PM 2007	1177	31	1141	5	0	0	0	0	953	16	937	0	47	27	0	20
% of Link Volume		2.63%	96.94%	0.42%						1.68%	98.32%	0.00%				
2008 PM	953	25	924	4	0	0	0	0	1130	19	1111	0	47	27	0	20

Appendix D - Localized Intersection Traffic Projection Data

AM

Rose & Raiders Way

N Leg

S Leg

SBRT

SBTH

SBLT

SB Total

NBRT

NBTH

NBLT

NB Total

2007

207

746

2

955

221

880

0

1101

% of Leg

21.68%

78.12%

0.21%

20.07%

79.93%

0.00%

2008

243

875

2

1120

195

775

0

970

Appendix D - Localized Intersection Traffic Projection Data

PM

Rose & Raiders Way

	N Leg			S Leg				
	SBRT	SBTH	SBLT	SB Total	NBRT	NBTH	NBLT	NB Total
2007	31	1141	5	1177	16	937	0	953
% of Leg	2.63%	96.94%	0.42%		1.68%	98.32%	0.00%	
2008	34	1260	6	1300	19	1111	0	1130

February 4, 2008

Mr. Tom Davies
Davies Properties
2225 Glastonbury Road
Westlake Village, CA 91361

Subject: **SWC of Rose and Channel Islands Church Parking Evaluation**

Dear Mr. Davies:

AllianceJB has conducted a parking study to assess the parking demand for the proposed New Progressive Christian Missionary Baptist Church development at the Southwest Corner of Rose Avenue & Channel Islands Boulevard in the City of Oxnard. Parking demand estimation procedures were discussed with Mr. Jason Samonte at the City of Oxnard. A review of the City of Oxnard church parking requirements and the proposed church parking is provided as follows:

City of Oxnard Church Parking Requirements

The City of Oxnard parking requirements for churches are as follows:

<i>Use</i>	<i>Requirements</i>	<i>Notes</i>
Churches and other places of public assembly, including mortuaries, banquet facilities, and convention facilities	One space per five seats within the main auditorium or one space per 35 square feet of gross floor area. Where pews or benches are provided, one space required for every ten lineal feet of pew or bench.	Twenty-four inches shall be considered one seat where pews or benches are provided. Ancillary uses such as offices shall provide additional parking spaces as required.

A direct application of the City of Oxnard parking requirements was conducted by assuming that the main auditorium and all of the ancillary building areas for the proposed New Progressive Christian Missionary Baptist Church building structure would be occupied and utilized simultaneously. Based on the attached building structure plan, City of Oxnard parking requirements indicate that the proposed church would require 50 parking spaces as shown in the attached parking analysis calculation sheet.

ITE Church Parking Requirements

The Institute of Transportation Engineers (ITE) Parking Generation manual was also consulted to estimate the parking demand for the proposed New Progressive Christian Missionary Baptist Church. Based on the total square footage of the church as shown on the attached building structure plan, ITE Parking Generation indicates that the proposed church would require 47 parking spaces as shown in the attached parking analysis calculation sheet.

ITE Hourly Shared Parking Analysis

The ITE Parking Generation manual was also consulted to prepare an hourly shared parking analysis for the proposed New Progressive Christian Missionary Baptist Church on a typical Sunday. Hourly parking demand estimates were prepared for the church, supermarket and retail project site uses. The attached ITE Hourly Shared Parking Analysis calculations indicate that a peak hourly parking demand of 110 parking spaces will occur between 10:00 AM and 12:00 Noon on a typical Sunday. Since the church, supermarket and retail project site uses propose a total of 125 parking spaces, an estimate of 15 parking spaces will be available during peak demand.

Actual Parking Requirements based on Proposed Church Uses:

The proposed New Progressive Christian Missionary Baptist Church building structure, as shown on the attached building structure plan, will provide 171 seats within the main auditorium. The auditorium seats will be used by the congregation, choir and pastor. A small choir room is provided for the choir to store their materials, and two small offices are provided for the pastor to interact privately with members of the congregation. Two small children's classrooms will provide an area for younger children to be supervised by their parents/guardians if needed during church services.

It is understood that a prime consideration in the design of the church structure was the desire for the structure to accommodate both church services and weddings with receptions. The lobby area was consequently designed to double as a reception area following a wedding held within the main auditorium. Likewise, the children's classrooms were designed to double as rooms for the bride/bridesmaids and groom/groomsmen. A small kitchen was placed at one side of the lobby for use during receptions. Restrooms were placed at the other side of the lobby for use during church services or weddings with receptions.

Application of the City's parking requirement that one space be provided for each five seats within the 171 seat main auditorium results in a parking requirement of 34 parking spaces for the proposed church. It is estimated that this parking requirement will accommodate the proposed uses within the structure during church services and during weddings with receptions. A justification of the parking requirements during church services and during weddings with receptions is provided as follows:

Parking Requirements during Church Services:

The 34 parking spaces required for the 171 seats within the main auditorium will accommodate the congregation, choir and pastor during church services. Key points are as follows:

- Church services will be held within the main auditorium, with the lobby functioning as a lobby area before and after the services. The congregation will either be in the main auditorium or passing through the lobby on their way to or from the main auditorium. The congregation will not be in two places at once.
- The choir members will be in the choir seating at the back of the platform during church services, and will use the choir room before and after church services. Choir members will not be in two places at once.
- The pastor will be on the platform during church services, could be in his office before or after church services, and could greet members of the congregation in the lobby before or after church services. The pastor will not be in two places at once.
- The small children's classrooms will provide an area for younger children to be supervised by their parents/guardians if needed during church services. The children will arrive in the same vehicles with their parents/guardians, and the parent/guardians will not be in two places at once.

Parking Requirements during Weddings with Receptions:

The 34 parking spaces required for the 171 seats within the main auditorium will accommodate the wedding attendees, choir and pastor during weddings and receptions. Key points are as follows:

- Wedding services will be held within the main auditorium, with the lobby available for receptions after the wedding services. The wedding attendees will either be in the main

auditorium or in the reception after the wedding. Wedding attendees will not be in two places at once.

- The choir members would be in the choir seating at the back of the platform during wedding services, and would use the choir room before and after the wedding services. Choir members will not be in two places at once.
- The pastor will be on the platform during wedding services, could be in his office before or after a wedding, and could be at the reception after a wedding. The pastor will not be in two places at once.
- The small children's classrooms would provide an area for the bride/bridesmaids and groom/groomsmen before and after wedding services. The bride/bridesmaids and groom/groomsmen will not be in two places at once.

Parking proposed by the Church:

The parking proposed by the New Progressive Christian Missionary Baptist Church, as shown on the attached site plan, will include a total of 35 parking spaces (34 required parking spaces plus 1 additional parking space). These parking spaces will include 34 standard spaces and 1 motorcycle parking space (per City parking requirements). There will be one handicap standard parking space and one handicap van accessible parking space provided at the site.

Conclusions:

The 35 parking spaces proposed by the New Progressive Christian Missionary Baptist Church will meet the City of Oxnard parking requirements during church services and during weddings with receptions.

AllianceJB appreciates the opportunity to provide traffic engineering services for this parking evaluation. If you have any questions, or need any additional traffic engineering services, please contact me at (805)-652-1149.

Very truly yours,



James A. Biega, P.E., T.E.
President, AllianceJB

Attachments: New Progressive Christian Missionary Baptist Church Building Structure
New Progressive Christian Missionary Baptist Church Site Plan



Parking Analysis - New Progressive Christian Missionary Baptist Church

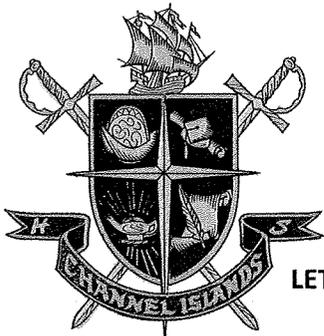
Required Parking per Oxnard Municipal Code					
<u>Area</u>	<u>Bldg. Sq. Ft.</u>	<u># of Seats</u>	<u>Parking * Based on Seats</u>	<u>Parking ** Based on Sq. Ft.</u>	<u>Total Parking per City Code</u>
Sanctuary	1,136	134	26.8		
Platform	<u>607</u>	<u>37</u>	<u>7.4</u>		
Subtotal	1,743	171	34.2		34.2
Office	102			0.408	
Pastor's office	149			0.596	
Choir Room/library	253			1.012	
Lobby	2,067			8.268	
Children's class room 1	141			0.564	
Children's class room 2	144			0.576	
Kitchen	307			1.228	
Electrical Room	71			0.284	
Men's Restroom	200			0.800	
Women's restroom	227			0.908	
Hallway	136			0.544	
Janitor Room	48			0.192	
Storage	17			<u>0.068</u>	
Total Net area	3,862			15.448	15.448
walls	360				
Total Gross Sq. Ft.	5,965				49.648
<p>* Parking is based on 1 space per 5 seats. The church will have fixed seats as opposed to pews or benches</p> <p>** A parking ratio of 1 space per 250 square feet was used for all ancillary areas.</p>					
Required Parking Per ITE Parking Generation Manual					
Church Square Feet	5965				
ITE Average Peak Rate	<u>7.81</u>	spaces per 1,000 square feet			
Require Parking	46.59				

ITE Hourly Shared Parking Analysis

<u>Land Use</u>	<u>Church</u>	<u>SuperMkt</u>	<u>Retail</u>
Building GFA (tsf)	5.965	13.929	5.630
ITE Land Use Code	560	850	820
Avg Peak Rate (veh/1000gfa)	7.81	4.75	2.04
Peak Parking Demand	46.58665	66.16275	11.4852
Parking Supply	34	90	

<u>Hour Beginning</u>	<u>Hour Ending</u>	<u>% of Peak</u>	<u>% of Peak</u>	<u>% of Peak</u>
6:00 AM	7:00 AM			
7:00 AM	8:00 AM			
8:00 AM	9:00 AM			
9:00 AM	10:00 AM	100	64	
10:00 AM	11:00 AM	100	83	78
11:00 AM	12:00 PM	100	80	94
12:00 PM	1:00 PM		85	100
1:00 PM	2:00 PM		93	93
2:00 PM	3:00 PM		96	95
3:00 PM	4:00 PM		100	94
4:00 PM	5:00 PM		93	87
5:00 PM	6:00 PM		99	81
6:00 PM	7:00 PM		96	69
7:00 PM	8:00 PM		81	
8:00 PM	9:00 PM			

<u>Hour Beginning</u>	<u>Hour Ending</u>	<u>Church Parking</u>	<u>SuperMkt Parking</u>	<u>Retail Parking</u>	<u>Total Parking Demand</u>	<u>Total Parking Supply</u>	<u>Available Parking Spaces</u>
6:00 AM	7:00 AM	0	0	0	0	125	125
7:00 AM	8:00 AM	0	0	0	0	125	125
8:00 AM	9:00 AM	0	0	0	0	125	125
9:00 AM	10:00 AM	47	42	0	89	125	36
10:00 AM	11:00 AM	47	55	9	110	125	15
11:00 AM	12:00 PM	47	53	11	110	125	15
12:00 PM	1:00 PM	0	56	11	68	125	57
1:00 PM	2:00 PM	0	62	11	72	125	53
2:00 PM	3:00 PM	0	64	11	74	125	51
3:00 PM	4:00 PM	0	66	11	77	125	48
4:00 PM	5:00 PM	0	62	10	72	125	53
5:00 PM	6:00 PM	0	66	9	75	125	50
6:00 PM	7:00 PM	0	64	8	71	125	54
7:00 PM	8:00 PM	0	54	0	54	125	71
8:00 PM	9:00 PM	0	0	0	0	125	125



Channel Islands High School

1400 East Raiders Way
Oxnard, CA 93033
(805) 385-2787 FAX (805) 385-2748

RECEIVED

FEB 11 5 2008

PLANNING DIVISION
CITY OF OXNARD

LETTER SUPPORTING DAVIES PROPERTIES' PROJECT AT CHANNEL ISLANDS BLVD.

Ms. Kathleen Mallory
Planning Department
City of Oxnard
214 South C Street
Oxnard, CA 93030

February 29, 2008

RE: Comments on project at Channel Islands Blvd. and Rose Ave.

Dear Ms. Mallory:

This letter is in support of the upcoming project of Mr. Tom Davies (Davies Properties) near Channel Islands High School. I had the pleasure of meeting Mr. Davies when he came here to meet with our Principal, Dr. Robert Q. "Rocky" Valles, earlier this school year. Mr. Davies was open and willing to address each and every one of our concerns regarding the project and the establishments that would be located across from our school. He likewise addressed many of the concerns expressed by the parents of our students and neighboring community members.

We here at C.I.H.S. look forward to future partnering opportunities with some of the business that will be our new neighbors. We appreciate the timing of the building project, and have been put at ease knowing there will be minimal (if any) disruptions of the learning process of our students. We discussed parking and traffic and feel confident that adequate planning and efforts have and will continue to be made to ensure a safe and sufficient access for all. Mr. Davies has also extended a pledge to help Channel Islands High School with some of our own building endeavors, and for that, we are extremely grateful.

Once again, we the stakeholders of the learning community of Channel Islands High School, would like to express our full support of the Davies Property project at Channel Islands Boulevard and Rose Avenue. For further questions and/or concerns, feel free to contact me directly.

Sincerely,

Mark C. Contreras
Assistant Principal, Facilities and Campus Supervision
Channel Islands High School
Fax: 385-2748
Office: 385-2746
Cell: 766-8194
e-mail: mark.contreras@ouhsd.k12.ca.us



Channel Islands High School

1400 East Raiders Way
Oxnard, CA 93033
(805) 385-2787

RECEIVED

MAR 04 2008

PLANNING DIVISION
CITY OF OXNARD

March 2, 2008

Ms. Kathleen Mallory
City of Oxnard
Planning department
214 W. C. St.
Oxnard, CA 93030

Re: Proposed project on the corner of Channel Islands Blvd & Rose Ave

Dear Ms. Mallory,

I am submitting this letter to you, as principal of Channel Islands High school, in support of the Tom Davies project on Channel Islands Blvd and Rose Ave. Tom and I have met, as well as with my parent organization in which Mr. Davies thoroughly discussed his project and answered questions. Not only are we comfortable with the regulations being imposed on his project by the state, we are also comfortable with the conditions the Oxnard Police Department is placing as well.

Senior police officer Cliff Waer and I have discussed the project at length and I, as well as, my parents groups, feel comfortable supporting the project.

I thank you for your time and if you should have any questions, please contact me at the number below.

Respectfully,

Dr. Rocky Valles
Channel Islands High School
Principal
Office: 385-2745
Cell: 207-1141

March 3, 2008

RECEIVED

MAR 04 2008

PLANNING DIVISION
CITY OF OXNARD

To whom it may concern,

My name is Mrs. Lisa Chavez. I am a parent at Channel Islands High School. I am writing to support the development project on the corner of Channel Islands Blvd. and Rose Ave of a Fresh and Easy market.

Thank you for your consideration in this matter.


Lisa K. Chavez

City of Oxnard
Planning Division

RECEIVED

MAR 04 2008

PLANNING DIVISION
CITY OF OXNARD

To whom it may concern:

I am writing in concern to the project being developed on the corner of Channel Islands and Rose by Davies Properties.

I am a parent of two Channel Islands High School students, a lifetime community member and also a Channel Islands Alumni.

I was very excited to hear that we will be getting another chain grocery store to the South Oxnard end of town. Due to the area I live in it is more convenient for my family to drive to Camarillo to do our weekly grocery shopping, since our closest chain grocery store is on Saviers Road. This new location would make it more convenient to run to the store more often or have one of my sons pick something up on his way home from school.

It has been thought that this center could provide employment opportunities for some of the students in this area.

The community and I are looking forward to the creation of this development beginning shortly in the future.

Sincerely,

A handwritten signature in black ink that reads "Nancy Brown". The signature is written in a cursive style with a large, sweeping initial "N".

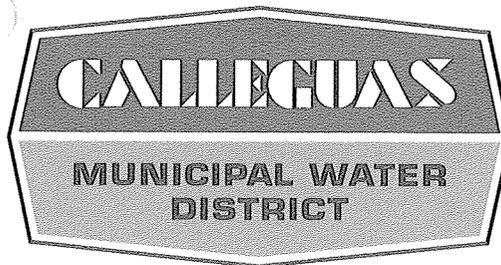
Nancy Brown

Ted Grandsen
President - Division 1
William R. Seaver
Vice President - Division 5
Donald G. Hauser
Secretary - Division 3

Jeffrey A. Borenstein
Treasurer - Division 2

Gail L. Pringle
Director - Division 4

Donald R. Kendall, Ph.D., P.E.
General Manager



2100 Olsen Road
Thousand Oaks
California 91360-6800

(805) 526-9323

Fax (805) 522-5730

Web site: www.calleguas.com

RECEIVED

FEB 19 2008

PLANNING DIVISION
CITY OF OXNARD

February 15, 2008

Susan L. Martin
Planning and Environmental Services Manager
City of Oxnard
305 West Third Street
Oxnard, CA 93030

Re: MND 07-11
PZ 07-600-06
PZ 07-750-04
PZ 07-300-13
PZ 07-500-14
PZ 07-500-19

Dear Ms Martin:

Thank you for sending Calleguas a Notice of Intention to Adopt a Mitigated Negative Declaration MND 07-11. Calleguas understands that the proposed project is located on Assessor's Parcel Number 221-0-232-515 at the southwest corner of Rose Avenue and Channel Islands Boulevard. The purpose of this letter is to confirm that the project lies within the present boundaries of Calleguas Municipal Water District and that annexation of the site to Calleguas will not be a necessary condition of approval.

Please advise the applicant that the project will be subject to the Calleguas Capital Construction Charge.

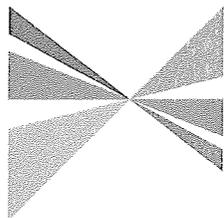
Thank you for keeping Calleguas in mind.

Sincerely yours,

A handwritten signature in black ink that reads "Cy Johnson". The signature is written in a cursive, flowing style.

Cy Johnson
Development Programs Administrator

cc: Robert Hearne



ASSOCIATION of GOVERNMENTS

Main Office

818 West Seventh Street
12th Floor
Los Angeles, California
90017-3435

t (213) 236-1800
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www.scag.ca.gov

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Transportation and Communications
Alan D. Wapner, Ontario

RECEIVED

FEB 25 2008

PLANNING DIVISION
CITY OF OXNARD

February 21, 2008

Ms. Kathleen Mallory
Project Planner
City of Oxnard Planning Division
214 South C Street
Oxnard, CA 93030

RE: SCAG Clearinghouse No. 1 20080092 Planning and Zoning Permit Nos. 07-600-06 (GPA), 07-570-04 (ZC), 07-300-13 (TPM), 07-500-14 (SUP), and 07-500-19 (SUP)

Dear Ms. Mallory:

Thank you for submitting the **Planning and Zoning Permit Nos. 07-600-06 (GPA), 07-570-04 (ZC), 07-300-13 (TPM), 07-500-14 (SUP), and 07-500-19 (SUP)** for review and comment. As areawide clearinghouse for regionally significant projects, SCAG reviews the consistency of local plans, projects and programs with regional plans. This activity is based on SCAG's responsibilities as a regional planning organization pursuant to state and federal laws and regulations. Guidance provided by these reviews is intended to assist local agencies and project sponsors to take actions that contribute to the attainment of regional goals and policies.

We have reviewed the **Planning and Zoning Permit Nos. 07-600-06 (GPA), 07-570-04 (ZC), 07-300-13 (TPM), 07-500-14 (SUP), and 07-500-19 (SUP)**, and have determined that the proposed Project is not regionally significant per SCAG Intergovernmental Review (IGR) Criteria and California Environmental Quality Act (CEQA) Guidelines (Section 15206). Therefore, the proposed Project does not warrant comments at this time. Should there be a change in the scope of the proposed Project, we would appreciate the opportunity to review and comment at that time.

A description of the proposed Project was published in SCAG's **February 1-15, 2008** Intergovernmental Review Clearinghouse Report for public review and comment.

The project title and SCAG Clearinghouse number should be used in all correspondence with SCAG concerning this Project. Correspondence should be sent to the attention of the Clearinghouse Coordinator. If you have any questions, please contact me at (213) 236-1857. Thank you.

Sincerely,

LAVERNE JONES, Planning Technician
Program Development and Evaluation Division

RESOURCE MANAGEMENT AGENCY
county of ventura

Planning Division

Kimberly L. Rodriguez
Director

March 5, 2008

City of Oxnard
Planning Division
305 W. Third Street
Oxnard, CA 93030
Attn: Kathleen Mallory

FAX #: (805) 385-7658 7417

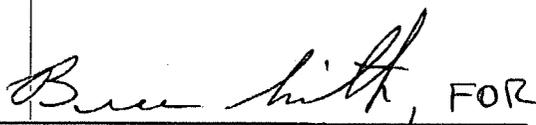
Subject: Comments on NOI to adopt MND Rose Ranch Shopping Center

Thank you for the opportunity to review and comment on the subject document. Attached are the comments that we have received resulting from intra-county review of the subject document.

Your proposed responses to these comments should be sent directly to the commenter, with a copy to Kari Finley, Ventura County Planning Division, L#1740, 800 S. Victoria Avenue, Ventura, CA 93009.

If you have any questions regarding any of the comments, please contact the appropriate respondent. Overall questions may be directed to Kari Finley at (805) 654-3327.

Sincerely,

 FOR

Kim Rodriguez
County Planning Director

G:\Planning Division\Outside Environmental Documents\Response Letters\

Attachment

County RMA Reference Number 08-002

03/05/08 WED 10:55 FAX 0451444

VENTURA CTY APCD

002

VENTURA COUNTY
AIR POLLUTION CONTROL DISTRICT
Memorandum

TO: Kari Finley/Krista Blankenbiller, Planning **DATE:** March 4, 2008

FROM: Alicia Stratton 

SUBJECT: Request for Review of Draft Mitigated Negative Declaration for the Fresh and Easy Grocery/Retail/Church General Plan Amendment and Zone Change, City of Oxnard (Reference No. 08-007)

Air Pollution Control District staff has reviewed the subject project, which is a proposal for a general plan amendment to change the land use designation on the southwest corner of Channel Islands Blvd. and Rose Avenue in the City of Oxnard. This will result in construction of a 13,929 sq. ft. Fresh and Easy grocery store and 5,630 sq. ft. of additional retail building space. The project also proposes construction of a 6,000 sq. ft. church facility to be located on the corner of Rose Avenue and Raiders Way. The entire project area is 4.05 acres.

Section C of the mitigated negative declaration addresses air quality. We concur with the findings of this discussion that air quality impacts from the project would be less than significant. Project-specific emissions were calculated using the URBEMIS 2002 computer model, and results do not exceed thresholds of significance. The mitigation measures described on Pages 13-14 will minimize fugitive dust and particulate matter that may result from construction activities on the site. No further air quality mitigation is necessary.

If you have any questions, please call me at (805) 645-1426.



**COUNTY OF VENTURA
PUBLIC WORKS AGENCY**

*ENGINEERING SERVICES DEPARTMENT
DEVELOPMENT & INSPECTION SERVICES DIVISION*

MEMORANDUM

DATE: March 3, 2008

TO: Kari Finley, Case Planner
RMA - Planning Division

FROM: Brian J. Trushinski, BES, MA, CFM
Engineer IV, Development and Inspection Services

SUBJECT: APPLICATION COMPLETENESS
PROJECT NO.: 08-007
LEAD AGENCY: City of Oxnard

Development & Inspection Services has reviewed the MND for the proposed development and offers the following comment.

The City's Engineering Geologist and Geotechnical Engineer should review the MND to verify the findings presented in the MND. No significant issues from a geologic standpoint.



VENTURA COUNTY
WATERSHED PROTECTION DISTRICT
PLANNING AND REGULATORY DIVISION
800 South Victoria Avenue, Ventura, California 93009
PAUL CALLAWAY, Permit Manager - 805 654-2011

DATE: March 4, 2008

TO: Kari Finley, Resource Management Agency
Planning Division

FROM: Sergio Vargas, Deputy Director
Watershed Protection District

SUBJECT: RMA 08-007.FRESH AND EASY GROCERY/RETAIL/
CHURCH GPA & ZONE CHANGE – city of Oxnard

The Watershed Protection District has reviewed the above Notice of Intent to Adopt Draft Mitigated Negative Declaration and our comments are as follows:

The ND describes construction related and operational impacts of the project on hydrology and water quality. The ND discussion concludes that water quality impacts associated with the construction activities and proposed uses would be less than significant. The discussion also cites a requirement to submit subsequent project plans and discusses City of Oxnard standards. However, the ND failed to include a site plan which included water quality source and treatment controls, typical engineering drawings for such water quality treatment Best Management Practices (BMPs), or any meaningful discussion of how source and treatment control BMPs would function at the project site. Therefore, it is impossible for the public and decision makers to know where source and treatment control BMPs would be located, how they would be designed, and how they would function and be maintained.

In addition, the ND fails to disclose the receiving water body (or its impairment status) for the additional stormwater that would be generated on the site following completion of construction. A discussion of these issues in the ND would possibly substantiate the conclusions regarding runoff and water quality impacts of the proposed project. However, because the applicable BMPs were not disclosed or discussed in the publicly circulated ND, the ND's analysis and conclusions of the project's operational impacts on stormwater runoff and water quality is unsupported.

As described in PRC Section 21002.1, the purpose of an ND is to identify the potentially significant effects on the environment, identify alternatives to the

Page 2 of 2 pages
Fresh and Easy Grocery/Retail/Church
General Plan Amendment and Zone Change
RMA 08-007
March 4, 2008

project and to indicate the manner in which those potentially significant effects can be mitigated.

Also, as described in PRC Section 21082.2, the lead agency determines whether a project may have a significant effect on the environment based on substantial evidence in light of the whole record. Argument, speculation, unsubstantiated opinion or narrative is not substantial evidence. Substantial evidence includes facts, reasonable assumptions predicated upon facts and expert opinion supported by facts.

Deferring an impact analysis or mitigation to a later date by preparing a subsequent plan (i.e., failing to disclose the land/area requirements for on site stormwater treatment features, the efficacy and maintenance requirements of the proposed stormwater treatment approach, and a conceptual design) may be a potential violation of CEQA (see *Sundstrom v. County of Mendocino* (1988) 202 Cal. App. 3d 296). Moreover, the public and decision makers have no way of knowing whether the NPDES, SWPPP, or SQUIMP provisions will adequately mitigate the stormwater and water quality impacts of the proposed development.

Therefore, the MND needs to be revised to include a full description of all feasible water quality and operational stormwater control mitigation measures that will be required, and an analysis how these measures would in fact reduce impacts on stormwater and water quality before reaching conclusions on the significance of the impacts or the adequacy of the mitigation measures. Further, in the spirit of PRC Section 21003, NPDES and/or SWPPP requirements should be routinely included in publicly circulated CEQA documents so as disclose and allow for public comment and consideration by decision makers.

The Watershed Protection District also made comments in their August 23, 2007 Memorandum to the city of Oxnard and requested a Drainage Study for review prior to making our comments. In reviewing the Mitigated Negative Declaration the District has reviewed the Hydrology study included and our comments on this study are as follows:

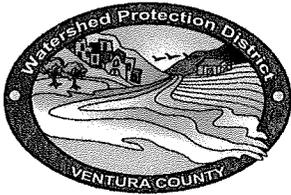
- 1) It is not a complete hydrology study, and does not provide enough info to evaluate whether or not the work was done correctly and whether the conclusions it presents are correct or not.

Page 3 of 3 pages
RMA 08-007
Fresh and Easy Grocery/Retail/Church
General Plan Amendment and Zone Change
March 4, 2008

- 2) It does not discuss the existing development and whether the proposed development will change the % imperviousness of the subareas. If it will, the peak flows will likely change and may need mitigation.
- 3) If the Tc calculations are done correctly, then the Tc's for two of the areas are greater than 5mins and acceptable for use for drainage system design. The other subarea has a Tc of less than 5 mins so the Tc calculator or VCRat model methodology cannot be used to evaluate the peak flow from this area. This can be done by evaluating the whole site as one subarea, using the longest pathway to calculate the overall Tc, and then pro-rating the resultant peak flow on a cfs/ac basis to use for drainage system design.
- 4) The Tc calculations use as many as 9 flowpath segments for the calculation, which is many more than is generally used for Tc calculations. Maps of the drainage system and flow paths and contributing areas should be provided as support for these calculations.

In addition all Watershed Protection District channels serving the city of Oxnard are considered impacted facilities and are deficient for capacities based on the current level of development. Therefore, there can be no increases in volume and quantity of water reaching District facilities over all frequencies.

End of Text



VENTURA COUNTY
WATERSHED PROTECTION DISTRICT
PLANNING AND REGULATORY DIVISION
800 South Victoria Avenue, Ventura, California 93009
PAUL CALLAWAY, Permit Manager - 805 654-2011

DATE: February 19, 2008

TO: Kathleen Mallory, Case Planner

FROM: Paul Callaway, Permit Manager
Planning and Regulatory

SUBJECT: TENTATIVE PARCEL MAP 07-300-13

The Watershed Protection District has reviewed the above project and finds it **INCOMPLETE** from our area of concern.

The developer needs to submit a drainage report addressing the increase in the peak runoff rate, if there is any, due to the increase in impervious area produced by construction of this proposed modification to the development per Watershed Protection District standard, which is that there must be no increase in peak runoff rate in any storm frequency. This report should also provide recommendations for mitigation of the increase in peak runoff rate, if there is any. The drainage report must be prepared by, signed and stamped by a California Registered Civil Engineer.

End of Text

ATTACHMENT D



Police Department

John Crombach, Police Chief

Date: February 12, 2008
To: Kathleen Mallory, AICP Planner
From: Cliff Waer, Senior Alcohol Compliance Officer
Subject: Channel Islands Blvd. at Rose Ave. (Fresh and Easy) **Re: PZ 07-500-14**

Site Information:

The proposed site is located on the southwest corner of Rose Avenue and Channel Islands Boulevard. The site is currently undeveloped and is proposed to be part of a small retail shopping center that will include the proposed 14,000 square foot market and an additional 5,600 square feet of retail space.

The site it is generally bordered by Channel Islands Boulevard on the north, Rose Avenue to the east, residences to the west and a high school to the south. The nearest residences are approximately 100 feet to the west and Channel Islands High School is approximately 350 feet to the south. The applicant has requested to obtain a California Alcoholic Beverage Control (ABC) License Type-21 which is an Off-Sale License that allows for the sale of beer, wine and distilled spirits.

Alcohol outlets located within 350 feet of the proposed site include:

BUSINESS NAME	LOCATION	LICENSE TYPE	LICENSE TITLE	BUSINESS TYPE	ALCOHOL ALLOWED
1. ARCO – AM/PM	2850 S. Rose Ave.	Type 20	Off-Sale Beer and Wine	Gas Station / Market	Beer and Wine

Alcohol outlets located within 1000 feet of the proposed site include:

BUSINESS NAME	LOCATION	LICENSE TYPE	LICENSE TITLE	BUSINESS TYPE	ALCOHOL ALLOWED
1. Sharks Grill	2701 S. Rose Ave.	Type 41	On-Sale Beer and Wine (food)	Restaurant	Beer and Wine
2. Tacos Mi Pueblo	2701 S. Rose Ave.	Type 41	On-Sale Beer and Wine (food)	Restaurant	Beer and Wine

Crime Statistic Review:

For comparison purposes the Police Department calculates the average number of part I and II crimes that occur per reporting district (grid) during a selected 12-month period. The average city-wide, per grid base number of Part I and II crimes is currently 139.

The average number of Part I and II crimes in the *applicant's* reporting district and all other districts within 1000 feet of the applicant is 151 during the same 12-month time period. This is 8.5% higher than the average crime rate citywide and is not considered to be significant. The heaviest concentration of incidents occurred east of the site near the residences that are approximately 1000 feet away. Disturbance-related calls were above average and those that listed alcohol as a contributing factor were below average.

For reference, the category of part I crimes include: murder, rape, robbery, theft, burglary, auto theft, assault, and arson. Part II crimes include: vandalism, weapons possession, other sex offenses, drug abuse violations, driving under the influence, liquor laws, drunkenness, and disorderly conduct.

Police Department Input:

The Beat Coordinator for the area was contacted and said the surrounding area is not generally considered a crime problem but cautioned that there are significant police problems just outside of the 1000 foot radius normally considered as the area of influence for alcohol outlets. To help prevent problems, he supported the inclusion of the standard police conditions in the Planning Commission Resolution

The Police Department's primary concern is the proximity of the proposed site to Channel Islands High School. The school is approximately 350 feet south of the proposed site and the shopping center is very likely to be a popular destination for students before and after school. Numerous studies have shown that youth exposure to alcohol significantly impacts underage consumption. Increased access and exposure to alcoholic beverages are directly related to incidences of youth consumption and should be carefully considered when determining appropriate locations for new outlets.¹⁻²

¹ G. Hastings, S. Anderson, E. Cooke, and R. Gordon, "Alcohol advertising and marketing and young people's drinking: a review of the research," *Journal of Public Health Policy* 26 (2005):296-311.

² L.B. Snyder, F.F. Milici, M. Slater, H. Sun, and Y. Strizhakova, "Effects of alcohol advertising exposure on drinking among youth," *Archives of Pediatrics and Adolescent Medicine* 160 (2006):18-24.

The Youth Services Officer who is assigned to Channel Islands High School was equally concerned about the outlet's proximity and expects that it will be a challenge to keep students from taking advantage of the situation. She said that the business will likely experience a significant influx of students after school and that the crowds may be difficult to manage or monitor. Among other things, she recommended specific conditions such as security cameras with feedback monitors that would give school staff the ability to review the recordings and help identify problem students.

Community Input:

The Responsible Alcohol Policy Action Coalition (RAPAC) has discussed this proposal at their monthly meetings and echoed the concern about the proximity to the high school. They believed that the overall shopping center would likely be an inviting "hang-out" for students and that such a use would be unfitting so close to the school. Members suggested that if the Permit is granted, the business should develop a formal policy of how to deal with wayward students and to work directly with the high school to determine the best methods of contacting school staff. They also recommended restricting certain alcoholic beverages that are appealing to underage drinkers (such as flavored malt beverages often referred to as alcopops). Recent studies have shown that in California, underage drinkers account for nearly half of all the alcopops sold in the entire state and that teenage girls account for the vast majority of the total sold to underage consumers.

The proposed use was discussed with the Principal at Channel Islands High School (Dr. Rocky Valles) who, in turn, shared the information with several groups including student organizations, boosters, PFO and school staff. In his report back to the Police Department, Principal Valles concluded that, while there were some risks associated with the use, they were not opposed as long as there were strict conditions to minimize problems.

Conclusion:

The statistical analysis shows the area to have a crime rate that is slightly higher than the city-wide average and that disturbance-related calls are also above average; however, the area is generally not considered to be a policing problem and the addition of an Off-Sale alcohol outlet should not significantly aggravate existing issues.

There is one similar use within 350 feet of the proposed site so there is an issue of undue concentration by local standards. The presumption of undue concentration has been rebutted by a preponderance of evidence, which shows that the establishment is in a retail center where restaurant establishments selling alcoholic beverages for consumption on the premises are appropriate. The guidelines for the Department of Alcoholic Beverage Control (ABC) use census tract populations to determine the "allowable" number of alcohol outlets in a particular census tract. In this case, the census tract surrounding this proposal allows for five Off-Sale Outlets. Currently, there is only one Off-Sale license in the tract so there is no undue concentration by state standards

The very close proximity to Channel Islands High School and to another similar use are the only significant issues to consider. The Police Department believes that the potential negative impacts of this use can be effectively mitigated through the adoption of all of the standard and special

conditions of operation detailed in the attached Planning Commission resolution. It is the experience of the Police Department that similar uses, when responsibly operated and adequately conditioned, do not typically increase policing problems.

ATTACHMENT E

RESOLUTION NO. 2008 – [PZ 07-500-14]

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OXNARD APPROVING PLANNING AND ZONING PERMIT NO. 07-500-14 (SPECIAL USE PERMIT FOR ALCOHOL SALES), TO ALLOW THE SALE OF ALCOHOL AT FRESH AND EASY (TESCO) GROCERY STORE LOCATED ON THE SOUTHWEST CORNER OF CHANNEL ISLANDS BLVD. AND ROSE AVENUE (APN 221-0-232-514) SUBJECT TO CERTAIN FINDINGS AND CONDITIONS. FILED BY CHURCHYARD DEVELOPMENT, LLC., 2225 GLASTONBURY ROAD, WESTLAKE VILLAGE, CA 91361.

WHEREAS, the Planning Commission of the City of Oxnard has considered an application for Planning and Zoning Permit No. 07-500-14, filed by Churchyard Development, LLC., in accordance with Section 16-530 through 16-553 of the Oxnard City Code and City Council Resolution No. 11,896; and

WHEREAS, in accordance with the California Environmental Quality Act, the Planning Manager provided public notice of the intent of the City to adopt a mitigated negative declaration for this project, and the Planning Commission considered the proposed mitigated negative declaration, together with any comments received during the public review process, finds on the basis of the whole record before it (including the initial study and any comments received) that with the imposition of mitigation measures as conditions of approval, there is no substantial evidence that the project will have a significant effect on the environment, further finds that the mitigated negative declaration reflects the independent judgment of the City, and adopts the mitigated negative declaration; and

WHEREAS, the documents and other materials that constitute the record of proceedings upon which the decision to adopt the mitigated negative declaration is based is located in the Planning and Environmental Services Division of the City of Oxnard, and the custodian of the record is the Planning and Environmental Services Manager; and

WHEREAS, the Planning Commission finds, after due study, deliberation and public hearing, that the following circumstances exist:

1. The proposed use is in conformance with the General Plan and other adopted policies of the City of Oxnard as amended by PZ 07-600-06.
2. The proposed use will not adversely affect or be materially detrimental to the adjacent uses, buildings or structures or to the public health, safety or general welfare.
3. The site for the proposed use is adequate in size and shape to accommodate the setbacks, parking, landscaping, and other City standards as amended by this approval.
4. The site for the proposed use will be served by streets and highways adequate in width and structure to carry the kind and quantity of traffic such use will generate.

5. The site for the proposed use will be provided with adequate sewerage, water, fire protection and storm drainage facilities.
6. The presumption of undue concentration has been rebutted by a preponderance of evidence, which shows that the establishment is in a retail center and the selling of alcoholic beverages for consumption off-site is appropriate.
7. The proposed use is not likely to create or significantly aggravate police problems within 1,000 feet of the location for which the special use permit is applied.

WHEREAS, the Planning Commission finds that the applicant agrees with the necessity of and accepts all elements, requirements, and conditions of this resolution as being a reasonable manner of preserving, protecting, providing for, and fostering the health, safety, and welfare of the citizenry in general and the persons who work, visit or live in this development in particular.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Oxnard hereby approves this permit subject to the following conditions. The decision of the Planning Commission is final unless appealed in accordance with the provisions of Section 16-545 of the Oxnard City Code.

Revised February 20, 2008

**STANDARD CONDITIONS OF APPROVAL
 FOR LAND USE PERMITS**

Note: The abbreviations below identify the City department or division responsible for determining compliance with these standard conditions. The first department or division listed has responsibility for compliance at plan check, the second during inspection and the third at final inspection, prior to issuance of a certificate of occupancy, or at a later date, as specified in the condition. If more than one department or division is listed, the first will check the plans or inspect the project before the second confirms compliance with the condition. The italicized code at the end of each condition provides internal information on the source of each condition: Some are standard permit conditions (e.g. *G-1*) while some are taken from environmental documents (e.g. *MND-S2*).

DEPARTMENTS AND DIVISIONS			
CA	City Attorney	PL	Planning Division
DS	Dev Services/Eng Dev/Inspectors	TR	Traffic Division
PD	Police Department	B	Building Plan Checker
SC	Source Control	FD	Fire Department
PK	Public Works, Landscape Design	CE	Code Compliance

GENERAL PROJECT CONDITIONS

1. This permit is granted for the property described in the application on file with the Planning and Environmental Services Division (“Planning Division”), and may not be transferred from one property to another. (PL, *G-1*).
2. This permit is granted for the plans received December 18, 2007, (“the plans”) on file with the Planning Division. The project shall conform to the plans, except as otherwise specified in these conditions, or unless a minor modification to the plans is approved by the Planning and Environmental Services Manager (“Planning Manager”) or a major modification to the plans is approved by the Planning Commission. A minor modification may be granted for minimal changes or increases in the extent of use or size of structures or of the design, materials or colors of structures or masonry walls. A major modification shall be required for substantial changes or increases in such items. (PL, *G-2*)
3. This permit shall automatically become null and void 24 months from the date of its issuance, unless Developer has diligently developed the proposed project, as shown by the issuance of a grading, foundation, or building permit and the construction of substantial improvements, or the beginning of the proposed use. (PL, *G-3*)
4. All required off-site and on-site improvements for the project, including structures, paving, and landscaping, shall be completed prior to occupancy unless the Development Services Manager allows Developer to provide security or an executed agreement approved by the City Attorney to ensure completion of such improvements. (DS, *G-4*)
5. By commencing any activity related to the project or using any structure authorized by this permit, Developer accepts all of the conditions and obligations imposed by this permit and waives any challenge to the validity of the conditions and obligations stated therein. (CA, *G-5*)
6. Developer shall complete the “Notice of Land Use Restrictions and Conditions” form, using the form provided by the City, for recording with the Ventura County Recorder. Before the City issues building permits, Developer shall submit the original completed, signed and notarized document, together with the required fees to the Planning Manager. (PL, *G-8*)
7. Developer shall provide off-street parking for the project, including the number of spaces, stall size, paving, striping, location, and access, as required by the City Code. (PL/B, *G-9*)
8. Before placing or constructing any signs on the project property, Developer shall obtain a sign permit from the City. Except as provided in the sign permit, Developer may not change any signs on the project property. (PL/B, *G-10*)
9. Developer shall obtain a building permit for any new construction or modifications to structures, including interior modifications, authorized by this permit. (B, *G-11*)
10. Developer shall not permit any combustible refuse or other flammable materials to be burned on the project property. (FD, *G-12*)

11. Developer shall not permit any materials classified as flammable, combustible, radioactive, carcinogenic or otherwise potentially hazardous to human health to be handled, stored or used on the project property, except as provided in a permit issued by the Fire Chief. (FD, G-13)
12. If Developer, owner or tenant fails to comply with any of the conditions of this permit, the Developer, owner or tenant shall be subject to a civil fine pursuant to the City Code. (CA, G-14)

LANDSCAPE STANDARD CONDITIONS

13. Before the City issues building permits or the proposed use is initiated, Developer shall submit two copies of landscape and irrigation plans, along with the appropriate permit application and fees, to the Development Services Division and obtain approval of such plans. (PK/DS, PK-2)
14. Before the City issues a certificate of occupancy, Developer shall install landscape and automatic irrigation systems that have been approved by Parks and Facilities Superintendent. (PK, PK-3)
15. Developer shall maintain landscape planting and all irrigation systems as required by the City Code and as specified by this permit. Failure of Developer to do so will result in the revocation of this permit and initiation of legal proceedings against Developer. (PK, PK-4)
16. Before the City issues a certificate of occupancy, Developer shall provide a watering schedule to the building owner or manager and to the Parks and Facilities Superintendent. The irrigation system shall include automatic rain shut-off devices, or instructions on how to override the irrigation system during rainy periods. (PK, PK-5)
17. All trees planted or placed on the project property by Developer shall be at least 24-inch-box size. All shrubs and vines shall be at least five-gallon size, except as otherwise specified by this permit. (PK, PK-6)
18. Developer shall install an irrigation system that includes a water sensor shut off device as a water conservation measure. (PK, PK-22)

PARKS SPECIAL CONDITIONS

19. Developer to join the existing Landscape Maintenance District and pay its prorata share of the annual landscape maintenance costs for the landscape median fronting the project as well as the landscape parkway improvements on Cota Circle. Contact Landscape Maintenance Assessment District Supervisor Joel Galaviz, at 805-385-8245, to coordinate and complete the procedure for joining the existing Landscape Maintenance District.
20. The west side of the 6'-0" high wall (adjacent to cul-de-sac) shall be planted with 5 gallon ficus vines at a maximum of 10' on center spacing (O.C.).

21. Developer shall plant 5 gallon vines at 10' O.C. spacing against the loading dock wall (Fresh and Easy Market location).

FIRE DEPARTMENT STANDARD CONDITIONS

22. Developer shall construct all vehicle access driveways on the project property to be at least 25 feet wide. Developer shall mark curbs adjacent to designated fire lanes in parking lots to prohibit stopping and parking in the fire lanes. Developer shall mark all designated fire lanes in accordance with the California Vehicle Code. (FD/B, *F-1*)
23. All roof covering materials on the project property shall be of non-combustible or fire retardant materials approved by the Fire Chief and in compliance with the City Code. (FD, *F-2*)
24. Before the City issues building permits, Developer shall obtain the Fire Chief's approval of a plan to ensure fire equipment access and the availability of water for fire combat operations to all areas of the project property. The Fire Chief shall determine whether or not the plan provides adequate fire protection. (FD/DS, *F-3*)
25. At Developer's expense, Developer shall obtain two certified fire flow tests for the project property. The first test shall be completed before City approval of building plans and the second shall be completed after construction and prior to the issuance of a certificate of occupancy. A mechanical, civil, or fire protection engineer must certify the tests. Developer shall obtain permits for the tests from the Engineering Division. Developer shall send the results of the tests to the Fire Chief and the City Engineer. (FD/DS, *F-4*)
26. All structures on the project property shall conform to the minimum standards prescribed in Title 19 of the California Code of Regulations. (FD, *F-5*)
27. The project shall meet the minimum requirements of the "Fire Protection Planning Guide" published by the Fire Department. (FD, *F-6*)
28. At all times during construction, developer shall maintain paved surfaces capable of handling loads of 46,000 pounds which will provide access for fire fighting apparatus to all parts of the project property. (FD/DS, *F-7*)
29. Developer shall identify all hydrants, standpipes and other fire protection equipment on the project property as required by the Fire Chief. (FD, *F-8*)
30. Developer shall provide central station monitoring of the fire sprinkler system and all control valves. (FD, *F-10*)
31. The turning radius of all project property driveways and turnaround areas used for emergency access shall be a minimum of 48 feet outside diameter for a semi-trailer. (FD, *F-11*)
32. Developer shall provide automatic fire sprinklers as required by the City Code and shall contact the Fire Chief to ascertain the location of all connections. (FD, *F-12*)

FIRE DEPARTMENT SPECIAL CONDITIONS

33. Before the City issues a certificate of occupancy, the Developer shall install a Knox Box key vault at a location on each building to be determined by the Fire Department.

POLICE DEPARTMENT CONDITONS

34. All managers or supervisors who are responsible for the daily coordination, supervision or managing of employees, shall complete a course in Responsible Beverage Sales and Service (RBSS) within sixty days of license granting and/or date of employment. Applicant can contact Senior Officer Cliff Waer at cliffwaer@oxnardpd.org to make arrangements. (PL/PD)
35. There shall be no advertising of alcoholic beverages visible from the outside of the establishment, including advertising directed to the exterior from within, promoting or indicating the availability of alcoholic beverages. (PL/PD)
36. The Police Chief or designee may immediately suspend operation of the uses approved by this permit pending a hearing on the revocation of this permit if the Chief finds that there have been significant violations of the use permit conditions and/or ABC permit, or there is a single serious violent crime or single significant incident to which multiple police units or multiple police jurisdictions respond associated with the operation of this use, which the Chief determines is detrimental to the public safety or health. The Chief shall immediately inform the Planning and Environmental Services Manager of the suspension and the Manager shall schedule a hearing on the revocation of the permit by the Planning Commission to be held no more than 30 days after the suspension begins. (PD)
37. Any signs, advertisements or decorations placed upon the windows shall not exceed 20% of the overall window area. Additionally, there shall be no other obstructions placed near the windows that exceed 20% of the overall viewing area including gondola display racks, stored products, shades or blinds.
38. Upon any individual transfer (person-to-person) of the subject Alcoholic Beverage Control License, or if the business is ever deemed a nuisance as defined by Oxnard City Code, the Police Department may initiate Planning Commission review of the existing SUP and the Planning Commission may apply or remove conditions as appropriate to mitigate existing or potential problems.
39. Any graffiti painted or marked upon the premises or on any adjacent area under the control of the Permittee shall be removed or painted over within twenty-four (24) hours of being applied. (PL/PD)
40. Permittee shall be responsible for maintaining free of litter the area adjacent to the premises over which they have control. This includes the rear of the business. (PL/PD)

41. Any rear door of the premises shall be equipped on the inside with an automatic locking device, shall be closed at all times, and shall not be used as a means of access by patrons to and from the licensed premises. Temporary use of these doors for delivery of supplies or disposal of trash does not constitute a violation. (PD)
42. Employees involved in the sale or service of alcoholic beverages shall not be allowed to consume alcoholic beverages at any time during their shift or at any time within eight (8) hours prior to the beginning of their shift. (PD)
43. The parking lot and adjacent areas of the premises shall be equipped with lighting of sufficient power to illuminate and make easily discernable the appearance and conduct of all persons on or about the area. This includes the rear of the business. The lighting plan shall be reviewed and approved by the Police Department in addition to Planning. (PL/PD)
44. There shall be no pay phones installed inside the premises nor shall there be any pay phones installed outside within 100 feet of the premises. (PL/PD)
45. Permittee shall regularly police the area under Permittee's control (including the rear of the business) in an effort to prevent the loitering of persons about the premises. (PL/PD)
46. In the areas surrounding the business the Permittee shall post prominent, permanent signs indicating that loitering, open containers and the consumption of alcoholic beverages is prohibited. This includes the parking lot and other adjacent areas under Permittee's reasonable control. (PD)
47. Coolers or displays containing alcoholic beverages shall be separate from other coolers or displays and shall be positioned so as to allow maximum visibility to cashiers, clerks or employees. Alcohol displays shall not be positioned near customer entry/exit doors, nor shall they be in a location that allows for an easy and unobstructed path to any entry or exit. It is recommended that the alcohol displays or coolers be positioned where the cashiers, clerks or supervisor stations have a clear view from their normal workstations of the activity of persons in the alcohol isle. (PD)
48. There shall be no amusement machines or video devices maintained on the premises at any time. (PD)
49. No single containers, 12oz., 16oz., 20oz., 24oz., 32oz., or 40oz., of beer or malt liquor shall be sold. (PD)
50. No fortified wine products shall be sold with an alcohol content greater than 14% by volume with the exception of the following products (provided they are consistent with those permitted by their Alcoholic Beverage Control License): Port, Sherry, Marsala, Saki, Madiera, Muscat and Vermouth. (PD)

51. Prominent signs shall be posted stating, in effect, "No persons under 21 will be served alcoholic beverages" and "Valid ID is required to purchase alcoholic beverages". These signs shall, at a minimum, be posted at each point of sale and near any alcohol display or areas. (PD)
52. No open floor displays of alcoholic beverages are allowed, including but not limited to "beer mountains" and portable coolers. (PD)
53. Alcoholic beverages shall not be sold between the hours 11:00 PM and 6:00 AM. (PD)
54. There shall be no self-service displays of any type of tobacco product including, but not limited to cigarettes, cigars and smokeless tobacco.
55. Security cameras shall be installed to monitor the premises and be positioned to monitor at minimum the entry/exit, all points of sale, alcohol coolers and the areas immediately surrounding the exterior of the business (including the rear). The camera system shall comply with the following minimum standards: (PD)
 - a. The cameras shall be color cameras, made by a reputable manufacturer and maintained to current industry standards. They shall have low light capability and be capable of identifying persons conducting transactions at the stores' registers or entering/exiting the business.
 - b. The system shall utilize a Digital Video Recorder (DVR). The use of videocassette recorders (VHS and other formats) is prohibited. The DVR shall allow recording, live viewing and playback of recorded video for a period of least 30 days. DVR shall perform all recording, viewing (local and remote), playback (local and remote), queries and backup functions simultaneously, with no interruption of any other function.
56. Permittee shall establish responsible cash handling procedures to reduce the likelihood of robberies and thefts.
57. Permittee shall bolt down all cash registers to service counters in order to prevent the entire device from being stolen during a burglary or robbery.
58. Permittee shall have drop-safes installed or other cash management procedures to allow employees to deposit daily receipts throughout the day as the amounts exceed allowable levels in the register (typically \$50).
59. Permittee shall install signage which indicates that employees do not possess keys to safes and that minimal levels of cash are available in register.
60. Permittee shall install height gauges at all exit doors.
61. Permittee shall install an electronic intrusion detection system that detects portal openings, glass break, and interior motion.

62. Permittee shall equip each point of sale with a silent robbery alarm that complies with Oxnard City Ordinance No. 2601.
63. A copy of these conditions must be maintained on the premises and made available upon the demand of any peace officer at all times. (PL/PD)

POLICE DEPARTMENT SPECIAL CONDITONS

64. Permittee shall install a video feedback monitor which displays the live feed from security cameras positioned nearby. The monitor shall, at a minimum, be displayed at each customer entrance. Permittee shall install an additional feedback monitor in the alcoholic beverage display area. (PD)
65. Any display of distilled spirits shall be maintained in locked cabinets or under the direct control of employees. No distilled spirits shall be accessible to customers without assistance from an employee. (PD)
66. Permittee shall cooperate fully with staff from Channel Islands High School and shall develop a written policy that specifies how employees will deal with teenage customers that are patronizing the business during normal school hours. The policy will include developing an efficient method of communication with school staff, the Youth Services Officer and how potential truants will be handled. (PD)
67. The sale of flavored malt beverages (commonly referred to as alcopops) is prohibited. Such products are generally sweetened, carbonated alcoholic beverages made with malt or spirits-based alcohol. Examples of such products include Smirnoff Ice, Mike's Hard Lemonade, Bacardi Silver and Jack Daniel's Lynchburg Lemonade. (PD)
68. There shall be no deliveries of alcoholic beverages permitted during typical high school commuting hours. Specifically, deliveries of alcohol are prohibited between 7:30 a.m. and 8:30 a.m. and between 2:30 p.m. to 3:30 p.m. (PD)

PLANNING DIVISION STANDARD CONDITIONS

69. The final building plans submitted by Developer with the building permit application shall depict on the building elevation sheets all building materials and colors to be used in construction. (PL/B, *PL-1*)
70. Any application for a minor modification to the project shall be accompanied by four copies of plans reflecting the requested modification, together with applicable processing fees. (PL, *PL-2*)
71. Before the City issues building permits, Developer shall include a reproduction of all conditions of this permit as adopted by resolution of the Planning Commission and/or the City Council in all sets of construction documents and specifications for the project. (PL, *PL-3*)

72. Before the City issues building permits, Developer shall provide to the Planning Division Manager color photographic reductions (8 1/2" by 11") of full-size colored elevations and any other colored exhibit approved by the Planning Commission. Developer may retain the full-size colored elevations after the reductions are so provided. (PL, *PL-4*)
73. Developer acknowledges that because of population limitations placed on the City by the Air Quality Management Program, approval of this permit does not guarantee that the City will issue building permits. The City's issuance of building permits may be delayed as a result of implementation of an air quality plan. (PL, *PL-5*)
74. Developer may not modify any use approved by this permit unless the Planning Division Manager determines that Developer has provided the parking required by the City Code for the modified use. (PL, *PL-7*)
75. During the plan check review process, the Developer shall provide a lighting plan that provides design details (light standards, bollards, wall mounted packs, etc.) and illumination site information within alleyways, pathways, streetscapes, and open spaces proposed throughout the development. An electrical engineer shall prepare the site lighting plan demonstrating that adequate lighting ranges will be provided throughout the development without creating light spillover, light pollution, or conflicts with surrounding factors such as tree locations, off-site or adjacent lighting. (PL)
76. Prior to issuance of building permits, Developer shall demonstrate that light standards illustrated on conceptual lighting plan do not conflict with tree locations. Developer shall submit a plan showing both the lighting and landscape on the same sheet.
77. Project on-site lighting shall be of a type and in a location that does not constitute a hazard to vehicular traffic, either on private property or on adjoining streets. To prevent damage from vehicles, standards in parking areas shall be mounted on reinforced concrete pedestals or otherwise protected. Developer shall recess or conceal under-canopy lighting elements so as not to be directly visible from a public street. Developer shall submit a lighting plan showing standard heights and light materials for design review and approval of the Planning Division Manager. (PL/B, *PL-8*)
78. In order to minimize light and glare on the project property, all parking lot and exterior structure light fixtures shall be high cut-off type that divert lighting downward onto the property and shall not cast light on any adjacent property or roadway. (PL, *PL-9*)
79. Developer agrees to participate in a water conservation program that includes refitting water fixtures existing on the project property with water conserving devices within residences or businesses in the City's water service area, if such a program is in effect when building permits are issued for this project. Among the requirements of such a program might be refitting existing toilets, faucets, shower heads, landscaping irrigation or other fixtures and items that consume water within the structure. (PL, *PL-14*)

80. Because of water limitations placed upon the City by its water providers, approval of this permit does not guarantee that the City will issue building permits. Issuance of building permits may be delayed as a result of implementation of a water conservation or allocation plan. (PL, *PL-15*)
81. Prior to issuance of building permits, Developer shall pay a document imaging fee for the planning files in an amount calculated by planning staff at the time of building permit review based on fees then in effect. (PL/B, *PL-16*).
82. Before the City issues building permits, Developer shall provide to the Planning Division Manager a disk in DWG format of a 100-foot scale site plan of the project as approved. (PL, *PL-50*)
83. Developer shall recess or screen roof heating and cooling systems and other exterior mechanical equipment from adjoining property and public streets, as required by this permit. Plumbing vents, ducts and other appurtenances protruding from the roof of structures shall be placed so that they will not be visible from the front of the property or other major public vantage points. Developer shall include a note on the construction plumbing drawings of exterior elevations to indicate to contractors that roof features shall be grouped and located in the described manner. Roof vents shall be shown on construction drawings and painted to match roof material color. (PL/B, *PL-41*)
84. Developer shall install all roof and building drainpipes and downspouts inside building elements. These items shall not be visible on any exterior building elevations. (PL, *PL-42*)
85. For any exterior utility meter panels, Developer shall paint such panels to match the structure upon which it is located. Such panels shall be located to take advantage of screening (e.g. landscaping or other building elements) from public right-of-ways, to the maximum extent feasible. (PL, *PL-43*)
86. Developer shall install toilets that have automatic flush sensors in all public restrooms. Such toilets shall be included on the plans submitted for a building permit and shall be maintained and in working order at all times. (PL, *PL-44*)
87. Developer shall install individual mirrors above each sink in a public restroom to the satisfaction of the Planning Division Manager. The details of such mirrors shall be approved prior to issuance of a building permit. Developer shall remove graffiti from the mirrors or replace the mirrors within 24 hours of graffiti appearance. (PL, *PL-45*)
88. Before the City issues building permits, Developer shall submit and obtain approval from the Planning Division of a master sign program for the project, which shall indicate on the site plan the elevations, the size, placement, materials, and color of all proposed free-standing and building signs. The square footage of all signs for the project shall not exceed that permitted in accordance with the City Code. (PL/ B, *PL-46*)

89. Prior to issuance of a certificate of occupancy, Developer shall remove all construction materials and vehicles from the subject property. (PL/B, PL-47)
90. Developer shall limit outdoor storage of materials to the locations shown on the project plans. Developer shall screen storage areas from adjacent properties and streets by a wall or similar screening. All fence and wall materials shall match the predominant design and materials of the main structure on the project property. (PL/B, PL-48)
91. Developer shall stripe loading zones placed partly or wholly within a structure for loading and unloading activities only and post to prohibit storage or other non-loading activity within the loading zone. (PL/B, PL-49)

PLANNING DIVISION SPECIAL CONDITIONS

92. Before the City issues building permits, Developer shall provide a Graffiti Deterrent Plan, subject to the approval the Planning Division Manager. Such plan shall include such elements as clear film on windows and/or mirrors, as well as washable paint and sealers on the building and perimeter walls. (PL)
93. This permit is granted subject to the City's approval of a tentative map and final map and recordation of the final map. The City shall issue building permits only after such recordation, unless otherwise approved by both the Planning Division Manager and the Development Services Manager. Before occupying any structures or initiating any use approved by this permit, Developer shall comply with all conditions of the tentative and final map. (PL/DS)
94. This permit is granted subject to the approval of a zone change for the project property. (PL)
95. This permit is granted subject to the approval of a general plan amendment for the project property. (PL)
96. This SUP permit for alcohol sales shall automatically be null and void 12 months from the date of issuance, unless Developer has received from the State Department of Alcoholic Beverage Control a license to sell alcoholic beverages on the project property. (PL)
97. Developer shall participate in the City's Art in Public Places Program, in accordance with City Council Resolution No. 13,103, by installing artwork on the project site prior to occupancy of the project. Design and final location of artwork shall be approved by the Art in Public Places Committee. Artwork location(s) shall be as shown on the approved site plan and artwork shall be installed prior to occupancy of the project. The artwork shall become a permanent part of this project. Relocation of or changes to the artwork require the approval of the Planning Division Manager and Art in Public Places Committee. Developer shall maintain the artwork in good condition. (PL)
98. Any outside seating or restaurant use shall be required to comply with City parking regulations and all required permits obtained prior to initiation of the use.

99. Prior to issuance of a certificate of occupancy, the mural to be installed on the easterly building elevation of the building on parcel 1 shall be completed.
100. No outside storage or box storage shall be permitted outside of the buildings on parcels 1 and 2.
101. A trash compactor shall be installed within the building to be constructed on parcel 1.
102. The doors screening the loading and unloading dock which are partially visible from Cota Circle shall only be open during deliveries.
103. Deliveries associated with parcel 1 shall only within the loading and unloading area in the rear of the building on parcel 1 and shall only occur from 6:00 a.m. to 7:30 a.m. and from 9:00 a.m. to 4:00 p.m.

Air Quality Mitigation

104. Developer shall ensure that all construction equipment is maintained and tuned to meet applicable Environmental Protection Agency (EPA) and California Air Resources Board (CARB) emission requirements. At such time as new emission control devices or operational modifications are found to be effective, Developer shall immediately implement such devices or operational modifications on all construction equipment.
105. At all times during construction activities, Developer shall minimize the area disturbed by clearing, grading, earth moving, or excavation operations to prevent excessive amounts of dust.
106. During construction and on non-construction days during periods of high wind (see C-6), Developer shall water the area to be graded or excavated prior to commencement of grading or excavation operations. Such application of water shall penetrate sufficiently to minimize fugitive dust during grading activities.
107. During construction, Developer shall control dust by the following activities:
 - All trucks hauling graded or excavated material offsite shall be required to cover their loads as required by California Vehicle Code §23114, with special attention to Sections 23114(b)(F), (e)(2) and (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads.
 - All graded and excavated material, exposed soils areas, and active portions of the construction site, including unpaved onsite roadways, shall be treated to prevent fugitive dust. Treatment shall include, but not necessarily be limited to, periodic watering, application of environmentally safe soil stabilization materials, and/or roll-compaction as appropriate. Watering shall be done as often as necessary and reclaimed water shall be used whenever possible.
108. During construction, Developer shall post and maintain onsite signs, in highly visible areas, restricting all vehicular traffic to 15 miles per hour or less.

109. During periods of high winds (i.e. wind speed sufficient to cause fugitive dust to impact adjacent properties), Developer shall cease all clearing, grading, earth moving, and excavation operations to prevent fugitive dust from being a nuisance or creating a hazard, either onsite or offsite.
110. Throughout construction, Developer shall sweep adjacent streets and roads at least once per day, preferably at the end of the day, so that any visible soil material and debris from the construction site is removed from the adjacent roadways.
111. Prior to grading permit approval, Developer shall include on the grading plans a reproduction of all conditions of this permit pertaining to dust control requirements.
112. Open material stockpiles shall be covered, seeded, periodically watered, or treated with environmentally safe dust suppressants.

Cultural Resources Mitigation

113. The Developer shall contract with an Archaeological Monitor and/or Native American Monitor to be present during all subsurface grading, trenching or construction activities deeper than 16 inches. The monitor shall provide a monthly report to the Planning Division summarizing their activities during the reporting period. A copy of the contract for these services shall be submitted to the Planning Division Manager for review and approval prior to issuance of any grading permits. The monitoring report(s) shall be provided to the Planning Division prior to approval of final certificate of occupancy.
114. In the event archaeological resources are discovered, the Archaeological Monitor and/or Native American Monitor shall be empowered to suspend work in the immediate area of the discovery until such time as a data recovery plan can be developed and implemented. Work outside of the area of the find shall proceed with the continuation of archaeological monitoring. If a data recovery plan is implemented, all cultural materials from testing, monitoring, and data recovery phases of the project, except burial-related artifacts and as otherwise required by law, shall be cleaned, catalogued and permanently curated at an institution meeting the standards defined in the State of California Guidelines of the Curation of Archaeological Collections (May 1993). Repatriation of materials shall be handled in accordance with the requirements of the California Native American Graves Protection and Repatriation Act. All artifacts and all faunal materials shall be analyzed.
115. Developer shall remove any and all graffiti from the project premises, including but not limited to graffiti within the building, such as in restrooms or fitting rooms, within 24 hours of its appearance. The surface of such affected areas shall be matched to blend in with the underlying colors and/or design, and shall not look like a paint patch. (PL)

Hazards and Hazardous Materials Mitigation

116. Prior to issuance of a grading permit, the applicant shall conduct a subsurface assessment for the presence of agricultural chemicals as recommended in the Phase I ESA.
117. If during future grading activities, suspect conditions including but not limited to stained or odorous soils are encountered at the project site, the applicant shall conduct further soil sampling. A qualified environmental scientist shall be retained to analyze the degree of contamination and all required state and county remediation standards and practices shall be followed for remediation. A report and recommendations for remediation shall be submitted to the project planner for review and approval prior to re-initiating further grading work.

Noise Mitigation

118. Construction times shall be limited to 7 a.m. to 7 p.m. Monday through Saturday or in accordance with City Ordinances restricting construction times at the time of construction, whichever is more restrictive.
119. All deliveries of construction material and equipment will occur on-site within the construction barricades and only during the hours of 7 am and 7 pm Monday through Saturday. The queuing of construction vehicles outside the site before 7 am or after 7 pm will be strictly prohibited. The vehicles shall all utilize mufflers and other devices to minimize noise levels. All materials and equipment will be stored on-site and within the confines of the construction barricades.
120. Truck traffic related to the construction will be limited to the routes specified by the City of Oxnard. Truck traffic through residential neighborhoods shall be minimized.
121. All construction related workers will be required to park on-site (i.e. behind the construction barricades or in designated off-site parking area) outside of the residential area to the west.
122. During construction activities, except as otherwise required by law, all vehicle horns shall remain silent except in the case of emergency.
123. Catering trucks providing service to construction workers at the site will be required to park within the site at all times. Catering trucks shall not be permitted to park on the street nor to sound their horns near or within the site.
124. Construction workers shall not be permitted to loiter on the jobsite or on any street, whether before, during or after work hours, on weekdays or on weekends.
125. Developers shall setup staging areas on-site to minimize off-site transportation of heavy construction equipment.
126. Construction equipment shall be fitted with modern sound-reduction equipment. (PL, 4.9-3)

127. During all grading on site, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.

Transportation Mitigation

128. Prior to issuance of a certificate of occupancy for the retail parcel (parcels 1 and 2) or the church (parcel 3), proof of a reciprocal parking, access and maintenance agreement shall be submitted to the project planner. Said agreement shall be recorded on all three parcels. The agreement shall be reviewed and approved by the project planner prior to recordation.

ENVIRONMENTAL RESOURCES DIVISION

129. To ensure that solid waste generated by the project is diverted from the landfill and reduced, reused or recycled, Developer shall complete and submit a "City of Oxnard C&D Environmental Resources Management & Recycling Plan" ("Plan") to the City for review and approval. The Plan shall provide that at least 50% of the waste generated on the project be diverted from the landfill. The Plan shall include the entire project area, even if tenants are pursuing or will pursue independent programs. The Plan shall be submitted to and approved by the Environmental Resources Division prior to issuance of a building permit. The Plan shall include the following information: material type to be recycled, reused, salvaged or disposed; estimated quantities to be processed; management method used; destination of material including the hauler name and facility location. Developer shall use the Plan form.
130. Developer shall follow the approved "City of Oxnard C&D Environmental Resources Management & Recycling Plan" and provide for the collection, recycling, and/or reuse of materials (i.e., concrete, wood, metal, cardboard, green waste, etc.) and document results during construction and/or demolition of the proposed project. After completion of demolition and/or construction, Developer shall complete and submit the "City of Oxnard C&D Environmental Resources Management & Recycling Report For Work Completed" ("Work Completed Report") and provide legible copies of weight tickets, receipts, or invoices for materials sent to disposal or reuse/recycling facilities. For other discarded or salvaged materials, Developer shall provide documentation, on the disposal facility's letterhead, identifying where the materials were taken, type of materials, and tons or cubic yards disposed, recycled or reused, and the project generating the discarded materials. Developer shall submit and obtain approval of the Work Completed Report prior to issuance of a certificate of occupancy.
131. Developer shall arrange for materials collection during construction, demolition, and occupancy with the City's Environmental Resources Division or Developer shall arrange for self-hauling to an authorized facility.
132. Developer shall make provisions to divert at least 50% of the waste material generated during occupancy through source reduction, recycling, reuse, and green waste programs. Developer shall complete and submit a "City of Oxnard C&D Environmental Resources Management & Recycling Occupancy Plan" ("Occupancy Plan") to the City's Environmental Resources

Division. An Occupancy Plan must be submitted and approved prior to issuance of a certificate of occupancy. A "City of Oxnard C&D Environmental Resources Management & Recycling Occupancy Report" shall be submitted to the Environmental Resources Division annually on the anniversary date of the certificate of occupancy for approval.

133. Developer shall dispose of sewage and solid waste from the project by City's wastewater and solid waste systems in a manner approved by the City Engineer.

DEVELOPMENT SERVICES DIVISION STANDARD CONDITIONS

134. Developer agrees, as a condition of approval of this resolution, to indemnify, defend and hold harmless, at Developer's expense, City and its agents, officers and employees from and against any claim, action or proceeding commenced within the time period provided for in Government Code Section 66499.37, to attack, review, set aside, void or annul the approval of this resolution or to determine the reasonableness, legality or validity of any condition attached thereto. City shall promptly notify Developer of any such claim, action or proceeding of which City receives notice, and City will cooperate fully with Developer in the defense thereof. Developer shall reimburse City for any court costs and attorney's fees that City may be required to pay as a result of any such claim, action or proceeding. City may, in its sole discretion, participate in the defense of any such claim, action or proceeding, but such participation shall not relieve Developer of the obligations of this condition. Developer's acceptance of this resolution or commencement of construction or operations under this resolution shall be deemed to be acceptance of all conditions thereof. (DS-18)
135. Prior to approval of the final map or parcel map, Developer shall provide the City Engineer with written evidence from the Ventura County Clerk's Office that Developer has executed and filed with the Clerk all certificates, statements and securities required by Government Code Sections 66492 and 66493. (DS-26)
136. By title sheet dedication at the time of filing the subdivision map, Developer shall dedicate all water rights for the project property to City. (DS-39)
137. Prior to release of the final map or parcel map for recordation, Developer shall provide the City Engineer with a 100-scale base map for addressing purposes. The map shall be drawn on 18-inch by 24-inch mylar and shall show the standard address map title block, north arrow, street names, tract number, phase boundary and lot numbers. The City will assign all addresses. (DS-56)

DEVELOPMENT SERVICES SPECIAL CONDITIONS

138. Prior to release of the final map or parcel map for recordation, Developer shall post a bond or other security satisfactory to the City Attorney, guaranteeing that all monuments will be set as required by the Government Code and the City Code. (DS-57)
139. Developer shall dedicate to the City approximately 4 feet of right-of-way along Channel Islands Boulevard as indicated on the engineering site plan. (DS)

140. Developer shall dedicate to the City approximately 6 feet of right-of-way along Rose Avenue as indicated on the engineering site plan. (DS)
141. Prior to recordation and in accordance with section 66499.20½ of the Subdivision Map Act, Developer shall include on the parcel map a written notation of abandonment by the City of a portion of the restriction on access to Rose Avenue and Channel Islands Boulevard at the locations indicated on the tentative parcel map. (DS)
142. Developer shall construct trash enclosure (one bin for recycle use) with a solid roof (8-foot minimum clearance) that prevents stormwater from entering the refuse bins. Developer shall construct all other components of the trash enclosure in accordance with the approved City Standard Plan on file with the Development Services Division. Developer shall finish the trash enclosure to match the major design elements of the main structure. The finish and roof appearance shall be indicated on the building plans and are subject to approval by the Planning and Environmental Services Division. The location and configuration of trash enclosures shall be reviewed and approved by the Solid Waste Collection Division. All refuse bins on the site shall be stored in an approved trash enclosure. No objects other than refuse bins may be stored in the trash enclosure without the written permission of the Solid Waste Collection Division. (DS)
143. If Source Control determines that any tenant within the project is required to install a grease interceptor for their operations, Developer shall provide a traffic-rated drain centered in the trash enclosure and connect it to the sewer system via the grease interceptor. If no grease interceptor is required for the project, Developer shall design the enclosure to surface drain to the storm drain system. (DS)

PASSED AND ADOPTED by the Planning Commission of the City of Oxnard on this 20th day of March, 2008, by the following vote:

AYES: Commissioners

NOES: Commissioners

ABSENT: Commissioners

Michael Sanchez, Chairman

ATTEST: _____
Susan L. Martin, Secretary

RESOLUTION NO. 2008-

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OXNARD RECOMMENDING TO THE CITY COUNCIL ADOPTION OF A GENERAL PLAN AMENDMENT (PZ 07-600-06) TO CHANGE THE LAND USE MAP TO DESIGNATE PROPERTY LOCATED ON THE SOUTHWEST CORNER OF CHANNEL ISLANDS BOULEVARD AND ROSE AVENUE (APN 221-0-232-515) FROM LOW MEDIUM DENSITY RESIDENTIAL TO GENERAL COMMERCIAL. FILED BY CHURCHYARD DEVELOPMENT, LLC., 2225 GLASTONBURY ROAD, WESTLAKE VILLAGE, CA 91361.

WHEREAS, the Planning Commission of the City of Oxnard has considered an application for Planning and Zoning Permit No. 07-600-06, filed by Churchyard Development to amend the land use map of the General Plan to change the land use designation on a 4.05 acre parcel from low medium density residential to general commercial; and

WHEREAS, the Planning Commission has held public hearings and received and reviewed written and oral comments related to proposed Planning and Zoning Permit No.07-600-06; and

WHEREAS, the Planning Commission finds after due study and deliberation that the public interest and general welfare require the adoption of Planning and Zoning Permit No.07-600-06; and

WHEREAS, in accordance with the California Environmental Quality Act, the Planning Manager provided public notice of the intent of the City to adopt a mitigated negative declaration for this project, and the Planning Commission has considered the proposed mitigated negative declaration before making its recommendation herein; and

WHEREAS, the documents and other material that constitute the record of proceedings upon which the decision to adopt the mitigated negative declaration is based is located at the Planning Division of the City of Oxnard and the custodian of the record is the Planning and Environmental Services Manager; and

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Oxnard hereby recommends to the City Council adoption of Planning and Zoning Permit No.07-600-06 as follows:

Part 1. Amending the 2020 General Plan Land Use Map (Figure V-5) land use designation as shown in Exhibit "A" attached hereto.

Resolution No. 2008-____
Page 2

PASSED and ADOPTED by the Planning Commission of the City of Oxnard on this 20th day of March, 2008 by the following vote:

AYES: Commissioner:

NOES: Commissioner:

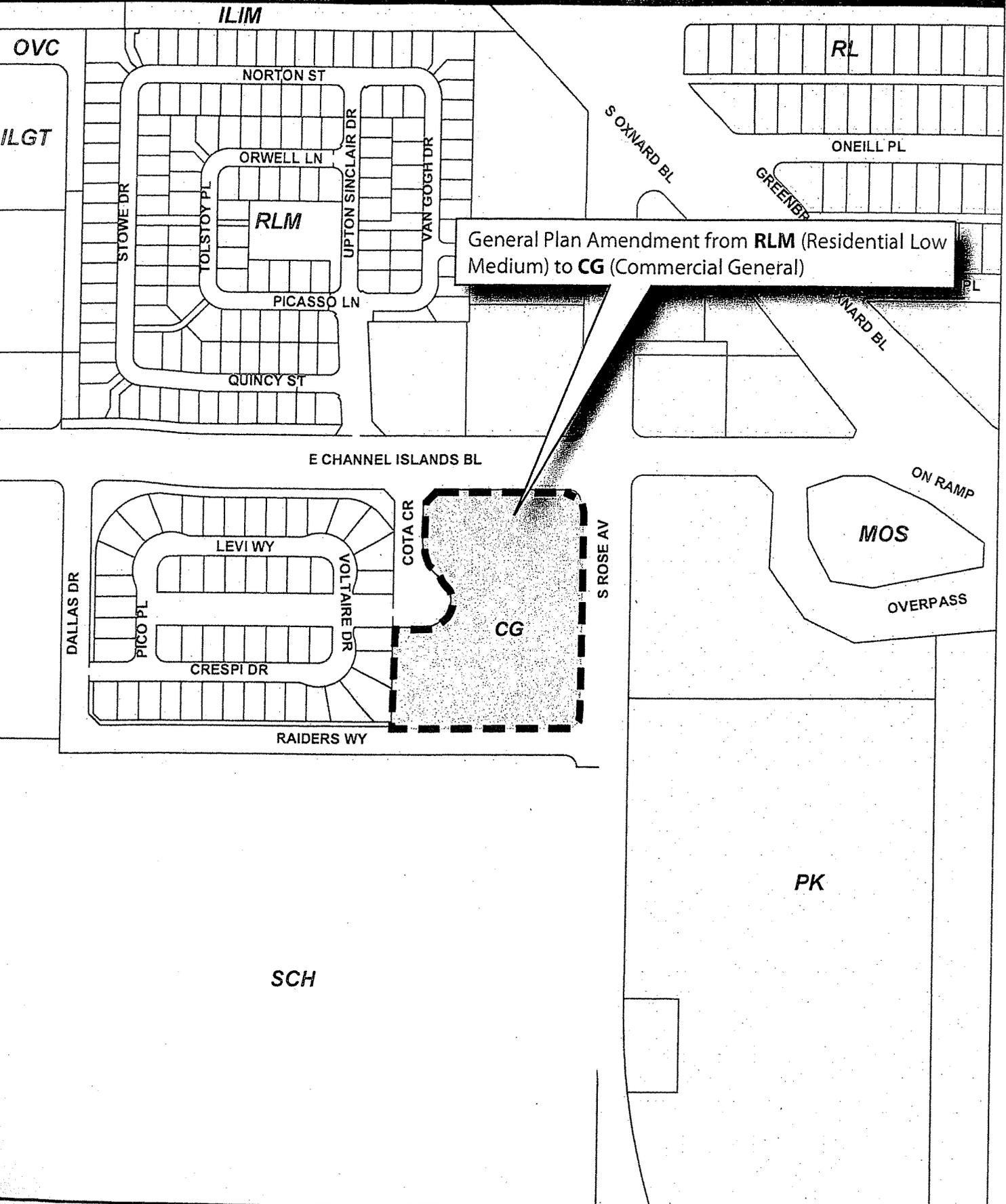
ABSENT: Commissioner:

Michael Sanchez, Chairman

ATTEST: _____
Susan L. Martin, Secretary

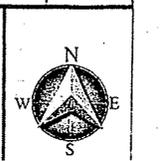
EXHIBIT 'A'
General Plan Amendment

General Plan Amendment



PZ 07-500-14 PZ 07-600-6
PZ 07-300-13 PZ 07-570-4
Location: SWC Rose Av & Channel Islands Bl
APN: 221023251
Churchyard Dev., LLC

General Plan Amendment



RESOLUTION NO. 2008-___

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OXNARD RECOMMENDING TO THE CITY COUNCIL ADOPTION OF A ZONE CHANGE (PZ 07-570-04), TO CHANGE THE ZONE DISTRICT FOR PARCELS 1-3 (PZ 07-500-14) LOCATED ROUGHLY ON THE SOUTHWEST CORNER OF CHANNEL ISLANDS BLVD. AND ROSE AVENUE (APN 221-0-232-515) FROM R-2 (MULTIPLE-FAMILY RESIDENTIAL) TO C-2-PD (COMMERCIAL GENERAL, PLANNED DEVELOPMENT). FILED BY THE CHURCHYARD DEVELOPMENT, 2225 GLASTONBURY ROAD, WESTLAKE VILLAGE, CA 91361

WHEREAS, the Planning Commission of the City of Oxnard has considered an application for Planning and Zoning Permit No. 07-570-04, filed by the Churchyard Development, LLP. to amend the zoning of the above-described property roughly on the Southwest corner of Channel Islands Blvd. and Rose Avenue from R-2 (Multiple-Family Residential) to C-2-PD (Commercial General, Planned Development); and

WHEREAS, the Planning Commission has held public hearings and received and reviewed written and oral comments related to proposed Planning and Zoning Permit No. 07-570-04; and

WHEREAS, the Planning Commission finds after due study and deliberation that the public interest and general welfare require the adoption of Planning and Zoning Permit No. 07-570-04; and

WHEREAS, in accordance with the California Environmental Quality Act, the Planning and Environmental Services Manager provided public notice of the intent of the City to adopt a mitigated negative declaration for this project, and the Planning Commission has considered the proposed mitigated negative declaration before making its recommendation herein;

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Oxnard hereby recommends to the City Council adoption of Planning and Zoning Permit No. 07-570-04, amending the City's official Zoning Map to change the zoning designation of ten parcels as shown in Exhibit "A", attached hereto and incorporated herein by reference.

Resolution No. 2007-____
PZ 07-570-04
Page 2
March 20, 2008

PASSED AND ADOPTED by the Planning Commission of the City of Oxnard on this 20th day of March 2008, by the following vote:

AYES: Commissioners:

NOES: Commissioners:

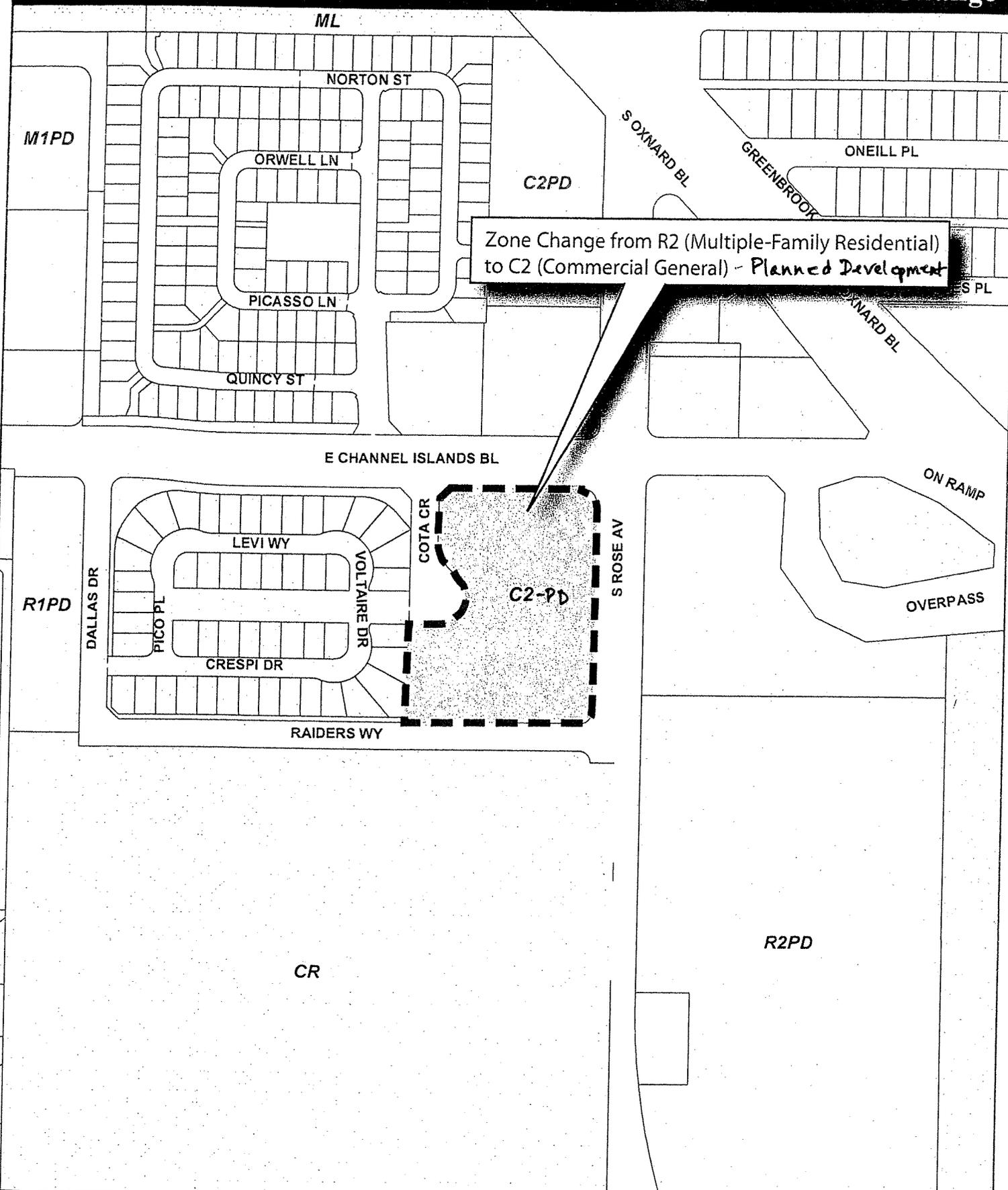
ABSENT: Commissioners:

Michael Sanchez, Chairman

ATTEST: _____
Susan L. Martin, Secretary

EXHIBIT 'A'
Zone Change

Zone Change



PZ 07-500-14 PZ 07-600-6
PZ 07-300-13 PZ 07-570-4
Location: SWC Rose Av & Channel Islands Bl
APN: 221023251
Churchyard Dev., LLC
0 100 200 300 400 Feet

Zone Change Exhibit "A"



RESOLUTION NO. 2007-

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OXNARD RECOMMENDING APPROVAL OF A PARCEL MAP (PLANNING AND ZONING PERMIT NO. 07-300-13), FOR PROPERTY LOCATED ON THE SOUTHWEST CORNER OF CHANNEL ISLANDS BLVD. AND ROSE AVENUE (APN 221-0-232-515), SUBJECT TO CERTAIN CONDITIONS OF APPROVAL. FILED BY CHURCHYARD DEVELOPMENT, LLC., 22225 GLASTONBURY ROAD, WESTLAKE VILLAGE, CA 91361

WHEREAS, the Planning Commission of the City of Oxnard has considered the parcel map (Planning and Zoning Permit No. 07-300-13), filed by Churchyard Development, LLC., in accordance with Chapter 15 of the Oxnard City Code; and

WHEREAS, said parcel map was referred to various public utility companies, City departments and the Development Advisory Committee for recommendations; and

WHEREAS, the Planning Commission finds the parcel map conforms to the City's General Plan and elements thereof as amended by Planning and Zoning permit number 07-600-06; and

WHEREAS, in accordance with the California Environmental Quality Act, the Planning and Environmental Services Manager provided public notice of the intent of the City to adopt a mitigated negative declaration for this project, and the Planning Commission has considered the proposed mitigated negative declaration before making its recommendation herein; and

WHEREAS, the Planning Commission finds that the applicant agrees with the necessity of and accepts all elements, requirements, and conditions of this resolution as being a reasonable manner of preserving, protecting, providing for, and fostering the health, safety, and welfare of the citizenry in general and the persons who work, visit or live in this subdivision in particular.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Oxnard hereby recommends to the City Council the approval of the tentative subdivision map, subject to the following conditions:

**STANDARD CONDITIONS OF APPROVAL
FOR LAND USE PERMITS**

Note: The abbreviations below identify the City department or division responsible for determining compliance with these standard conditions. The first department or division listed has responsibility for compliance at plan check, the second during inspection and the third at final inspection, prior to issuance of a certificate of occupancy, or at a later date, as specified in the condition. If more than one department or division is listed, the first will check the plans or inspect the project before the second confirms compliance with the condition. The italicized code at the end of each condition provides internal information on the source of each condition: Some are standard permit conditions (e.g. *G-1*) while some are taken from environmental documents (e.g. *MND-S2*).

DEPARTMENTS AND DIVISIONS			
CA	City Attorney	PL	Planning Division
DS	Dev Services/Eng Dev/Inspectors	TR	Traffic Division
PD	Police Department	B	Building Plan Checker
SC	Source Control	FD	Fire Department
PK	Public Works, Landscape Design	CE	Code Compliance

GENERAL PROJECT CONDITIONS

1. This permit is granted for the property described in the application on file with the Planning and Environmental Services Division (“Planning Division”), and may not be transferred from one property to another. (PL, *G-1*).
2. This permit is granted for the plans dated December 18, 2007, (“the plans”) on file with the Planning Division. The project shall conform to the plans, except as otherwise specified in these conditions, or unless a minor modification to the plans is approved by the Planning and Environmental Services Manager (“Planning Manager”) or a major modification to the plans is approved by the Planning Commission. A minor modification may be granted for minimal changes or increases in the extent of use or size of structures or of the design, materials or colors of structures or masonry walls. A major modification shall be required for substantial changes or increases in such items. (PL, *G-2*)
3. This permit shall automatically become null and void 24 months from the date of its issuance, unless Developer has diligently developed the proposed project, as shown by the issuance of a grading, foundation, or building permit and the construction of substantial improvements, or the beginning of the proposed use. (PL, *G-3*)
4. All required off-site and on-site improvements for the project, including structures, paving, and landscaping, shall be completed prior to occupancy unless the Development Services Manager allows Developer to provide security or an executed agreement approved by the City Attorney to ensure completion of such improvements. (DS, *G-4*)
5. By commencing any activity related to the project or using any structure authorized by this permit, Developer accepts all of the conditions and obligations imposed by this permit and waives any challenge to the validity of the conditions and obligations stated therein. (CA, *G-5*)

DEVELOPMENT SERVICES DIVISION STANDARD CONDITIONS

6. Developer shall pay plan check and processing fees in effect at the time of construction plan submittal and shall pay development fees, encroachment permit fees, and other applicable fees in effect at the time the City issues building permits. (DS-1)
7. Developer's Engineer shall design parking lot structural sections based on an analysis of the soils R-value and a traffic index (T.I.) approved by the City Engineer. The minimum structural section for parking lots is two inches of asphalt on four inches of base material. Developer shall show the proposed structural section on the site improvement plans. (DS-2)
8. Developer shall have the site improvement plans prepared on standard Development Services Division mylars by a civil engineer licensed in the State of California. The plans shall incorporate recommendations from soil engineering and geology reports. Prior to issuance of a grading permit, improvement plans must be approved by the City Engineer and the original ink-on-mylar plans filed with the Development Services Division. (DS-3)
9. Developer shall submit improvement plans and drainage calculations that demonstrate that storm drainage from the project property and all upstream areas will be safely conveyed to an approved drainage facility. The design and conveyance route shall be compatible with the City's Master Plan of Drainage and shall be approved by the City Engineer prior to approval of improvement plans. (DS-4)
10. Developer shall protect building pads from inundation during a 100-year storm. (DS-5)
11. Developer shall remove and replace all improvements that are damaged during construction. (DS-6)
12. Each structure shall be served by separate sewer and water services. There shall be no interconnections between structures. (DS-8)
13. Curb cut widths and design shall conform to City ordinances, standards, and policies in effect at the time City issues an encroachment permit. (DS-9)
14. If the existing sewer lateral is larger than four inches in diameter, Developer's site improvement plans shall include an on-site sewer plan. (DS-10)
15. Developer shall install on-site and off-site utility services underground in accordance with City ordinances in effect at the time City issues the building permit. Services shall be installed underground to the nearest suitable riser pole as determined by the appropriate utility service provider. (DS-12)
16. Developer shall enter into an agreement, approved as to form by the City Attorney, to install and construct all public improvements required by this permit and by the City Code and shall post security satisfactory to the Finance Director, guaranteeing the installation and construction of all required improvements within the time period specified in the agreement or any approved time extension. (DS-14)

17. A civil engineer licensed in the State of California shall prepare the public improvement plans and documents for this project in accordance with City standards and shall submit all such plans to the City Engineer. Such plans and documents shall include, but not be limited to, grading, street, drainage, sewer, water and other appurtenant improvement plans; a master utility plan showing the layout and location of all on-site and off-site utility improvements that serve the project; construction cost estimates, soils reports, and all pertinent engineering design calculations. City will not accept an application for the final map or parcel map for the project or issue a grading, site improvement or building permit until the City Engineer has approved all improvement plans. (DS-15)
18. Developer shall process permanent master planned improvements that are eligible for reimbursement in accordance with City policies, resolutions, and ordinances in effect at the time of recordation of the final map or parcel map or if there is no such map, then at the time of public improvement plan approval. (DS-17)
19. Developer shall remove graffiti from the project, including graffiti on offsite public infrastructure under construction by Developer, within 24 hours of its appearance. If Developer fails to remove graffiti in accordance with this condition, the City may at the discretion of the Development Services Manager issue a stop work order until such time as the graffiti is removed. (DS-20)
20. The conditions of this resolution shall prevail over all omissions, conflicting notations, specifications, dimensions, typical sections, and the like, that may or may not be shown on the improvement plans. (DS-21)
21. Developer shall pay the cost of all inspections of on-site and off-site improvements. (DS-22)
22. Developer shall be responsible for all project-related actions of Developer's employees, contractors, subcontractors, and agents until City accepts the improvements. (DS-23)
23. Prior to beginning construction, Developer shall designate in writing an authorized agent who shall have complete authority to represent and to act for Developer. The authorized agent shall be present at the work site whenever work is in progress. Developer or the authorized agent shall make arrangements acceptable to City for any emergency work. When City gives orders to the authorized agent to do work required for the convenience and safety of the general public because of inclement weather or any other cause, and the orders are not immediately acted upon by the authorized agent, City may do or have such work done by others at Developer's expense. (DS-24)
24. "Standard Specifications for Public Works Construction," latest edition, and any modifications thereto by City, and City of Oxnard Standard Land Development Specifications and all applicable City Standard Plans, shall be the project specifications, except as noted otherwise on the approved improvement plans. City reserves the right to upgrade, add to, or revise these specifications and plans and all other City ordinances, policies, and standards. If the improvements required of this project are not completed within 12 months from the date of City's approval of the improvement plans, Developer shall comply with and conform to any and all upgraded, additional or revised specifications, plans, ordinances, policies and standards. (DS-27)

25. Developer shall retain a Civil Engineer licensed in the State of California to ensure that the construction work conforms to the approved improvement plans and specifications and to provide certified "as-built" plans after project completion. Developer's submittal of the certified "as-built" plans is a condition of City's final acceptance of the project. (DS-29)
26. All grading shall conform to City's grading ordinance and any recommendations of Developer's soils engineer that have been approved by the City Engineer. Developer shall conform to all applicable notes specified on the site improvement/grading plan cover sheet and grading permit. (DS-30)
27. In order to mitigate any potential flooding or erosion affecting adjacent properties and public rights-of-way, Developer shall construct required drainage facilities concurrently with the rough grading operations, or with prior approval of the City Engineer, provide interim drainage improvements on a temporary basis. (DS-31)
28. Storm drain, sewer and water facilities shall conform to applicable City Master Plans. Developer shall prepare plans for these facilities in accordance with City's engineering design criteria in effect at the time of improvement plan submittal. Developer shall submit plans with pertinent engineering analyses and design calculations for review and approval by the City Engineer prior to issuance of a site improvement permit. (DS-34)
29. Each lot shall drain into a street, alley, or approved drain so that there will be no undrained depressions. (DS-35)
30. Prior to issuance of a site improvement permit, Developer shall provide to the City Engineer easements or written consents from all affected landowners for any diversion of historical flows or change in drainage conditions caused by the project, as evidence that such landowners accept any additional water flowing over their property. (DS-36)
31. Developer shall dispose of sewage and solid waste from the project by City's wastewater and solid waste systems in a manner approved by the City Engineer. (DS-38)
32. Developer shall install water mains, fire hydrants and water services in conformance with City Standard Plans and specifications as directed by the City Engineer. (DS-41)
33. Developer shall install adequately sized water services and meters to each lot or unit in accordance with City standards in effect at the time City issues building permits. There shall be no interconnections between structures. (DS-42)
34. Prior to issuance of building permits, Developer shall present to the City Engineer a "Proof of Payment - Authorization for Building Permits" form issued by the Calleguas Municipal Water District. (DS-44)
35. Developer shall install City approved backflow prevention devices for water connections if so ordered by the City Engineer. (DS-45)
36. Prior to designing the water system for the project, Developer shall have a certified fire flow test performed to determine existing water pressure and flow characteristics. The water system shall be designed to allow for a 10 psi drop in the static water pressure measured during the fire

- flow test. After construction and before City issues a certificate of occupancy, the City Engineer may require a second test. Before performing the tests, Developer shall obtain permits from the City Engineer. Developer shall have all tests certified by a mechanical, civil, or fire protection engineer and provide written results of all tests to the City Engineer. (DS-47)
37. Developer shall construct all street and road improvements in conformance with the City Code, the City's 2020 General Plan, and any applicable specific plan. (DS-48)
 38. Developer shall dedicate and improve to City standards all sidewalks, parkways, streets, alleys, and street appurtenances. City will name all streets in accordance with adopted City guidelines. (DS-49)
 39. Developer shall improve all streets, alleys, sidewalks, curbs, and gutters adjacent to the project in accordance with City standards, as necessary to provide safe vertical and horizontal transitions. (DS-52)
 40. Developer shall provide soils reports, "R" value tests, and compaction tests for all streets. Determination of the actual structural sections shall be based on City's design procedure, applying the appropriate traffic index specified in City standards. (DS-53)
 41. Developer shall submit a landscape irrigation plan prepared by a licensed professional, showing proper water meter size, backflow prevention devices, and cross-connection control. (DS-59)
 42. Developer shall be responsible for and bear the cost of replacement of all existing survey monumentation (e.g., property corners) disturbed or destroyed during construction, and shall file appropriate records with the Ventura County Surveyor's Office. (DS-64)
 43. Developer shall provide adequate vehicle sight distance as specified by CalTrans specifications at all driveways and intersections. (TR-71)
 44. Developer, property owner or tenant shall participate in a Traffic Management Association (TMA) if one is formed and shall implement and participate in all programs and strategies established by the TMA. This condition shall be included in all leases and rental agreements for the project property. (TR-72)
 45. Developer shall install bike racks in accordance with City standards at locations approved by City Traffic Engineer. (TR-73)
 46. Prior to issuance of a building permit, all traffic signal, pavement marking and sign plans shall be prepared by a registered California traffic engineer and approved by the City Engineer prior to issuance of a grading, site improvement or a building permit. (TR-74)
 47. Prior to issuance of an encroachment permit, Developer's shall obtain City's approval of a contractor qualified to install traffic signals, pavement markings and signs. (TR-76)

STORMWATER QUALITY CONDITIONS

48. Developer shall comply with all National Pollutant Discharge Elimination System (NPDES) permit Best Management Practice (BMP) requirements in effect at the time of grading or building permit issuance. Requirements shall include, but not be limited to, compliance with the Ventura Countywide Stormwater Quality Urban Impact Mitigation Plan (SQUIMP). (DS-78)
49. Developer shall design parking lot and other drive areas to minimize degradation of stormwater quality. Using Best Management Practices (BMPs), such as oil and water separators, sand filters, landscaped areas for infiltration, basins or approved equals, Developer shall intercept and effectively prevent pollutants from discharging to the storm drain system. The stormwater quality system design shall be approved by the City Engineer prior to the issuance of a site improvement permit. (DS-81)
50. Using forms provided by the Development Services Division, Developer shall submit a stormwater quality control measures maintenance program ("the Program") for this project. If the BMPs implemented with this project include proprietary products that require regular replacement and/or cleaning, Developer shall provide proof of a contract with an entity qualified to provide such periodic maintenance. The property owner is responsible for the long-term maintenance and operation of all BMPs included in the project design. Upon request by City, property owner shall provide written proof of ongoing BMP maintenance operations. No grading or building permit shall be issued until the Development Services Manager approves the Program and Developer provides an executed copy for recordation. (DS-82)
51. Developer shall clean on-site storm drains at least twice a year; once immediately before the first of October (the beginning of the rainy season) and once in January. The City Engineer may require additional cleaning. (DS-83)
52. Developer shall maintain parking lots free of litter and debris. Developer shall sweep sidewalks, drive aisles, and parking lots regularly to prevent the accumulation of litter and debris. When swept or cleaned, debris must be trapped and collected to prevent entry into the storm drain system. Developer may not discharge any cleaning agent into the storm drain system. (DS-84)
53. Prior to issuance of a certificate of occupancy, on-site storm drain inlets shall be labeled "Don't Dump - Drains to Ocean" in accordance with City standards. Before City issues a site improvement permit, the requirement to label storm drain inlets shall be shown on the civil engineering plans. (DS-85)
54. Prior to issuance of a grading permit or commencement of any clearing, grading or excavation, Developer shall provide the City Engineer with a copy of a letter from the California State Water Resources Control Board, Storm Water Permit Unit assigning a permit identification number to the Notice of Intent (NOI) submitted by Developer in accordance with the NPDES Construction General Permit. Developer shall comply with all additional requirements of the General Permit, including preparation of a Stormwater Pollution Prevention Plan (SWPPP). The SWPPP shall identify potential pollutant sources that may affect the quality of discharges to stormwater and shall include the design and placement of recommended Best Management Practices (BMPs) to effectively prohibit pollutants from the construction site entering the storm drain system. Developer shall keep the SWPPP updated to reflect current site conditions at all

times and shall keep a copy of the SWPPP and the NOI on the site and make them available for City or designated representative to review upon request. (DS-86)

DEVELOPMENT SERVICES DIVISION SPECIAL CONDITIONS

55. Prior to issuance of a site improvement permit, Developer shall provide to the Development Services Division a compact Disc (CD) containing digital copies of the final subdivision map, address map, and civil improvements drawings in DWG format. Prior to improvement bond release, Developer shall provide an updated CD containing all changes that occur during construction. (DS-101)
56. Developer shall pay to the County of Ventura a road mitigation fee in accordance with the agreement between the City and the County of Ventura. Proof of payment shall be provided to the Development Services Division prior to issuance of a building permit. (DS-105)
57. Developer shall replace all broken, uplifted or missing sidewalk along the project's street frontages. Required repairs shall be shown on the grading/site improvement plan. (DS)
58. Prior to issuance of a site improvement permit, Developer shall dedicate to the City approximately 4 feet of right-of-way along Channel Islands Boulevard as indicated on the engineering site plan. Proposed sidewalk improvements along Channel Islands Boulevard shall be included on the project site plan. (DS)
59. Prior to issuance of a site improvement permit, Developer shall dedicate to the City approximately 6 feet of right-of-way along Rose Avenue as indicated on the engineering site plan. Proposed widening of Rose Avenue shall be included on the project site plan. Rose Avenue widening (curb, gutter, base, asphalt, and sidewalk) is reimbursable under the traffic impact fee. (DS)
60. Developer shall provide a minimum of 30" between the front of a parking stall and proposed 3 foot high screen wall. (DS)
61. Developer shall construct proposed walkways that cross vehicular drive aisles of colored enhanced concrete. The concrete color shall contrast with the parking lot asphalt to clearly identify pedestrian areas. (DS)
62. Developer shall construct trash enclosure (one bin for recycle use) with a solid roof (8-foot minimum clearance) that prevents stormwater from entering the refuse bins. Developer shall construct all other components of the trash enclosure in accordance with the approved City Standard Plan on file with the Development Services Division. Developer shall finish the trash enclosure to match the major design elements of the main structure. The finish and roof appearance shall be indicated on the building plans and are subject to approval by the Planning and Environmental Services Division. The location and configuration of trash enclosures shall be reviewed and approved by the Solid Waste Collection Division. All refuse bins on the site shall be stored in an approved trash enclosure. No objects other than refuse bins may be stored in the trash enclosure without the written permission of the Solid Waste Collection Division. (DS)

63. If Source Control determines that any tenant within the project is required to install a grease interceptor for their operations, Developer shall provide a traffic-rated drain centered in the trash enclosure and connect it to the sewer system via the grease interceptor. If no grease interceptor is required for the project, Developer shall design the enclosure to surface drain to the storm drain system. (DS)
64. Developer shall construct a concrete apron along the length of the trash enclosure opening that extends a minimum of 15 feet from the face of the enclosure. (DS)
65. Developer shall provide onsite fire hydrants such that all points of all structures are within one hundred fifty (150) feet of a fire hydrant, or as otherwise approved by the Fire Department. Fire hydrant line improvements shall be designed in accordance with City standards and shown on the civil engineer's improvement plans prior to issuance of a site improvement/grading permit. Developer shall provide the City with an easement over the waterline using standard City format. (DS)
66. Developer shall design the grass swale filter in accordance with the Technical Guidance Manual for Stormwater Quality Control Measures. Design calculations shall be included in the project drainage report. (DS)
67. Developer's engineer shall provide City with written confirmation that they have reviewed the landscape construction drawings within the NPDES grass swale filter areas and that the proposed landscaping conforms to SQUIMP standards for grass swale filters. (DS)
68. Developer shall provide a 6-inch minimum vertical drop between the flow line of the parking lot concrete gutter and the flow line of the filter swale at each location where stormwater enters the filter swale. The transition between gutter flow line and filter swale flow line shall be constructed similar to a concrete ribbon gutter. (DS)
69. Developer shall install a perforated underdrain below all grass-filter swales constructed with a longitudinal slope of less than 1%. Underdrain shall connect to a point of safe discharge as approved by the Development Services Manager. (DS)
70. Developer shall provide proof of recordation of a document that holds all property owners subject to this permit equally responsible for implementation and perpetual maintenance of all proposed long-term post-construction BMPs within this project. (DS)
71. Prior to issuance of a site improvement permit, Developer shall provide proof of recordation of CC&Rs (or similar document) providing reciprocal access, sewage and drainage among the various lots of this project. (DS)
72. Developer shall repair proposed street cuts within Rose Avenue in accordance with City Standard Plate 602 sheet 2 of 3 for streets less than 10 years old (resurfaced). Street repair shall extend from the most northerly trench to the most southerly trench in accordance with the "Parallel to Centerline" detail. The cost of this repair is not reimbursable under the traffic impact fee. (DS)

PASSED AND ADOPTED by the Planning Commission of the City of Oxnard on this 20th day of March, 2008, by the following vote:

AYES: Commissioners

NOES: Commissioners

ABSENT: Commissioners

Michael Sanchez, Chairman

ATTEST: _____
Susan L. Martin, Secretary